

TELECONFERENCED MEETING

CITY COUNCIL REGULAR MEETING – 6:00 PM

DECEMBER 15, 2020

[Pursuant to Governor Executive Order N-29-20]

There Will Not Be a Physical Location for Attending the Meeting

The Public May Observe the Meeting and Offer Public Comment As Follows:

STEP 1

Install the Free Zoom App or Visit the Free Zoom Website at [<https://zoom.us/>](https://zoom.us/)

STEP 2

Get Meeting ID Number and Password by emailing zoom@moval.org or calling (951) 413-3001, no later than 5:00 p.m. on Tuesday, December 15, 2020

STEP 3

Select Audio Source

Computer Speakers/Microphone

or

Telephone

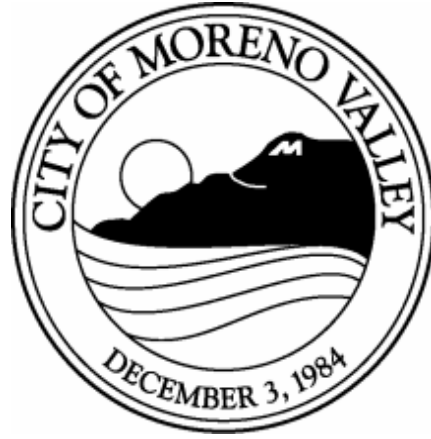
STEP 3

Public Comments May be Made Via Zoom

During the Meeting, the Mayor Will Explain the Process for Submitting Public Comments

ALTERNATIVE

If you do not wish to make public comments, you can view the meeting on Channel MVTV-3, the City's website at www.moval.org or YouTube



AGENDA

**CITY COUNCIL OF THE CITY OF MORENO VALLEY
MORENO VALLEY COMMUNITY SERVICES DISTRICT
CITY AS SUCCESSOR AGENCY FOR THE
COMMUNITY REDEVELOPMENT AGENCY OF
THE CITY OF MORENO VALLEY
MORENO VALLEY HOUSING AUTHORITY
MORENO VALLEY PUBLIC FINANCING AUTHORITY
BOARD OF LIBRARY TRUSTEES**

December 15, 2020

REGULAR MEETING – 6:00 PM

City Council Study Sessions

Second Tuesday of each month – 6:00 p.m.

City Council Meetings

Special Presentations – 5:30 P.M.

First & Third Tuesday of each month – 6:00 p.m.

City Council Closed Sessions

Will be scheduled as needed at 4:30 p.m.

City Hall Council Chamber – 14177 Frederick Street

Upon request, this agenda will be made available in appropriate alternative formats to persons with disabilities, in compliance with the Americans with Disabilities Act of 1990. Any person with a disability who requires a modification or accommodation in order to participate in a meeting should direct such request to Guy Pegan, ADA Coordinator, at 951.413.3120 at least 72 hours before the meeting. The 72-hour notification will enable the City to make reasonable arrangements to ensure accessibility to this meeting.

Dr. Yxstian A. Gutierrez, Mayor

Victoria Baca, Mayor Pro Tem
Ulises Cabrera, Council Member

David Marquez, Council Member
Dr. Carla J. Thornton, Council Member

**AGENDA
JOINT MEETING OF THE
CITY COUNCIL OF THE CITY OF MORENO VALLEY
MORENO VALLEY COMMUNITY SERVICES DISTRICT
CITY AS SUCCESSOR AGENCY FOR THE
COMMUNITY REDEVELOPMENT AGENCY OF THE
CITY OF MORENO VALLEY
MORENO VALLEY HOUSING AUTHORITY
MORENO VALLEY PUBLIC FINANCING AUTHORITY
AND THE BOARD OF LIBRARY TRUSTEES**

***THE CITY COUNCIL RECEIVES A SEPARATE STIPEND FOR CSD
MEETINGS***

REGULAR MEETING – 6:00 PM

DECEMBER 15, 2020

CALL TO ORDER

Joint Meeting of the City Council, Community Services District, City as Successor Agency for the Community Redevelopment Agency, Housing Authority and the Board of Library Trustees - actions taken at the Joint Meeting are those of the Agency indicated on each Agenda item.

ROLL CALL

INTRODUCTIONS

**PUBLIC COMMENTS ON ANY SUBJECT ON THE AGENDA AND NOT ON THE
AGENDA UNDER THE JURISDICTION OF THE CITY COUNCIL**

Any person wishing to address the Mayor and City Council on any matter, either under the Public Comments section of the Agenda or scheduled items or public hearings, must follow the procedures set forth above and wait to be identified to speak by the Mayor. Members of the public may be limited to three minutes per person or the allowed time set by the Mayor, except for the applicant. The Mayor and City Council may establish an overall time limit for comments on a particular Agenda item. Members of the public must direct their questions to the Mayor and not to other members of the City Council, the applicant, the Staff, or the audience.

SPECIAL ORDER OF BUSINESS

1. RESOLUTION RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION HELD ON NOVEMBER 3, 2020, DECLARING THE OFFICIAL RESULTS AND SUCH OTHER MATTERS AS PROVIDED BY LAW (REPORT OF: CITY CLERK)

Recommendation: That the City Council:

1. Adopt a Resolution of the City Council of the City of Moreno Valley, California, reciting the fact of the General Municipal Election held on November 3, 2020, declaring the results and such other matters as provided by law.

ADMINISTRATION OF THE OATHS OF OFFICE TO NEWLY ELECTED OFFICIALS

Council Member District 1 - Oath and Remarks

Council Member District 3 - Oath and Remarks

Mayor - Oath and Remarks

RECOGNITION TO NEWLY ELECTED CITY OFFICIALS BY OTHER PUBLIC ELECTED OFFICIALS

JOINT CONSENT CALENDARS (SECTIONS A-E)

All items listed under the Consent Calendars, Sections A, B, C, D, and E are considered to be routine and non-controversial, and may be enacted by one motion unless a member of the City Council, Community Services District, City as Successor Agency for the Community Redevelopment Agency, Housing Authority or the Board of Library Trustees requests that an item be removed for separate action. The motion to adopt the Consent Calendars is deemed to be a separate motion by each Agency and shall be so recorded by the City Clerk. Items withdrawn for report or discussion will be heard after public hearing items.

A. CONSENT CALENDAR-CITY COUNCIL

- A.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- A.2. MINUTES - CITY COUNCIL - REGULAR MEETING - DEC 1, 2020 6:00 PM

Recommendation: Approve as submitted.

A.3. 2021 CITY COUNCIL COMMISSION, BOARD, AND INTER-AGENCY APPOINTMENTS (Report of: City Clerk)

Recommendation: That the City Council:

1. Ratify the appointments to the various committees and regional bodies as noted on the 2021 Council Committee Participation List – terms end either June 30, 2021 or December 31, 2021.

A.4. APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE (Report of: City Clerk)

Recommendation:

1. Receive and confirm the following appointments:

Citizens Public Safety Committee

<u>Name</u>	<u>Position</u>	<u>Term</u>
Jorge Alvarado	Member	Ending 06/30/21
Tanya Price	Member	Ending 06/30/21
Rosemary Rios	Member	Ending 06/30/21
Karen Sykes	Member	Ending 06/30/21
Liz Williams	Member	Ending 06/30/21
Wayne Woodliff	Member	Ending 06/30/21
Marquita Clay	Member	Ending 06/30/21

Each of the recommended appointees for the Citizens Public Safety Committee were screened and interviewed by the Citizens Public Safety Committee leaders consisting of Chair Mayor Pro Tem Baca and Vice Chair Council Member Thornton. Each recommended appointee was also subject to a criminal background check which is standard practice for all applicants for any City advisory commission or committee.

Additional appointees are undergoing background checks.

A.5. MAYORAL APPOINTMENTS TO THE EMERGING LEADERS COUNCIL, AND THE UTILITIES COMMISSION

Recommendation:

1. Receive and confirm the slate of Mayoral appointments as follows:

EMERGING LEADERS COUNCIL

<u>Name</u>	<u>Position</u>	<u>Term</u>
Steven Serrano Yepez	Member	Ending 05/31/22

UTILITIES COMMISSION

<u>Name</u>	<u>Position</u>	<u>Term</u>
Brianna Solomon	Public Member	Ending 06/30/23

A.6. COLLEGE PROMISE MEMORANDUM OF UNDERSTANDING (Report of: City Manager)

Recommendations:

1. Approve the Memorandum of Understanding (MOU) between the City of Moreno Valley (City) and the Riverside Community College District Foundation acting on behalf of Moreno Valley College (College) allowing for the allocation of \$50,000 to support the College Promise Initiative.
2. Authorize the City Manager to execute the MOU as to form and all necessary documents with the College.

A.7. LIST OF PERSONNEL CHANGES (Report of: Financial & Management Services)

Recommendation:

1. Ratify the list of personnel changes as described.

A.8. COVID-19 PANDEMIC RESOLUTIONS EXTENDING THE LOCAL STATE OF EMERGENCY AND CERTAIN EMERGENCY MEASURES (RESO. NO. 2020-69) (Report of: Financial & Management Services)

Recommendations:

1. That the City Council adopt a Resolution Extending the Local State of Emergency and Certain Emergency Measures related to the Local, State and National Declarations of a State of Emergency related to the COVID-19 Pandemic.
2. Authorize the City Manager to amend any agreements as necessary to continue the operation of the State of California's Great Plates Delivered program, locally known as Senior Eats.

A.9. RECEIVE THE ANNUAL AB1600 COMPLIANCE REPORT FOR FISCAL YEAR 2019-20 (Report of: Financial & Management Services)

Recommendations:

1. Receive and file the Annual AB 1600 Compliance Report for FY 2019-20 in compliance with California Government Code sections 66006 and 66001.
2. Approve the finding that staff has demonstrated a continuing need to hold unexpended Development Impact Fees.

A.10. APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO UPGRADE AGING SERVERS AND DISASTER RECOVERY TECHNOLOGY TO MEET INCREASED DEMANDS AND AWARD OF CONTRACT FOR SERVER, STORAGE, BACKUPS, AND DISASTER RECOVERY TECHNOLOGY– RFP NO. 2020-020 (Report of: Financial & Management Services)

Recommendations:

1. Award contract to Nth Generation Computing, Inc., the highest qualified proposal for \$833,766, (\$757,969 for server, storage, backups, and disaster recovery technology plus \$75,797 (10% contingency)).
2. Authorize the Assistant City Manager to execute the Contract with Nth Generation Computing, Inc., subject to the approval of the Interim City Attorney.
3. Authorize expenditures for improvements to the disaster recovery site for an amount not to exceed \$27,500 (\$25,000 for improvements, plus \$2,500 (10% contingency)).
4. Authorize amendments to the Fiscal Year 2020/21 budget from the Technology Replacement Reserve balance for these expenses as outlined in the Fiscal Impact section.

A.11. PAYMENT REGISTER- OCTOBER 2020 (Report of: Financial & Management Services)

Recommendation:

1. Receive and file the Payment Register.

A.12. PROJECT SPECIFIC AGREEMENT FOR ON-CALL CONSTRUCTION INSPECTION SERVICES WITH NV5, INC. (Report of: Public Works)

1. Approve the Project Specific Agreement for On-Call Professional Services for Construction Inspection Services with NV5, Inc. and authorize the City Manager to execute the Project Specific Agreement with NV5, Inc., subject to the approval of the City Attorney;
2. Authorize the issuance of a purchase order to NV5, Inc. in the amount of \$270,000.00 for Fiscal Year (FY) 2020/2021 and authorize the Chief Financial Officer to approve subsequent related purchase orders for annual extensions up to a maximum of two annual extensions with NV5, Inc. not to exceed \$270,000.00 annually, for a total not to exceed of \$810,000.00, in accordance with the approved terms of the Agreement, using funds collected from development and utility company projects; and
3. Authorize the Public Works Director/City Engineer to execute subsequent extensions and/or amendments to the Agreements, including the authority to authorize subsequent associated purchase orders in accordance with the terms of the Agreements, subject to the approval of the City Attorney.

A.13. APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM FOR RESIDENTIAL AND SMALL BUSINESSES, PROVIDE FUNDING OF \$250,000 FOR THE COVID-19 UTILITY ASSISTANCE PROGRAM, APPROVE A SUSPENSION OF LATE FEES AND DISCONNECTS, AND APPROVE A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY TO CONFIRM THE ELECTRIC RATES FOR MORENO VALLEY UTILITY (MVU) (Report of: Public Works)

Recommendations:

1. Approve a COVID-19 Utility Assistance Program for residential and small business customers;
2. Approve a budget allocation of \$250,000 to fund the new COVID-19 Utility Assistance Program;
3. Approve suspension of late fees and disconnects for non-payment until the Emergency Resolution is lifted by the City; and
4. Approve Resolution No. 2020-XX. A Resolution of the City Council of the City of Moreno Valley, California, to confirm the Electric Rates for Moreno Valley Utility (MVU) as currently required by increased energy costs, operating costs, operating agreements, and City resolution.

- A.14. ADOPT RESOLUTION NO. 2020-XX, A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING A RENEWABLE ENERGY PROCUREMENT PLAN, COMPLIANCE PERIOD 3 (2017-2020) FOR MORENO VALLEY UTILITY (Report of: Public Works)

Recommendation:

1. Adopt Resolution 2020-XX, a Resolution of the City of Moreno Valley adopting a Renewable Energy Resources Procurement Plan, Compliance Period 3 (2017-2020) pursuant to Public Utilities Code § 399.30(a)

- A.15. SECOND READING AND CONSIDERATION OF ADDING CHAPTER 2.65 “CAMPAIGN CONTRIBUTION REGULATIONS” TO TITLE 2 “ADMINISTRATION AND PERSONNEL” OF THE MORENO VALLEY MUNICIPAL CODE TO ESTABLISH REGULATIONS AND LIMITATIONS ON CAMPAIGN CONTRIBUTIONS FOR CITY ELECTED OFFICIALS (ORD. NO. 973) (Report of: City Attorney)

RECOMMENDATION:

1. That the City Council adopt the attached Ordinance No. 973 to add a new Chapter 2.65 (“Campaign Contribution Regulations”) to Title 2 (“Administration and Personnel”) of the Moreno Valley Municipal Code.

- A.16. SECOND READING AND CONSIDERATION OF AMENDING CHAPTER 3.48 (“WESTERN RIVERSIDE COUNTY MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE PROGRAM”) TO ELIMINATE LOW INCOME RESIDENTIAL HOUSING AS AN EXEMPTION TO THE MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE (ORD. NO. 972) (Report of: City Attorney)

RECOMMENDATION:

1. That the City Council adopt the attached Ordinance No. 972 amending Chapter 3.48 (“Western Riverside County Multiple Species Habitat Conservation Plan Fee Program”) to Title 3 (“Revenue and Finance”) of the Moreno Valley Municipal Code to eliminate “Low Income Residential Housing” as an exemption.

- A.17. HOUSING ELEMENT AND DRAFT HOUSING ELEMENT SITES INVENTORY OVERVIEW (Report of: Community Development)

1. Receive and file the staff report and supporting information

- A.18. ADOPT A RESOLUTION OF THE CITY OF MORENO VALLEY, CALIFORNIA, APPROVING AN ANNUAL ADJUSTMENT TO QUIMBY IN-LIEU FEES PURSUANT TO CHAPTER 3.40.110 OF THE MORENO VALLEY MUNICIPAL CODE (Report of: Parks & Community Services)

Recommendation:

1. Adopt Resolution No. 2020-____, authorizing an adjustment to Quimby In-Lieu fees.

- A.19. ADOPTION OF RESOLUTION FOR UPDATE OF THE CITY USER FEE SCHEDULE – PARKS AND COMMUNITY SERVICES USER FEES FOR FY 2020/2021 (Report of: Parks & Community Services)

Recommendation:

1. Adopt Resolution 2020-XX approving a CPI adjustment to the Parks and Community Services User Fees.

B. CONSENT CALENDAR-COMMUNITY SERVICES DISTRICT

- B.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- B.2. MINUTES - CITY COUNCIL - REGULAR MEETING - DEC 1, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

C. CONSENT CALENDAR - HOUSING AUTHORITY

- C.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- C.2. MINUTES - CITY COUNCIL - REGULAR MEETING - DEC 1, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

D. CONSENT CALENDAR - BOARD OF LIBRARY TRUSTEES

- D.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- D.2. MINUTES - CITY COUNCIL - REGULAR MEETING - DEC 1, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

E. CONSENT CALENDAR - PUBLIC FINANCING AUTHORITY

- E.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- E.2. MINUTES - CITY COUNCIL - REGULAR MEETING - DEC 1, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

F. PUBLIC HEARINGS

Questions or comments from the public on a Public Hearing matter are limited to five minutes per individual and must pertain to the subject under consideration.

Those wishing to speak should follow the teleconference procedures.

- F.1. PROPOSED GENERAL PLAN AMENDMENT AND CHANGE OF ZONE AMENDING FIGURE 2-2 "LAND USE MAP" OF THE MORENO VALLEY GENERAL PLAN AND THE CITY ZONING ATLAS, RESPECTIVELY, AND PROPOSED CONDITIONAL USE PERMITS FOR A DRIVE THROUGH RESTAURANT WITH OUTDOOR SEATING, A FUELING STATION AND CONVENIENCE STORE WITH ALCOHOL SALES AT THE NORTHEAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE (Report of: Community Development)

Recommendations:

1. ADOPT Resolution 2020-XX: A Resolution of the City Council of the City of Moreno Valley CERTIFYING that the Initial Study/Mitigated Negative Declaration, prepared for General Plan Amendment PEN19-0206, Change of Zone PEN19-0207, and Conditional Use Permits PEN19-0204 and PEN19-0205, on file with the Community Development Department, incorporated herein by this reference, completed in compliance with the California Environmental Quality Act Guidelines, and that the City Council reviewed and considered the

information contained in the Initial Study/ Mitigated Negative Declaration, and that the document reflects the City's independent judgment and analysis, and ADOPTING the Mitigation Monitoring and Reporting Program prepared for General Plan Amendment PEN19-0206, Change of Zone PEN19-0207, and Conditional Use Permits PEN19-0204 and PEN19-0205;

2. ADOPT Resolution 2020-XX: A Resolution of the City Council of the City of Moreno Valley approving General Plan Amendment application PEN19-0206 to amend the General Plan Land Use map to amend the land use designation of 2.04-acres from Residential/Office (R/O) General Plan land use designation to Commercial (C) General Plan land use designation for the areas described in the Resolution, based on the findings contained in the Resolution, and the revised Land Use Map;
3. INTRODUCE and conduct the first reading by title only of Ordinance No. XXX, approving a Change of Zone PEN19-0207 to amend the City Zoning Atlas to rezone 2.04-acres from Office Commercial (OC) District and Office (O) District to Community Commercial (CC) District for the areas described in the Ordinance, based on the findings contained in the Ordinance, and the revised Zoning Atlas;
4. ADOPT Resolution No. 2020-XX, A Resolution of the City Council of the City of Moreno Valley approving Conditional Use Permit PEN19-0204 based on the findings contained in this resolution, and subject to the conditions of approval included as Exhibit A;
5. ADOPT Resolution No. 2020-XX, A Resolution of the City Council of the City of Moreno Valley approving Conditional Use Permit PEN19-0205 based on the findings contained in this resolution, and subject to the conditions of approval included as Exhibit A; and
6. SCHEDULE the second reading and adoption of Ordinance No. XXX for the next regular City Council meeting.

F.2. MUNICIPAL CODE AMENDMENTS MODIFYING VARIOUS SECTIONS WITHIN TITLE 9 PLANNING AND ZONING: CHAPTER 9.02 PERMITS AND APPROVALS, CHAPTER 9.09 SPECIFIC USE DEVELOPMENT STANDARDS, CHAPTER 9.11 PARKING, PEDESTRIAN AND LOADING REQUIREMENTS, CHAPTER 9.12 SIGN REGULATIONS, AND TITLE 12 VEHICLES AND TRAFFIC, ADDING CHAPTER 12.24 DRIVING UPON ACCESS RAMPS PROHIBITED. (Report of: Community Development)

Recommendations:

1. Introduce and conduct the first reading of Ordinance No. XXX amending Sections 9.02.030, 9.02.080, 9.02.150, 9.02.240, 9.02.280, 9.09.080, 9.11.040, 9.11.070, 9.12.060, and 9.12.070 of Title 9 the

City of Moreno Valley Municipal Code to provide greater flexibility and clarity regarding existing requirements and to streamline certain entitlement procedures in order to promote economic development within the City;

2. Introduce and conduct the first reading of Ordinance No. XXX adding Chapter 12.24 of Title 12 of the City of Moreno Valley Municipal Code related to the prohibition of driving on City maintained sidewalks and pedestrian access ramps; and
3. Schedule the second reading and adoption of Ordinance Nos. XXX and XXX for the next regular Council meeting.

G. GENERAL BUSINESS

G.1. FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW AND APPROVAL OF THE FIRST QUARTER BUDGET AMENDMENTS AND REVIEW OF THE OPERATING AND CAPITAL CARRYOVERS FROM FISCAL YEAR 2019/20 (RESO NO. 2020-XX, CSD 2020-XX, HA 2020-XX, SA 2020-XX) (Report of: Financial & Management Services)

Recommendations: That the City Council:

1. Receive and file the Fiscal Year 2020/21 First Quarter Budget Review.
2. Adopt Resolution No. 2020-XX. A resolution of the City Council of the City of Moreno Valley, California, adopting the revised budgets for fiscal year 2020/21.
3. Approve the City Position Summary.
4. Receive and file the Fiscal Year 2019/20 Carryover Memo.

Recommendation: That the CSD:

1. Adopt Resolution No. CSD 2020-XX. A resolution of the Moreno Valley Community Services District of the City of Moreno Valley, California, adopting the revised operating and capital budgets for Fiscal Year (FY) 2020/21.

Recommendation: That the HA:

1. Adopt Resolution No. HA 2020-XX. A resolution of the Moreno Valley Housing Authority of the City of Moreno Valley, California, adopting the revised operating and capital budget for Fiscal Year 2019/20.

Recommendation: That the City Council as Successor Agency:

1. Adopt Resolution No. Successor Agency 2020-XX. A Resolution of the City Council of the City of Moreno Valley, California, Serving as Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley, California, adopting the revised operating and capital budget for Fiscal Year 2019/20.
- G.2. City Council Reorganization - Selection of Mayor Pro Tem (Report of: City Clerk)

Recommendation: That the City Council:

1. Conduct the reorganization of the City Council by selecting one Council Member to serve a one-year term as Mayor Pro Tem.

H. ITEMS REMOVED FROM CONSENT CALENDARS FOR DISCUSSION OR SEPARATE ACTION

I. REPORTS

I.1. CITY COUNCIL REPORTS

(Informational Oral Presentation - not for Council action)

March Joint Powers Commission (JPC)

Riverside County Habitat Conservation Agency (RCHCA)

Riverside County Transportation Commission (RCTC)

Riverside Transit Agency (RTA)

Western Riverside Council of Governments (WRCOG)

Western Riverside County Regional Conservation Authority (RCA)

School District/City Joint Task Force

I.2. CITY MANAGER'S REPORT

(Informational Oral Presentation - not for Council action)

CLOSING COMMENTS AND/OR REPORTS OF THE CITY COUNCIL, COMMUNITY SERVICES DISTRICT, CITY AS SUCCESSOR AGENCY FOR THE COMMUNITY REDEVELOPMENT AGENCY, HOUSING AUTHORITY, PUBLIC FINANCING AUTHORITY, AND THE BOARD OF LIBRARY TRUSTEES.

ADJOURNMENT

PUBLIC INSPECTION

The contents of the agenda packet are available for public inspection on the City's website at www.moval.org and in the City Clerk's office at 14177 Frederick Street during normal business hours.

Any written information related to an open session agenda item that is known by the City to have been distributed to all or a majority of the City Council less than 72 hours prior to this meeting will be made available for public inspection on the City's website at www.moval.org and in the City Clerk's office at 14177 Frederick Street during normal business hours.

CERTIFICATION

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, certify that 72 hours prior to this Regular Meeting, the City Council Agenda was posted on the City's website at: www.moval.org and in the following three public places pursuant to City of Moreno Valley Resolution No. 2007-40:

City Hall, City of Moreno Valley
14177 Frederick Street

Moreno Valley Library
25480 Alessandro Boulevard

Moreno Valley Senior/Community Center
25075 Fir Avenue

Pat Jacquez-Nares, CMC & CERA
City Clerk

Date Posted: December 10, 2020



Report to City Council

TO: Mayor and City Council

FROM: Pat Jacquez-Nares, City Clerk

AGENDA DATE: December 15, 2020

TITLE: RESOLUTION RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION HELD ON NOVEMBER 3, 2020, DECLARING THE OFFICIAL RESULTS AND SUCH OTHER MATTERS AS PROVIDED BY LAW

RECOMMENDED ACTION

Recommendation: That the City Council:

1. Adopt a Resolution of the City Council of the City of Moreno Valley, California, reciting the fact of the General Municipal Election held on November 3, 2020, declaring the results and such other matters as provided by law.

SUMMARY

Pursuant to Sections 10262 and 10263 of the California Elections Code, the elections official shall certify the results to the governing body and the governing body shall adopt a resolution reciting the fact of the election, declaring the official results and such other matters as provided by law, no later than the next regularly scheduled City Council meeting following presentation of the canvass, or at a special meeting called for that purpose. The official canvass of the election was received on Friday, December 4, 2020. Adoption of the proposed resolution will comply with these requirements.

DISCUSSION

On Tuesday, November 3, 2020, the City of Moreno Valley conducted a General Municipal Election for the purpose of electing Council Members in Districts 1 and 3, and one Mayor, to be elected City-wide. The Riverside County Registrar of Voters has completed the canvass of the election as provided in Chapter 4 of the California Elections Code, which is attached hereto as Exhibit A.

NOTIFICATION

Publication of the agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Pat Jacquez-Nares
City Clerk

Department Head Approval:
Pat Jacquez-Nares
City Clerk

CITY COUNCIL GOALS

None

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Resolution Canvass of Election

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/07/20 4:47 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/07/20 6:37 PM

RESOLUTION NO. 2020-

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION HELD ON NOVEMBER 3, 2020, DECLARING THE RESULTS AND SUCH OTHER MATTERS AS PROVIDED BY LAW

WHEREAS, a General Municipal Election was held and conducted in the City of Moreno Valley, California, on Tuesday, November 3, 2020 as required by law; and

WHEREAS, notice of the election was given in time, form and manner as provided by law; voting precincts were properly established; election officers were appointed and in all respects the election was held and conducted and the votes were cast, received and canvassed, and the returns made and declared in time, form and manner as required by the provisions of the Elections Code of the State of California for the holding of elections in general law cities; and

WHEREAS, the Riverside County Registrar of Voters canvassed the returns of the election and has certified the results to this City Council, the results are received, attached and made a part hereof as Exhibit "A";

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE, DECLARE, DETERMINE AND ORDER AS FOLLOWS:

SECTION 1. That the County of Riverside Registrar of Voters Office has counted the total number of votes (ballots) cast in November 3, 2020 General Municipal Election for the City of Moreno Valley and has provided the vote totals for each office by candidate name.

SECTION 2. That the names of persons and the offices for which they were voted for at the election are as follows:

<u>For Mayor at Large (Two Year Term)</u>	<u>Votes</u>
L. Lawrence Baird	3,187
Daryl Terrell	5,945
Yxstian Gutierrez	38,088
Mary McBean	21,314
<u>For Member of the City Council District 1 (Four Year Term)</u>	<u>Votes</u>
Victoria Baca	6,344
Dwayne Lewis	2,382
D. La Donna Jempson	3,062

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION

<u>For Member of the City Council District 3 (Four Year Term)</u>	<u>Votes</u>
Rafael Brugueras	5,387
Jorge Zavala	1,975
David Giron	2,695
David Marquez	6,053

SECTION 3. That the number of votes given at each precinct and the number of votes given in the City to each of the persons above named for the respective offices for which the persons were candidates as listed in Exhibit "A" attached.

SECTION 4. The City Council does declare and determine that Yxstian Gutierrez was elected as Mayor for a full term of two years; that Victoria Baca was elected as a Member of the City Council for District 1 for a full term of four years; and that David Marquez was elected as a Member of the City Council for District 3 for a full term of four years.

SECTION 5. That the City Clerk shall enter on the records of the City Council of the City, a statement of the results of the election showing: (1) the whole number of votes (ballots) cast in the City; (2) the names of the persons voted for; (3) for what office each person was voted for; (4) the number of votes given at each precinct to each person; (6) the total number of votes given to each person.

SECTION 6. That the City Clerk shall immediately make and deliver to each of the persons so elected a Certificate of Election signed by the City Clerk and authenticated; that the City Clerk shall also administer to each person elected the Oath of Office prescribed in the Constitution of the State of California and shall have them subscribe to it and file it in the office of the City Clerk. Each and all of the persons so elected shall then be inducted into the respective office to which they have been elected.

SECTION 7. That the City Clerk shall certify to the passage and adoption of this resolution and enter it into the book of original resolutions.

PASSED, APPROVED AND ADOPTED this 15th day of December, 2020.

 Dr. Yxstian A. Gutierrez
 Mayor
 City of Moreno Valley

APPROVED AS TO FORM:

ATTEST:

 Steve Quintanilla
 Interim City Attorney

 Pat Jacquez-Nares
 City Clerk

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Resolution No. 2020-xx was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the 15th day of December, 2020 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION



REBECCA SPENCER
Registrar of Voters

ART TINOCO
Assistant Registrar of Voters

REGISTRAR OF VOTERS
COUNTY OF RIVERSIDE

**CERTIFICATE OF REGISTRAR OF VOTERS
TO THE RESULTS OF THE CANVASS OF ELECTION RETURNS**

State of California)
) ss.
County of Riverside)

I, **REBECCA SPENCER**, Registrar of Voters of said County, do hereby certify that, in pursuance of the provisions of Sections 15301, 15372, and 15374 of the California Elections Code, and the resolution adopted by the City Council, I did canvass the returns of the votes cast on November 3, 2020, as part of the Consolidated General Election in the


CITY OF MORENO VALLEY

and I further certify that the statement of votes cast, to which this certificate is attached, shows the whole number of votes for each candidate for elective office at said election, in said City, and in each precinct therein, and that the totals as shown for each candidate are full, true, and correct.

Dated this 3rd day of December 2020

REBECCA SPENCER
Registrar of Voters





Melissa Kouba
Chief Deputy Registrar of Voters

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF THE GENERAL MUNICIPAL ELECTION

Moreno Valley City Mayor (Vote for 1) **** - Insufficient Turnout to Protect Voter Privacy

Precinct	Times Cast	Registered Voters
Countywide		
Electionwide		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
Electionwide - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	73,498	98,948
STATE		
CALIFORNIA		
0050100	0	0
0050110	1,951	2,836

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
Countywide		
Electionwide		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
Electionwide - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	3,187	5,945
STATE		
CALIFORNIA		
0050100	0	0
0050110	76	182

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
Countywide			
Electionwide			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
Electionwide - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Countywide - Total	38,088	21,314	68,534
STATE			
CALIFORNIA			
0050100	0	0	0
0050110	952	594	1,804

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
CALIFORNIA - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	73,498	98,948
Congressional		
41st Congressional District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
CALIFORNIA - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	3,187	5,945
Congressional		
41st Congressional District		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
CALIFORNIA - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
STATE - Total	38,088	21,314	68,534
Congressional			
41st Congressional District			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
41st Congressional District - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	73,498	98,948
State Senate		
31st Senatorial District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
41st Congressional District - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	3,187	5,945
State Senate		
31st Senatorial District		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
41st Congressional District - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Congressional - Total	38,088	21,314	68,534
State Senate			
31st Senatorial District			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
31st Senatorial District - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	73,498	98,948
State Assembly		
61st Assembly District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
31st Senatorial District - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	3,187	5,945
State Assembly		
61st Assembly District		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
31st Senatorial District - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
State Senate - Total	38,088	21,314	68,534
State Assembly			
61st Assembly District			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
0050436	0	0
61st Assembly District - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	73,498	98,948
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	73,498	98,948
Cumulative		

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
0050436	0	0
61st Assembly District - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	3,187	5,945
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	3,187	5,945
Cumulative		

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTTEREZ	MARY E. MCEBEAN	Total Votes
0050436	0	0	0
61st Assembly District - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
State Assembly - Total	38,088	21,314	68,534
State Board of Equalization			
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	38,088	21,314	68,534
Cumulative			

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	73,498	98,948
County		
Riverside County		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
Riverside County - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	73,498	98,948

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	3,187	5,945
County		
Riverside County		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
Riverside County - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	3,187	5,945

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
Cumulative	0	0	0
Cumulative - Total	0	0	0
State Board of Equalization - Total	38,088	21,314	68,534
County			
Riverside County			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
Riverside County - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
County - Total	38,088	21,314	68,534

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
County Supervisor		
5th Supervisorial District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
5th Supervisorial District - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	73,498	98,948
Cities		
City of Moreno Valley		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
County Supervisor		
5th Supervisorial District		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
5th Supervisorial District - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	3,187	5,945
Cities		
City of Moreno Valley		
0050100	0	0
0050110	76	182
0050119	187	415

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
County Supervisor			
5th Supervisorial District			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
5th Supervisorial District - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
County Supervisor - Total	38,088	21,314	68,534
Cities			
City of Moreno Valley			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
City of Moreno Valley - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	73,498	98,948
Unincorporated		
Unincorporated		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
0050203	5,560	6,878
0050220	6,560	8,424
0050235	8,296	10,264
0050239	2,379	3,037

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
City of Moreno Valley - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	3,187	5,945
Unincorporated		
Unincorporated		
0050100	0	0
0050110	76	182
0050119	187	415
0050126	12	18
0050127	247	623
0050203	319	451
0050220	316	508
0050235	489	854
0050239	126	186

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
City of Moreno Valley - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Cities - Total	38,088	21,314	68,534
Unincorporated			
Unincorporated			
0050100	0	0	0
0050110	952	594	1,804
0050119	2,322	1,220	4,144
0050126	135	73	238
0050127	3,577	1,653	6,100
0050203	2,772	1,606	5,148
0050220	3,514	1,732	6,070
0050235	3,782	2,508	7,633
0050239	1,189	712	2,213

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
0050403	6,387	9,088
0050405	825	1,246
0050424	6,954	9,131
0050427	5,124	6,356
0050435	0	0
0050436	0	0
Unincorporated - Total	73,498	98,948
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	73,498	98,948

Precinct	L. LAWRENCE BAIRD	DARYL TERRELL
0050300	79	139
0050309	300	626
0050313	0	0
0050324	64	99
0050328	315	500
0050403	228	411
0050405	34	50
0050424	185	485
0050427	210	398
0050435	0	0
0050436	0	0
Unincorporated - Total	3,187	5,945
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	3,187	5,945

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	YXSTIAN GUTIERREZ	MARY E. MCBEAN	Total Votes
0050300	990	506	1,714
0050309	4,024	2,440	7,390
0050313	0	0	0
0050324	540	371	1,074
0050328	3,987	2,104	6,906
0050403	3,655	1,730	6,024
0050405	484	207	775
0050424	3,686	2,141	6,497
0050427	2,479	1,717	4,804
0050435	0	0	0
0050436	0	0	0
Unincorporated - Total	38,088	21,314	68,534
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Unincorporated - Total	38,088	21,314	68,534

Moreno Valley City Council, Dist 1 (Vote for 1) **** - Insufficient Turnout to Protect Voter Privacy

Precinct	Times Cast	Registered Voters
Countywide		
Electionwide		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
Electionwide - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	13,130	19,438
STATE		
CALIFORNIA		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
CALIFORNIA - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	13,130	19,438
Congressional		
41st Congressional District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503

Precinct	VICTORIA BACA	DWAYNE LEWIS
Countywide		
Electionwide		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
Electionwide - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	6,344	2,382
STATE		
CALIFORNIA		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
CALIFORNIA - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	6,344	2,382
Congressional		
41st Congressional District		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	D. LA DONNA JEMPSON	Total Votes
Countywide		
Electionwide		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
Electionwide - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	3,062	11,788
STATE		
CALIFORNIA		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
CALIFORNIA - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	3,062	11,788
Congressional		
41st Congressional District		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
41st Congressional District - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	13,130	19,438
State Senate		
31st Senatorial District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
31st Senatorial District - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	13,130	19,438
State Assembly		
61st Assembly District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
61st Assembly District - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	13,130	19,438

Precinct	VICTORIA BACA	DWAYNE LEWIS
41st Congressional District - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	6,344	2,382
State Senate		
31st Senatorial District		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
31st Senatorial District - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	6,344	2,382
State Assembly		
61st Assembly District		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
61st Assembly District - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	6,344	2,382

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	D. LA DONNA JEMPSON	Total Votes
41st Congressional District - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	3,062	11,788
State Senate		
31st Senatorial District		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
31st Senatorial District - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	3,062	11,788
State Assembly		
61st Assembly District		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
61st Assembly District - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	3,062	11,788

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	13,130	19,438
County		
Riverside County		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
Riverside County - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	13,130	19,438
County Supervisor		
5th Supervisorial District		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503

Precinct	VICTORIA BACA	DWAYNE LEWIS
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	6,344	2,382
County		
Riverside County		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
Riverside County - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	6,344	2,382
County Supervisor		
5th Supervisorial District		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	D. LA DONNA JEMPSON	Total Votes
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	3,062	11,788
County		
Riverside County		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
Riverside County - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	3,062	11,788
County Supervisor		
5th Supervisorial District		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
5th Supervisorial District - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	13,130	19,438
Cities		
City of Moreno Valley		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
City of Moreno Valley - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	13,130	19,438
Unincorporated		
Unincorporated		
0050100	0	0
0050110	1,951	2,836
0050119	4,406	6,636
0050126	257	463
0050127	6,516	9,503
Unincorporated - Total	13,130	19,438
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	13,130	19,438

Precinct	VICTORIA BACA	DWAYNE LEWIS
5th Supervisorial District - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	6,344	2,382
Cities		
City of Moreno Valley		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
City of Moreno Valley - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	6,344	2,382
Unincorporated		
Unincorporated		
0050100	0	0
0050110	916	389
0050119	2,121	811
0050126	123	61
0050127	3,184	1,121
Unincorporated - Total	6,344	2,382
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	6,344	2,382

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	D. LA DONNA JEMPSON	Total Votes
5th Supervisorial District - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	3,062	11,788
Cities		
City of Moreno Valley		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
City of Moreno Valley - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	3,062	11,788
Unincorporated		
Unincorporated		
0050100	0	0
0050110	463	1,768
0050119	1,062	3,994
0050126	57	241
0050127	1,480	5,785
Unincorporated - Total	3,062	11,788
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	3,062	11,788

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Moreno Valley City Council, Dist 3 (Vote for 1) **** - Insufficient Turnout to Protect Voter Privacy

Precinct	Times Cast	Registered Voters
Countywide		
Electionwide		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
Electionwide - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	18,283	25,086
STATE		
CALIFORNIA		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
CALIFORNIA - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	18,283	25,086
Congressional		
41st Congressional District		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259

Precinct	RAFAEL BRUGUERAS	JORGE ZAVALA
Countywide		
Electionwide		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
Electionwide - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Countywide - Total	5,387	1,975
STATE		
CALIFORNIA		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
CALIFORNIA - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
STATE - Total	5,387	1,975
Congressional		
41st Congressional District		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	DAVID GIRON	DAVID MARQUEZ	Total Votes
Countywide			
Electionwide			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
Electionwide - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Countywide - Total	2,695	6,053	16,110
STATE			
CALIFORNIA			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
CALIFORNIA - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
STATE - Total	2,695	6,053	16,110
Congressional			
41st Congressional District			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
41st Congressional District - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	18,283	25,086
State Senate		
31st Senatorial District		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
31st Senatorial District - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	18,283	25,086
State Assembly		
61st Assembly District		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
61st Assembly District - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	18,283	25,086

Precinct	RAFAEL BRUGUERAS	JORGE ZAVALA
41st Congressional District - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Congressional - Total	5,387	1,975
State Senate		
31st Senatorial District		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
31st Senatorial District - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Senate - Total	5,387	1,975
State Assembly		
61st Assembly District		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
61st Assembly District - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Assembly - Total	5,387	1,975

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	DAVID GIRON	DAVID MARQUEZ	Total Votes
41st Congressional District - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Congressional - Total	2,695	6,053	16,110
State Senate			
31st Senatorial District			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
31st Senatorial District - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
State Senate - Total	2,695	6,053	16,110
State Assembly			
61st Assembly District			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
61st Assembly District - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
State Assembly - Total	2,695	6,053	16,110

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	18,283	25,086
County		
Riverside County		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
Riverside County - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	18,283	25,086
County Supervisor		
5th Supervisorial District		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259

Precinct	RAFAEL BRUGUERAS	JORGE ZAVALA
State Board of Equalization		
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
State Board of Equalization - Total	5,387	1,975
County		
Riverside County		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
Riverside County - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County - Total	5,387	1,975
County Supervisor		
5th Supervisorial District		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797

Precinct	DAVID GIRON	DAVID MARQUEZ	Total Votes
State Board of Equalization			
St Bd of Equalization 4 (Por. SB, SD, Or & Imp)			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
St Bd of Equalization 4 (Por. SB, SD, Or & Imp) - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
State Board of Equalization - Total	2,695	6,053	16,110
County			
Riverside County			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
Riverside County - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
County - Total	2,695	6,053	16,110
County Supervisor			
5th Supervisorial District			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	Times Cast	Registered Voters
5th Supervisorial District - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	18,283	25,086
Cities		
City of Moreno Valley		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
City of Moreno Valley - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	18,283	25,086
Unincorporated		
Unincorporated		
0050300	1,837	2,757
0050309	7,892	10,568
0050313	0	0
0050324	1,163	1,502
0050328	7,391	10,259
Unincorporated - Total	18,283	25,086
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	18,283	25,086

Precinct	RAFAEL BRUGUERAS	JORGE ZAVALA
5th Supervisorial District - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
County Supervisor - Total	5,387	1,975
Cities		
City of Moreno Valley		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
City of Moreno Valley - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Cities - Total	5,387	1,975
Unincorporated		
Unincorporated		
0050300	537	246
0050309	2,282	813
0050313	0	0
0050324	320	119
0050328	2,248	797
Unincorporated - Total	5,387	1,975
Cumulative		
Cumulative	0	0
Cumulative - Total	0	0
Unincorporated - Total	5,387	1,975

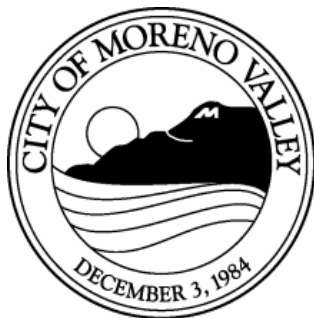
Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

Precinct	DAVID GIRON	DAVID MARQUEZ	Total Votes
5th Supervisorial District - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
County Supervisor - Total	2,695	6,053	16,110
Cities			
City of Moreno Valley			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
City of Moreno Valley - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Cities - Total	2,695	6,053	16,110
Unincorporated			
Unincorporated			
0050300	286	524	1,593
0050309	1,161	2,718	6,974
0050313	0	0	0
0050324	168	416	1,023
0050328	1,080	2,395	6,520
Unincorporated - Total	2,695	6,053	16,110
Cumulative			
Cumulative	0	0	0
Cumulative - Total	0	0	0
Unincorporated - Total	2,695	6,053	16,110

Attachment: Resolution Canvass of Election [Revision 1] (4236 : RESOLUTION RECITING THE FACT OF

**MINUTES
CITY COUNCIL REGULAR MEETING OF THE CITY OF MORENO VALLEY
December 1, 2020**

TELECONFERENCED MEETING



**CITY COUNCIL REGULAR MEETING - 6:00 PM
DECEMBER 1, 2020
[Pursuant to Governor Executive Order N-29-20]**

There Will Not Be a Physical Location for Attending the Meeting

The Public May Observe the Meeting and Offer Public Comment As Follows:

STEP 1

Install the Free Zoom App or Visit the Free Zoom Website at [<https://zoom.us/>](https://zoom.us/)

STEP 2

Get Meeting ID Number and Password by emailing zoom@moval.org
[<mailto:zoom@moval.org>](mailto:zoom@moval.org) or calling (951) 413-3001

STEP 3

Select Audio Source

*Computer Speakers/Microphone
or
Telephone*

STEP 3

Public Comments May be Made Via Zoom

During the Meeting, the Mayor Will Explain the Process for Submitting Public Comments

ALTERNATIVE

If you do not wish to make public comments, you can view the meeting on Channel MVTV-3, the City's website at www.moval.org or YouTube

Minutes Acceptance: Minutes of Dec 1, 2020 6:00 PM (CONSENT CALENDAR-CITY COUNCIL)

**MINUTES
JOINT MEETING OF THE
CITY COUNCIL OF THE CITY OF MORENO VALLEY
MORENO VALLEY COMMUNITY SERVICES DISTRICT
CITY AS SUCCESSOR AGENCY FOR THE
COMMUNITY REDEVELOPMENT AGENCY OF THE
CITY OF MORENO VALLEY
MORENO VALLEY HOUSING AUTHORITY
MORENO VALLEY PUBLIC FINANCING AUTHORITY
BOARD OF LIBRARY TRUSTEES**

REGULAR MEETING – 6:00 PM

December 1, 2020

CALL TO ORDER

The Joint Meeting of the City Council, Community Services District, City as Successor Agency for the Community Redevelopment Agency of the City of Moreno Valley, Moreno Valley Housing Authority, Moreno Valley Public Financing Authority and the Board of Library Trustees was called to order at 6:00 p.m. by Mayor Gutierrez in the Council Chamber located at 14177 Frederick Street.

Mayor Gutierrez announced that the City Council receives a separate stipend for CSD meetings.

ROLL CALL

Council:	Dr. Yxstian A. Gutierrez	Mayor
	David Marquez	Council Member
	Ulises Cabrera	Council Member
	Dr. Carla J. Thornton	Council Member

Absent:	Victoria Baca	Mayor Pro Tem
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INTRODUCTIONS

Staff:	Pat Jacquez-Nares	City Clerk
	Steve Quintanilla	Interim City Attorney
	Mike Lee	City Manager
	Marshall Eyerman	Assistant City Manager/Chief Financial Officer
	Michael Wolfe	Assistant City Manager/Director of Public Works/City Engineer
	Manuel Mancha	Community Development Director

Minutes Acceptance: Minutes of Dec 1, 2020 6:00 PM (CONSENT CALENDAR-CITY COUNCIL)

Patti Solano
John Salisbury

Parks & Community Services Director
Chief of Police

PUBLIC COMMENTS ON ANY SUBJECT ON THE AGENDA AND NOT ON THE AGENDA UNDER THE JURISDICTION OF THE CITY COUNCIL

Angel Lopez

1. Congratulated the winning City Council and School Board members.
2. Commended District 3 voters for reelecting Council Member Marquez.
3. Expressed his dismay at the speakers who continue to criticize Council Member Marquez.

Keri Then

1. Astounded that the City Council requested an Ordinance that does not limit campaign contributions.
2. Upset that the Public can't view the City Council members during the meetings.
3. Requested that the draft minutes for the November 17th meeting be corrected.
4. Commented on the unethical campaigns of some City Council candidates.

Louise Palomarez

1. Critical of the comments made by the previous speakers.
2. Disparaged Council Member Marquez.
3. Praised the City Council.

Brandon Carn

1. Condemned Super Pac's and their unlimited influence over elections.

JOINT CONSENT CALENDARS (SECTIONS A-E)

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Ulises Cabrera, Council Member
SECONDER:	Dr. Carla J. Thornton, Council Member
AYES:	Dr. Yxstian A. Gutierrez, David Marquez, Ulises Cabrera, Dr. Carla J. Thornton
ABSENT:	Victoria Baca

A. CONSENT CALENDAR-CITY COUNCIL

- A.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- A.2. City Council - Regular Meeting - Nov 17, 2020 6:00 PM

Recommendation: Approve as submitted.

- A.3. COUNCIL DISCRETIONARY EXPENDITURE REPORTS FOR FISCAL YEAR 2020/2021 FROM JULY 1, 2020 THROUGH OCTOBER 31, 2020 (Report of: City Clerk)

Recommendation:

1. Receive and file the Fiscal Year 2020/2021 Council Discretionary Expenditure Report for July 1, 2020 through October 31, 2020

- A.4. LIST OF PERSONNEL CHANGES (Report of: Financial & Management Services)

Recommendation:

1. Ratify the list of personnel changes as described.

- A.5. SECOND READING AND CONSIDERATION OF ADOPTION OF ORDINANCE 971, AMENDING CHAPTER 5.05 COMMERCIAL CANNABIS REGULATORY ACTIVITY TO PROVIDE CLEAN UP LANGUAGE TO AVOID DUPLICATE OR CONFLICT WITH THE STATE'S REGULATIONS (ORD. NO. 971) (Report of: Financial & Management Services)

Recommendation: That the City Council:

1. Adopt Ordinance 971, amending Chapter 5.05 Commercial Cannabis Regulatory Activity to provide clean up language to avoid duplicate or conflict with the State's Cannabis Regulations.

- A.6. PURSUANT TO LANDOWNER PETITION, ANNEX CERTAIN PARCELS INTO COMMUNITY FACILITIES DISTRICT NO. 2014-01 (MAINTENANCE SERVICES) - AMENDMENT NO. 49, AMENDMENT NO. 50, AMENDMENT NO. 51, AND AMENDMENT NO. 52 (RESO. NOS. 2020-74 - 77) (Report of: Financial & Management Services)

Recommendations:

1. Acting as the legislative body of Community Facilities District No. 2014-01 (Maintenance Services), adopt Resolution No. 2020-74, a Resolution of the City Council of the City of Moreno Valley, California, ordering the annexation of territory to City of Moreno Valley Community Facilities District No. 2014-01 (Maintenance Services) and approving the amended map for said District. (Amendment No. 49). (Courtyards at Cottonwood, LP, located on the northeast corner of Cottonwood Ave. and Indian St.).
2. Acting as the legislative body of Community Facilities District No. 2014-01 (Maintenance Services), adopt Resolution No. 2020-75, a Resolution of the City Council of the City of Moreno Valley, California,

ordering the annexation of territory to City of Moreno Valley Community Facilities District No. 2014-01 (Maintenance Services) and approving the amended map for said District. (Amendment No. 50). (FH II located on the northwest corner of Quincy St. and Brodiaea Ave.).

3. Acting as the legislative body of Community Facilities District No. 2014-01 (Maintenance Services), adopt Resolution No. 2020-76, a Resolution of the City Council of the City of Moreno Valley, California, ordering the annexation of territory to City of Moreno Valley Community Facilities District No. 2014-01 (Maintenance Services) and approving the amended map for said District. (Amendment No. 51). (FR Nandina Avenue located southeast of Nandina Ave. and Indian St.)
 4. Acting as the legislative body of Community Facilities District No. 2014-01 (Maintenance Services), adopt Resolution No. 2020-77, a Resolution of the City Council of the City of Moreno Valley, California, ordering the annexation of territory to City of Moreno Valley Community Facilities District No. 2014-01 (Maintenance Services) and approving the amended map for said District. (Amendment No. 52). (Right Solutions located on the southeast corner of Dunlavy Ct. and Davis St.).
- A.7. PEN19-0213 – ADOPTION OF THE PROPOSED RESOLUTION FOR THE SUMMARY VACATION OF A EASTERLY PORTION OF CARDINAL AVENUE LOCATED 1,433 FEET EAST OF HEACOCK STREET TO ITS TERMINUS AT THE PERRIS VALLEY STORM DRAIN, A DISTANCE OF 2,089 FEET DEVELOPER: MOORPARK COUNTRY PROPERTIES (RESO. NO. 2020-78) (Report of: Public Works)

Recommendations:

1. Adopt Resolution No. 2020-78. A Resolution of the City Council of the City of Moreno Valley, California, Ordering the Summary Vacation of the easterly portion of Cardinal Avenue located 1,433 feet east of the intersection of Heacock Street to its terminus at the Perris Valley Storm Drain.
2. Direct the City Clerk to certify said resolution and transmit a copy of the resolution to the County Recorder's office for recording.

- A.8. PEN18-0224 (TR 35931) – APPROVE TRACT MAP 35931 LOCATED ON THE SOUTHWEST CORNER OF IRIS AVENUE AND VIA DEL LAGO, DEVELOPER: VIA DEL LAGO HOMES, LLC (Report of: Public Works)

Recommendations:

1. Approve Tract Map 35931 (A Condominium Map).
2. Authorize the City Clerk to sign the map and transmit said map to the County Recorder's Office for recordation.

- A.9. AUTHORIZATION TO AWARD A CONSTRUCTION CONTRACT TO ELECNOR BELCO ELECTRIC, INC. FOR THE ADVANCED DILEMMA ZONE DETECTION SYSTEMS PROJECT (PROJECT 808 0018) (AGMT. NO. 2020-266) (Report of: Public Works)

Recommendations:

1. Reject all other bids, waive any and all minor irregularities, and award a construction contract to Elecnor Belco Electric, Inc. for the Advanced Dilemma Zone Detection Systems project in the amount of \$812,979.00 and authorize the City Manager to execute the contract;
2. Authorize the issuance of a Purchase Order to Elecnor Belco Electric, Inc. in the amount of \$934,925.85 (\$812,979.00 bid amount plus a 15% contingency), using grant funds from the Highway Safety Improvement Program; and
3. Authorize the Public Works Director/City Engineer to execute any subsequent related change orders to the contract, not to exceed the contingency amount.

- A.10. RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY TO SUPPORT THE CREATION OF A MARCH AIR RESERVE BASE COMPATIBILITY USE STUDY UPDATE (CUS) OF THE 2010 MARCH AIR RESERVE BASE/INLAND PORT JOINT LAND USE STUDY (2010 JLUS) TO ASSURE COMPATIBLE LAND USES NEAR MARCH AIR RESERVE BASE TO PROTECT THE FUTURE AVIATION OPERATIONS OF THE UNITED STATES MILITARY (RESO. NO. 2020-79) (Report of: Community Development)

Recommendations:

1. Approve Resolution No 2020-79. A Resolution of the City Council of the City of Moreno Valley, California, to support the creation of a March Air Reserve Base Compatibility Use Study Update (CUS) of the 2010 March Air Reserve Base/Inland Port Joint Land Use Study (2010 JLUS) to assure compatible land uses near March Air Reserve

Base to protect the future aviation operations of the United States Military.

- A.11. AMENDING CHAPTER 3.48 (“WESTERN RIVERSIDE COUNTY MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE PROGRAM”) TO ELIMINATE LOW INCOME RESIDENTIAL HOUSING AS AN EXEMPTION TO THE MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE (ORD. NO. 972) (Report of: City Attorney)

RECOMMENDATION:

1. That the City Council adopt the attached ordinance amending Chapter 3.48 (“Western Riverside County Multiple Species Habitat Conservation Plan Fee Program”) to Title 3 (“Revenue and Finance”) of the Moreno Valley Municipal Code to eliminate “Low Income Residential Housing” as an exemption.

B. CONSENT CALENDAR-COMMUNITY SERVICES DISTRICT

- B.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- B.2. MINUTES - CITY COUNCIL - REGULAR MEETING - NOV 17, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

- B.3. PURSUANT TO LANDOWNER PETITIONS, ANNEX CERTAIN PARCELS INTO COMMUNITY FACILITIES DISTRICT NO. 1 (PARK MAINTENANCE) — AS ANNEXATION NO. 2020-54, ANNEXATION NO. 2020-57, ANNEXATION NO. 2020-58, AS ANNEXATION NO. 2020-59, AND ANNEXATION NO. 2020-61 (RESO. NOS. CSD 2020-30 - 34) (Report of: Financial & Management Services)

Recommendations:

1. Acting as the legislative body of Community Facilities District No. 1 (Park Maintenance) adopt Resolution No. CSD 2020-30, a Resolution of the Board of Directors of the Moreno Valley Community Services District, California, ordering the annexation of territory for Annexation No. 2020-54 to its Community Facilities District No. 1 and approving the amended map for said District. (Courtyards at Cottonwood, LP, located on the northeast corner of Cottonwood Ave. and Indian St.).

2. Acting as the legislative body of Community Facilities District No. 1 (Park Maintenance) adopt Resolution No. CSD 2020-31, a Resolution of the Board of Directors of the Moreno Valley Community Services District, California, ordering the annexation of territory for Annexation No. 2020-57 to its Community Facilities District No. 1 and approving the amended map for said District. (FH II located on the northwest corner of Quincy St. and Brodiaea Ave.).
3. Acting as the legislative body of Community Facilities District No. 1 (Park Maintenance) adopt Resolution No. CSD 2020-32, a Resolution of the Board of Directors of the Moreno Valley Community Services District, California, ordering the annexation of territory for Annexation No. 2020-58 to its Community Facilities District No. 1 and approving the amended map for said District. (Right Solutions located on the southeast corner of Dunlavy Ct. and Davis St.).
4. Acting as the legislative body of Community Facilities District No. 1 (Park Maintenance) adopt Resolution No. CSD 2020-33, a Resolution of the Board of Directors of the Moreno Valley Community Services District, California, ordering the annexation of territory for Annexation No. 2020-59 to its Community Facilities District No. 1 and approving the amended map for said District. (Juan Franco located at 12065 Webb St.).
5. Acting as the legislative body of Community Facilities District No. 1 (Park Maintenance) adopt Resolution No. CSD 2020-34, a Resolution of the Board of Directors of the Moreno Valley Community Services District, California, ordering the annexation of territory for Annexation No. 2020-61 to its Community Facilities District No. 1 and approving the amended map for said District. (Nafeesah Kahala located at 11668 Perris Blvd.).

C. CONSENT CALENDAR - HOUSING AUTHORITY

- C.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- C.2. MINUTES - CITY COUNCIL - REGULAR MEETING - NOV 17, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

D. CONSENT CALENDAR - BOARD OF LIBRARY TRUSTEES

- D.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- D.2. MINUTES - CITY COUNCIL - REGULAR MEETING - NOV 17, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

E. CONSENT CALENDAR - PUBLIC FINANCING AUTHORITY

- E.1. ORDINANCES - READING BY TITLE ONLY - THE MOTION TO ADOPT AN ORDINANCE LISTED ON THE CONSENT CALENDAR INCLUDES WAIVER OF FULL READING OF THE ORDINANCE.

Recommendation: Waive reading of all Ordinances.

- E.2. MINUTES - CITY COUNCIL - REGULAR MEETING - NOV 17, 2020 6:00 PM (See A.2)

Recommendation: Approve as submitted.

F. PUBLIC HEARINGS

- F.1. PUBLIC HEARING TO ADOPT UPDATES TO THE CARES ACT AMENDMENT TO THE 2019-2020 ANNUAL ACTION PLAN - 3rd ROUND ALLOCATION FOR CDBG-CV (Report of: Financial & Management Services)

Assistant City Manager/Chief Financial Officer Eyerman provided the report.

Mayor Gutierrez opened the Public Hearing at 6:20 p.m.

There being no comments in support or opposition, Mayor Gutierrez closed the Public Hearing at 6:20 p.m.

Recommendations: That the City Council:

1. Conduct a Public Hearing to allow public comment on the proposed updates to the CARES Act Amendment to the 2019-2020 Annual Action Plan.
2. Review and adopt the proposed updates to the CARES Act Amendment to the 2019-2020 Annual Action Plan.

3. Authorize a budget amendment as set forth in the fiscal impact section and authorize the Chief Financial Officer to allocate grant funds between HUD- approved grant activities.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	David Marquez, Council Member
SECONDER:	Dr. Carla J. Thornton, Council Member
AYES:	Dr. Yxstian A. Gutierrez, David Marquez, Ulises Cabrera, Dr. Carla J. Thornton
ABSENT:	Victoria Baca

F.2. PUBLIC HEARING FOR SEVEN NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM MAIL BALLOT PROCEEDINGS (Report of: Financial & Management Services)

Assistant City Manager/Chief Financial Officer Eyerman provided the report.

Mayor Gutierrez opened the Public Hearing at 6:21 p.m.

There being no comments in support or opposition, Mayor Gutierrez closed the Public Hearing at 6:21 p.m.

Recommendations: That the City Council:

1. Conduct the Public Hearing and accept public testimony for the mail ballot proceedings for the National Pollutant Discharge Elimination System (NPDES) Common Interest, Commercial, Industrial, and Quasi-Public Use Regulatory Rate or Residential Regulatory Rate to be applied to the property tax bill of the parcels identified herein for Courtyards at Cottonwood, LP located at the northeast corner of Cottonwood Ave. and Indian St. FR Nandina Avenue located southeast of Nandina Ave. and Indian St., Family Service Association located at the southeast corner Ironwood Ave. and Kilgore St., Juan Franco located at 12065 Webb St., Nafeesh Kahala located at 11668 Perris Blvd., Right Solutions located on the southeast corner of Dunlavy Ct. and Davis St., and FH II located on the northwest corner of Quincy St. and Brodiaea Ave.);
2. Direct the City Clerk to open and count the returned NPDES ballots;

RESULT: APPROVED [UNANIMOUS]
MOVER: David Marquez, Council Member
SECONDER: Ulises Cabrera, Council Member
AYES: Dr. Yxstian A. Gutierrez, David Marquez, Ulises Cabrera, Dr. Carla J. Thornton
ABSENT: Victoria Baca

3. Verify and accept the results of the mail ballot proceedings as maintained by the City Clerk on the Official Tally Sheet and if approved, set the rate and impose the NPDES Common Interest, Commercial, Industrial, and Quasi-Public Use Regulatory Rate or the Residential Regulatory Rate, as applicable, on the Assessor’s Parcel Numbers as mentioned;
4. Receive and file the Official Tally Sheet with the City Clerk’s office.

RESULT: APPROVED [UNANIMOUS]
MOVER: David Marquez, Council Member
SECONDER: Ulises Cabrera, Council Member
AYES: Dr. Yxstian A. Gutierrez, David Marquez, Ulises Cabrera, Dr. Carla J. Thornton
ABSENT: Victoria Baca

G. GENERAL BUSINESS

G.1. ADDING CHAPTER 2.65 “CAMPAIGN CONTRIBUTION REGULATIONS” TO TITLE 2 “ADMINISTRATION AND PERSONNEL” OF THE MORENO VALLEY MUNICIPAL CODE TO ESTABLISH REGULATIONS AND LIMITATIONS ON CAMPAIGN CONTRIBUTIONS FOR CITY ELECTED OFFICIALS (ORD. NO. 973) (Report of: City Attorney)

Interim City Attorney Quintanilla provide the report.

Council Member Marquez asked if the Ordinance is revocable.

Interim City Attorney Quintanilla responded in the affirmative.

Mayor Gutierrez queried what other cities are doing in response to the bill.

Interim City Attorney Quintanilla answered that several cities, including Riverside have no limits and noted that imposing limits could increase donations from PACs which have no oversight.

Council Member Cabrera remarked that although the intent of the legislation is good, the desired outcome will not occur.

Minutes Acceptance: Minutes of Dec 1, 2020 6:00 PM (CONSENT CALENDAR-CITY COUNCIL)

RECOMMENDATION:

1. That the City Council adopt the attached ordinance to add a new Chapter 2.65 (“Campaign Contribution Regulations”) to Title 2 (“Administration and Personnel”) of the Moreno Valley Municipal Code.

RESULT:	APPROVED [3 TO 1]
MOVER:	Ulises Cabrera, Council Member
SECONDER:	Dr. Carla J. Thornton, Council Member
AYES:	Dr. Yxstian A. Gutierrez, Ulises Cabrera, Dr. Carla J. Thornton
NAYS:	David Marquez
ABSENT:	Victoria Baca

H. ITEMS REMOVED FROM CONSENT CALENDARS FOR DISCUSSION OR SEPARATE ACTION - NONE**I. REPORTS****I.1. CITY COUNCIL REPORTS**

(Informational Oral Presentation - not for Council action)

March Joint Powers Commission (JPC) - None

Riverside County Habitat Conservation Agency (RCHCA) - Council Member Marquez

Council Member Marquez reported that the US Fish and Wildlife Service will propose a change to the classification of the Stephens' Kangaroo Rat from endangered to threatened species.

Riverside County Transportation Commission (RCTC) - None

Riverside Transit Agency (RTA) - Council Member Marquez

Council Member Marquez reported that the Board of Directors voted to utilize hydrogen fuel cell buses.

Western Riverside Council of Governments (WRCOG) - None

Western Riverside County Regional Conservation Authority (RCA) - None

School District/City Joint Task Force - None

I.2. CITY MANAGER'S REPORT

(Informational Oral Presentation - not for Council action)

City Manager Lee announced that the City's three libraries are open and that free masks are still available. He also encouraged residents to use the City's app to report code violations, graffiti, and pot holes. At the request of Mayor Gutierrez, he provided details of the virtual tree lighting ceremony.

CLOSING COMMENTS AND/OR REPORTS OF THE CITY COUNCIL, COMMUNITY SERVICES DISTRICT, CITY AS SUCCESSOR AGENCY FOR THE COMMUNITY REDEVELOPMENT AGENCY, HOUSING AUTHORITY, PUBLIC FINANCING AUTHORITY, AND THE BOARD OF LIBRARY TRUSTEES.

Council Member Thornton

1. Participated in the Phi Beta Sigma trail clean up on November 23rd with Council Member Cabrera.
2. Solicited partnerships for additional clean ups.
3. Urged individuals to get tested for Covid-19.
4. Pleaded with residents to contact Senator Roth's office regarding the trash on the freeways and off-ramps.
5. Announced that she was appointed to the LOCC's Housing Community and Economic Development Board.

Council Member Marquez

1. Attended the first Sheriff's Task-force meeting. Noted Captain Vaughn's willingness to assist with cleaning the littered off ramps.
2. Remarked that the task force will initially focus on seat belts and impaired driving.
3. Asked residents to contact him with any questions.

Council Member Cabrera

1. Participated in the clean up on Hidden Springs.
2. Invited residents to attend the District 4 clean up on Saturday, December 5, 2020.
3. Noted that road repairs are concluding.
4. Remarked on the Governor's Covid-19 relief package.
5. Wished everyone Happy Holidays and encouraged them to get tested for Covid-19.

Mayor Gutierrez

1. Reiterated the fact the off ramps and the freeways are the jurisdiction of Caltrans and assured residents that the City will take every course of action available to move them to act.
2. Reminded residents to continue to follow the CDC's social distancing precautions.
3. Called for residents to attend the Light It Up MoVal light show and the tree lighting ceremony.

4. Mourned the loss of his cousin, Mark Tanaka.

ADJOURNMENT

There being no further business to come before the City Council, Mayor Gutierrez adjourned the meeting in memory of Mark Tanaka at 6:54 p.m.

Submitted by:

Pat Jacquez-Nares, CMC & CERA
 City Clerk
 Secretary, Moreno Valley Community Services District
 Secretary, City as Successor Agency for the Community
 Redevelopment Agency of the City of Moreno Valley
 Secretary, Moreno Valley Housing Authority
 Secretary, Board of Library Trustees
 Secretary, Public Financing Authority

Approved by:

Dr. Yxstian A. Gutierrez
 Mayor
 City of Moreno Valley
 President, Moreno Valley Community Services District
 Chairperson, City as Successor Agency for the Community
 Redevelopment Agency of the City of Moreno Valley
 Chairperson, Moreno Valley Housing Authority
 Chairperson, Board of Library Trustees
 Chairperson, Public Financing Authority



Report to City Council

TO: Mayor and City Council

FROM: Pat Jacquez-Nares, City Clerk

AGENDA DATE: December 15, 2020

TITLE: 2021 CITY COUNCIL COMMISSION, BOARD, AND INTER-AGENCY APPOINTMENTS

RECOMMENDED ACTION

Recommendation: That the City Council:

1. Ratify the appointments to the various committees and regional bodies as noted on the 2021 Council Committee Participation List – terms end either June 30, 2021 or December 31, 2021.

SUMMARY

The previous Council Committee Participation appointments were for either six months or one year and have come to their end. Mayor Gutierrez has compiled the new 2021 Council Committee Participation appointments with the terms to end on June 30, 2021 or December 31, 2021 (Attachment A).

NOTIFICATION

The Form 806 (Attachment B) which lists all the paid appointed positions to which an official will vote to appoint themselves was posted on the City's website on December 10, 2020 pursuant to FPPC Regulation 18702.5 and posting of the agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Pat Jacquez-Nares
City Clerk

Department Head Approval:
Pat Jacquez-Nares
City Clerk

CITY COUNCIL GOALS

None

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Attachment A - 2021 Council Committee Participation
- 2. Attachment B - FPPC Form 806

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/08/20 9:56 AM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/08/20 10:07 AM

Revised 12/01/20
MAYOR'S RECOMMENDATIONS
2021 COUNCIL COMMITTEE PARTICIPATION

CITY COUNCIL ADVISORY COMMISSIONS/BOARDS:	Primary	Alternate(s)	Term
Arts Commission	Cabrera		6/30/2021
Emerging Leaders Council	Cabrera		6/30/2021
Environmental and Historical Preservation Board	Thornton		6/30/2021
Library Commission	Baca		6/30/2021
Parks, Community Services and Trails Committee	Cabrera		6/30/2021
Senior Citizens' Board	Baca		6/30/2021
Traffic Safety Commission	Thornton		6/30/2021
Utilities Commission	Thornton		6/30/2021

CITY COUNCIL SUBCOMMITTEES:

Economic Development Subcommittee <i>Appoint 2 Primary</i>	Gutierrez/Cabrera		6/30/2021
Finance Subcommittee <i>Appoint 2 Primary</i>	Gutierrez/Cabrera		6/30/2021
Citizens Public Safety Committee	Thornton Chair	Baca Vice Chair	6/30/2021
Public Safety Subcommittee <i>Appoint 2 Primary</i>	Thornton/Baca	Gutierrez	6/30/2021
Parks and Community Services Council Committee <i>Appoint 2 Primary and 2 Alternates</i>	Gutierrez/Baca		6/30/2021

INTER-AGENCY:

March Joint Powers Commission (JPC) <i>Appoint 2 Primary</i>	Baca/Thornton		12/31/2021
School Districts/City Joint Task Force <i>Appoint 2 Primary</i>	Baca/Cabrera		12/31/2021
Riverside County Habitat Conservation Agency (RCHCA)	Marquez		12/31/2021
Riverside County Transportation Commission (RCTC)	Gutierrez	Thornton	12/31/2021
Riverside Transit Agency (RTA)	Marquez	Gutierrez	12/31/2021
Western Riverside Council of Governments (WRCOG)	Thornton	Baca	12/31/2021
Western Riverside County Regional Conservation Authority (RCA)	Marquez		12/31/2021

**Agency Report of:
Public Official Appointments**

A Public Document


1. Agency Name City of Moreno Valley		California Form 806 For Official Use Only	
Division, Department, or Region (If Applicable)			
City Clerk			
Designated Agency Contact (Name, Title) Pat Jacquez-Nares, CMC & CERA		Date Posted: 12/10/2020 <small>(Month, Day, Year)</small>	Page <u>1</u> of <u>2</u>
Area Code/Phone Number 951.413.3010	E-mail patjn@moval.org		

2. Appointments

Agency Boards and Commissions	Name of Appointed Person	Appt Date and Length of Term	Per Meeting/Annual Salary/Stipend
March Joint Powers Commission (MJPC)	▶ Name <u>Baca, Victoria</u> <small>(Last, First)</small>	▶ <u>12 / 31 / 20</u> <small>Appt Date</small>	▶ Per Meeting: \$ <u>100.00</u>
	Alternate, if any <u>Thornton, Carla</u> <small>(Last, First)</small>	▶ <u>12 Months</u> <small>Length of Term</small>	▶ Estimated Annual: <input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000 <input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> Other
Riverside County Habitat Conservation Agency (RCHCA)	▶ Name <u>Marquez, David</u> <small>(Last, First)</small>	▶ <u>12 / 31 / 20</u> <small>Appt Date</small>	▶ Per Meeting: \$ <u>100.00</u>
	Alternate, if any _____ <small>(Last, First)</small>	▶ <u>12 Months</u> <small>Length of Term</small>	▶ Estimated Annual: <input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000 <input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> Other
Riverside County Transportation Commission (RCTC)	▶ Name <u>Gutierrez, Yxstian</u> <small>(Last, First)</small>	▶ <u>12 / 31 / 20</u> <small>Appt Date</small>	▶ Per Meeting: \$ <u>100.00</u>
	Alternate, if any <u>Thornton, Carla</u> <small>(Last, First)</small>	▶ <u>12 Months</u> <small>Length of Term</small>	▶ Estimated Annual: <input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000 <input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> Other
Riverside Transit Agency (RTA)	▶ Name <u>Marquez, David</u> <small>(Last, First)</small>	▶ <u>12 / 31 / 20</u> <small>Appt Date</small>	▶ Per Meeting: \$ <u>150.00</u>
	Alternate, if any <u>Gutierrez, Yxstian</u> <small>(Last, First)</small>	▶ <u>12 Months</u> <small>Length of Term</small>	▶ Estimated Annual: <input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000 <input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> Other

3. Verification

I have read and understand FPPC Regulation 18702.5. I have verified that the appointment and information identified above is true to the best of my information and belief.

 Signature of Agency Head or Designee	Pat Jacquez-Nares Print Name	City Clerk Title	12/10/2020 (Month, Day, Year)
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Comment: _____

**Agency Report of:
Public Official Appointments
Continuation Sheet**

1. Agency Name
City of Moreno Valley

Date Posted: 12/10/2020
(Month, Day, Year)

2. Appointments

Agency Boards and Commissions	Name of Appointed Person	Appt Date and Length of Term	Per Meeting/Annual Salary/Stipend
Western Riverside Council of Governments (WRCOG)	<p>▶ Name <u>Thornton, Carla</u> <i>(Last, First)</i></p> <p>Alternate, if any <u>Baca, Victoria</u> <i>(Last, First)</i></p>	<p>▶ <u>12 / 31 / 20</u> <i>Appt Date</i></p> <p>▶ _____ <i>Length of Term</i></p>	<p>▶ Per Meeting: \$ <u>150.00</u></p> <p>▶ Estimated Annual:</p> <p><input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000</p> <p><input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> _____ <i>Other</i></p>
Western Riverside County Regional Conservation Authority (RCA)	<p>▶ Name <u>Marquez, David</u> <i>(Last, First)</i></p> <p>Alternate, if any _____ <i>(Last, First)</i></p>	<p>▶ <u>12 / 31 / 20</u> <i>Appt Date</i></p> <p>▶ <u>12 Months</u> <i>Length of Term</i></p>	<p>▶ Per Meeting: \$ <u>100.00</u></p> <p>▶ Estimated Annual:</p> <p><input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000</p> <p><input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> _____ <i>Other</i></p>
	<p>▶ Name _____ <i>(Last, First)</i></p> <p>Alternate, if any _____ <i>(Last, First)</i></p>	<p>▶ _____ <i>Appt Date</i></p> <p>▶ _____ <i>Length of Term</i></p>	<p>▶ Per Meeting: \$ _____</p> <p>▶ Estimated Annual:</p> <p><input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000</p> <p><input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> _____ <i>Other</i></p>
	<p>▶ Name _____ <i>(Last, First)</i></p> <p>Alternate, if any _____ <i>(Last, First)</i></p>	<p>▶ _____ <i>Appt Date</i></p> <p>▶ _____ <i>Length of Term</i></p>	<p>▶ Per Meeting: \$ _____</p> <p>▶ Estimated Annual:</p> <p><input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000</p> <p><input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> _____ <i>Other</i></p>
	<p>▶ Name _____ <i>(Last, First)</i></p> <p>Alternate, if any _____ <i>(Last, First)</i></p>	<p>▶ _____ <i>Appt Date</i></p> <p>▶ _____ <i>Length of Term</i></p>	<p>▶ Per Meeting: \$ _____</p> <p>▶ Estimated Annual:</p> <p><input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000</p> <p><input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> _____ <i>Other</i></p>
	<p>▶ Name _____ <i>(Last, First)</i></p> <p>Alternate, if any _____ <i>(Last, First)</i></p>	<p>▶ _____ <i>Appt Date</i></p> <p>▶ _____ <i>Length of Term</i></p>	<p>▶ Per Meeting: \$ _____</p> <p>▶ Estimated Annual:</p> <p><input type="checkbox"/> \$0-\$1,000 <input type="checkbox"/> \$2,001-\$3,000</p> <p><input type="checkbox"/> \$1,001-\$2,000 <input type="checkbox"/> _____ <i>Other</i></p>



Report to City Council

TO: Mayor and City Council

FROM: Pat Jacquez-Nares, City Clerk

AGENDA DATE: December 15, 2020

TITLE: APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE

RECOMMENDED ACTION

Recommendation:

1. Receive and confirm the following appointments:

Citizens Public Safety Committee

<u>Name</u>	<u>Position</u>	<u>Term</u>
Jorge Alvarado	Member	Ending 06/30/21
Tanya Price	Member	Ending 06/30/21
Rosemary Rios	Member	Ending 06/30/21
Karen Sykes	Member	Ending 06/30/21
Liz Williams	Member	Ending 06/30/21
Wayne Woodliff	Member	Ending 06/30/21
Marquita Clay	Member	Ending 06/30/21

Each of the recommended appointees for the Citizens Public Safety Committee were screened and interviewed by the Citizens Public Safety Committee leaders consisting of Chair Mayor Pro Tem Baca and Vice Chair Council Member Thornton. Each recommended appointee was also subject to a criminal background check which is standard practice for all applicants for any City advisory commission or committee.

Additional appointees are undergoing background checks.

CITY COUNCIL GOALS

Advocacy. Develop cooperative intergovernmental relationships and be a forceful advocate of City policies, objectives, and goals to appropriate external governments, agencies and corporations.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Jorge A. Alvarado_Redacted
- 2. Tanya Price_Redacted
- 3. Rosemary Rios_Redacted
- 4. Karen Sykes_Redacted
- 5. Liz Williams_Redacted
- 6. Wayne Woodliff_Redacted
- 7. Marquita L. Clay_Redacted

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/08/20 9:54 AM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/08/20 10:08 AM

CITY CLERK
MORENO VALLEY
RECEIVED
20 AUG 13 AM 10:01

For City Clerk's Use
Stamp Date and Time Received



City of Moreno Valley

Boards and Commissions

Membership Application Form

Name: JORGE A. ALVARADO
Home Address: [REDACTED]
MORENO VALLEY, CALIFORNIA 92557

How long have you resided in Moreno Valley? 20+ YEARS

CONFIDENTIAL INFORMATION

Home Phone No.: _____ Driver's License No.: [REDACTED]
Work Phone No.: _____ Email Address: [REDACTED]
Cell Phone No.: [REDACTED] Date of Birth: [REDACTED]

Employer Name: CANYON CHILDREN'S LEGAL SERVICES (CCLS) Position: EXECUTIVE DIRECTOR
Address: 11875 Pigeon Pass Rd. Suite B-13, #131
MORENO VALLEY, CALIFORNIA 92557

Board or Commission applying for*: 1st Choice CITIZENS PUBLIC SAFETY COMMITTEE
2nd Choice _____

- *If applying for the Accessibility Appeals Board, please indicate which position you are applying for:
 Physically Challenged Person Person Experienced in Construction Public Member
- *If applying for the Utilities Commission, please indicate which position you are applying for:
 Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility
- *If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:
 Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?
MY UNIQUE EXPERIENCES AS AN ATTORNEY FOR OVER 36 YEARS HAS PROVIDED ME A BALANCED VIEW OF THE PUBLIC SAFETY ISSUES FOR WHICH THIS COMMISSION SHALL SERVE. I SEED TO PROVIDE QUALITY INPUT IN WHAT ISSUES ARE TACKLED BY THIS COMMISSION WITH THE ULTIMATE GOAL OF ENHANCING QUALITY OF LIFE AND SAFETY IN THE COMMUNITY.

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:
IN MY 36 YEARS OF PRACTICE AS AN ATTORNEY, I HAVE PROVIDED QUALITY LEGAL REPRESENTATION TO POOR PEOPLE IN CRIMINAL AND CIVIL CASES. I HAVE PROVIDED LEGAL ASSISTANCE TO VICTIMS OF CRIMES, ESPECIALLY THOSE OF DOMESTIC VIOLENCE, IN HOPES OF THEIR SAFE TRANSITION IN LIVING LIFE FREE OF VIOLENCE. I FOUNDED CCLS TO ESPECIALLY HELP CHILDREN FOR A BRIGHTER FUTURE.

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.
THE COMMISSION WILL SERVE AS AN ADVISORY BOARD TO THE CITY COUNCIL IN AREAS AFFECTING PUBLIC SAFETY. IN PRESENT TIMES MANY PEOPLE HAVE DIFFERENT IDEAS ON HOW TO ENHANCE PUBLIC SAFETY WITHOUT TURNING SOLELY TO LAW ENFORCEMENT. THE COMMISSION WILL IN THE COMMUNITY TO IDENTIFY CONCERNS AND VIABLE ALTERNATIVES ON DELIVERING POLICE SERVICES IN THE COMMUNITY.

What do you hope to accomplish by your participation?
TO HELP ENHANCE THE QUALITY OF LIFE WE HAVE IN MORENO VALLEY SO OUR RESIDENTS, ESPECIALLY THE CHILDREN, WILL FEEL SAFE AND DEVELOP INTO PRODUCTIVE MEMBERS OF THIS SOCIETY.

Attachment: Jorge A. Alvarado_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

CANYON CHILDREN'S LEGAL SERVICES - 6/1/209 TO PRESENT; PUBLIC LAW CENTER - KEN BABCOCK, EXECUTIVE DIRECTOR, [REDACTED] 5/2017-5/2019; ENLACE COMUNITARIO - ANTOINETTE SEDILLO-LOPEZ, SENATOR [REDACTED] 6/2016-5/2017; LAW OFFICES OF THE NEW MEXICO STATE PUBLIC DEFENDER - BEN BAUR, CHIEF PUBLIC DEFENDER [REDACTED] 11/2013-4/2017; NEW MEXICO PUBLIC DEFENDER - KATHY VANCE [REDACTED]

What other areas of interest do you have in our City government?

CITY COUNCIL ACTIVITIES; PLANNING AND DEVELOPMENT; PARKS AND RECREATION

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

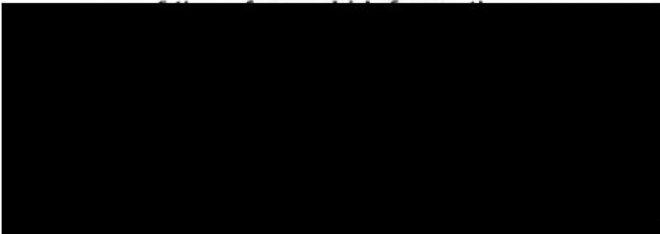
Date(s) of the meeting(s) attended: NEW COMMISSION AND NO MEETINGS SET YET.

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the



AUGUST 13, 2020

Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.



City of Moreno Valley Boards and Commissions

CITY CLERK
MORENO VALLEY
RECEIVED
20 JUL -6 AM 11:46

For City Clerk's Use
Stamp Date and Time Received

Membership Application Form

Name: Tanya Price

Home Address: [REDACTED]
Moreno Valley, CA 92557

How long have you resided in Moreno Valley? 20 years

CONFIDENTIAL INFORMATION

Home Phone No.: N/A Driver's License No.: [REDACTED]
Work Phone No.: [REDACTED] Email Address: [REDACTED]
Cell Phone No.: [REDACTED] Date of Birth: [REDACTED]

Employer Name: Altura Credit Union Position: Facilities&Security Manager

Address: 2847 Campus Parkway
Riverside, CA 92507

Board or Commission applying for*: 1st Choice Moreno Valley Citizens Public Safety Committee
2nd Choice NA

- *If applying for the Accessibility Appeals Board, please indicate which position you are applying for:
 Physically Challenged Person Person Experienced in Construction Public Member
- *If applying for the Utilities Commission, please indicate which position you are applying for:
 Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility
- *If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:
 Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?
At Altura Credit Union, I am responsible for developing and implementing physical security programs and implementing policies and procedures to ensure our members and employees are safe. With my experience and knowledge of safety and security, I fell like I can contribute valuable insight to assist with the success of the board. I have been a resident of Moreno Valley for 20years. city safety is extremely important to me.
List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:
I am currently enrolled in Arizona State University to receive my bachelor's degree in public and private policy with an emphasis in Homeland Security and Emergency Management.

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.
The Board and/or Commissions are responsible for understanding the goals of the organization, provide feedback and recommendations, approve or decline programs and changes, and to oversee the soundness of what they govern.

What do you hope to accomplish by your participation?
I hope to gain additional knowledge in the public safety arena, as well as contribute my skillset in safety and security in keeping the city safe.

Attachment: Tanya Price_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

I am not a member of a city/organization group, however, as an employee of Altura Credit Union, through our Altura Cares program, I volunteer for many events that benefit Riverside County. Some of this volunteer work includes work at March Air force Base, Salvation Army, VFW, and Ronald McDonald house.
Michelle Gardner, Altura Director of Corporate Events. [REDACTED] contact person

What other areas of interest do you have in our City government?

I am interested in Parks and Recreation, animal care, military, police and fire agencies.

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

Date(s) of the meeting(s) attended: NA

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.

[REDACTED]

July 6th 2020
Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.



City of Moreno Valley

Boards and Commissions

CITY CLERK
MORENO VALLEY
RECEIVED
20 JUN 31 AM 11:17

For City Clerk's Use
Stamp Date and Time Received

Membership Application Form

Name: Rosemary Rios
Home Address: [REDACTED]
Moreno Valley CA 92557
How long have you resided in Moreno Valley? 2/2006-7/2009 and 12/2018 to present

CONFIDENTIAL INFORMATION

Home Phone No.: _____ Driver's License No.: [REDACTED]
Work Phone No.: _____ Email Address: [REDACTED]
Cell Phone No.: [REDACTED] Date of Birth: [REDACTED]
Employer Name: N/A Position: Retired
Address: [REDACTED]

Board or Commission applying for*: 1st Choice MV Citizens Public Safety Committee
2nd Choice _____

- *If applying for the Accessibility Appeals Board, please indicate which position you are applying for:
 Physically Challenged Person Person Experienced in Construction Public Member
- *If applying for the Utilities Commission, please indicate which position you are applying for:
 Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility
- *If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:
 Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?
As a 'stakeholder' of the beautiful city of Moreno Valley where I plan to reside permanently, serving on this board would fulfill my desire to support and help better my community. I would appreciate the opportunity to interact with other community members, law enforcement, fire department and our elected officials with the focus and goal of addressing issues for safety and quality of life.

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:
I am newly retired from a career of 43 years as a customer service representative with United Airlines. I dealt with our customers from all parts of the world and learned to be attentive to their needs and concerns by listening and caring. I had a great appreciation and understanding of their individual experiences and made every effort to help their travel run smoothly.

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.
My understanding is that this board will be comprised of elected officials, community members, non-profit faith community representatives, business representatives, police and fire department representatives and representatives of the city managers office. Our goal would be to identify and respond to issues before they become problems to improve safety and quality of life.

What do you hope to accomplish by your participation?
I hope to play a part in the continued improvement of my city, Moreno Valley. I hope to become educated in the process of how the city addresses issues and resolves them. I also hope to offer a different view point based on my life experiences and make suggestions and recommendations that would be positive for all our citizens.

Attachment: Rosemary Rios_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

United Airlines

Noel Garcia (supervisor) [REDACTED]

June 10, 1977 to June 30, 2020 (43 great years)

What other areas of interest do you have in our City government?

I am very interested in helping with the homeless situation and providing them a better quality of life. I see many displaying mental issues and I believe as a community it is our responsibility to help them. There is also the segment with drug addictions that turn to crime to survive and a segment of those affected by our current economic situation who need a hand. I do not have the answer to solve the problem but I would like to help.

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

Date(s) of the meeting(s) attended: No meetings attended but would definitely attend when advised.

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.

[REDACTED Signature]

June 25, 2020

Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.



City of Moreno Valley Boards and Commissions

CITY CLERK
MORENO VALLEY
RECEIVED

20 JUN 29 AM 9:51

For City Clerk's Use
Stamp Date and Time Received

Membership Application Form

Name: Karen Sykes

Home Address: [REDACTED]

Riverside

How long have you resided in Moreno Valley? We have a business in the City of Moreno Valley since 2013 (7 yrs)

CONFIDENTIAL INFORMATION

Home Phone No.: _____ Driver's License No.: [REDACTED]

Work Phone No.: [REDACTED] Email Address: _____

Cell Phone No.: [REDACTED] Date of Birth: [REDACTED]

Employer Name: CrossWord Christian Church Position: Associate Pastor/Administrator

Address: 21401 Box Springs Road

Moreno Valley, CA 92557

Board or Commission applying for*: 1st Choice Moreno Valley Citizens Public Safety Committee

2nd Choice _____

*If applying for the Accessibility Appeals Board, please indicate which position you are applying for:

- Physically Challenged Person Person Experienced in Construction Public Member

*If applying for the Utilities Commission, please indicate which position you are applying for:

- Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility

*If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:

- Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?

I believe my experience as a community leader within Moreno Valley, and Associate Pastor at a local MV church, will be an asset to the MV Citizens Public Safety Committee. Not only am I willing to learn but I have valuable insight to be a contributing member of that Committee.

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:

Recipient of various awards within the Inland Empire; completed the Eleanor Jean Grier Leadership Academy; received a certificate "Caring for People God's Way" from the American Association of Christian Counselors; completed coursework from Cal Baptist Univ Moody Bible Institute in Biblical Studies and currently pursuing B.S. in Applied Management from Grand Canyon University.

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.

The position operates in an advisory capacity to identify community concerns, quality of life concerns, to gather information on police services and practices, as well as assist in developing potential positions of advocacy for the City Council.

What do you hope to accomplish by your participation?

I desire to assist in making a positive, impactful difference in the lives of the residents of Moreno Valley and visitors to this great city.

Attachment: Karen Sykes_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

CrossWord Christian Church, Moreno Valley // Assoc. Pastor, Administrator and Leader (Present)

Parkview Community Hospital African American Advisory Board (2016-2019)

Foreign Missionary work to Sierra Leone, Ghana and Kenya

What other areas of interest do you have in our City government?

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

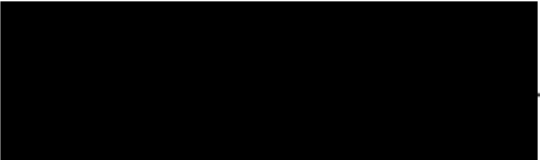
Date(s) of the meeting(s) attended: _____

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.



June 23, 2020
Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.

Attachment: Karen Sykes_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)



July 30, 2020

Mayor Yxstian A. Gutierrez
City of Moreno Valley
14177 Frederick Street
Moreno Valley, CA 92553

Dear Mayor Gutierrez,

Thank you, in advance, for taking time to read this letter. At the beginning of June, I learned there was an opportunity to participate on the Moreno Valley Citizens Public Safety Committee. I completed the application and it was hand delivered to the City Hall office for your consideration on or around June 29th.

It is my hope to be among those selected to participate on this Committee as I believe there is much I can offer as a vested community partner. You may know me through our church, CrossWord Christian Church in Moreno Valley as we have remained diligent in serving our mutual community. A majority of our membership resides within the City and we have been in the March ARB/Moreno Valley area for 17 years now. Your serious consideration of my application to be part of the MV Citizens Public Safety Committee is appreciated.

I am available should you wish to discuss my qualifications or express any concerns you may have. Once again, thank you.

Sincerely,



Associate Pastor

Enclosure: Copy of City of Moreno Valley Board and Commissions Application

20 AUG - 3 AM 10: 48
RECEIVED
CITY COUNCIL
MORENO VALLEY, CA

Attachment: Karen Sykes_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)



City of Moreno Valley Boards and Commissions

A.4.e

CITY CLERK
MORENO VALLEY
RECEIVED
20 JUN 23 AM

For City Clerk's Use
Stamp Date and Time Received

Membership Application Form

Name: Liz Williams
Home Address: [Redacted]

How long have you resided in Moreno Valley? 25 years

CONFIDENTIAL INFORMATION

Home Phone No.: [Redacted] Driver's License No.: [Redacted]
Work Phone No.: [Redacted] Email Address: [Redacted]
Cell Phone No.: [Redacted] Date of Birth: [Redacted]

Employer Name: Inland Christian Home Position: Director of Home Care
Address: 1950 S. Mountain Ave Ontario

Board or Commission applying for*: 1st Choice Public Safety Committee
2nd Choice _____

- *If applying for the Accessibility Appeals Board, please indicate which position you are applying for:
 Physically Challenged Person Person Experienced in Construction Public Member
- *If applying for the Utilities Commission, please indicate which position you are applying for:
 Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility
- *If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:
 Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?
I want to be a part of "Being the Change" in our community

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:
Just a concerned citizen

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.
No understanding at all. It's my time to be involved with my community

What do you hope to accomplish by your participation?
Bringing insight & adding to and being a part of change & bridging the gap between the races.

Attachment: Liz Williams_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

Volunteer for American Cancer Society for 11 years

What other areas of interest do you have in our City government?

Police Reform

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

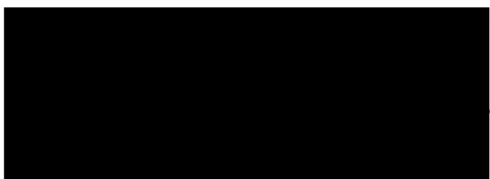
Date(s) of the meeting(s) attended: _____

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.



6/23/2020
Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.

Attachment: Liz Williams_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)



City of Moreno Valley Boards and Commissions

CITY CLERK
MORENO VALLEY
RECEIVED
20 JUL -8 AM 9:54

For City Clerk's Use
Stamp Date and Time Received

Membership Application Form

Name: Wayne Woodliff
Home Address: [REDACTED]
Palm Springs Ca. 92262

How long have you resided in Moreno Valley? 23060 Alessandro Blvd

CONFIDENTIAL INFORMATION

Home Phone No.: _____ Driver's License No.: [REDACTED]
Work Phone No.: _____ Email Address: _____
Cell Phone No.: [REDACTED] Date of Birth: [REDACTED]

Employer Name: Woodys Restaurant / Craft Brewery Position: Owner
Address: 23060 Alessandro Blvd
Moreno Valley Ca, 92553

Board or Commission applying for*: 1st Choice MV Citizens Public Safety Committee
2nd Choice _____

*If applying for the Accessibility Appeals Board, please indicate which position you are applying for:
 Physically Challenged Person Person Experienced in Construction Public Member

*If applying for the Utilities Commission, please indicate which position you are applying for:
 Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility

*If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:
 Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?
As a commercial property owner and business owner in the city of Moreno Valley, I have a vested interest in the continued positive direction the city has been achieving in economic and residential growth.

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:
Self-employed since 1985 ; I am currently (2011) the CEO of The Palm House LLC operating Woodys Palm House, Woodys Brewhouse and Palm House Brewing Company

I am the President of Carmona Properties Inc. (a real estate invesment company)

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.
The Moreno Valley Citizens Public Safety Committee engages community stakeholders, identifies community concerns, identifies quality of life concerns, gathers information on police services and practices, and develops potential positions of advocacy for the City Council.

What do you hope to accomplish by your participation?
Develop relationships with the community and the city so I will be able to effectively advocate appropriately.

Attachment: Wayne Woodliff_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

What other areas of interest do you have in our City government?

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

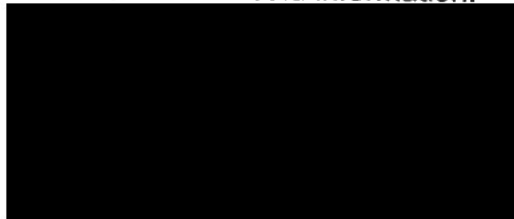
Date(s) of the meeting(s) attended: _____

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.



7/6/2020
Date

_____ on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.

Attachment: Wayne Woodliff_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)



City of Moreno Valley Boards and Commissions

CITY CLERK
MORENO VALLEY
RECEIVED
20 JUL - 8 AM 9:36

For City Clerk's Use
Stamp Date and Time Received

Membership Application Form

Name: Marquita L Clay

Home Address: [Redacted] Moreno Valley, CA 92551

How long have you resided in Moreno Valley? 20 years

CONFIDENTIAL INFORMATION

Home Phone No.: [Redacted] Driver's License No.: [Redacted]

Work Phone No.: [Redacted] Email Address: [Redacted]

Cell Phone No.: [Redacted] Date of Birth: [Redacted]

Employer Name: Department of Veteran Affairs Position: Social Service Assistant

Address: 11201 Benton St, Loma Linda, CA 92357

Board or Commission applying for*: 1st Choice MV Citizens Public Safety Committee

2nd Choice

*If applying for the Accessibility Appeals Board, please indicate which position you are applying for:

- Physically Challenged Person Person Experienced in Construction Public Member

*If applying for the Utilities Commission, please indicate which position you are applying for:

- Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility

*If applying for the MV Citizens Public Safety Committee, please indicate which position you are applying for:

- Non-Profit Business Public Member

Why do you wish to serve on this Board and/or Commission?

The reason I wish to serve on the Citizens Public Safety Committee is to be involved in and give back to the community I was raised in and am raising my sons in. As well as apply the knowledge and skills I have acquired in my Social Work education to address community concerns, develop and implement new programs to improve the communities quality of life regarding safety.

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:

U.S Navy Veteran

Bachelor's Degree in Social Work, Currently obtaining a Master's Degree in Social Work (anticipated graduation May 2021)

Peer Support Certification, Mental Health First Aid Certification, Suicide First Aid Certification

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.

My understanding of the Citizens Public Safety Committee is the members act as a liaison between the public and the City Council providing recommendations to the council on the citizens behalf regarding police services and quality of life concerns that impact their safety. The members will aid with program development and implementation pertaining to outreach events. The limitations of the committee involve having general power and responsibilities from an advisory capacity to the City Council.

What do you hope to accomplish by your participation?

With my participation I hope to assist with the improvements of community outreach and involvement, identify and support the City Council with a solution to community public safety concerns. Also, I hope to provide the community members a support system that they can believe in regarding their areas of concern.

Attachment: Marquita L. Clay_Redacted (4221 : APPOINTMENTS TO THE CITIZENS PUBLIC SAFETY COMMITTEE)

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

(Employment) Department of Veteran Affairs, Nov 2018 - present; Nicholas Perez, [REDACTED]

(Volunteer/internship) Path of Life Ministries, June 2016 - Present; Michelle Leary, [REDACTED]

(volunteer/internship) 211 Community Connect (Crisis/Hotline), July 2019 - May 2020; Jennifer Carson, [REDACTED]

What other areas of interest do you have in our City government?

I have an interest in involvement with community outreach, unity and program development on multi levels. Specific boards of interest include the

Environmental and Historical Preservation Board.

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to appointment.

Date(s) of the meeting(s) attended: June 16, 2020

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.

[REDACTED]

7/6/2020
Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.



Report to City Council

TO: Mayor and City Council

FROM: Pat Jacquez-Nares, City Clerk

AGENDA DATE: December 15, 2020

TITLE: MAYORAL APPOINTMENTS TO THE EMERGING LEADERS COUNCIL, AND THE UTILITIES COMMISSION

RECOMMENDED ACTION

Recommendation:

1. Receive and confirm the slate of Mayoral appointments as follows:

EMERGING LEADERS COUNCIL

<u>Name</u>	<u>Position</u>	<u>Term</u>
Steven Serrano Yopez	Member	Ending 05/31/22

UTILITIES COMMISSION

<u>Name</u>	<u>Position</u>	<u>Term</u>
Brianna Solomon	Public Member	Ending 06/30/23

CITY COUNCIL GOALS

Advocacy. Develop cooperative intergovernmental relationships and be a forceful advocate of City policies, objectives, and goals to appropriate external governments, agencies and corporations.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development

- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Steven Serrano Yopez_Redacted
- 2. Brianna Solomon_Redacted

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/10/20 4:47 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/10/20 4:47 PM



City of Moreno Valley EMERGING LEADERS COUNCIL

Membership Application Form

CITY CLERK
MORENO VALLEY
RECEIVED
20 DEC -2 PM 3:09

For City Clerk's Use
Stamp Date and Time Received

The purpose of the Emerging Leaders Council (ELC) is to identify college or high school students with a desire and potential to become community leaders, educate and engage young adults in local government, and focus efforts on service to the Moreno Valley community. The Emerging Leaders Council was established as a standing committee with two-year terms by Resolution 2014-30. The attached Resolution No. 2015-31 modifies the existing provisions governing the Emerging Leaders Council.

Meeting Schedule: Fourth Monday of each month at 6:00 p.m., City Hall - Council Chamber, 14177 Frederick Street

Qualifications include: Moreno Valley residency, enrollment in high school or college, must be between the ages of 17 and 25. Please attach at least one letter of recommendation/reference.

Name: Steven Serrano Yopez
Home Address: [Redacted] Moreno Valley CA
92553

CONFIDENTIAL INFORMATION

Home Phone No.: [Redacted] E-mail Address: [Redacted]
Work Phone No.: [Redacted] Date of Birth: [Redacted]
Cell Phone No.: [Redacted]

College or High School: California State University San Bernardino
Major: Political Science

Why do you want to serve on the City's Emerging Leaders Council?
I want to get involved in city, be the voice of the youth, be vocal of youth issue along of city issues, overall just be involv in the city.

If selected to serve on the Emerging Leaders Council, what do you believe you would be able to contribute to the ELC and your community? In what way(s) are you an "emerging leader"?
I believe I would be able to contribute leadership and knowledge as current citizen of Moreno Valley and college student. I'm an emerging leader by being college student part of future. As a professor said all college students are leaders of our future.

Attachment: Steven Serrano Yopez_Redacted (4255 : MAYORAL APPOINTMENTS TO THE EMERGING LEADERS COUNCIL AND THE UTILITIES

List any volunteer work that you have performed. Please provide the name(s) of the organization(s) and dates served:

- St Christopher Church youth group 03/01/2019 - Present
- Rescue work force 06/01/2019 - 04/20/2019
- high school events / CSUSB events

How would you define Moreno Valley's strengths? Weaknesses? Why?

Moreno Valley strength is rising emerging city with economic terms for business. Weaknesses is that there is always issues emerging such as homeless; cleaning city more.

Briefly explain your understanding of the functions of municipal government.

It overall intent was to be the primary source of democracy functioning, people should bring up issues and work to resolve instead of complaining to State Senator.

Do you have any experience chairing and/or participating in meetings that are regulated by the Brown Act?

I personally do not but wish to gain experience.

What do you hope to accomplish by your participation on the Emerging Leaders Council? How will your participation enhance your future goals and objectives?

More knowledge and experience of city issues and local issues how the city functions. It would enhance my goal of getting into politics.

Are you able to commit to participating in one meeting per month? Each meeting can last approximately two hours. Yes No

Do you have any means of transportation to arrive to meetings on time? Yes No

Have you ever been removed or asked to resign from a job or volunteer position? Yes No

May we contact the person who wrote your letter of recommendation? Yes No

I hereby authorize that the City of Moreno Valley may obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No

Pursuant to Resolution 2016-42 all board, commission, or council members must be registered voters of the City of Moreno Valley, provided they are at least 18 years old.

I hereby agree to attend all Board meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or the appointing authority from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission

within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.

[Redacted Signature]

11/30/2020
Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.

Attachment: Steven Serrano Yopez_Redacted (4255 : MAYORAL APPOINTMENTS TO THE EMERGING LEADERS COUNCIL AND THE UTILITIES

To Whom whoever this regards to,

It is my absolute pleasure to recommend Steven Serrano to the Moreno Valley Emerging Leader Council. Steven Serrano is a great Political science student who is involved at California. He's a great student who is on top of his classes. He's been involved in his local community by being an active member at local city council meetings voicing concerns of the district he lives in. Steven is also active with his city by giving back to the community by helping out in city sponsored events such as cleaning up different parts of the city and their local lake. Steven has been heavily involved since high school years in clubs such as listed, "Eco-Club, History Club, Math Club, National Honor Society, Science Club and AVID". He is a future leader in the making who always looks forward to helping others and improving himself by being involved in more organizations. He recently won an award 2019 Jamil Dada Award for being outstanding youth leader in Riverside County and being involved with Moreno Valley Youth opportunity center by getting involved in internships for job opportunities then ending up with first job. His written work is both clear and concise interesting read, he will use his skill of analyze to develop best solution needed for the Moreno Valley Emerging Leader Council. . Steven Serrano is the best candidate to represent the youth Moreno Valley Emerging Leader Council. . I believe he will do what's best for the youth of moreno valley in this council. He is highly motivated, intelligent and analytical, and an excellent communicator to community youth. I would therefore highly recommend Steven Serrano. His performance in all fields is a good indication of how she would perform as a Moreno Valley Emerging Leader Council member, he would be an extremely positive asset.

If I can be of any further assistance, or provide you with any further information, please do not hesitate to contact me.

Yours sincerely,

[sender's signature]

[REDACTED]

[sender's name and title,contact information]

Valenzuela, Beatriz, Moreno Valley Highschool teacher,

[REDACTED]



City of Moreno Valley
Boards and Commissions
Membership Application Form

CITY CLERK
MORENO VALLEY
RECEIVED
20 MAR 27 AM 11:16

A.5.b

For City Clerk's Use
Stamp Date and Time Received

Name: Brianna Solomon
Home Address: [Redacted] Moreno Valley, CA 92551

How long have you resided in Moreno Valley? (10) ten years.

CONFIDENTIAL INFORMATION

Home Phone No.: _____ Driver's License No.: [Redacted]
Work Phone No.: _____ Email Address: [Redacted]
Cell Phone No.: [Redacted] Date of Birth: [Redacted]

Employer Name: Berkshire Hathaway HomeServices Position: Mortgage/Real Estate Consultant
Address: _____

Board or Commission applying for*: 1st Choice Utility Commission
2nd Choice _____

*If applying for the Accessibility Appeals Board, please indicate which position you are applying for:
 Physically Challenged Person Person Experienced in Construction Public Member

*If applying for the Utilities Commission, please indicate which position you are applying for:
 Public Member Customer of Moreno Valley Utility Business Customer of Moreno Valley Utility

Why do you wish to serve on this Board and/or Commission?
I'm merely responding to the city of Moval e-mail I just received that:
"The city of Moval is looking for volunteers to fill on the Utility Commission position."
And I would like to contribute my time to the City of Moval.

List any education, training, or special skills, you have which may be relevant or of particular benefit to this Board and/or Commission:
Business administration: Mortgage/Real Estate Consultant.

Explain briefly your understanding of what this Board and/or Commission does, including its powers and limitations.

What do you hope to accomplish by your participation?

Attachment: Brianna Solomon_Redacted (4255 : MAYORAL APPOINTMENTS TO THE EMERGING LEADERS COUNCIL AND THE UTILITIES

List any employment, volunteer work, or membership in a service/community organization that you have served on, or are now a member of. Please provide the name(s) of the agency (ies), contact person, and dates served:

What other areas of interest do you have in our City government?

Would you be available for meetings during the day or evening?

Attendance of at least one (1) meeting is required prior to the appointment.

Date(s) of the meeting(s) attended: _____

Pursuant to Resolution 2016-42 all board and commission members must be registered voters of the City of Moreno Valley.

I authorize the City of Moreno Valley to obtain and review, on a confidential basis, such information regarding me as may be contained in the California State Summary Criminal History and in records of the California Department of Motor Vehicles. Yes No (The application shall not be considered if the NO box is checked.)

I hereby agree to attend all board or commission meetings, unless excused, and understand that I may be removed for lack of attendance, pursuant to Municipal Code, Subsection 2.06.010(C) which states, "If a member is absent without advance permission of the board or commission or of the appointing authority, from three consecutive regular meetings or from 25% of the duly scheduled meetings of the board or commission within any fiscal year, the membership shall thereupon become vacant and shall be filled as any other vacancy."

CERTIFICATE OF APPLICANT: I certify that all statements in this application are true and complete to the best of my knowledge. I understand that any false statements of material fact will subject me to disqualification or dismissal if appointed. I release the City of Moreno Valley from any liability for the use of the aforesaid information.

Signature

March 25, 2020
Date

Please Note: Applications will be kept on file for potential future vacancies for one year after the application submittal date. Applications are accepted year-round. All applications are public record; personal information may be redacted to protect applicants' privacy.



Report to City Council

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: COLLEGE PROMISE MEMORANDUM OF UNDERSTANDING

RECOMMENDED ACTION

Recommendations:

1. Approve the Memorandum of Understanding (MOU) between the City of Moreno Valley (City) and the Riverside Community College District Foundation acting on behalf of Moreno Valley College (College) allowing for the allocation of \$50,000 to support the College Promise Initiative.
2. Authorize the City Manager to execute the MOU as to form and all necessary documents with the College.

SUMMARY

This report recommends an approval of the MOU between the City of Moreno Valley and the Riverside Community College Foundation for the continuation of the College Promise Initiative operated by Moreno Valley College. The report outlines the duties of the parties and authorizes the expenditure from the City to the College in the amount of \$50,000 for fiscal year 2020-2021.

DISCUSSION

On August 15, 2017, the Moreno Valley City Council approved an MOU with the Riverside Community College District making a \$50,000 commitment to Moreno Valley students enrolled at Moreno Valley College through the College Promise Initiative. Since that time the City Council has supported the program with an annual \$50,000 commitment solidifying a strong partnership between the City and the College to support students as they pursue their educational goals to receive an Associate's

degree, transfer to a four-year university and/or complete eligible Career and Technical Education programs.

The College Promise program, also known as the First Year Experience Program, provides a bridge allowing Moreno Valley high school students enrolled at Moreno Valley Unified and Val Verde Unified School Districts to successfully transfer into College life. The program provides for gap funding to offset first year college expenses such as tuition, fees and textbooks. Serving some of the neediest students on campus, many of whom are the first in their families to attend college, the program also offers advanced services such as access to counselors and student support services.

The program serves as a prime example of the positive benefits to the individual and the community at large when key institutions partner for student success. As such, the program was awarded a 2018 Program Award of Excellence by the California Association for Local Economic Development (CALED) reinforcing the City's investment in students as a key economic engine driving the local economy.

As the Coronavirus pandemic continues to affect local economies, the College Promise Initiative is more critical than ever as California's Community College students prove to be more vulnerable as they manage family, school and work responsibilities making them more susceptible to dropping out and in need of support services like online tutoring, virtual mental health services, academic counseling and social services like the on-campus food pantry.

The College Promise program provides much needed support to Moreno Valley College students at a time of great uncertainty allowing for students to stay connected to school, critical support services and a future made more secure with education.

ALTERNATIVES

1. Approve the attached MOU and authorize staff to execute all necessary documents with the College for the operation of the College Promise Program. This alternative will allow for the continued partnership and support to eligible Moreno Valley residents enrolled at the College and support the City's economic and workforce efforts to increase educational attainment and train the future workforce. Staff recommends this alternative.
2. Decline to approve the attached MOU and decline to authorize staff to execute the supporting documents. This alternative will result in the end of the City's financial support for the College Promise program and result in reduced funding for eligible Moreno Valley residents seeking to benefit from the program and may result in an ultimate reduction in completion rates at the College. Staff does not recommend this alternative.

FISCAL IMPACT

Funding to operate the College Promise Initiative has been included in the City's 2020-2021 budget. No additional funds are required.

NOTIFICATION

Publication of the Agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Jackie Melendez
Deputy City Manager

Department Head Approval:
Mike Lee
City Manager

CITY COUNCIL GOALS

Positive Environment. Create a positive environment for the development of Moreno Valley's future.

Community Image, Neighborhood Pride and Cleanliness. Promote a sense of community pride and foster an excellent image about our City by developing and executing programs which will result in quality development, enhanced neighborhood preservation efforts, including home rehabilitation and neighborhood restoration.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development
2. Public Safety
3. Library
4. Infrastructure
5. Beautification, Community Engagement, and Quality of Life
6. Youth Programs

ATTACHMENTS

1. Promise Initiative MOU

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/02/20 5:20 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/02/20 5:23 PM

**MEMORANDUM OF UNDERSTANDING
BY AND BETWEEN
THE CITY OF MORENO VALLEY,
RIVERSIDE COMMUNITY COLLEGE DISTRICT FOUNDATION
AND
MORENO VALLEY COLLEGE**

MORENO VALLEY COLLEGE PROMISE INITIATIVE

THIS MEMORANDUM OF UNDERSTANDING (“MOU”) is entered into this 23rd day of November 2020, by and between the City of Moreno Valley (“City”), the Riverside Community College District Foundation (“RCCD Foundation”) and Moreno Valley College (“College”), collectively referred herein as “the Parties.”

RECITALS

WHEREAS, the Moreno Valley College Promise Initiative ensures that a college degree is an achievable goal for students in need and provides enrolled students financial assistance and ongoing academic guidance; and

WHEREAS, the Promise Initiative for the 2020-21 academic year is a commitment to help students complete their associate degree requirements, transfer requirements, and/or workforce certificate requirements in a timely and efficient manner; and

WHEREAS, donations for the Promise Initiative are accepted through the RCCD Foundation; and

WHEREAS, the Parties have a mutual interest in providing the residents of Moreno Valley with educational opportunities and advancement for the purpose of improving the quality of life for residents and the community at large and the Parties also mutually believe education is the greatest pathway to greater success and opportunity; and

WHEREAS, although the College provides a quality education for students at a relatively affordable cost, many residents still find it difficult to pay such costs due to the challenges presented by the COVID-19 Pandemic and other economic challenges beyond their control; and

WHEREAS, the Moreno Valley City Council desires to enhance local residents’ access to a college education, in recognition of the overall benefits a higher number of college educated residents bring to the community at large; and

WHEREAS, in light of the foregoing, the City Council has determined that it serves a legitimate governmental purpose to enhance and facilitate local residents’ access to the educational opportunities provided by the College through the City’s participation, sponsorship and support of the College's Promise Initiative.

NOW THEREFORE, the Parties understand and agree as follows:

Section 1. General Terms and Conditions

The following terms and conditions shall apply to the funds described in this MOU:

a) The City shall donate via the RCCD Foundation a total of FIFTY THOUSAND DOLLARS (\$50,000) to the Moreno Valley College Promise Initiative to be used as source of financial aid and assistance for residents of Moreno Valley who are currently enrolled as Moreno Valley College students and participants in the College's Promise Initiative.

b) Any financial aid or assistance provided to an eligible student pursuant to the terms and conditions of this MOU may only be used to pay the cost of tuition/registration and books not covered by other public or private financial aid/assistance programs.

c) An eligible student be a resident of the City for the twelve (12) consecutive months preceding the date the student applies for any financial aid or assistance provided under this MOU, and the student shall remain a current resident of the City during the academic year in which he or she has received any funding under this MOU.

d) To maintain eligibility for any financial aid or assistance under this MOU, an eligible student must be in good standing at the College (not on suspension, expelled, etc.) and maintain compliance with all applicable requirements and standards set forth in the Promise Initiative.

e) Any eligible student who receives any financial aid or assistance under this MOU must complete the academic period for which funding has been received in good standing (not on suspension, expelled, etc.).

f) If a student, who has received funding under this MOU, does not complete the academic period for which funding has been received in good standing, the student shall be deemed ineligible to apply for and receive any additional financial aid or assistance under this MOU.

Section 2. RCCD Foundation and College Responsibilities

The RCCD Foundation and the College shall have the following responsibilities:

a) The RCCD Foundation and/or the College shall be responsible for determining eligibility, awarding and distributing the funds provided under this MOU to eligible students.

b) The College shall determine specifically how the funds may be used by an eligible student consistent with the applicable terms and conditions of this MOU.

c) The College shall collect certain data pertaining to the eligible students who receive assistance under this MOU, related to the students' academic success rates, demographics, and use

of funds. Such data shall be provided to the City on a periodic basis as determined by City, which the City shall use for auditing purposes and for determining whether the City needs to make any necessary revisions to the financial aid and assistance program as set forth and described in this MOU for subsequent academic years.

d) The College shall require all eligible students prior to being awarded any funds under this MOU to execute a FERPA release and waiver that will allow the College to collect the necessary information for the data the College is obligated to provide to the City under this MOU.

e) The College shall provide the City with a Partnership Letter acknowledging the City's contribution to and participation in the Promise Initiative.

f) The College shall provide regular updates to the City and the City Council on the impact this MOU has on the students who have receive funding under the terms and conditions of this MOU.

g) The College shall provide both academic counseling and financial aid guidance to eligible students to ensure that the financial aid and assistance set forth in this MOU are made available to those promising students with an urgent need of financial support for tuition/registration and/or text books.

Section 3. Indemnification

Except as to the sole negligence or willful misconduct of the City, the RCCD Foundation and the College agree to indemnify, protect and hold harmless the City from and against any claim for damage, charge, lawsuit, action, judicial, administrative, regulatory or arbitration proceeding, damage, cost, expense (including counsel fees), judgement, civil fine and penalties, liabilities or losses of any kind, whether actual, threatened or alleged, which arise out of, pertain to, or relate to, or are a consequence of the performance of or under this MOU. This indemnification shall apply to any acts, omissions, negligence, recklessness, or willful misconduct, whether active or passive, on the part of the College or anyone employed by or working on behalf of the College.

Section 4. Entire Understanding

This MOU constitutes the final, complete and exclusive statement of the terms of the understanding between the City, the RCCD Foundation, and the College pertaining to the subject matter of this MOU, and supersedes all prior and contemporaneous understandings or agreements of the Parties.

Section 5. Amendments

This MOU may be modified or amended only by a signed, written agreement executed by the City, the RCCD Foundation, and the College.

Section 6. Term

The term of the MOU shall be one year designated to cover the 2020-2021 academic year.

Section 7. Waiver

No action or failure to act by either Party shall constitute a waiver of any right or duty afforded under this MOU, nor shall any such action or failure to act constitute approval of or acquiescence in any breach thereafter, except as may be specifically agreed in writing.

Section 8. California Law

This MOU shall be governed by and construed in accordance with the laws of the State of California.

Section 9. Venue


Any action at law or in equity brought by either of the Parties for the purpose of enforcing this MOU shall be brought in a court of competent jurisdiction in the County of Riverside, California, and the Parties hereby waive all provisions of law providing for a change of venue to any other county.

IN WITNESS WHEREOF, the City, the College and the RCCD Foundation have caused this MOU to be duly executed as of the day and year as first written above.

Mike Lee, City Manager
City of Moreno Valley


Robin Steinback (Dec 3, 2020 11:16 PST)

Dr. Robin Steinback, President
Moreno Valley College



Launa Wilson, Executive Director
RCCD Foundation

Attest:

Approved as to form:

Pat Jacquez-Nares, City Clerk
City of Moreno Valley

Steve Quintanilla, Interim City Attorney
City of Moreno Valley

Attachment: Promise Initiative MOU (4223 : COLLEGE PROMISE MOU)



Report to City Council

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: LIST OF PERSONNEL CHANGES

RECOMMENDED ACTION

Recommendation:

1. Ratify the list of personnel changes as described.

DISCUSSION

The attached list of personnel changes scheduled since the last City Council meeting is presented for City Council ratification.

Staffing of City positions ensures assignment of highly qualified and trained personnel to achieve Momentum MoVal priorities, objectives and initiatives.

FISCAL IMPACT

All position changes are consistent with appropriations previously approved by the City Council.

PREPARATION OF STAFF REPORT

Prepared By:
Vanessa Leccese
Executive Assistant

Department Head Approval:
Marshall Eyerman
Assistant City Manager
Chief Financial Officer/City Treasurer

CITY COUNCIL GOALS

None

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Personnel Changes

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/02/20 4:55 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/02/20 5:05 PM

**City of Moreno Valley
Personnel Changes
December 15, 2020**

New Hires

None

Promotions

None

Transfers

None

Separations

Kathleen Gross, Executive Assistant I, Economic Development Department

Sandra Brieño, Senior Management Analyst, Parks & Community Services Department, Parks & Community Services Division



Report to City Council

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager
Steve Quintanilla, Interim City Attorney

AGENDA DATE: December 15, 2020

TITLE: COVID-19 PANDEMIC RESOLUTIONS EXTENDING THE LOCAL STATE OF EMERGENCY AND CERTAIN EMERGENCY MEASURES (RESO. NO. 2020-69)

RECOMMENDED ACTION

Recommendations:

1. That the City Council adopt a Resolution Extending the Local State of Emergency and Certain Emergency Measures related to the Local, State and National Declarations of a State of Emergency related to the COVID-19 Pandemic.
2. Authorize the City Manager to amend any agreements as necessary to continue the operation of the State of California's Great Plates Delivered program, locally known as Senior Eats.

SUMMARY

The Disaster Council recommends that the City Council adopt a Resolution Extending the Local State of Emergency and Certain Emergency Measures related to the Local, State and National Declarations of a State of Emergency related to the COVID-19 Pandemic.

The City Council initially declared a Local State of Emergency at its March 17, 2020 meeting and closed all City facilities to the public to minimize and mitigate the spread of the coronavirus. Recognizing the financial hardship caused by the Coronavirus Pandemics the City established various assistance programs and modified certain operations and procedures to provide certain essential services to the public and/or to ease some of the financial burdens on residents and businesses such as, but not limited to, temporarily suspending certain fines, fees, penalties and interest for late payment of

utility bills, parking citations, business license fines, and library fines until January 1, 2021.

DISCUSSION

On March 17, 2020, the City Council declared a Local State of Emergency in response to the COVID-19 Pandemic, which prompted the Disaster Council to convene. The Disaster Council consists of the Mayor, City Manager/Director of Emergency Services and the Fire Chief. The Disaster Council's purpose is to develop and recommend for adoption by the City Council emergency plans, mutual aid plans, agreements, ordinances, resolutions and any necessary rules and regulations to implement the aforementioned.

Since the commencement of the Local State of Emergency, the City Council adopted via various resolutions and/or orders ("emergency measures") related to the following:

- Declaring and subsequently extending the Existence of a Local State of Emergency due to the COVID-19 Pandemic;
- Approving the Pandemic Influenza Preparedness Plan;
- Ratifying, Adopting and Approving the Amended Closure Plan Regarding its Termination Date;
- Directing the City Disaster Council and/or City Manager/Emergency Services Director to Seek, Apply for and Accept any Financial Assistance, Grants, Reimbursements the City is Eligible to Receive Under any State or Federal Programs, Agencies or Offices including but not limited to the Governor's Office of Emergency Services, the United States Department of Health and Human Services; Centers for Disease Control and Prevention and/or the Federal Emergency Management Agency;
- Authorizing the City Manager/Emergency Services Director to Obtain Vital Supplies, Equipment and Property Found Lacking and Needed for the Protection of Life and Property During the Local State of Emergency;
- Temporarily Waiving the City Manager's/Emergency Services Director's Level of Procurement Signature Authority to Mitigate or Prevent the Spread and Transmission of COVID-19;
- Authorizing the City Manager/Emergency Services Director to Suspend the Purchasing Procedures Set Forth in Chapter 3.12 of the Municipal Code Related to the COVID-19 Virus Pandemic Emergency;
- Authorizing the City Manager/Emergency Director to Take any Directly Related and Immediate Action Required by the COVID-19 Virus Pandemic Emergency and Procure the Necessary Public Works Construction Contracts for those Purposes, Without Giving Notice for Bids to Let Contracts;
- Granting Qualified Immunity to Certain Medical Professionals and Veterinarians or Registered Veterinary Technicians Who Render Services During the City's Local State of Emergency at the Express or Implied Request of the City Disaster Council, City Manager/Emergency Services Director and/or City Council;

- Imposing a Moratorium on Late Fees Related to the Nonpayment of Rent Due to Inability to Pay Related to COVID-19;
- Granting provisions for expanded restaurant outdoor seating and streamlined Temporary Use Permit (TUP) process and fee waiver to temporarily allow existing restaurants within the City to expand outdoor seating capacity on private property in order to implement social distancing measures;
- Authorizing the operation of the State of California's Great Plates Delivered program, locally known as Senior Eats, which pays local restaurants for the delivery of hot meals to seniors.
- Granting provisions for the Temporary Outdoor Business operations and streamlined Temporary Use Permit (TUP) process and fee waiver to temporarily allow existing businesses within the City to conduct outdoor use on private property in order to implement social distancing measures; and
- Adopting a Declaration of a fiscal emergency.

It is important to note that all the emergency measures are temporary. Each were set to terminate at such time that the Governor's State of Emergency is terminated by a subsequent proclamation of the Governor or a concurrent resolution of the State Legislature, unless the emergency measures are terminated earlier by the City Council. Notwithstanding the foregoing, and in order to prevent inconsistencies, the Disaster Council or the City Council may suspend the effectiveness of any of the emergency measures in the event that the President of the United States, the United States Congress, the Governor of the State of California, the California State Legislature or the Public Health Officer of the County of Riverside adopts legislation, a law, a regulation or order that supersedes any given emergency measure.

Notwithstanding the above, under the California Emergency Services Act, the City Council must review the need for continuing the existence of the Local State of Emergency at least once every 60 days until the City Council terminates the Local State of Emergency. Pursuant to the California Emergency Services Act, the City Council must terminate the Local State of Emergency at the earliest possible day that the conditions warrant.

RESOLUTION EXTENDING LOCAL EMERGENCY AND EMERGENCY MEASURES

In light of the foregoing, the Disaster Council recommends that the City Council adopt the attached Resolution that:

1. Extends the Existence of a Local State of Emergency due to the COVID-19 Pandemic;
2. Extends the Direction to the City Disaster Council and/or City Manager/Emergency Services Director to Seek, apply for and accept any Financial Assistance, Grants, Reimbursements the City is Eligible to Receive Under any State or Federal Programs, Agencies or Offices including but not limited to the Governor's Office of Emergency Services, the United States Department of Health and Human Services, Centers for Disease Control and Prevention and/or the Federal Emergency Management Agency;

3. Extends the Authorization of the City Manager/Emergency Services Director to Obtain Vital Supplies, Equipment and Property Found Lacking and Needed for the Protection of Life and Property During the Local State of Emergency;
4. Extends the Temporary Waiver Granted to the City Manager's/Emergency Services Director's Level of Procurement Signature Authority to Mitigate or Prevent the Spread and Transmission of COVID-19;
5. Extends the Authorization of the City Manager/Emergency Services Director to Suspend the Purchasing Procedures Set Forth in Chapter 3.12 of The Municipal Code Related to the COVID-19 Virus Pandemic Emergency;
6. Extends the Authorization of the City Manager/Emergency Director to Take any Directly Related and Immediate Action Required by the COVID-19 Virus Pandemic Emergency and Procure the Necessary Public Works Construction Contracts for Those Purposes, Without Giving Notice for Bids to Let Contracts;
7. Extends the Grant of Qualified Immunity to Certain Medical Professionals and Veterinarians or Registered Veterinary Technicians Who Render Services During the City's Local State of Emergency at the Express or Implied Request of the Disaster Council, City Manager/Emergency Services Director and/or City Council;
8. Extends the Moratorium on Late Fees Related to the Nonpayment of Rent Due to Inability to Pay Related to COVID-19;
9. Extends the Provisions for Expanded Restaurant Outdoor Seating and a Streamlined Temporary Use Permit (TUP) Process and Fee Waiver to Temporarily Allow Existing Restaurants Within the City to Expand Outdoor Seating Capacity on Private Property in Order to Implement Social Distancing Measures;
10. Extends the provisions for the Temporary Outdoor Business operations and streamlined Temporary Use Permit (TUP) process and fee waiver to temporarily allow existing businesses within the City to conduct outdoor use on private property in order to implement social distancing measures; and
11. Continues the Declaration of a Fiscal Emergency.

Upon adoption of the attached Resolution, all of the above emergency measures will remain in full force and effect until such time that the Governor's State of Emergency is lifted either by the Governor or a joint resolution of the State Legislature, unless terminated earlier by the City Council, which pursuant to the California Services Act, the City Council is obligated to terminate at the earliest possible day that the conditions warrant.

Extending the Existence of a Local Emergency due to the COVID-19 Pandemic

Under a prior order of the City Council, the City Manager/Emergency Services Director was directed to provide an update to the City Council every 60 days to determine whether the Local State of Emergency needs to remain in effect.

The Disaster Council has determined there remains a need to continue (extend) the City Council's proclamation of the existence of a Local State of Emergency due to the ongoing nature of the COVID-19 Virus Pandemic, which continues to endanger the

health and welfare of the residents and visitors of the City of Moreno Valley, as reflected in the guidance and related findings published by Riverside County Public Health Officer, the Governor's Office, the California Department of Public Health, the Centers of Disease Control and Prevention and the World Health Organization.

Pandemic Influenza Preparedness Plan

The Pandemic Influenza Preparedness Plan which was prepared under the direction and oversight of the Disaster Council, serves as the City's "Emergency Operations Plan." Pursuant to the Municipal Code, the Disaster Council is responsible for the development and maintenance of the City's Emergency Operations Plan, which must provide for the effective mobilization of all of the resources of the City, both public and private, to meet any conditions which may arise during the Local State of Emergency. It also provides for the organization, powers and duties and services of certain City employees, who all became "Disaster Workers" upon the adoption the Declaration of the Local State of Emergency. As Disaster Workers, some City employees may be assigned duties outside the scope of their regular job duties. The Plan also addresses issues such as telecommuting and financial tracking of emergency expenditures for purposes of qualifying for emergency assistance from Governor's Operations of Emergency Services and/or FEMA.

Closure Plan

Under the direction and oversight of the Disaster Council, a "Closure Plan" was developed which identifies which City facilities would either be closed during certain times and on certain days. The Closure Plan also identifies any adjustment in the hours of operation related to providing certain services to the public, such as those provided through the City's Libraries, Parks & Community Services, Employment Resource Center, Animal Services, etc.

Waiving Limitation on City Manager's Purchasing Authority

Currently, the City Manager/Emergency Services Director has the discretion to purchase and procure certain materials, equipment, supplies and services, provided that no single transaction exceeds \$50,000 or \$75,000 for public works contracts. This emergency measure waives these dollar limitations only for those purchases and procurement of materials, equipment, supplies and services which are related to mitigating or preventing the spread and transmission of COVID-19. This emergency measure also ratified any and all purchases of equipment, supplies and other materials in response to the arrival of 195 individuals who may have been exposed to COVID-19 at March Air Reserve Base on or about January 29, 2020, and who were subjected to a mandatory 14-Day federal COVID-19 quarantine at the Base, without prior sufficient notice being provided to the City first

Suspending Purchasing Procedures

This emergency measure authorizes the City Manager/Emergency Services Director to suspend the purchasing procedures set forth in Chapter 3.12 "Purchasing" of the Municipal Code to procure the necessary equipment, services, and supplies in order to respond immediately and effectively to the COVID-19 Virus Pandemic Emergency. Suspension of the purchasing procedures essentially authorizes the City Manager/Emergency Services Director to approve the direct purchase of any supplies, materials, equipment or contractual services where immediate procurement is essential to prevent delays which may otherwise hinder the City's efforts to implement programs and provide services intended to prevent or mitigate the risk of spreading and transmitting COVID-19. This also allows the City Manager/Emergency Services Director to suspend any requirements for preparing and publishing "Notices Inviting Bids," soliciting prospective vendors and consultants via "Requests for Proposals" (RFPs) or "Request for Quotes," awarding a contract or purchase to the "Lowest Responsible Bidder," requiring "Bidder's Security," and implementing "Protest Procedures."

Suspending Public Contract Bidding Requirements

This emergency measure allows for a temporary suspension of the competitive bidding process associated with public works contracts. Under the State's Public Contract Code, such a temporary suspension is permitted during a State of Emergency. Basically, this authorizes the City Manager/Emergency Services Director to cause the repair or replacement of any public facility directly related to the COVID-19 Virus Pandemic Emergency, which requires immediate action without having to give notice for bids to let contracts as otherwise required under the Public Contract Code.

Obtaining Vital Supplies, Equipment and Property

This emergency measure authorizes the City Manager/Emergency Services Director or designee, for the duration of the Local State Emergency, to obtain vital supplies, equipment and property identified as lacking and necessary for the protection of life and property and to bind the City for the fair value thereof.

Application & Acceptance of State & Federal Emergency Aid

FEMA has announced that certain emergency protective measures taken by cities to respond to the COVID-19 emergency may be eligible for reimbursement. In addition, there may also be some emergency funding made available by the Health and Human Services or the Centers for Disease Control and Prevention for certain emergency protective measures, the City may implement. Such funding may be made available for costs associated with management, control and reduction of immediate threats to public health and safety, such as Emergency Operation Center costs, training specific to the declared event and disinfection of eligible public facilities, medical facility services and supplies, temporary medical facilities and/or enhanced medical/hospital capacity, use of specialized medical equipment, medical waste disposal, emergency medical transport, medical sheltering, etc. Moreover, it is expected that under the California Disaster Assistance Act, the State may be providing financial assistance for local costs such as, but not limited to, personnel costs, equipment costs, and the cost of supplies and

materials used during disaster response activities; matching fund assistance for cost sharing required under federal disaster assistance programs; and indirect administrative costs and any other assistance deemed necessary by the Director of the Office of Emergency Services.

Moratorium on Late Fees Related to the Nonpayment of Rent

This emergency measure prevents a landlord from evicting a tenant for nonpayment of rent if the tenant demonstrates that the tenant is unable to pay rent due to financial impacts related to COVID-19. In addition, a landlord may not charge or collect any interest or any late fee for rent that is delayed due to the nonpayment of rent caused by the COVID-19 Pandemic. This shall not, however, relieve a tenant of liability for the unpaid rent, and no other legal remedies available to the landlord are affected by this emergency measure. This emergency measure also does not prevent a landlord from evicting a tenant who failed to pay rent when due prior to the Governor's Proclamation of a State of Emergency on March 4, 2020 or for any other lease violation not related or caused by the COVID-19 pandemic.

Granting Qualified Immunity to Medical Professionals and Veterinarians

This emergency measure authorizes the City Manager/Emergency Services Director, for the duration of the Local State of Emergency, to request, expressly or impliedly, the services of certain medical professionals and facilities for purposes related to the Local State of Emergency. This applies to the services provided by any physician or surgeon (whether licensed in California or any other state), hospital, pharmacist, respiratory care practitioner, nurse, or dentist for purposes related to the COVID-19. Pursuant to the California Emergency Services Act, any physician or surgeon (whether licensed in California or any other state), hospital, pharmacist, respiratory care practitioner, nurse, or dentist who renders services during the Local State of Emergency at the express or implied request of the City Disaster Council, City Manager/Emergency Services Director and/or City Council shall have no liability for any injury sustained by any person by reason of such services, regardless of how or under what circumstances or by what cause those injuries are sustained; provided, however, that the immunity herein granted shall not apply in the event of a willful act or omission.

This emergency measure also applies to any veterinarian or registered veterinary technician who renders services during the Local State of Emergency at the express or implied request of the City Disaster Council, City Manager/Emergency Services Director and/or City Council. They too shall have no liability for any injury sustained by any animal by reason of those services, regardless of how or under what circumstances or by what cause those injuries are sustained; provided, however, that the immunity herein granted shall not apply in the event of a willful act or omission.

Providing For Expanded Restaurant Outdoor Seating

This Resolution authorizes the City Manager or designee to set forth a streamlined Temporary Use Permit (TUP) process and fee waiver to temporarily allow existing

restaurants within the City to expand outdoor seating capacity on private property in order to implement social distancing measures. In association with this plan to facilitate operations for existing City restaurant businesses, staff has developed a checklist that provides applicants with a clear and simple understanding of the associated requirements. An over-the-counter approval process has also been made available.

Providing For Temporary Outdoor Business Operations

This Resolution ratified the City Manager's actions on July 22, 2020, to set forth a streamlined Temporary Use Permit (TUP) process and fee waiver to temporarily allow existing businesses to conduct outdoor operations within the City on private property in order to implement social distancing measures and in accordance with applicable State guidelines. In association with this plan to facilitate operations for existing City businesses, staff developed a checklist that provides applicants a clear and simple understanding of the associated requirements. An over-the-counter approval process is also available.

Continuing the Declaration of a Fiscal Emergency

It is difficult to predict with certainty the ultimate reduction in General Fund revenues caused by the COVID-19 Pandemic but the impact has been and likely will continue to be significant. Although the City has taken immediate actions to balance the budget for the best case scenario of a \$9.9 million shortfall in FY 2020/21, due to the ongoing impacts of the Governor's Executive Orders and the potential for additional State takeaways from local government in future State budget revisions or other actions, along with not receiving any financial support from either the State or the Federal Government, this situation may continue to create a severe economic crisis at the federal, state and local levels.

Continuing the Declaration of a Fiscal Emergency as a result of COVID-19 provides, in part, for the City Manager/Emergency Services Director to investigate and recommend further actions to mitigate the fiscal impact to the City's 2019/20 and 2020/21 Fiscal Year Budgets, including such measures relating to personnel costs, operations, reduction in service levels, or other measures deemed necessary and reasonable to minimize the accelerated and significant reduction to the General Fund budget and reserves.

The decision to declare a Fiscal Emergency was not made lightly. The City's immediate and significant loss of revenue due to COVID-19 is unprecedented and represents a sudden change of circumstances beyond the City's control and will ultimately draw down its General Fund reserves beyond a traditionally recommended level. Unlike the Great Recession of 2008-2011, where the City had an opportunity to implement cost saving measures over a longer period of time, the COVID-19 Local State of Emergency is immediate, severe and is highly likely going to extend through the next few fiscal years.

The State now has a blueprint for reducing COVID-19 in the State with revised criteria for loosening and tightening restrictions on activities, which includes categories identified as “Minimal” (most indoor business operations are open with modifications), “Moderate” (some indoor business operations are open with modifications), “Substantial” (some non-essential indoor business operations are closed), and “Widespread” (many non-essential indoor business operations are closed), which are applied on a county by county basis throughout the State based on a 7-day average of daily COVID-19 cases per 100,000 and a 7-day average of all COVID-19 tests performed that are positive. In addition, the County of Riverside now aligns itself with the State’s Orders as they now exist or may be issued or amended in the future.

Providing For Seniors Eats, a California Great Plates Delivered Program

Meal delivery for the Senior Eats program began in Moreno Valley on Monday, May 18, 2020. The program has been extended several times with the State submitting a request for approval to FEMA in 30 day increments, based on need. The program is currently extended until January 7, 2021, with additional 30-day extensions pending FEMA approval. The program allows for the delivery of lunch and dinner for 150 seniors, Monday through Friday at \$50 per day (\$17 for lunch, \$28 for dinner, \$5 for incidentals) for 150 seniors at approximately \$37,500 per week. The program was adopted with an anticipated reimbursement from the California Office of Emergency Services at 18.75%, FEMA at 75% and a 6.25% final contribution from the City as the local administrator. Continuation of this program will coincide with 30-day FEMA’s approval process, however the City reserves the right to end participation at any time.

Based on current emergency resolutions, the City Manager has been previously authorized to amend the current contracts with the restaurants for the continuation of the program within the State’s guidance and direction. The current restaurant participation agreements through the end of the calendar year are set at an amount not to exceed \$120,000, based on the number of meals delivered. Continuation of the program may require a \$40,000 amendment for each 30 day extension.

ALTERNATIVES

1. Adopt the recommended actions set forth within the staff report. This would allow the City Manager/Emergency Services Director to respond in a timely manner in time sensitive situations where delays may frustrate or impede the City’s emergency efforts to abate or mitigate the spread and transmission of COVID-19.
2. Reject the recommended actions set forth within the staff report, which would impact the City’s ability to respond in a timely manner in time sensitive situations where delays may frustrate or impede on the City’s emergency efforts to abate or mitigate the spread and transmission of COVID-19 in a timely manner.

FISCAL IMPACT

See above discussion regarding Resolution Declaring Fiscal Emergency and the Senior

Eats Program.

PREPARATION OF STAFF REPORT

Prepared By:
Marshall Eyerman
Assistant City Manager/Chief Financial Officer

Department Head Approval:
Mike Lee
City Manager

Concurred By:
Steve Quintanilla
Interim City Attorney

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

Public Safety. Provide a safe and secure environment for people and property in the community, control the number and severity of fire and hazardous material incidents, and provide protection for citizens who live, work and visit the City of Moreno Valley.

Public Facilities and Capital Projects. Ensure that needed public facilities, roadway improvements, and other infrastructure improvements are constructed and maintained.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development
2. Public Safety
3. Library
4. Infrastructure
5. Beautification, Community Engagement, and Quality of Life
6. Youth Programs

ATTACHMENTS

1. Resolution Extending Local Emergency and COVID-19 Emergency Measures

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/03/20 6:41 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/03/20 7:38 PM

RESOLUTION NO. 2020-____

**A RESOLUTION OF THE CITY COUNCIL
OF THE CITY OF MORENO VALLEY, CALIFORNIA,
EXTENDING THE LOCAL STATE OF EMERGENCY AND CERTAIN EMERGENCY
MEASURES RELATED TO THE LOCAL, STATE AND NATIONAL DECLARATIONS OF
EMERGENCY RELATED TO THE COVID-19 PANDEMIC
AND EXTENDING THE DECLARATION OF A FISCAL EMERGENCY**

WHEREAS, on March 4, 2020, the Governor of the State of California proclaimed a State of Emergency to make additional resources available, formalize emergency actions already underway across multiple state agencies and departments, and help the state prepare for broader spread of COVID-19; and

WHEREAS, on March 7, 2020, Riverside County Public Health Officer Dr. Cameron Kaiser declared a Local Health Emergency; and

WHEREAS, on March 10, 2020, the Riverside County Board of Supervisors ratified the Local Health Emergency and activated the Medical Health Department Operations Center to better coordinate public messaging and planning among community partners as Riverside County officials prepare for the spread of COVID-19; and

WHEREAS, on March 11, 2020, the California Department of Public Health issued guidance that in order to protect public health and slow the rate of transmission of COVID-19, by recommending placing restrictions on gatherings of people and minimum social distancing of six feet; and

WHEREAS, on March 12, 2020, the Governor of the State of California issued Executive Order N-25-20 providing that all residents are to heed any orders and guidance of state and local public health officials, including but not limited to the imposition of social distancing measures, to control the spread of COVID-19 and that authorized local legislative bodies to hold public meetings via teleconferencing and to make public meetings accessible telephonically or otherwise electronically to all members of the public seeking to attend and to address the local legislative body, during the period in which local public officials impose or recommend measures to promote social distancing, including but not limited to limitations on public events; and

WHEREAS, on March 13, 2020, the President of the United States of America proclaimed and declared a National Emergency Concerning the Novel Coronavirus Disease (COVID-19) Outbreak and announced that the federal government would make emergency funding available to assist state and local governments in preventing the spread of and addressing the effects of COVID-19; and

WHEREAS, on March 17, 2020, the Governor issued Executive Order N-33-20 ordering that to protect public health, that all individuals living in the State of California stay home or at their place of residence (“Shelter in Place”) except as needed to maintain continuity of operations of the federal critical infrastructure sectors, as outlined at <https://www.cisa.gov/critical-infrastructure-sectors>; and

WHEREAS, on March 17, 2020, the City Council declared a Local State of Emergency in response to the COVID-19 Pandemic, which prompted the Disaster Council to convene; and

WHEREAS, the Disaster Council consists of the Mayor, City Manager/Director of Emergency Services and the Fire Chief; and

WHEREAS, the Disaster Council's purpose is to develop and recommend for adoption by the City Council emergency plans, mutual aid plans, agreements, ordinances, resolutions and any necessary rules and regulations to implement the aforementioned; and

WHEREAS, since the commencement of the Local State of Emergency, the City Council adopted various temporary emergency measures related to the Local State of Emergency; and

WHEREAS, each of the temporary emergency measures were set to terminate at such time that the Governor's State of Emergency is terminated by a subsequent proclamation of the Governor or a concurrent resolution of the State Legislature, unless the emergency measures are terminated earlier by the City Council, but notwithstanding the foregoing, and in order to prevent inconsistencies, the Disaster Council or the City Council may suspend the effectiveness of any of the emergency measures in the event that the President of the United States, the United States Congress, the Governor of the State of California, the California State Legislature or the Public Health Officer of the County of Riverside adopts legislation, a law, a regulation or order that supersedes any given emergency measure; and

WHEREAS, notwithstanding the above, under the California Emergency Services Act, the City Council must review the need for continuing the existence of the Local State of Emergency at least once every 60 days until the City Council terminates the Local State of Emergency, and pursuant to the California Emergency Services Act, the City Council must terminate the Local State of Emergency at the earliest possible day that the conditions warrant; and

WHEREAS, the Disaster Council has determined there remains a need to continue (extend) the City Council's proclamation of the existence of a Local State of Emergency due to the COVID-19 Virus Pandemic, which continues to endanger the health and welfare of the residents and visitors of the City of Moreno Valley, as reflected in the guidance and related findings published by Riverside County Public Health Officer, the Governor's Office, the California Department of Public Health, the Centers of Disease Control and Prevention and the World Health Organization; and

WHEREAS, on June 2, 2020, at a duly noticed regular meeting of the City Council, the City Council adopted Resolution No. 2020-41 extending the following emergency measures, as described in their respective adopting resolutions, until such time that the Governor's State of Emergency is lifted either by the Governor or a joint resolution of the State Legislature, unless terminated earlier by the City Council, which pursuant to the California Services Act, the City Council is obligated to terminate at the earliest possible day that the conditions warrant:

- a. The Existence of a Local State of Emergency due to the COVID-19 Pandemic
- b. Direction to the City Disaster Council and/or City Manager/Emergency Services Director to Seek, Apply For and Accept any Financial Assistance, Grants, Reimbursements the City is Eligible to Receive Under any State or Federal Programs, Agencies or Offices including but not limited to the Governor's Office of Emergency Services; the United States Department of Health and Human Services; Centers for Disease Control and Prevention and/or the Federal Emergency Management Agency;

- c. Authorization of the City Manager/Emergency Services Director to Obtain Vital Supplies, Equipment and Property Found Lacking and Needed for the Protection of Life and Property During the Local State of Emergency;
- d. Temporary Waiver Granted to the City Manager's/Emergency Services Director's Level of Procurement Signature Authority to Mitigate or Prevent the Spread and Transmission of COVID-19;
- e. Authorization of the City Manager/Emergency Services Director to Suspend the Purchasing Procedures Set Forth in Chapter 3.12 of The Municipal Code Related to the COVID-19 Virus Pandemic Emergency;
- f. Authorization of the City Manager/Emergency Director to Take any Directly Related and Immediate Action Required by the COVID-19 Virus Pandemic Emergency and Procure the Necessary Public Works Construction Contracts for Those Purposes, Without Giving Notice for Bids to Let Contracts;
- g. Grant of Qualified Immunity to Certain Medical Professionals and Veterinarians or Registered Veterinary Technicians Who Render Services During the City's Local State of Emergency at the Express or Implied Request of the Disaster Council, City Manager/Emergency Services Director and/or City Council; and
- h. Moratorium on Late Fees Related to the Nonpayment of Rent Due to Inability to Pay Related to COVID-19; and

WHEREAS, on June 2, 2020, at a duly noticed regular meeting of the City Council, the City Council adopted Resolution No. 2020-42, an emergency measure authorizing the City Manager/Emergency Director to set forth a streamlined Temporary Use Permit Process and fee waiver to temporarily allow existing restaurants to expand outdoor seating capacity in order to provide social distancing measures during the COVID-19 Pandemic; and

WHEREAS, on June 2, 2020, at a duly noticed regular meeting of the City Council, the City Council adopted Resolution No. 2020-43, unanimously determining and declaring the existence of a Fiscal Emergency within the City of Moreno Valley for the purpose of providing the City with the rights and authorities granted to the City Council, without limitation, under Article XIII C, section 2(b) of the California Constitution and Government Code section 3504.5, to ensure that the City has the resources and opportunities available to it that are necessary to preserve and protect public health, safety and welfare for the benefit of the City's residents, business owners, and visiting public; and

WHEREAS, on September 1, 2020, at a duly noticed regular meeting of the City Council, the City Council adopted Resolution No. 2020-61, an emergency measure authorizing the City Manager/Emergency Director to set forth a streamlined Temporary Use Permit Process and fee waiver to temporarily allow existing business operations to conduct outdoor use on private property in order to implement social distancing measures during the COVID-19 Pandemic; and

WHEREAS in light of the foregoing, the Disaster Council recommends that the City Council adopt the attached Resolution that extends the existence of a Local State of Emergency due to the COVID-19 Pandemic and extends certain emergency measures, and affirms the need to maintain the state of a Fiscal Emergency, previously adopted by the City Council pursuant to the findings set forth in the recitals contained and set forth in the resolutions adopting said emergency measures; and

WHEREAS, the State has a blueprint for reducing COVID-19 in the state with revised criteria for loosening and tightening restrictions on activities, which includes categories identified as "Minimal" (most indoor business operations are open with modifications), "Moderate" (some

indoor business operations are open with modifications), “Substantial” (some non-essential indoor business operations are closed), and “Widespread” (many non-essential indoor business operations are closed), which are applied on a county by county basis throughout the State based on a 7-day average of daily COVID-19 cases per 100,000 and a 7-day average of all COVID-19 tests performed that are positive; and

WHEREAS, the County of Riverside now aligns itself with the State’s Orders as they now exist or may be issued or amended in the future.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY HERBY FINDS, ORDERS AND RESOLVES:

1. THAT following emergency measures shall be extended and remain in full force and effect, as described in their respective adopting resolutions, until such time that the Governor’s State of Emergency is lifted either by the Governor or a joint resolution of the State Legislature, unless terminated earlier by the City Council, which pursuant to the California Services Act, the City Council is obligated to terminate at the earliest possible day that the conditions warrant:
 - a. The Existence of a Local State of Emergency due to the COVID-19 Pandemic; Direction to the City Disaster Council and/or City Manager/Emergency Services Director to Seek, Apply For and Accept any Financial Assistance, Grants, Reimbursements the City is Eligible to Receive Under any State or Federal Programs, Agencies or Offices including but not limited to the Governor’s Office of Emergency Services; the United States Department of Health and Human Services; Centers for Disease Control and Prevention; and/or the Federal Emergency Management Agency;
 - b. Authorization of the City Manager/Emergency Services Director to Obtain Vital Supplies, Equipment and Property Found Lacking and Needed for the Protection of Life and Property During the Local State of Emergency;
 - c. Temporary Waiver Granted to the City Manager’s/Emergency Services Director’s Level of Procurement Signature Authority to Mitigate or Prevent the Spread and Transmission of COVID-19;
 - d. Authorization of the City Manager/Emergency Services Director to Suspend the Purchasing Procedures Set Forth in Chapter 3.12 of the Municipal Code Related to the COVID-19 Virus Pandemic Emergency;
 - e. Authorization of the City Manager/Emergency Director to Take any Directly Related and Immediate Action Required by the COVID-19 Virus Pandemic Emergency and Procure the Necessary Public Works Construction Contracts for Those Purposes, Without Giving Notice for Bids to Let Contracts;
 - f. Grant of Qualified Immunity to Certain Medical Professionals and Veterinarians or Registered Veterinary Technicians Who Render Services During the City’s Local State of Emergency at the Express or Implied Request of the Disaster Council, City Manager/Emergency Services Director and/or City Council;
 - g. Moratorium on Late Fees Related to the Nonpayment of Rent Due to Inability to Pay Related to COVID-19;

- h. Order authorizing the City Manager/Emergency Director to set forth a streamlined Temporary Use Permit Process and fee waiver to temporarily allow existing restaurants to expand outdoor seating capacity in order to provide social distancing measures during the COVID-19 Pandemic; and
 - i. Order authorizing the City Manager/Emergency Director to set forth a streamlined Temporary Use Permit Process and fee waiver to temporarily allow existing business operations to conduct outdoor use on private property in order to implement social distancing measures during the COVID-19 Pandemic; and
- 2. THAT the economic downturn due to the impact of COVID-19 continues to create an unforeseen situation that poses a threat to the public health, safety and welfare which continues the need for immediate action since there continues to be substantial uncertainty as to whether the City's revenues in the next two years will be sufficient to cover the expenditures necessary to provide a service level consistent with public health and safety demands and expectations of the residents and businesses of the City of Moreno Valley, for the reasons set forth in Resolution No. 2020 - 43; and
- 3. THAT in light of the foregoing and the fact that the City's need for additional revenue is immediate and will likely continue for the remainder of this Fiscal Year 2020 and well into Fiscal Year 2021 and the extension of the declaration of fiscal emergency is necessary to ensure the City has the resources and opportunities necessary to preserve and protect public health, safety and welfare, the City Council also hereby extends the declaration of a fiscal emergency; and
- 4. THAT the City Council has determined that the purpose of adopting and implementing protective measures to mitigate and/or abate the transmission of COVID-19, is to prevent harm to those who may violate any protective measure since any such violations may result in exposure to the COVID-19 which may lead to illness and death to the violator and those in the presence of the violator since there is no effective cure or vaccine available; and
- 5. THAT the City Council has determined that the purpose of adopting and implementing and extending the aforementioned emergency measures, including extending the declaration of fiscal emergency, is to mitigate and/or abate the spread and transmission of COVID-19.; and
- 6. THAT a violation of any of the aforementioned emergency orders by any member of the public shall be subject to any and all other remedies, civil, equitable or criminal, afforded to the City under any City, County, State and Federal laws or regulations; and
- 7. THAT any section, subdivision, subsection, sentence, clause, or phrase in this Resolution or its application to any person or circumstances, is for any reason held invalid, the validity of the remainder of this Resolution, or the application of such provision contained therein to other persons or circumstances, shall not be affected thereby; and
- 8. THAT the City Council hereby declares that it would have adopted this Resolution and each section, subdivision, subsection, sentence, clause, or phrase thereof, irrespective of the fact that one or more sections, subdivisions, subsections, sentences, clauses, or phrases, or the application thereof to any person or circumstance, be held invalid; and

- 9. THAT notwithstanding the foregoing, and in order to prevent inconsistencies, the City Council, Disaster Council or City Manager/Emergency Services Director may suspend the effectiveness of this Resolution in the event that the President of the United States, the United States Congress, the Governor of the State of California, the California State Legislature or the Public Health Officer of the County of Riverside adopts legislation, a law, a regulation or order that supersedes this Resolution.

APPROVED AND ADOPTED this ___th day of December, 2020

Mayor of the City of Moreno Valley

ATTEST

APPROVED AS TO FORM

City Clerk

Interim City Attorney

Resolution No. 2020-
Date Adopted: December ___, 2020

Attachment: Resolution Extending Local Emergency and COVID-19 Emergency Measures [Revision 2] (4209 : COVID-19 PANDEMIC



Report to City Council

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: RECEIVE THE ANNUAL AB1600 COMPLIANCE REPORT FOR FISCAL YEAR 2019-20

RECOMMENDED ACTION

Recommendations:

1. Receive and file the Annual AB 1600 Compliance Report for FY 2019-20 in compliance with California Government Code sections 66006 and 66001.
2. Approve the finding that staff has demonstrated a continuing need to hold unexpended Development Impact Fees.

SUMMARY

Government Code Section 66006 requires cities that impose impact fees to render an annual accounting of the fees and to provide findings that support the retention of any fees that have been held in excess of five years and remain unexpended or have not been committed to projects. The City has no Development Impact Fees (DIF) that are unexpended and uncommitted for a period of five years or more.

Government Code Section 66001 requires cities that impose impact fees must make certain findings described in section 66001(d)(1) every five years as a component of the annual report.

The information included in this staff report is provided to comply with State law.

DISCUSSION

Government Code Section 66006 requires cities imposing impact fees to undertake an annual accounting of such fees within 180 days of the fiscal year end, and that the accounting be made available for public review. The accounting must provide the

beginning and ending balances for the fiscal year, receipts, disbursements, interest earned and any other income that was received. The report must also include a description of how the fees were expended during the past year. If fees are unexpended, whether committed or uncommitted for a period of five or more years, the report must include a finding regarding the continuing need for the fees. If a continuing need cannot be shown, State law requires that the City refund the unused, uncommitted fees. The City's report contains no such instances of Development Impact Fees that remain unexpended and uncommitted.

The attached Annual AB 1600 Compliance Report is for the fiscal year ended June 30, 2020, and has been prepared in compliance with the California Government Code Section 66006 regarding the annual accounting for impact fees. The accounting was completed and the required information was made available to the public within the required time frame of 180 days subsequent to fiscal year end. The report has been on file in the City Clerk's office since October 18, 2020, and available for public review which is also in compliance with state law requiring that the report be made available at least 15 days prior to being considered by the City Council.

This report does not include any findings that require the return of unexpended or uncommitted DIF fees. This report does make a finding for continuing to hold previously collected development impact fees since all funds collected and held by the City as of June 30, 2020, within each of the 13 respective Development Impact Fee funds, have been designated for specific capital projects, consistent with the Development Impact Fee Study Final Report approved by the City Council on December 11, 2012, and the Capital Improvement Plan approved by the City Council on June 4, 2019.

Interest earnings attributable to the DIF funds totaled \$433,227 for the year.

Government Code Section 66001(d)(1) requires that at least every five years certain findings be made with respect to each impact fee being assessed. The following information is provided to satisfy the four requirements of this code section:

(A) Identify the purpose to which the fee is to be put – The purpose of the development impact fee program is to ensure that new development is paying its share of the transportation infrastructure and facility costs associated with the growth resulting from that development. The program includes projects related to Arterial Street Improvements, Traffic Signal Improvements, Fire Facilities, Police Facilities, Park Improvements, Recreation Centers, Libraries, City Hall, Corporate Yard, Interchange Improvements, Maintenance Equipment, Animal Shelter Facilities and Impact Fee Administration.

(B) Demonstrate a reasonable relationship between the fee and the purpose for which it is charged – The fees are based on the relationship between the needed transportation infrastructure and facility costs associated with the growth resulting from new development.

(C) Identify all sources and amounts of funding anticipated for incomplete improvements – Facilities to be funded from development impact fees are also funded by other sources including gas tax, Measure A, General Fund, and grant funding. The specific funding sources utilized for each project depend on funds availability at the time a project is moved forward.

(D) Designate the approximate dates on which the funding is expected to be deposited into the appropriate account or fund – The receipt of funding and the construction of improvements is dependent upon when undeveloped land remaining in the City is developed. Facilities constructed utilizing development impact fee funding are constructed when all required funding is available and the City Engineer has determined that it is appropriate for the project to move forward.

ALTERNATIVES

The following alternatives are available to the City Council:

1. Approve and accept the Annual AB 1600 Compliance Report for FY 2019-20 in compliance with California Government Code Section 66006 and approve the finding that staff has demonstrated a continuing need to hold unexpended Development Impact Fees. *Staff recommends this alternative to comply with the reporting requirements of the California Government Code.*
2. Approve and accept the Annual AB 1600 Compliance Report for FY 2019-20 in compliance with California Government Code Section 66006 but reject the finding that staff has demonstrated a continuing need to hold unexpended Development Impact Fees. *Staff does not recommend this alternative in that this action could result in the need to refund unexpended fees such that projects and debt service intended to be funded through these fees would be left without a funding source.*

FISCAL IMPACT

There is no fiscal impact resulting from the recommended action; the information included in the staff report is provided to comply with State law.

NOTIFICATION

Publication of the agenda. The Annual AB 1600 Compliance Report for FY 2019-20 was made available for public review in the City Clerk's Office on October 18, 2020.

PREPARATION OF STAFF REPORT

Prepared By:
Brooke McKinney
Treasury Operations Division Manager

Department Head Approval:
Marshall Eyeran
Assistant City Manager/Chief Financial Officer/City
Treasurer

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. AB 1600 Report FY 2020 for Staff Report

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/02/20 5:29 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/03/20 3:05 PM

City of Moreno Valley

Annual AB 1600
Compliance Report

For the Fiscal Year Ended
June 30, 2020



**City of Moreno Valley
Annual AB1600 Compliance Report
For the Fiscal Year Ended June 30, 2020**

Pursuant to Government Code Section 66006, the following report on the receipt, use and retention of development impact fees for fiscal year ended June 30, 2020 is hereby presented to the City Council for review and approval.

Fund Number / Fund Name	Beginning Fund Balance July 1, 2019	Receipts	Disbursements	Transfers In	Interest Earnings	Ending Fund Balance June 30, 2020
2901 Arterial Streets Development Impact Fee	\$ 2,332,119	\$ 955,667	\$ (1,275,756)	\$ 1,060,756	\$ 54,024	\$ 3,126,810
2902 Traffic Signal Development Impact Fee	\$ 537,876	\$ 356,060	\$ -	\$ 12,158	\$ 15,223	\$ 921,317
2903 Fire Facility Development Impact Fee	\$ 3,624,462	\$ 753,773	\$ (241,738)	\$ -	\$ 75,030	\$ 4,211,527
2904 Police Facility Development Impact Fee	\$ (4,305,390)	\$ 379,609	\$ (643,235)	\$ -	\$ -	\$ (4,569,016)
2905 Parkland Facilities Development Impact Fee	\$ 5,043,395	\$ 686,611	\$ (2,208,359)	\$ 750,000	\$ 97,016	\$ 4,368,663
2907 Recreation Center Development Impact Fee	\$ 679,694	\$ 174,605	\$ (679,694)	\$ 157,625	\$ -	\$ 332,230
2908 Libraries Development Impact Fee	\$ 4,421,729	\$ 82,496	\$ (1,707,049)	\$ 2,100,199	\$ 102,623	\$ 4,999,998
2909 City Hall Development Impact Fee	\$ 541,862	\$ 138,150	\$ -	\$ -	\$ 12,106	\$ 692,118
2910 Corporate Yard Development Impact Fee	\$ 1,881,564	\$ 418,704	\$ (197,000)	\$ -	\$ 38,245	\$ 2,141,513
2911 Interchange Improvements Development Impact Fee	\$ 915,251	\$ 531,446	\$ (657,000)	\$ -	\$ 20,059	\$ 809,756
2912 Maintenance Equipment Development Impact Fee	\$ 909,037	\$ 117,275	\$ -	\$ -	\$ 18,901	\$ 1,045,213
2913 Animal Shelter Development Impact Fee	\$ 106,052	\$ 49,531	\$ -	\$ -	\$ -	\$ 155,583
2914 Administration Development Impact Fee	\$ 440,703	\$ 92,940	\$ (165,032)	\$ -	\$ -	\$ 368,611

The reservation of Fund Balance and disbursement information for each of the above funds is as follows:

Fund 2901 - Arterial Streets Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Debt Service – 2013 Refunding Lease Revenue Bonds Current Year	631,993	100%
Debt Service – 2014 Refunding Lease Revenue Bonds Current Year	428,763	100%
Indian Street - Cardinal Avenue Bridge	215,000	5%
	<u>\$ 1,275,756</u>	

<u>Fund Balance Designations:</u>	
Existing Debt Service and Future Arterial Streets Development	<u>\$ 3,126,810</u>
Unreserved Fund Balance	None
Funds unexpended or uncommitted for five years or more	None

Fund 2902 - Traffic Signal Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
No Disbursements	-	
	<u>\$ -</u>	

<u>Fund Balance Designations:</u>	
Future Traffic Signal Development	<u>\$ 921,317</u>
Unreserved Fund Balance	None
Funds unexpended or uncommitted for five years or more	None

Fund 2903 - Fire Facility Development Impact Fees

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Debt Service – 2013 Refunding Lease Revenue Bonds	144,084	100%
Debt Service – 2014 Refunding Lease Revenue Bonds	97,654	100%
	<u>\$ 241,738</u>	

<u>Fund Balance Designations:</u>	
Future Fire Facility	<u>\$ 4,211,527</u>
Unreserved Fund Balance	None
Funds unexpended or uncommitted for five years or more	None

Fund 2904 - Police Facility Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Debt Service – 2013 Refunding Lease Revenue Bonds	384,074	100%
Debt Service – 2014 Refunding Lease Revenue Bonds	259,161	100%
	<u>\$ 643,235</u>	

<u>Fund Balance Designations:</u>	
Future Police Facility	<u>\$ (4,569,016)</u>
Unreserved Fund Balance	None
Funds unexpended or uncommitted for five years or more	None

Fund 2905 - Parkland Facilities Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Civic Center Amphitheater	2,208,359	28%
	<u>\$ 2,208,359</u>	
<u>Fund Balance Designations:</u>		
Future Parkland Facility	<u>\$ 4,368,663</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2907- Recreation Center Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Civic Center Amphitheater	679,694	9%
	<u>\$ 679,694</u>	
<u>Fund Balance Designations:</u>		
Future Recreation Center	<u>\$ 332,230</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2908 - Libraries Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Iris Plaza Branch Library	1,707,049	100%
	<u>\$ 1,707,049</u>	
<u>Fund Balance Designations:</u>		
Future Libraries	<u>\$ 4,999,998</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2909 - City Hall Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
No Disbursements	-	
	<u>\$ -</u>	
<u>Fund Balance Designations:</u>		
Future City Hall	<u>\$ 692,118</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2910 - Corporate Yard Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
Corporate Yard Master Plan Improvements	197,000	100%
	<u>\$ 197,000</u>	
<u>Fund Balance Designations:</u>		
Future Corporate Yard	<u>\$ 2,141,513</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2911 - Interchange Improvements

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
SR-60 / Moreno Beach Drive Interchange (Phase 2)	657,000	14%
	<u>\$ 657,000</u>	
<u>Fund Balance Designations:</u>		
Future Interchange Improvements	<u>\$ 809,756</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2912 - Maintenance Equipment Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
No Disbursements	-	
	<u>\$ -</u>	
<u>Fund Balance Designations:</u>		
Future Maintenance Equipment	<u>\$ 1,045,213</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2913 - Animal Shelter Development Impact Fee

<u>Disbursements:</u>		<u>% Funded by Impact Fees</u>
No Disbursements	-	
	<u>\$ -</u>	
<u>Fund Balance Designations:</u>		
Future Animal Shelter	<u>\$ 155,583</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Fund 2914 Administration Development Impact Fee

		% Funded by Impact Fees
<u>Disbursements:</u>		
DIF Update	165,032	100%
	<u>\$ 165,032</u>	
<u>Fund Balance Designations:</u>		
Future Development Impact Fee Study	<u>\$ 368,611</u>	
Unreserved Fund Balance	None	
Funds unexpended or uncommitted for five years or more	None	

Five-Year Reporting Requirements

Government Code Section 66001(d)(1) requires that at least every five years certain findings be made with respect to each impact fee being assessed. The following information is provided to satisfy this requirement:

(A) Identify the purpose to which the fee is to be put – The purpose of the development impact fee program is to ensure that new development is paying its share of the transportation infrastructure and facility costs associated with the growth resulting from that development. The program includes projects related to Arterial Street Improvements, Traffic Signal improvements, Fire Facilities, Police Facilities, Park Improvements, Recreation Centers, Libraries, City Hall, Corporate Yard, Interchange Improvements, Maintenance Equipment, Animal Shelter Facilities and Impact Fee Administration.

(B) Demonstrate a reasonable relationship between the fee and the purpose for which it is charged – The fees are based on the relationship between the needed transportation infrastructure and facility costs associated with the growth resulting from new development.

(C) Identify all sources and amounts of funding anticipated for incomplete improvements – Facilities to be funded from development impact fees are also funded by other sources including gas tax, Measure A, General Fund, and grant funding. The specific funding sources utilized for each project depend on funds availability at the time a project is moved forward.

(D) Designate the approximate dates on which the funding is expected to be deposited into the appropriate account or fund – The receipt of funding and the construction of improvements is dependent upon when undeveloped land remaining in the City is developed. Facilities constructed utilizing development impact fee funding are constructed when all required funding is available and the City Engineer has determined that it is appropriate for the project to move forward.

Development Impact Fee Rate Table

RESIDENTIAL IMPACT FEES City-Wide (Except as otherwise noted)

Impact Fee Description	Single Family (DU)	Multi-family (DU)	Mobile/Senior (DU)
Police Facilities	\$ 493.64	\$ 191.73	\$ 125.86
Fire Facilities	\$ 980.93	\$ 261.58	\$ 392.37
Libraries	\$ 327.90	\$ 280.31	\$ 128.37
Park Improvements	\$ 2,728.51	\$ 2,532.44	\$ 1,068.16
Community & Recreation Cntrs	\$ 694.29	\$ 393.50	\$ 271.80
Arterial Streets	\$ 1,125.17	\$ 787.62	\$ 506.33
Traffic Signals	\$ 764.56	\$ 535.19	\$ 344.05
Interchange Improvements	\$ 700.84	\$ 490.59	\$ 315.38
City Hall	\$ 180.49	\$ 48.13	\$ 72.20
Animal Shelter	\$ 196.74	\$ 168.18	\$ 77.02
Corporate Yard	\$ 543.24	\$ 144.86	\$ 217.30
Maintenance Equipment	\$ 152.41	\$ 40.64	\$ 60.97
Impact Fee Administration Fee	\$ 177.77	\$ 117.49	\$ 69.08
Total	\$ 9,066.49	\$ 5,992.26	\$ 3,523.03

NON-RESIDENTIAL IMPACT FEES City-Wide (Except as otherwise noted)

Impact Fee Description	Commercial General (KSF)	Commercial Regional (KSF)	Industrial (KSF)	Industrial High-Cube (KSF)	Office (KSF)
Police Facilities	\$ 646.34	\$ 553.26	\$ 115.77	\$ 115.77	\$ 246.73
Fire Facilities	\$ 360.31	\$ 360.31	\$ 257.36	\$ 257.36	\$ 300.25
Libraries	No Fee	No Fee	No Fee	No Fee	No Fee
Park Improvements	No Fee	No Fee	No Fee	No Fee	No Fee
Community & Recreation Cntrs	No Fee	No Fee	No Fee	No Fee	No Fee
Arterial Streets	\$ 1,479.77	\$ 1,297.79	\$ 729.66	\$ 170.48	\$ 1,022.89
Traffic Signals	\$ 1,005.51	\$ 881.85	\$ 495.80	\$ 115.84	\$ 695.05
Interchange Improvements	\$ 921.71	\$ 808.36	\$ 454.48	\$ 106.19	\$ 637.13
City Hall	\$ 66.30	\$ 66.30	\$ 47.35	\$ 47.35	\$ 55.25
Animal Shelter	No Fee	No Fee	No Fee	No Fee	No Fee
Corporate Yard	\$ 199.54	\$ 199.54	\$ 142.53	\$ 142.53	\$ 166.28
Maintenance Equipment	\$ 55.98	\$ 55.98	\$ 39.99	\$ 39.99	\$ 46.65
Impact Fee Administration Fee	\$ 94.71	\$ 84.47	\$ 45.66	\$ 19.91	\$ 63.40
Total	\$ 4,830.17	\$ 4,307.86	\$ 2,328.60	\$ 1,015.42	\$ 3,233.63

City-Wide except MV Ranch and TownGate SP
See Planning staff for projects within TownGate Specific Plan (SP200)

DU=Dwelling Unit
KSF=1,000 gross square feet of building space

Attachment: AB 1600 Report FY 2020 for Staff Report [Revision 2] (4203 : RECEIVE THE ANNUAL AB



Report to City Council

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO UPGRADE AGING SERVERS AND DISASTER RECOVERY TECHNOLOGY TO MEET INCREASED DEMANDS AND AWARD OF CONTRACT FOR SERVER, STORAGE, BACKUPS, AND DISASTER RECOVERY TECHNOLOGY– RFP NO. 2020-020

RECOMMENDED ACTION

Recommendations:

1. Award contract to Nth Generation Computing, Inc., the highest qualified proposal for \$833,766, (\$757,969 for server, storage, backups, and disaster recovery technology plus \$75,797 (10% contingency)).
2. Authorize the Assistant City Manager to execute the Contract with Nth Generation Computing, Inc., subject to the approval of the Interim City Attorney.
3. Authorize expenditures for improvements to the disaster recovery site for an amount not to exceed \$27,500 (\$25,000 for improvements, plus \$2,500 (10% contingency)).
4. Authorize amendments to the Fiscal Year 2020/21 budget from the Technology Replacement Reserve balance for these expenses as outlined in the Fiscal Impact section.

SUMMARY

COVID-19 highlighted the need to improve technology to meet the needs of the residents and the development community, while enabling remote workers, providing collaborative tools, improving secure file sharing, increasing cyber security, and expanded disaster recovery capabilities into City technology. Current systems were

able to accommodate remote workers and offered the traditional collaboration tools (e.g., e-mail, file sharing, etc.), but are limiting when it comes to collaboration tools and do nothing to build the City's disaster recovery capabilities. Additionally, the system will also see increased demands through new development and planning software implementations.

Additionally, the City's technology infrastructure is due for replacement based on its age (average of 5.34 years, ranging to over 15 years), making this an opportune time to replace aging infrastructure with modern hardware and software. This RFP addresses the hardware infrastructure; software infrastructure is addressed every three years when evaluating software licensing with Microsoft. The next software evaluation is March 2021; staff expects to present significantly more cloud options than ever before to the Council in the first quarter of 2021.

When technology assets are purchased, the City sets aside depreciation costs in the Technology Replacement Reserve so that the assets can be replaced when required. With reductions in computer server hardware prices over the years, the replacement fund is well-positioned to replace the existing servers and to invest in servers for disaster recovery. With the huge increase in computer viruses, Ransomware, and cybersecurity attacks on public entities, the need for disaster recovery capabilities is heightened.

The ability to recover systems from a technology disaster is measured by two means, RTO (Recovery Time Objective) and RPO (Recovery Point Objective). RTO measures how long it takes to recover from an incident. RPO specified how far back in time one can recover so that work does not have to be redone. The City's current RTO and RPO are on the order of multiple days. Recalling backup tapes, loading them, and restoring files and systems for productive use would take several days. Many employees would be unproductive during those days.

The proposed hardware and disaster recovery capabilities give the City the ability to lower its RTO and RPO to hours instead of days. RTO and RPO goals for the most common failures will become four hours.

DISCUSSION

This report recommends approval of a contract with the highest qualified proposal for server, storage, backups, and disaster recovery technology for the City's systems. This project involves replacing all but two computer servers, all data storage platforms, upgrading backup software, and adding a separate disaster recovery location with adequate technology to replicate the primary site and maintain system availability in case of a failure. The City's existing servers (over 90 of them) average 5.34 year old with some servers over twice that age; the storage device is also over five and one-half years old, and tape backups are the primary means of recovery from a disaster. Industry best practices recommend having disaster recovery hardware, and that it be modern disaster recovery equipment, including backup storage in the cloud instead of on tapes.

On August 17, 2020, bid 2020-020 was issued via the City’s electronic proposal management system with a due date of September 18, 2020. Thirty-four contractors expressed interest in the RFP (request for proposal). Three contractors paid to download the proposal documents and attended the optional pre-bid meetings. Six members of the Technology Services Division evaluated proposals and vendor presentations. The highest qualified proposal is from Nth Generation Computing, Inc. with a cost of \$833,766. Therefore, the requested contract award is for \$833,767, (\$757,969 plus a 10% contingency of \$75,797).

The two other bidders were ConvergeOne, Inc. and CPACinc.com.

An additional \$27,500 is requested to prepare the disaster recovery site for secure and climate-controlled operations. These expenditures will add air conditioning, electrical capacity, Internet connections, a server rack, UPS (uninterrupted power supply), and the cabling necessary for the site to adequately support disaster recovery capabilities. The site already possesses a backup generator capable of accommodating the additional load of the disaster recovery equipment.

The full scope of the replacement project includes several subprojects. All of the projects and their estimated costs are given below.

SERVER, STORAGE, BACKUPS, AND DISASTER RECOVERY		
		Estimated Costs
Servers, Storage, Backups (part of bid)	\$620,404	
Disaster Recovery Site	\$137,565	
Prepare Disaster Recovery Site	\$20,000	
Reconfigure Existing Data Center	\$5,000	
Subtotal		\$782,969
Contingency (10%)	\$78,297	
TOTAL		\$861,266

ALTERNATIVES

1. Approve and authorize the six recommended actions as presented in this staff report. *Staff recommends this alternative because it will ensure the continuous operation of its technology systems and add disaster recovery capabilities.*
2. Do not approve or authorize the recommended actions as presented in this staff report. *Staff does not recommend this alternative because it will negatively affect the ability of the City’s technology to operate without failure and will not enhance the City’s disaster recovery capabilities.*

FISCAL IMPACT

Funding for this project will be provided by the dedicated Technology Replacement

Reserve; no General Fund money needs to be allocated for this project. When technology assets are purchased, the City sets aside depreciation costs in the Technology Replacement Reserve so that the assets can be replaced when required. These funds are held in a reserve account until they are needed because they are dedicated to the replacement of technology.

The Technology Replacement Reserve (7230) has adequate balance to expend \$861,266 of the balance for this project due to the City Council's diligent budget policies in past years.

Description	Fund	GL Account No.	Type (Rev/Exp)	FY 20/21 Budget	Proposed Adjustments	FY 20/21 Amended Budget
Technology Replacement Reserve – Transfer Out	7230	7230-99-99-97230-927220	Exp	\$200,000	\$861,266	\$1,061,266
Technology Services Asset Fund – Transfer In	7220	7220-99-99-97220-827230	Rev	\$200,000	\$861,266	\$1,061,266
Technology Services Asset Fund	7220	7220-30-39-25455-620299	Exp	\$0	\$861,266	\$861,266

PREPARATION OF STAFF REPORT

Prepared By:
Steve Hargis
Strategic Initiatives Manager

Department Head Approval:
Marshall Eyerman
Assistant City Manager/Chief Financial Officer/City Treasurer

CITY COUNCIL GOALS

Positive Environment. Create a positive environment for the development of Moreno Valley's future.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development
2. Public Safety
3. Library
4. Infrastructure
5. Beautification, Community Engagement, and Quality of Life
6. Youth Programs

ATTACHMENTS

1. Contract with Nth Generation Computing

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/03/20 2:08 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/07/20 6:40 PM

City of Moreno Valley

AGREEMENT FOR ON-SITE AND/OR PROFESSIONAL SERVICES

This Agreement is made by and between the City of Moreno Valley, California, a municipal corporation, with its principal place of business at 14177 Frederick Street, Moreno Valley, CA 92552, hereinafter referred to as the “City”, and **Nth Generation Computing, Inc.**, a Corporation, with its principal place of business at 17055 Camino San Bernardo, San Diego, CA, 92127, hereinafter referred to as the “Contractor,” based upon City policies and the following legal citations:

RECITALS

- A. Government Code Section 53060 authorizes the engagement of persons to perform special services as independent contractors;
- B. Contractor desires to perform and assume responsibility for the provision of professional services for server and storage infrastructure replacement contracting services required by the City on the terms and conditions set forth in this Agreement. Contractor represents that it is experienced in providing professional server, storage, backup, and disaster recovery infrastructure replacement contracting services, is licensed in the State of California, if applicable;
- C. City desires to engage Contractor to render such services for the server, storage, backup, and disaster recovery as set forth in this Agreement;
- D. The public interest, convenience, necessity and general welfare will be served by this Agreement; and
- E. This Agreement is made and entered into effective the date the City signs this Agreement.

TERMS**1. CONTRACTOR INFORMATION:**

Contractor’s Name: Nth Generation Computing, Inc.
 Address: 17055 Camino San Bernardo
 City: San Diego State: CA Zip: 92127
 Business Phone: 858-451-2383 Fax No. 858-673-8431
 Business License Number: (in process)
 Federal Tax I.D. Number: 33-0451285

2. CONTRACTOR SERVICES, FEES, AND RELEVANT DATES:

- A. The Contractor’s scope of service is described in Exhibit “A” attached hereto and incorporated herein by this reference.
- B. The City’s responsibilities, other than payment, are described in Exhibit “B” attached hereto and incorporated herein by this reference.

- C. Payment terms are provided in Exhibit “C” attached hereto and incorporated herein by this reference.
- D. The term of this Agreement shall be from **December 15, 2020 to June 30, 2021** unless terminated earlier as provided herein. The City acknowledges that it will not unreasonably withhold approval of the Contractor’s requests for extensions of time in which to complete the work required. The Contractor shall not be responsible for performance delays caused by others or delays beyond the Contractor’s reasonable control (excluding delays caused by non-performance or unjustified delay by Contractor, his/her/its employees, or subcontractors), and such delays shall extend the time for performance of the work by the Contractor.

3. **STANDARD TERMS AND CONDITIONS:**

- A. Control of Work. Contractor is solely responsible for the content and sequence of the work, and will not be subject to control and direction as to the details and means for accomplishing the anticipated results of services. The City will not provide any training to Contractor or his/her/its employees.
- B. Intent of Parties. Contractor is, and at all times shall be, an independent contractor and nothing contained herein shall be construed as making the Contractor or any individual whose compensation for services is paid by the Contractor, an agent or employee of the City, or authorizing the Contractor to create or assume any obligation or liability for or on behalf of the City, or entitling the Contractor to any right, benefit, or privilege applicable to any officer or employee of the City.
- C. Subcontracting. Contractor may retain or subcontract for the services of other necessary contractors with the prior written approval of the City. Payment for such services shall be the responsibility of the Contractor. Any and all subcontractors shall be subject to the terms and conditions of this Agreement, with the exception that the City shall have no obligation to pay for any subcontractor services rendered. Contractor shall be responsible for paying prevailing wages where required by law [See California Labor Code Sections 1770 through 1777.7].
- D. Conformance to Applicable Requirements. All work prepared by Contractor shall be subject to the approval of City.
- E. Substitution of Key Personnel. Contractor has represented to City that certain key personnel will perform and coordinate the services under this Agreement. Should one or more of such personnel become unavailable, Contractor may substitute other personnel of at least equal competence upon written approval of City. In the event that City and Contractor cannot agree as to the substitution of key personnel, City shall be entitled to terminate this Agreement for cause. As discussed below, any personnel who fail or refuse to perform the services in a manner acceptable to the City, or who are determined by the City to be uncooperative, incompetent, a threat to the adequate or timely completion of the project or a threat to the safety of persons or property, shall be promptly removed from the project by the Contractor at the request of the City. The key

personnel for performance of this Agreement are as follows: **Richard Tengdin**.

- F. City's Representative. The City hereby designates the City Manager, or his or her designee, to act as its representative for the performance of this Agreement ("City's Representative"). Contractor shall not accept direction or orders from any person other than the City's Representative or his or her designee.
- G. Contractor's Representative. Contractor hereby designates **Steve Hargis**, or his or her designee, to act as its representative for the performance of this Agreement ("Contractor's Representative"). Contractor's Representative shall have full authority to represent and act on behalf of the Contractor for all purposes under this Agreement. The Contractor's Representative shall supervise and direct the services, using his or her best skill and attention, and shall be responsible for all means, methods, techniques, sequences and procedures and for the satisfactory coordination of all portions of the services under this Agreement.
- H. Legal Considerations. The Contractor shall comply with applicable federal, state, and local laws in the performance of this Agreement. Contractor shall be liable for all violations of such laws and regulations in connection with services. If the Contractor performs any work knowing it to be contrary to such laws, rules and regulations and without giving written notice to the City, Contractor shall be solely responsible for all costs arising therefrom. Contractor shall defend, indemnify and hold City, its officials, directors, officers, employees and agents free and harmless, pursuant to the indemnification provisions of this Agreement, from any claim or liability arising out of any failure or alleged failure to comply with such laws, rules or regulations.
- I. Standard of Care; Performance of Employees. Contractor shall perform all services under this Agreement in a skillful and competent manner, consistent with the standards generally recognized as being employed by professionals in the same discipline in the State of California. Contractor represents and maintains that it is skilled in the profession necessary to perform the services. Contractor warrants that all employees and subcontractor shall have sufficient skill and experience to perform the services assigned to them. Finally, Contractor represents that it, its employees and subcontractors have all licenses, permits, qualifications and approvals of whatever nature that are legally required to perform the services and that such licenses and approvals shall be maintained throughout the term of this Agreement. Any employee of the Contractor or its subcontractors who is determined by the City to be uncooperative, incompetent, a threat to the adequate or timely completion of the project, a threat to the safety of persons or property, or any employee who fails or refuses to perform the services in a manner acceptable to the City, shall be promptly removed from the project by the Contractor and shall not be re-employed to perform any of the services or to work on the project.
- J. Contractor Indemnification. Contractor shall indemnify, defend and hold the City, the Moreno Valley Housing Authority, and the Moreno Valley Community Services District (CSD), their officers, agents and employees harmless from any and all claims, damages, losses, causes of action and

demands, including, without limitation, the payment of all consequential damages, expert witness fees, reasonable attorney’s fees and other related costs and expenses, incurred in connection with or in any manner arising out of Contractor’s performance of the work contemplated by this Agreement and this Agreement. Acceptance of this Agreement signifies that the Contractor is not covered under the City’s general liability insurance, employee benefits, or worker’s compensation. It further establishes that the Contractor shall be fully responsible for such coverage. Contractor’s obligation to indemnify shall survive expiration or termination of this Agreement, and shall not be restricted to insurance proceeds, if any, received by the City, the Moreno Valley Housing Authority, and the CSD, and their officers, agents and employees.

- K. Additional Indemnity Obligations. Contractor shall defend, with counsel of City’s choosing and at Contractor’s own cost, expense and risk, any and all claims, suits, actions or other proceedings of every kind covered by Section “J” that may be brought or instituted against City, the Moreno Valley Housing Authority, and the CSD, and their officers, agents and employees. Contractor shall pay and satisfy any judgment, award or decree that may be rendered against City, the Moreno Valley Housing Authority, and the CSD, and their officers, agents and employees as part of any such claim, suit, action or other proceeding. Contractor shall also reimburse City for the cost of any settlement paid by City, the Moreno Valley Housing Authority, and the CSD, and their officers, agents and employees as part of any such claim, suit, action or other proceeding. Such reimbursement shall include payment for City’s attorney’s fees and costs, including expert witness fees. Contractor shall reimburse City, the Moreno Valley Housing Authority, and the CSD, and their officers, agents and employees for any and all legal expenses and costs incurred by each of them in connection therewith or in enforcing the indemnity herein provided.
- L. Insurance Requirements. The Contractor will comply with the following insurance requirements at its sole expense. Insurance companies shall be rated (A Minus: VII—Admitted) or better in Best’s Insurance Rating Guide and shall be legally licensed and qualified to conduct business in the State of California:

The Contractor shall procure and maintain, at its sole expense, Workers’ Compensation Insurance in such amounts as will fully comply with the laws of the State of California and which shall indemnify, insure and provide legal defense for the Contractor and the City, the Housing Authority and CSD against any loss, claim, or damage arising from any injuries or occupational diseases happening to any worker employed by the Contractor in the course of carrying out the Agreement. This coverage may be waived if the Contractor is determined to be functioning as a sole proprietor and the city provided form “Exception to Worker’s Compensation Coverage” is signed, notarized and attached to this Agreement

General Liability Insurance—to protect against loss from liability imposed by law for damages on account of bodily injury, including death, and/or property damage suffered or alleged to be suffered by any person or persons

whomever, resulting directly or indirectly from any act or activities of the Contractor, sub-Contractor, or any person acting for the Contractor or under its control or direction. Such insurance shall be maintained in full force and effect throughout the terms of the Agreement and any extension thereof in the minimum amounts provided below:

- Bodily Injury \$1,000,000 per occurrence/ \$2,000,000 aggregate
- Property Damage \$500,000 per occurrence/ \$500,000 aggregate

~~Professional Errors and Omission Insurance such coverage shall not be less than \$1,000,000 per claim and aggregate.~~

Liability and Property Damage Insurance coverage for owned and non-owned automotive equipment operated on City/CSD/Housing Authority premises. Such coverage limits shall not be less than \$1,000,000 combined single limit.

A Certificate of Insurance and appropriate additional insured endorsement evidencing the above applicable insurance coverage shall be submitted to the City prior to the execution of this Agreement. The Certificate of Insurance or an appropriate binder shall bear an endorsement containing the following provisions:

Solely as respect to services done by or on behalf of the named insured for the City of Moreno Valley, it is agreed that the City of Moreno Valley, the Moreno Valley Housing Authority, and the Moreno Valley Community Services District, their officers, employees and agents are included as additional insured under this policy and the coverage(s) provided shall be primary insurance and not contributing with any other insurance available to the City of Moreno Valley, the Moreno Valley Housing Authority, and the Moreno Valley Community Services District, its officers, employees and agents, under any third party liability policy

The terms of the insurance policy or policies issued to provide the above coverage shall neither be amended to reduce the required insurance limits and coverages nor shall such policies be canceled by the carrier without thirty (30) days prior written notice by certified or registered mail of amendment or cancellation to the City, except that cancellation for non-payment of premium shall require ten (10) days prior written notice by certified or registered mail. In the event the insurance is canceled, the Contractor shall, prior to the cancellation date, submit new evidence of insurance in the amounts established.

M. Intellectual Property. Any system or documents developed, produced or provided under this Agreement, including any intellectual property discovered or developed by Contractor in the course of performing or otherwise as a result of its work, shall become the sole property of the City unless explicitly stated otherwise in this Agreement. The Contractor may retain copies of any and all

material, including drawings, documents, and specifications, produced by the Contractor in performance of this Agreement. The City and the Contractor agree that to the extent permitted by law, until final approval by the City, all data shall be treated as confidential and will not be released to third parties without the prior written consent of both parties.

- N. Entire Agreement. This Agreement constitutes the entire agreement between the parties. There are no understandings, agreements, or representations of warranties, expressed or implied, not specified in this Agreement. This Agreement applies only to the current proposal as attached. This Agreement may be modified or amended only by a subsequent written Agreement signed by both parties. Assignment of this Agreement is prohibited without prior written consent.
- O. (a) The City may terminate the whole or any part of this Agreement at any time without cause by giving at least ten (10) days written notice to the Contractor. The written notice shall specify the date of termination. Upon receipt of such notice, the Contractor may continue work through the date of termination, provided that no work or service(s) shall be commenced or continued after receipt of the notice which is not intended to protect the interest of the City. The City shall pay the Contractor within thirty (30) days after receiving any invoice after the date of termination for all non-objected to services performed by the Contractor in accordance herewith through the date of termination.
- (b) Either party may terminate this Agreement for cause. In the event the City terminates this Agreement for cause, the Contractor shall perform no further work or service(s) under the Agreement unless the notice of termination authorizes such further work.
- (c) If this Agreement is terminated as provided herein, City may require Contractor to provide all finished or unfinished documents and data and other information of any kind prepared by Contractor in connection with the performance of services under this Agreement. Contractor shall be required to provide such documents and other information within fifteen (15) days of the request.
- (d) In the event this Agreement is terminated in whole or in part as provided herein, City may procure, upon such terms and in such manner as it may determine appropriate, similar to those terminated.
- P. Payment. Payments to the Contractor pursuant to this Agreement will be reported to Federal and State taxing authorities as required. The City will not withhold any sums from compensation payable to Contractor. Contractor is independently responsible for the payment of all applicable taxes. Where the payment terms provide for compensation on a time and materials basis, the Contractor shall maintain adequate records to permit inspection and audit of the Contractor's time and materials charges under the Agreement. Such records shall be retained by the Contractor for three (3) years following completion of the services under the Agreement.
- Q. Restrictions on City Employees. The Contractor shall not employ any City employee or official in the work performed pursuant to this Agreement. No

officer or employee of the City shall have any financial interest in this Agreement in violation of federal, state, or local law.

- R. Choice of Law and Venue. The laws of the State of California shall govern the rights, obligations, duties, and liabilities of the parties to this Agreement, and shall govern the interpretation of this Agreement. Any legal proceeding arising from this Agreement shall be brought in the appropriate court located in Riverside County, State of California.
- S. Delivery of Notices. All notices permitted or required under this Agreement shall be given to the respective parties at the following address, or at such other address as the respective parties may provide in writing for this purpose:

Contractor:

Nth Generation Computing, Inc.
17055 Camino San Bernardo
San Diego, CA 92127
Attn: Garrett Ono

City:

City of Moreno Valley
14177 Frederick Street
P.O. Box 88005
Moreno Valley, CA 92552
Attn: Steve Hargis, Strategic Initiatives Manager

Such notice shall be deemed made when personally delivered or when mailed, forty-eight (48) hours after deposit in the U.S. Mail, first class postage prepaid and addressed to the party at its applicable address. Actual notice shall be deemed adequate notice on the date actual notice occurred, regardless of the method of service.

- T. Time of Essence. Time is of the essence for each and every provision of this Agreement.
- U. City’s Right to Employ Other Contractors. City reserves right to employ other contractors in connection with this project.
- V. Amendment; Modification. No supplement, modification, or amendment of this Agreement shall be binding unless executed in writing and signed by both parties.
- W. Waiver. No waiver of any default shall constitute a waiver of any other default or breach, whether of the same or other covenant or condition. No waiver, benefit, privilege, or service voluntarily given or performed by a party shall give the other party any contractual rights by custom, estoppel, or otherwise.
- X. No Third Party Beneficiaries. There are no intended third party beneficiaries of any right or obligation assumed by the parties.

- Y. Counterparts. This Agreement may be signed in counterparts, each of which shall constitute an original.
- Z. Invalidity; Severability. If any portion of this Agreement is declared invalid, illegal, or otherwise unenforceable by a court of competent jurisdiction, the remaining provisions shall continue in full force and effect.
- AA. Assignment or Transfer. Contractor shall not assign, hypothecate, or transfer, either directly or by operation of law, this Agreement or any interest herein without the prior written consent of the City. Any attempt to do so shall be null and void, and any assignees, hypothecates or transferees shall acquire no right or interest by reason of such attempted assignment, hypothecation or transfer.
- BB Supplementary General Conditions (for projects that are funded by Federal programs). The following provisions, pursuant to 44 Code of Federal Regulations, Part 13, Subpart C, Section 13.36, as it may be amended from time to time, are included in the Agreement and are required to be included in all subcontracts entered into by CONTRACTOR for work pursuant to the Agreement, unless otherwise expressly provided herein. These provisions supersede any conflicting provisions in the General Conditions and shall take precedence over the General Conditions for purposes of interpretation of the General Conditions. These provisions do not otherwise modify or replace General Conditions not in direct conflict with these provisions. Definitions used in these provisions are as contained in the General Conditions.
1. CONTRACTOR shall be subject to the administrative, contractual, and legal remedies provided in the General Conditions in the event CONTRACTOR violates or breaches terms of the Agreement.
 2. CITY may terminate the Agreement for cause or for convenience, and CONTRACTOR may terminate the Agreement, as provided the General Conditions.
 3. CONTRACTOR shall comply with Executive Order 11246 of September 24, 1965, entitled Equal Employment Opportunity, as amended by Executive Order 11375 of October 13, 1967, and as supplemented in Department of Labor regulations (41 CFR chapter 60). (All construction contracts awarded in excess of \$10,000 by CITY and/or subcontracts in excess of \$10,000 entered into by CONTRACTOR.)
 4. CONTRACTOR shall comply with the Copeland Anti-Kickback Act (18 U.S.C. 874) as supplemented in Department of Labor regulations (29 CFR Part 3) (All contracts and subcontracts for construction or repair.)
 5. CONTRACTOR shall comply with the Davis-Bacon Act (40 U.S.C. 276a to 276a7) as supplemented by Department of Labor regulations (29 CFR Part 5).
 6. CONTRACTOR shall comply with Sections 103 and 107 of the Contract Work Hours and Safety Standards Act (40 U.S.C. 327330) as supplemented by Department of Labor regulations (29 CFR Part 5).

- 7. CONTRACTOR shall observe CITY requirements and regulations pertaining to reporting included in the General Conditions.
- 8. Patent rights with respect to any discovery or invention which arises or is developed in the course of or under the Agreement shall be retained by the CITY.
- 9. Copyrights and rights in data developed in the course of or under the Agreement shall be the property of the CITY. FEMA/CalOES reserve a royalty-free, nonexclusive, irrevocable license to reproduce, publish or otherwise use or authorize to others to use for federal purposes a copyright in any work developed under the Agreement and/or subcontracts for work pursuant to the Agreement.
- 10. CONTRACTOR shall provide access by the City, the Federal grantor agency, the Comptroller General of the United States, or any of their duly authorized representatives to any books, documents, papers, and records of the contractor which are directly pertinent to that specific contract for the purpose of making audit, examination, excerpts, and transcriptions.
- 11. CONTRACTOR shall retain all required records for three years after CITY makes final payments and all other pending matters relating to the Agreement are closed.
- 12. CONTRACTOR shall comply with all applicable standards, orders, or requirements issued under section 306 of the Clean Air Act (42 U.S.C. 1857(h)), section 508 of the Clean Water Act (33 U.S.C. 1368), Executive Order 11738, and Environmental Protection Agency regulations (40 CFR part 15). (This provision applies to contracts exceeding \$100,000 and to subcontracts entered into pursuant to such contracts.)
- 13. CONTRACTOR shall comply with mandatory standards and policies relating to energy efficiency which are contained in the State energy conservation plan issued in compliance with the Energy Policy and Conservation Act (Pub. L. 94163, 89 Stat. 871).

SIGNATURE PAGE TO FOLLOW

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

IN WITNESS HEREOF, the parties have each caused their authorized representative to execute this Agreement.

City of Moreno Valley

Nth Generation Computing, Inc.

BY: _____
Dr. Yxstian A. Gutierrez
Mayor

BY: _____

TITLE: _____

Date

Date

BY: _____

TITLE: _____

Date

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

INTERNAL USE ONLY

ATTEST:

City Clerk

APPROVED AS TO LEGAL FORM:

City Attorney

Date

RECOMMENDED FOR APPROVAL:

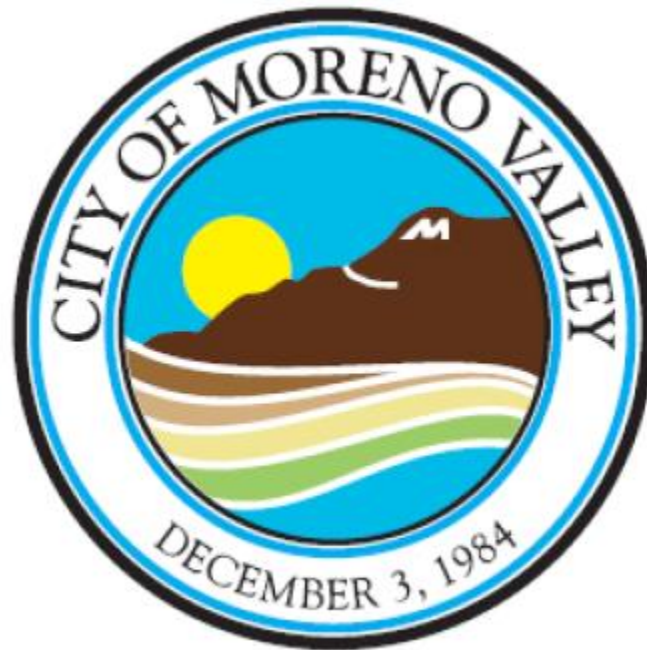
Assistant City Manager, CFO

Date

EXHIBIT A
CONTRACTOR'S SCOPE OF SERVICE

The Contractor's scope of service begins on the next page.

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO



Nth Generation Computing, Inc.'s Proposal

For RFP #2020-020 Professional Services for Server and Storage Infrastructure Replacement

Prepared Especially for City of Moreno Valley

Submittal Deadline: October 27, 2020, 2:00 pm, PST

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

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Confidentiality Notice

This Nth Generation Proposal and all associated documents and attachments contained and included herewith, includes proprietary and confidential data that shall not be disclosed outside of Client Companies and Nth Generation Computing, Inc. and shall not be duplicated, used, or disclosed in whole or in part for any purpose other than to evaluate this proposal. This restriction does not limit any right to use information contained in this proposal if it is obtained from another source without restriction.

A. Section 1: Executive Summary

1. Cover Letter

October 27, 2020

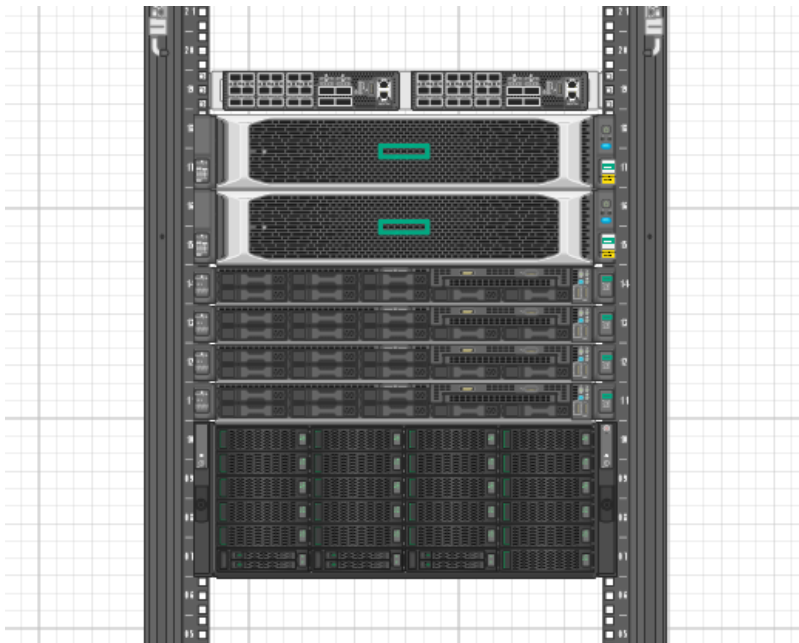
Attention:
City of Moreno Valley, Purchasing Division
PurchasingDivision@moval.org

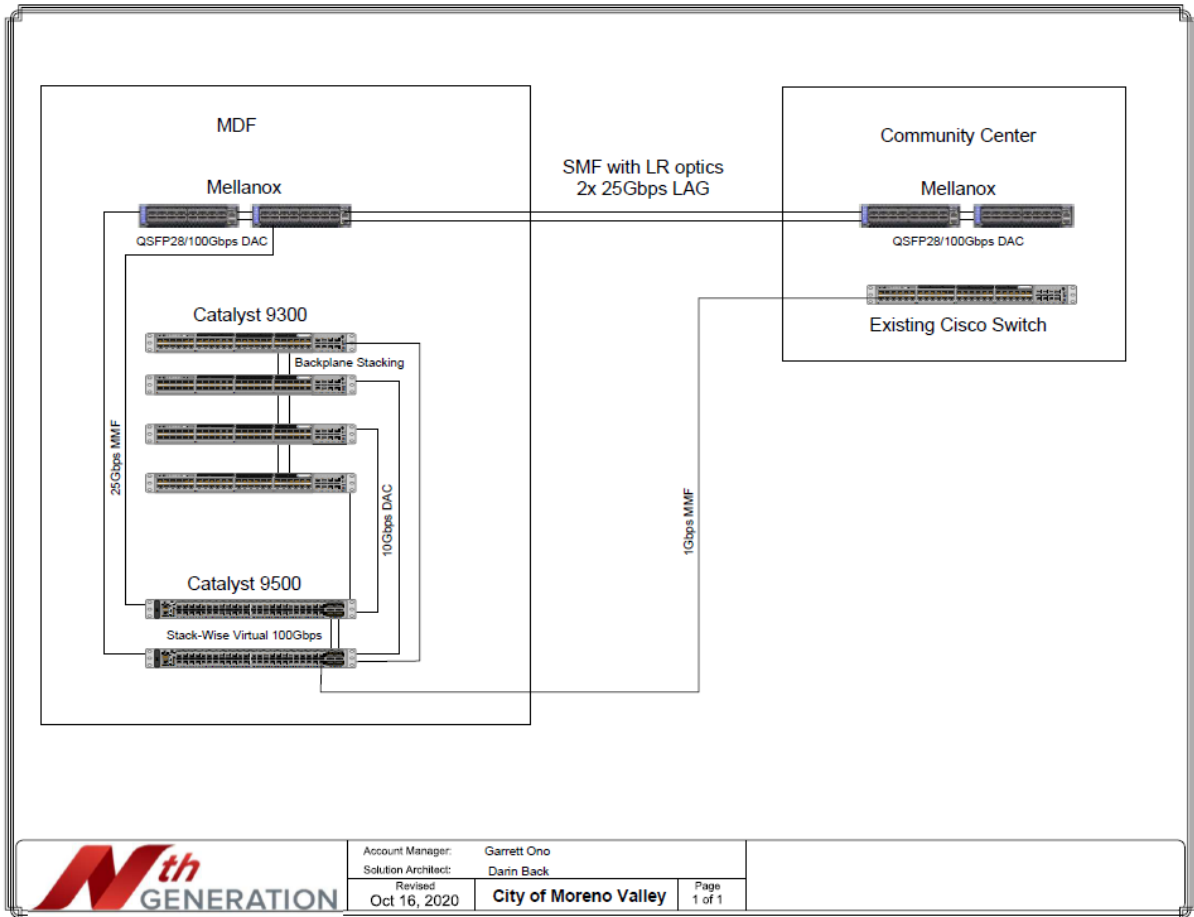
RE: Nth Generation Computing, Inc.'s Proposal to City of Moreno Valley, RFP #2020-020 Professional Services for Server and Storage Infrastructure Replacement

Dear City of Moreno Valley,

Nth Generation Computing, Inc. ("Nth") is pleased to present herein our proposal in response to City of Moreno Valley's ("City" or "Client") RFP #2020-020 for Professional Services for Server and Storage Infrastructure Replacement.

Nth is proposing an All-in-One *Disaggregated hyper-converged infrastructure* (dHCI) Solution and HPE StoreOnce refresh for your production and disaster recovery sites. The proposed solution provides the following hardware for the Primary and DR locations:





- 2x M-series (Mellanox) SN2010 25GbE switches in a common 1U rack mount each with 18 SFP28 ports and 4x QSFP28 ports for stacking
 - 4x 10GbE SFP+ optics for City network connection
 - 4x 25GbE SFP+ LC LR 10km SMF Transceiver
 - 4x 10GbE 3m DAC cables for storage
 - 16x 25GbE 3m DAC cables for host connections
- HPE StoreOnce 3640 48 TB appliance with
 - 48 TB Capacity upgrade
 - 10/25GbE network card
 - Encryption LTU
 - 2x 25GbE 1m DAC cables
 - Array provides 72TiB backup target storage capacity for Commvault backups
- 4x DL360 Gen10 servers each with
 - 2x Xeon-Gold 6148 processors (20 cores, 2.4 Ghz)
 - 768 GB of RAM
 - 4x 10/25GbE network ports (2 for iSCSI, 2 for general VMware)
 - 4x GbE ports (2 for DMZ switch access, 2 for general VMware)
 - 1x GbE ILO/OneView management port

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- Nimble HF40 Array with
 - 126TB Raw data pack (165.6 TiB effective at 2:1 data reduction, 90% occupancy)
 - 4x 10GbE network ports for iSCSI traffic
 - Array is rated at 50k random read, 85k random write performance
- Core Switches
 - 2x Cisco Catalyst 9500 in MDF to replace Catalyst 6509 Aggregation Ports
 - 4x Cisco Catalyst 9300 in MDF to replace Catalyst 6509 Access Ports

The solution requires 13U as installed, we recommend reserving 4U below the Nimble array to allow for storage expansion

HPE dHCI solution with Nimble Storage

HPE Nimble dHCI Combines Benefits of both CI and HCI with the flexibility of converged and the simplicity of HCI. Grow compute and storage independently, extended across a hybrid cloud, with industry-leading data efficiency. It disaggregates compute and storage and integrates hyperconverged control to give enterprises simple infrastructure management on a flexible architecture. HPE Nimble Storage dHCI extends the hyperconverged experience to workloads with unpredictable growth, allowing independent scaling of compute and storage. This gives enterprises the flexibility of converged and the simplicity of HCI, delivering sub-milliseconds data response times for latency-sensitive databases and up to 21X data reduction – Accelerating time-to market on an architecture built for the unpredictable with a resilient design center for 99.9999% availability (With Nimble Storage) with no single point of failure and advanced data integrity to tolerate three simultaneous drive failures. Built with the world's Best-Selling server, HPE ProLiant, and the Self-Managing. HPE dHCI automation software, advanced data services, and HPE InfoSight are all free. Although HCI is an aggregated architecture that combines storage, compute and virtualization in the same chassis and software stack; HPE Nimble dHCI consists of HPE ProLiant servers packaged with Nimble Storage arrays, with automation software to help with deployment. The simplicity of hyper-converged experience with the flexibility of converged. A converged infrastructure packages servers and storage separately, as in a traditional infrastructure, but does not combine them into a single chassis and integrated stack, as HCI does.

HPE StoreOnce Appliance with Catalyst technology

We will leverage your existing Commvault platform and updated your backup physical backup repository with HPE StoreOnce to help minimize training on new products and services. Ransomware cannot affect what it cannot see and the best backup solution is useless if ransomware can access your backup repositories. HPE StoreOnce is a purpose-built backup appliance (or virtual machine) that includes HPE StoreOnce Catalyst stores to effectively isolate critical data where attackers cannot have impact on it without resorting to direct physical interactions that ultimately would result in the destruction of some or all of the hardware itself. Even when physical destruction is achieved at a single location, whether from malware or a natural disaster, the more advanced implementation of HPE StoreOnce Federated Catalyst stores (distributed implementation) would effectively protect mission critical data by effectively isolating it from traditional lines of communication and command sets leveraged by ransomware attackers. The single most important feature of HPE StoreOnce Catalyst is its ability to completely isolate data from being tampered with unintentionally. HPE has hidden the Catalyst store from attackers in plain sight but behind an application programming interface (API) that both enhances and simplifies the process of backing up and deduplicating data while making it practically impossible for ransomware to attack it directly. HPE StoreOnce Catalyst was designed for use as a disk-based

solution capable of leveraging deduplication, compression, encryption, and data isolation for backup and archiving processes. HPE StoreOnce Catalyst prevents ransomware from accessing or encrypting backup data stores, thereby ensuring data integrity. The HPE Cloud Bank Storage feature of HPE StoreOnce lets you leverage the economics, agility, and flexibility of the cloud for modernized data protection. You can natively, securely, and cost-effectively move backup data to the public, private, or hybrid cloud—providing long-term retention and reliable disaster recovery that is simple and efficient.

In conclusion Nth Generation is proposing an all-HPE solution to simplify support and management with only one phone number and email address for the HPE help desk...plus Nth is only a call or click away! Nth's proposed solution reduces the complexity of implementing multiple vendors that may or may not play well together. Finally, an all-HPE solution allows the City to get up and running very quickly due to your vast experience with HPE hardware, software and related tools. HPE Nimble Storage dHCI (disaggregated HCI) simplifies the life of VM admins struggling to keep up with the changing demands of their business. It scales to support workloads with unpredictable growth, it is simple to manage through VMware vCenter and has HPE InfoSight, which means it's simple to support. Rather than monolithic all-in-one nodes, dHCI products ship separate compute and storage nodes so you can decide the right mix of each without scaling compute and storage linearly in lockstep.

Required Information:

- a) Company's full legal name, address, phone, fax, email, website;
Nth Generation Computing, Inc.
17055 Camino San Bernardo
San Diego, CA 92127
Phone: 858-451-2383
Fax: 858-673-8431
Email: bids@nth.com
Website: www.nth.com

- b) Prior company names (if any);
None.

- c) Organizational structure (corp., LLC, etc.);
Corporation

- d) Names and titles of the principal owner(s);

Janis Baldwin, CEO

Richard Baldwin, CSO/Secretary

Dan Molina, CTO

Joyce Russell, EVP/CFO

- e) Person(s) authorized to make commitments for your company;
Joyce Russell, EVP/CFO

- f) Company history, experience (brief), and years in business;

Nth Generation Computing, Inc. (DUNS No: 78-112-3211 / SIC No: 7373, 7379) an IT consulting and engineering firm in Southern California, building adaptive IT infrastructure solutions that enable business agility. An “adaptive infrastructure” can respond proactively to changing market conditions, thus becoming a strategic weapon and helping to secure a competitive advantage in the marketplace. Founded in 1991, with over 29 years of extensive data center expertise, Nth consistently maintains the highest levels of technical certification to ensure superior quality deliverables and service. Today Nth offers key services and solutions in areas including: Security, Mobility (including BYOD), Networking, Unified Communications, Data Management (including Big Data, Backup, Archive, Search and eDiscovery), Data Analytics, Enterprise Storage, Servers, Converged and Hyper-Converged Infrastructure, Software-Defined Data Centers, Converged Cloud, and Managed Services. At the heart of Nth’s value-add is our staff of highly trained engineers, business analysts, account executives, and project managers with decades of front line experience in the aforementioned areas. With Nth’s high caliber talent, our clients enjoy the benefit of having the industry’s top professionals analyze, design, implement and support their IT investments.

Since 1991, Nth Generation has provided industry leading, consultative IT services, encompassing a suite of IT and security solutions. We empower clients to accelerate achieving optimal results through boosting agility, efficiency, and profitability, while mitigating business risks.

Whether your need or initiative is IT security; governance, risk mitigation, and compliance; cloud computing / XaaS; managed services; hyperconverged infrastructure; networking; hybrid IT; IoT; edge computing; automation; artificial intelligence; or other transformative solutions; Nth offers proven expertise to be your trusted industry advisor.

What sets Nth apart is our distinction of tenured experts — account managers, project managers, senior certified security consultants, solution architects, and engineers — who have achieved top level certifications with leading manufacturers. In addition, through Nth Labs, we test, validate, and demonstrate key functionalities of a wide set of cutting-edge technologies that are relevant to your organization.

In a perpetually changing, competitive landscape, Nth Generation helps you accomplish your unique business goals both now and in the future.

- g) Current number of employees by category;
Total Employees: 69
Executive/Managerial: 11
Engineering/Technical Professionals: 24
Operations/Administration: 16
Sales: 18

- h) Federal Identification Number;

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

33-0451285

- i) Contracts terminated for cause, pending litigation or legal issues.
None.

We are grateful for this opportunity with City of Moreno Valley. Should you need any other information to move this process forward and further validate your decision, please let us know.

Sincerely,

Handwritten signature of Joyce Russell in blue ink.

Joyce Russell
EVP/CFO
Nth Generation Computing, Inc.
17055 Camino San Bernardo
San Diego, CA 92127
858-451-2383
888-674-4684 (fax)
joyce.russell@nth.com

Handwritten signature of Garrett Ono in blue ink.

Garrett Ono
Account Manager
Nth Generation Computing, Inc.
17055 Camino San Bernardo
San Diego, CA 92127
858-451-2383
888-674-4684 (fax)
garrett.ono@nth.com

2. Exceptions

B. Section 2: Supplemental Company Information (Optional)

Requirement:

1. Provide any supplemental information not specifically requested by City that you would like City to consider in evaluating your proposal.
2. Ensure information is relevant to City's current or potential future needs.
3. Limit this section to a maximum of one page.

Nth's Response:

Most recent accolades include – Inc. Magazine's list of "Best Workplaces 2020"; #3 in San Diego's Best Places to work (mid-size companies) in 2019; 2018 and 2019 HPE Top Partner of the Year in the West; HPE Platinum Partner; Darktrace Platinum Partner; Nutanix Platinum Partner; Woman-Owned Company.

Pre-sales Support – Client's dedicated account manager, inside sales representative and solution architects are ready to support the client on this and future projects relating to server, storage, networking, big data, cloud, security, mobility and general computing solutions. Nth Generation solution architects can assist clients in preparing solutions, configurations and budgetary quotes. In addition, Nth Generation solution architects have the ability to deliver HPE product roadmap presentations.

Professional Services – Nth has local engineers in Southern California to serve the client for post-sales support. These engineers have achieved the top level certifications with leading manufacturers. Nth is recognized by HPE to have some of the most highly skilled and experienced solution engineers in Southern California.

Operational Support - Nth's highly certified Solution Architects (SAs) work closely with our clients to understand their primary business goals and IT enabled initiatives and design solutions that meet those goals. Nth's ability to utilize the extensive knowledge of our Solutions Architects allows us to review a client's existing hardware, software and virtual infrastructure and provide integrated solutions for our clients, leveraging both existing infrastructure and new technology.

Nth Labs – Nth Labs is a multi-million dollar, cutting edge environment, where we evaluate, validate, and demonstrate key functionalities of a wide set of leading technologies that are relevant to your organization. At Nth Labs we show complete solutions beyond a point product. Nth is a beta site for HPE, as well as other technologies. This allows us to provide feedback on developing technologies, maintain awesome relationships with engineering teams, and be fully up to speed when new solutions are ready for release.

Professional Services

Nth Generation excels in the non-disruptive implementation of enterprise IT solutions, particularly in 7X24, mission-critical datacenter environments. Nth consultants have the skill and expertise to ensure successful project delivery. Capabilities include:

- IT security
- governance, risk mitigation, and compliance

- cloud computing / XaaS
- managed services
- hyperconverged infrastructure
- networking
- hybrid IT
- IoT
- artificial intelligence

Client Testimonials

- **Security** - Manufacturing, Retail, and Entertainment:
<https://www.security.nth.com/testimonials>
- **Public Sector:** <https://www.nthgentech.com/publicsector>

C. Section 3: Subcontractor Company Information

Requirement:

1. For every subcontractor or partner providing Professional Services provide the following information and any supplemental information not specifically requested by City that you would like City to consider in evaluating your proposal.
 - a) Company’s full legal name, address, phone, fax, email, website;
 - b) Organizational structure (corp., LLC, etc.);
 - c) Company’s parent company, if applicable;
 - d) Description of the company’s lines of business, and areas where they will participate in this engagement;
 - e) Company’s experience in responsibilities similar to their participation in the engagement (e.g., years in business, year authorized to install manufacturers product(s), description of geographic reach and market penetration in the target area, number of similar systems installed or services performed for a similarly sized and complex engagement.
2. Limit this section to a maximum of two pages per subcontractor.

Nth’s Response:

Nth will not be utilizing any subcontractors for the proposed work. Nth is able to provide the resources needed to deliver our solution with Nth staff. Nth does have relationships and partnerships with a variety of subcontractors, should additional scope be requested that is not included in Nth’s proposal.

D. Section 4: Professional Team Assignments

Requirement:

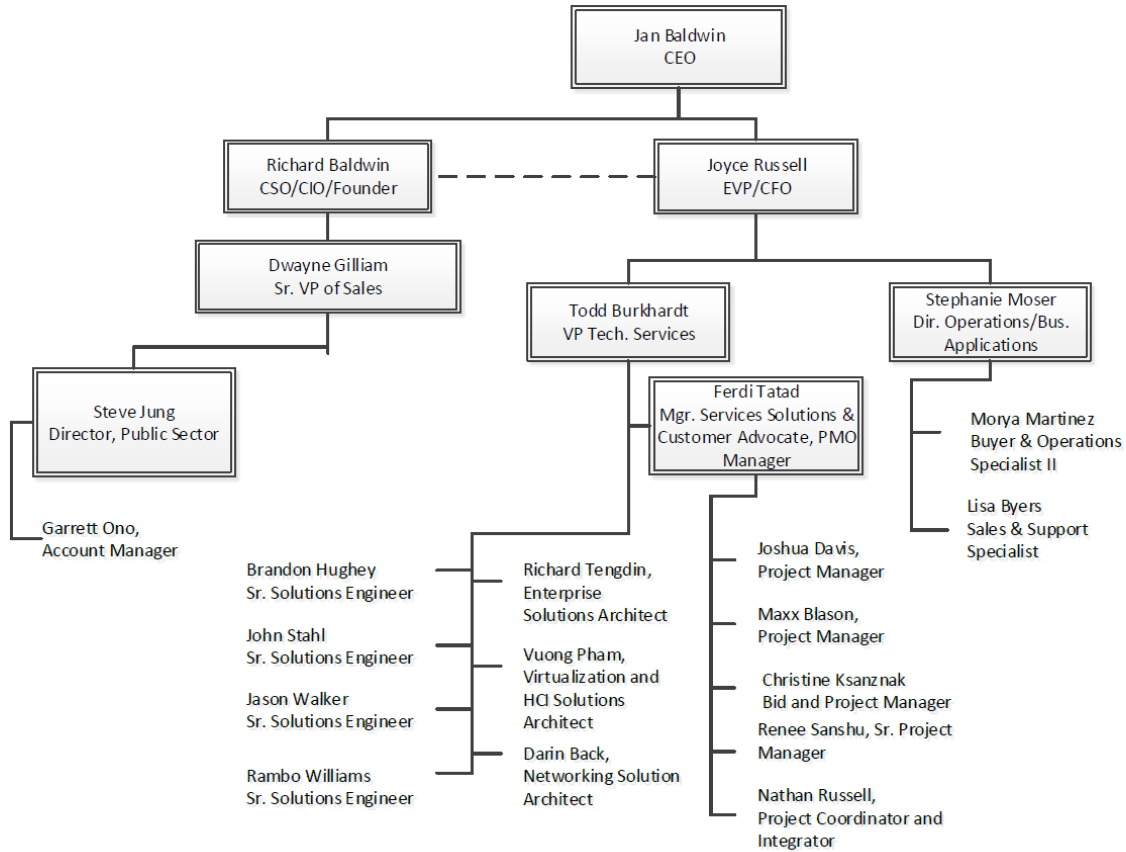
1. Note any key personnel who are expected to remain in service until completion of the project.
2. Provide detail regarding the team to be assigned for these services.
3. Provide resumes of all team members.
4. Provide an organizational chart of all team members, titles, and a very brief description of their relevant responsibilities.
5. Limit this section to a maximum of two pages plus resumes and org chart.

Nth's Response:

The following team will be assigned to execute the proposed solution. Upon award, an Nth Project Manager will be assigned to manage the scope, schedule, budget and resources. Resumes are located in Section II, E. of Attachment A.

Name	Role/Title
Richard Tengdin	Enterprise Solution Architect
Vuong Pham	Sr. SA & Virtualization Specialist
Darin Back	Sr. Networking Consultant
Brandon Hughey	Sr. Solutions Engineer – Servers and Virtualization
John Stahl	Sr. Solutions Engineer – Storage
Robert Rambo Williams	Sr. Solutions Engineer – Storage
Jason Walker	Sr. Solutions Engineer – Storage, Servers, Backup and Data Management
Ferdi Tatad	Services, Solutions & PMO Manager
Renee Sanshu	Sr. Project Manager
Joshua Davis	Project Manager
Maxx Blason	Project Manager
Chrissie Stubbs	Bid and Project Manager
Nathan Russell	Project Coordinator, Integrator

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO



E. Section 5: Proposal Costs

Requirement:

1. Submit all pricing on *Exhibit B using the form provided.
2. Provide pricing for each of the required line items.
3. Provide pricing for optional proposer recommendations.
4. See payment terms in Exhibit B for additional details.

*These forms are provided by City in the submittal forms section.

Nth's Response:

Please find the completed Exhibit B: Pricing form on the following page. **Lease Options are available upon request.**

RFP #2021-020 Professional Services for Server and Storage Replacement August 2020

Exhibit B: Pricing

I. Equipment and Professional Service Rates***

A. Apportion your costs among the established categories below. Be sure to include all costs in one of the categories.

Five-Year Total Cost Summary						
Item	Total	Year 1	Year 2	Year 3	Year 4	Year 5
Primary Site Hardware	\$208,019.00	\$208,019.00	N/A	N/A	N/A	N/A
DR Site Hardware	\$137,565.00	\$137,565.00	N/A	N/A	N/A	N/A
Software Licensing	\$110,522.00	\$110,522.00	included	included	included	included
Third-Party Software	N/A	N/A	N/A	N/A	N/A	N/A
Installation Services	\$4,000.00	\$4,000.00	N/A	N/A	N/A	N/A
Configuration of Operating Environments	\$46,226.00	\$46,226.00	N/A	N/A	N/A	N/A
Data Loading, Conversion, or Transfer	\$30,000.00	\$30,000.00	N/A	N/A	N/A	N/A
Documentation & Training	\$10,000.00	\$10,000.00	N/A	N/A	N/A	N/A
Project Management	\$13,580.00	\$13,580.00	N/A	N/A	N/A	N/A
Miscellaneous	N/A	N/A	N/A	N/A	N/A	N/A
Maintenance	\$171,124.00	\$171,124.00	included	included	included	included
Subtotal:	\$731,038.00	\$731,038.00		N/A	N/A	N/A
Sales Tax	\$26,932.51	\$26,932.51	N/A	N/A	N/A	N/A
Total:	\$757,970.51	\$757,970.51	N/A	N/A	N/A	N/A

II. Hourly Rates***

A. These rates are not anticipated to be used; however, if the scope of work increases during the contract, City would like to have established rates. Include the title and rate for all staff that may provide services.

LN	Title of Person Performing Service	Hourly Rate
1	Solution Engineer - Storage	\$ 300.00
2	Solution Engineer - Server, Virtualization	\$ 300.00
3	Solution Engineer - Networking	\$ 300.00
4	Solution Engineer - Installation	\$ 300.00
5	Solution Engineer - Microsoft	\$ 300.00
6	Project Manager	\$ 160.00
7	Security Consultant	\$ 375.00
8	N-BBQ-Rambo	\$ included
9		\$
10		\$

*** Attach additional detail sheets as necessary.

(CONTINUED ON NEXT PAGE)

III. Pricing Terms and Conditions

- A. **Primary Site Hardware:** Describe the hardware components of the system located in the primary data center; group related components together.

Nth is proposing an All-in-One *Disaggregated hyper-converged infrastructure* (dHCI) Solution and HPE StoreOnce refresh for your production and disaster recovery sites. The proposed solution provides the following hardware for the Primary and DR locations:

- 2x M-series (Mellanox) SN2010 25GbE switches in a common 1U rack mount each with 18 SFP28 ports and 4x QSFP28 ports for stacking
 - 4x 10GbE SFP+ optics for City network connection
 - 4x 25GbE SFP+ LC LR 10km SMF Transceiver
 - 4x 10GbE 3m DAC cables for storage
 - 16x 25GbE 3m DAC cables for host connections
- HPE StoreOnce 3640 48 TB appliance with
 - 48 TB Capacity upgrade
 - 10/25GbE network card
 - Encryption LTU
 - 2x 25GbE 1m DAC cables
 - Array provides 72TiB backup target storage capacity for Commvault backups
- 4x DL360 Gen10 servers each with
 - 2x Xeon-Gold 6148 processors (20 cores, 2.4 Ghz)
 - 768 GB of RAM
 - 4x 10/25GbE network ports (2 for iSCSI, 2 for general VMware)
 - 4x GbE ports (2 for DMZ switch access, 2 for general VMware)
 - 1x GbE iLO/OneView management port
- Nimble HF40 Array with
 - 126TB Raw data pack (165.6 TiB effective at 2:1 data reduction, 90% occupancy)
 - 4x 10GbE network ports for iSCSI traffic
 - Array is rated at 50k random read, 85k random write performance
- Core Switches
 - 2x Cisco Catalyst 9500 in MDF to replace Catalyst 6509 Aggregation Ports
 - 4x Cisco Catalyst 9300 in MDF to replace Catalyst 6509 Access Ports

The solution requires 13U as installed, we recommend reserving 4U below the Nimble array to allow for storage expansion

- B. **DR Site Hardware:** Describe the hardware components of the system in the DR site; group related components together.

Same as above is proposed for the DR Site.

- C. **Software Licensing:** List, describe, and record the licensing, implementation, maintenance, and support fees associated with your proposed software.

Software licensing pricing is included in Exhibit B.

- D. **Third-Party Software:** List and describe each piece of software (including Operating Systems) required to optimally run the software.

None.

- E. **Installation Services:** Describe any labor, equipment, supplies, or other costs associated with installing your proposed system into our current architecture and systems.

Descriptions of Professional Services is included in the Work Plan. Pricing is included in Exhibit B.

- F. **Configuration of Operating Environments:** List and describe activities associated with post-installation configuration of all equipment, accessories, Operating Systems, software, etc. required to optimize the system. This includes setting up an environment of data collection and metric reporting, activity required to integrate the system into the City's architecture and back-end systems.

Descriptions of Professional Services is included in the Work Plan. Pricing is included in Exhibit B.

- G. **Data Loading, Conversion, or Transfer:** Describe any labor, equipment, or other costs associated with importing data from current systems into the new system.

Descriptions of Professional Services is included in the Work Plan. Pricing is included in Exhibit B.

- H. **Documentation & Training:** If there are fees associated with your user or technical documentation, list them here.

Descriptions of Professional Services is included in the Work Plan. Pricing is included in Exhibit B.

- I. **Project Management:** If there are project management fees associated with your proposed software, list and describe them here.

Descriptions of Professional Services is included in the Work Plan. Pricing is included in Exhibit B.

- J. **Miscellaneous:** List and describe any other costs associated with your proposed software solution. These cost may include per diems or reimbursable expenses.

None.

- K. **Maintenance:** Describe any other ongoing costs associated with the operation and maintenance of your proposed system. Quantities: listed Line Items are annual estimates

based on historical information or anticipated and may vary significantly. City does not imply or make any commitment to purchase any specific quantity.

Maintenance and support is included in Exhibit B per the scope.

L. Cost/Price: include everything but sales tax.

Please see the Exhibit B table.

M. Delivery Costs: must be included in unit price.
Included.

N. Sales Tax: add the appropriate tax, if applicable to the item type.
Please see the Exhibit B table.

O. Additional Charges: none; do not charge any fees or charges not listed in the Price Sheets.
None.

P. Fixed Prices: prices are fixed during the agreement.
Prices are fixed price per Exhibit B table.

Q. Proposal Price Sheet: The awarded Provider's Price Sheet, as accepted by City, will be incorporated into the resultant Agreement.

Please see the Exhibit B table.

F. Section 6: Response Template

Requirement:

1. Complete City-provided Response Template with your answers to City’s questions. Provide thorough responses with sufficient detail to enable City to evaluate your understanding of City’s requirements, the suitability of your services and/or product(s) to meet City’s requirements, the strength of your work plan, previous experience, and available resources.
2. Note these responses carry significant weight in City’s evaluation of your proposal to provide and implement requested services.
3. Limit this section to a maximum of ten pages.

Nth’s Response:

Please see Nth’s responses below, in-line.

Attachment A: Required Response Template

Nth Generation Computing, Inc.

(Bidder’s Company Name)

Instructions:

Prospective proposers must provide the information requested below and include it in their proposal. City has provided the form as a separate Word document for bidder’s use. Use this form or copy and paste it into your own document; in either case, please provide your responses under each of the number points. Do not omit or renumber any sections. Refer to attached documents sparingly and only as necessary; and ensure that any documents referred to are numbered according to the outline below.

I. Company Information: Name, Contacts, History, Scope of Services

Please provide the following information about your company. Repeat this information for all services subcontractors.

- A. Your company’s full legal name, address, phone, fax, email, website.

Nth Generation Computing, Inc.
17055 Camino San Bernardo
San Diego, CA 92127
Phone: 858-451-2383
Fax: 858-673-8431
Email: bids@nth.com
Website: www.nth.com

- B. Prior company names (if any) and years in business; mergers, buyouts, etc.

None.

- C. Organizational structure (i.e. corp., LLC, sole proprietorship, etc.).

Corporation

D. Names and titles of the principal owner(s).

Janis Baldwin, CEO
Richard Baldwin, CSO/Secretary
Dan Molina, CTO
Joyce Russell, EVP/CFO

E. Person(s) authorized to make commitments for your company.

Joyce Russell, EVP/CFO

F. Company history, experience, years in business for current company name.

Nth Generation Computing, Inc. (DUNS No: 78-112-3211 / SIC No: 7373, 7379) an IT consulting and engineering firm in Southern California, building adaptive IT infrastructure solutions that enable business agility. An “adaptive infrastructure” can respond proactively to changing market conditions, thus becoming a strategic weapon and helping to secure a competitive advantage in the marketplace. Founded in 1991, with over 29 years of extensive data center expertise, Nth consistently maintains the highest levels of technical certification to ensure superior quality deliverables and service. Today Nth offers key services and solutions in areas including: Security, Mobility (including BYOD), Networking, Unified Communications, Data Management (including Big Data, Backup, Archive, Search and eDiscovery), Data Analytics, Enterprise Storage, Servers, Converged and Hyper-Converged Infrastructure, Software-Defined Data Centers, Converged Cloud, and Managed Services. At the heart of Nth’s value-add is our staff of highly trained engineers, business analysts, account executives, and project managers with decades of front line experience in the aforementioned areas. With Nth’s high caliber talent, our clients enjoy the benefit of having the industry’s top professionals analyze, design, implement and support their IT investments.

Since 1991, Nth Generation has provided industry leading, consultative IT services, encompassing a suite of IT and security solutions. We empower clients to accelerate achieving optimal results through boosting agility, efficiency, and profitability, while mitigating business risks.

Whether your need or initiative is IT security; governance, risk mitigation, and compliance; cloud computing / XaaS; managed services; hyperconverged infrastructure; networking; hybrid IT; IoT; edge computing; automation; artificial intelligence; or other transformative solutions; Nth offers proven expertise to be your trusted industry advisor.

What sets Nth apart is our distinction of tenured experts — account managers, project managers, senior certified security consultants, solution architects, and engineers — who have achieved top level certifications with leading manufacturers. In addition, through Nth Labs, we test, validate, and demonstrate key functionalities of a wide set of cutting-edge technologies that are relevant to your organization.

In a perpetually changing, competitive landscape, Nth Generation helps you accomplish your unique business goals both now and in the future.

G. Current number of employees by category (i.e., engineering, service, installation, administrative, etc.).

Total Employees: 69
Executive/Managerial: 11
Engineering/Technical Professionals: 24
Operations/Administration: 16
Sales: 18

H. Tax ID number.

33-0451285

I. Contracts terminated for cause, pending litigation or legal issues.

None.

II. Resources: Staffing, Facilities, Equipment

Provide the following information relative to required services:

A. Names and titles of key management personnel.

Todd Burkhardt, VP of Technical Services
Ferdi Tatad, Services, Solutions & PMO Manager
Steve Jung, Director of Public Sector
Garrett Ono, Account Manager

B. Team to be assigned for these services.

Name	Role/Title
Richard Tengdin	Enterprise Solution Architect
Vuong Pham	Sr. SA & Virtualization Specialist
Darin Back	Sr. Networking Consultant
Brandon Hughey	Sr. Solutions Engineer – Servers and Virtualization
John Stahl	Sr. Solutions Engineer – Storage
Robert Rambo Williams	Sr. Solutions Engineer – Storage
Jason Walker	Sr. Solutions Engineer – Storage, Servers, Backup and Data Management
Ferdi Tatad	Services, Solutions & PMO Manager
Renee Sanshu	Sr. Project Manager
Joshua Davis	Project Manager
Maxx Blason	Project Manager
Chrissie Stubbs	Bid and Project Manager
Nathan Russell	Project Coordinator, Integrator

C. Qualifications of specific individuals who will work on the project.

Each member of the Nth team as successfully delivered on hundreds of projects in their specialized areas. Many of the team members below has worked in collaboration on infrastructure, virtualization, and data center solutions for well over 10 years. Some of the team's specific technical specialities and credentials are listed below for reference.

Name	Resource type	Qualifications	Credentials
Vuong Pham	Server, Virtualization, Hyperconverged Infrastructure (HCI)	HCI, DevOps, CNA, HPE Blades/Server, VMware, Hyper-V, Cloud	VCP 3,4,5,6, VCP DTM 7, VCAP Design, VCAP Administration, EMCIE. VxRail Xpert, Nutanix NPP, SimpliVity Certified, VMware vExpert 2020, VMware MCS
Darin Back	Networking	Networking, Security, Wireless, and UC, Cisco Routing and Switching, UC, Data Center, Meraki, Extreme Networks, Aruba/HPE Networks, Juniper Networks, Fortinet, and Palo Alto	Aruba: ACDP, ASDX Written, ACMA, ACSA, ACSP Cradlepoint: CPCNA, CPCNP Cisco: CCENT, CCNA Extreme: ECS Campus Fabric, S Campus EXOS, ECS Management Center, EDS Campus Fabric, EDS Data Center, EDS IP Campus, EDS Management Center
Brandon Hughey	Server and Virtualization, VDI Engineer	HPE Blades/Servers and VMware	HPE ASE certified, VMware VCP certified, Microsoft MCSE1
Jason Walker	Server, Data Protection Engineer	HPE Blades/Servers, Microsoft Hyper-V, Commvault, Networking	HPE APT HPN network certified, Microsoft MCSE1, Nutanix NPP, Commvault Simpana, Aruba Networks
John Stahl	Storage, SANs	HPE 3PAR, MSA and Servers	HPE Master ASE storage, MCSE1
Bob Williams	Storage, SANS, Backup	HPE 3PAR, MSA and servers, Commvault Simpana	HPE Partner Ready 3PAR, MSA, MSL, SAN certified
Richard Tengdin	Server, Storage, Data Protection Architect	HPE Server, Storage, Commvault Simpana, Veeam, VMware	HPE Master ASE Storage, Commvault Simpana, Veeam, Aruba networks, VMware VTSP
Christine Stubbs	Project Management	Commvault TSP, PMP	PMP

D. Amount of time and involvement of key personnel who will be involved in respective portions of the project.

Task	Duration	Resource
Award	Day 1	City
Project Kick-off	1 hr.	PM, City, Nth Technical Resources
Installation Planning	1 day	PM, City, Nth Technical Resources
Install new systems in racks, two locations	2 days	Nathan, Rambo, Brandon
Core Network Startup and Configuration	3 days	Darin
- Gather existing configuration of existing Catalyst 6509.		
- Staging and configuration at Nth		
- Installation		
- Bring up switch and migrate routing		
- Decommission C6509		
dHCI startup services 2 locations	10 days	Rambo
Network startup services 2 locations	2 days	Darin
Operation readiness review - vCenter remediation - vSphere virtual network topology review and design - P2V detailed planning and server identification - virtualized DMZ network design - Commvault design review	3 days	Vuong, Brandon, Jason, City
Gap analysis & remediation	1 Day	Vuong, Brandon, Jason, City
Set up required DvSwitches including virtual DMZ	1 day	Brandon

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Set up Shared-nothing vMotion from production cluster	.5 day	Brandon
Demonstrate VM migration	.5 day	Brandon, City
Verify Shared nothing vMotion from DMZ host and demonstrate	.5 day	Brandon, City
Deploy two CommVault MA VMs as new installs	1 day	Jason
Start up StoreOnce and configure Catalyst to Mas (2 locations)	2 days	Rambo, Jason
Configure CVLT to use new Media Agent(s) and StoreOnce appliances	1 day	Rambo, Jason
P2V non-Commvault hosts	10 days	Brandon, Vuong
Wait for data on physical Media Agents to age off	30 days (age off time)	City
Remove old MAs from CVLT and retire hardware	2 days	Jason
Create As-Built documentation	2 days	Nth Technical Resources
Project review, Knowledge review, Best Practices	2 days	City, Nth Technical Resources
Overall Project Management	10 days	Nth PM

- E. Resumes of all team members; provide only names and titles only; attach current resumes to proposal.

Account Team:

Steve Jung, Director of Public Sector

Steve is dedicated to serving our public sector accounts and is responsible for all pre-sales efforts, including but not limited to Client services, vendor engagement, on-site meetings, conference calls, quotes, configurations, bids, and all Client facing activities. In addition, Steve, manages Nth’s Public Sector team.

Steve is an Information Technology manager with over 20 years experience delivering comprehensive IT solutions and successful business outcomes to your most difficult information technology challenges. Over the years Steve has engaged with enterprise organizations in the public sector to help secure, protect and optimize their IT infrastructure with best-of-breed solutions including Data Storage/Management, Hyper-Converged Infrastructure, Software Defined Data Center, Security, Networking, Big Data, Disaster Recovery, Virtualization, Cloud, and more.

Garrett Ono, Account Manager

Garret is focused on serving our public sector accounts and has been with Nth since 2018. He has over 10 years of experience in IT, with a focus on IT Sales and Account Administration since 2015. Garrett is responsible for Client service, vendor management, account coordination, and all Client facing activities. He believes that providing a Client experience based in culture, loyalty, trust, and reliability helps our clients improve their IT ecosystem, as well as their revenue or budgets, to optimize the right solution for each client's needs. This includes continuously honing his own knowledge and expertise, and that of his team, around the latest solutions for Infrastructure, Hyper-Converged, Storage and Data Management, Security, Networking, DR, Virtualization, Cloud and SaaS. Garrett currently holds certifications in VMSP, VMTSP, Fortinet NSE 1-3, Dell/EMC Storage, and NCSR L1-3.

Solution Architects:**Richard Tengdin, Senior Solutions Architect**

Richard has been working with networked computers since 1986 and PCs since 1982. He started as a technical user, managed a four node Apollo workstation cluster, and managed the Pathworks environment for 600 PCs and 800 users at Rocketdyne. After moving to a small ISV he got his first Microsoft certifications and completed his MCSE on NT 4.0. Richard came to NTH Generation in December 1999 and has been working as an Implementation Engineer and Solutions Architect. He is an HP Master ASE -StorageWorks Solutions, HP Networking ASC and ASP, MCSE on Windows Server 2003, Brocade Certified Fabric Administrator, Network Engineer and SAN Designer, VMware VCP and is certified by Xiotech, CommVault, Emulex on their products. Richard has been involved with backup issues for many years, both designing and troubleshooting backup environments. He has experience with backup environments running CommVault Galaxy and Simpana, Data Protector, Backup Exec, ArcServe, NetBackup and Networker. Richard also has a BS from Penn State, and he served in the US Navy. He also remembers playing the ancestral version of Angry Birds on a GE mainframe computer, just don't ask him when.

Vuong Pham, Senior Solutions Architect

Vuong Pham is a Senior Solutions Architect who has been in IT for 20 years in many aspects of IT. Pre-sales, Design, Implementation, and Operations of small, medium and enterprise environments across multiple industries. He is SME in virtualization, data protection and storage solutions for multiple vendors. His current focus is HCI solutions, DevOps and CNA. VCP 3,4,5,6, VCP DTM 7, VCAP Design, VCAP Administration, EMCIE. VxRail Xpert, Nutanix NPP, SimpliVity Certified, VMware vExpert 2015-2018, You can follow Vuong on Twitter as: @Digital_kungfu

Darin Back, Sr. Networking Solution Architect

Darin is a newer addition to Nth, having joined us in early 2019; however, he has been working in IT and networking since 1993. Darin is a Senior Networking Solution Engineer and Architect focusing on Networking, Security, Wireless, and UC. Typical solutions consist of Cisco Routing and Switching, UC, Data Center, Meraki, Extreme Networks, Aruba/HPE Networks, Juniper Networks, Fortinet, and Palo Alto. He designs and implements solutions to fit Client needs

while working with sales teams and Clients through the whole process. Darin's skills in both solution design and hands on implementation are a vital part of any project team.

Solution Engineers:

John Stahl – Sr. Storage and SAN Solutions Engineer

John is a long time member of the NTH post-sales engineering team. He has worked in the computer industry since the early 1980's and since the early 1990's with focus on storage. John has been with Nth Generation's engineering department deploying enterprise class storage for more than 20 years. His focus since the late 1990's has been Storage Area Networks and he is skilled in many areas of SAN deployment including SAN virtualization, upgrades, architecture, Fibre Channel switched networks, data migration, data replication for Disaster Recovery, clustering, and DR clusters. John holds many certifications including Microsoft MCSE, MCP, HP Master ASE SAN Architect, Brocade Certified Professional, VMware VCP, and more.

Bob "Rambo" Williams – Solutions Engineer

Rambo came to Nth after many years with HPE and Compaq. Rambo has an outstanding knowledge of SAN based storage and backup hardware and has proven to always place the Client first. He is familiar with several OS' from Unix to Widows and is working on his VMware certification. He also has an in-depth knowledge of ISCSI, HPE 3PAR, LeftHand networks, Quantum STORnext File system, Dell Compellent Storage and X-IO to name a few. Rambo's knowledge and out of the box thinking has helped many Clients come up with a storage solution that best meets their needs, including but not limited to Warner Bros, GE medical, Behr Products and The Irvine company.

Brandon Hughey, Sr. Solutions Engineer

Brandon Hughey has over 15 years of computer experience, MCSE 2003, Master ASE SAN Architect and Master ASE ProLiant High Availability and Clustering Solutions. ASE HPE BladeSystem, AIS BladeSystem, VMware Certified Professional (VCP), VDM Jumpstart Accreditation, Health Check Accreditation, Business Continuity Accreditation, P2v Service Accreditation.

Jason Walker, Sr. Solutions Engineer

Jason is an Information Technology professional with over 14 years in the field as a Microsoft Systems Engineer/Consultant and Backup Systems Administrator. With over 7 years of professional services deployments focused primarily on CommVault, HPE Server & Storage Technology, Server Virtualization (ESX and Hyper-V) and Microsoft System Center, he has experience in a wide variety of Client environments.

Joining Nth Generation in January of 2007, Jason has helped with developing the engineering team by furthering his skills required for successful engagements with new technology. His extensive background with administering backup products and specific CommVault skill set has added value to the team's overall ability to competently deliver complex data management solutions. He continues to work on career development with the Microsoft System Center product suite as well as keeping up with CommVault and HPE related technologies.

Project Management:**Ferdi Tatad, Director of Professional Services and PMO Manager**

Ferdi Tatad has been with Nth Generation for over 10 years. He joined Nth following a 23 year career at Hewlett Packard (formerly DEC, then Compaq) where he held a number of different positions, including Field Engineer, Unit Manager and Project Manager. Currently, Ferdi manages Nth's Post Sales Engineering and Technical Editing teams, as well as oversees all technical escalations, working with each of Nth's partner manufacturers, as applicable. Lastly, Ferdi also manages Nth's extensive reach of technical certifications, ensuring all needed certifications are assigned, attained and maintained, as applicable. Ferdi holds a BS degree in Business Management.

Christine Kszanznak Stubbs, Bid and Project Manager

Christine is a certified Project Manager (PMP) with over 20 years of administrative organization experience and 10 years of progressive Project Management experience. Christine has successfully delivered on hundreds of projects, within budget and scope, and with a profound ability to adapt and maneuver through the politics and challenges that commonly arise. Her experience spans diverse industries and projects of varying sizes. Christine's years of administrative experience, working at different levels of the project cycle, have forged her skills in organization, planning, Client service and problem resolution. Similarly, her experience has equipped her with a keen understanding of the complexities associated with both the corporate and small business environments. Christine's progressive experience as a project manager continues to sharpen her ability to assess an issue and develop a fast resolution. Christine has been with Nth Generation for 6 and half years. She currently manages a multitude of projects, both internal and external to Nth Generation. Christine is also Nth's Bid Manager, managing Nth's bid process and coordinating bid development to ensure time lines and compliance are met per client's requirements.

Renee Sanshu, Sr. Project Manager

Renee has over 20 years of project management experience, ranging from full software development lifecycle management to infrastructure upgrades and data migrations. Renee has managed two \$15M+ multi-year infrastructure upgrades starting at vendor selection all the way to device installation and operational acceptance. Having worked as an IT project management consultant for government, state, local and Fortune 100 companies, Renee's breath of experience enables her to align IT with corporate strategy, gain consensus from Executive Leadership, and effectively communicate the project vision and objectives with key stakeholders and team members. She works well with geographically diverse, cross-functional teams and recently completed a large data migration project for an international healthcare manufacture; migrating or decommissioning over 75 enterprise applications. Renee has been with Nth Generation for over three years. She currently manages a wide range of projects, from hardware installation, software deployment, information security assessment, and business continuity planning. Her background in education, technology, organizational change management and account management makes her an effective, visionary leader.

Joshua Davis, Project Manager

Joshua has over 10 years of administrative organization and project coordination experience and 4 years of progressive Project Management experience. Joshua manages projects in Security, Application, and Infrastructure from start to finish; leading projects and building relationships with clients in various industries including Education, Government, Health Care, Private and Public companies. He also manages internal, external and remote teams including 3rd party vendors (vendor include as HPE, VMware, Nutanix, Varonis, SecureAuth and Cylance) to deliver client solutions. Joshua manages Nth's yearly Symposium event which includes participation from companies such as HPE, Nutanix, Veeam, Cylance, VMware, Aruba, and Zerto.

Maxx Blason, Project Manager

Maxx has over 5 years of progressive project management experience. He is a passionate, adaptive, and hardworking individual with focused organizational skills and experience across a diverse range of industries. Maxx's project management background consists of managing infrastructure, software development, web development, hyper-convergence, security, Citrix, networking, storage, pick/nosql database conversions, risk assessments, disaster recovery and hosted virtual environment projects. He leads projects and manages client relationships and expectations by utilizing project management tools to ensure that his projects are well communicated, planned and executed. Maxx is also involved in the management of Nth's annual Symposium as well as Nth internal IT Projects. He has also spearheaded training on the project management of specific project types including networking, VDI, VMware/Server Virtualization and Agile project management methodology.

III. Minimum Qualifications

- A. Do you have at least three years of experience conducting the specific type of services required herein and have experience with at least three other clients performing like services as described herein or have performed satisfactory work for City within the past three years?

Yes, Nth has been providing Infrastructure Solutions and Services since 1991. Please see Attachment C: Client References for client contact details for references specific to this proposal. Additional examples may be provided upon request.

- B. Is your firm capable of providing the required services beginning on November 9, 2020? Work will be conducted during normal City work hours, Monday to Thursday 7:30 am to 5:30 pm.

Yes, Nth resources are available to begin work upon award and contract execution. Resources are available beginning on November 9, 2020 assuming said contract is executed prior to that date. Hardware availability is based upon Manufacturer availability.

- C. Is it agreeable that Proposers staff, when onsite, will work at City Hall, 14177 Frederick St, Moreno Valley, CA 92552 or at the City's Emergency Operations Center, 22870 Calle San Juan De Los Lagos, Moreno Valley, CA 92552 (across the parking lot from City Hall)?

Yes, Nth resources will be available to work onsite, following any State or Federal Covid-19 mandates, at the City's locations in Moreno Valley California.

- D. Is it agreeable that some Friday, early morning, late evening, and weekend work is anticipated to not disrupt existing City systems running production workloads. Have you provided costs for this contingency in the cost proposal?

Yes, it is agreeable to provide some services during non-standard business hours as needed or requested to mitigate disruption risks. Costs for this contingency are included in Nth's cost proposal for the tasks included in the scope of work.

- E. Does your firm maintain current certification(s), and is this described in your response to the RFP? Please list page of proposal response where this information is provided.

Nth maintains current certifications for all of the solutions and products we are proposing. The list below includes current Nth certifications. Please also see Section II, C., pages 19 for specific certifications and qualifications of Nth team members for this project.

HPE Platinum Partner Ready Certifications

- Platinum Converged Infrastructure Specialist
- Gold Storage Specialist
- Gold Aruba Networking Specialist
- Gold Server Specialist
- Gold ServiceOne Specialist
- Gold Cloud Builder Specialist
- Gold ArcSight Specialist
- Qualified Software Authorization
- BCS Product Authorization
- Storage Product Authorization
- Networking Product Authorization
- Silver IT Software Management
- HPE Hyper-converged System Partner

HPE Technical Certifications

- 4- HPE MASE -Master SAN Architects (MASE)
- HPE Master certification in Network Infrastructure
- HPE ASE- Server Solutions Architect v2
- HPE ASE-Data Center and Cloud Architect v2
- HPE Arcsight Certified

- HPE AIS- Server Automations v9
- HPE ATP- DataProtector
- HPE ATP- FlexNetwork Solutions v2

HPE PointNext Delivery Partner Ready (formerly ServiceOne)

- HPE 3PAR StoreServ
- HPE Nimble
- HPE Simplivity
- HPE Synergy
- HPE StoreSure
- HPE StoreOnce
- HPE c-Class Blade systems
- HPE-StoreEasy
- HPE StoreVirtual

VMware- Solutions Provider- Premier Level

- VMware Premier Enterprise Partner
- MSC- Master Services Competency in Data Center Virtualization
- vSAN Specialist
- VMware certified in the following Competencies: NSX, Data Management, Application Modernization, Infrastructure Virtualization, Management, Desktop Virtualization, Business Continuity, Virtualization of Business Critical Apps, and Cloud IaaS, SDN
- 5 VCPs, 6 VTSP, 19 VSP VMware certified professionals



**VMware
Master Services
Competency**

DATA CENTER VIRTUALIZATION

Achievement of advanced technical certifications, proof of high-level service capability and expertise validated by customers.

Commvault-Premier Level

- Partner Advantage Partner
- Simpana CVCSA Certified Commvault Engineers

Veeam

- Gold Partner
- VMSP- Sales certification
- VMTSP- Pre-Sales Technical
- VMCE- Technical Certification

Nutanix

- Nutanix – NPP (Nutanix Platform Certified)

DELL

- Authorized Gold Partner
- Compellent Authorized Installation Professional

Microsoft – Silver Partner in Messaging Competency

- Seven Microsoft Certified MCTIP/MCP/MCSE Engineers

RedHat

Red Hat Linux Advanced Business Partner

Networks

- Palo Alto Networks certified- Silver level
- Fortinet- Platinum
- F5 Networks Product and System Engineer certified
- A10 Networks certified
- Silver Peak Partner
- Aruba Networks Certified

Security

- Certified Information System Security Professional (CISSP)
- Certified Information Security Manager (CISM)
- Certified Control Objectives for IT (COBIT)
- GIAC Certified as Penetration Tester, Security Essentials, Forensic Examiner

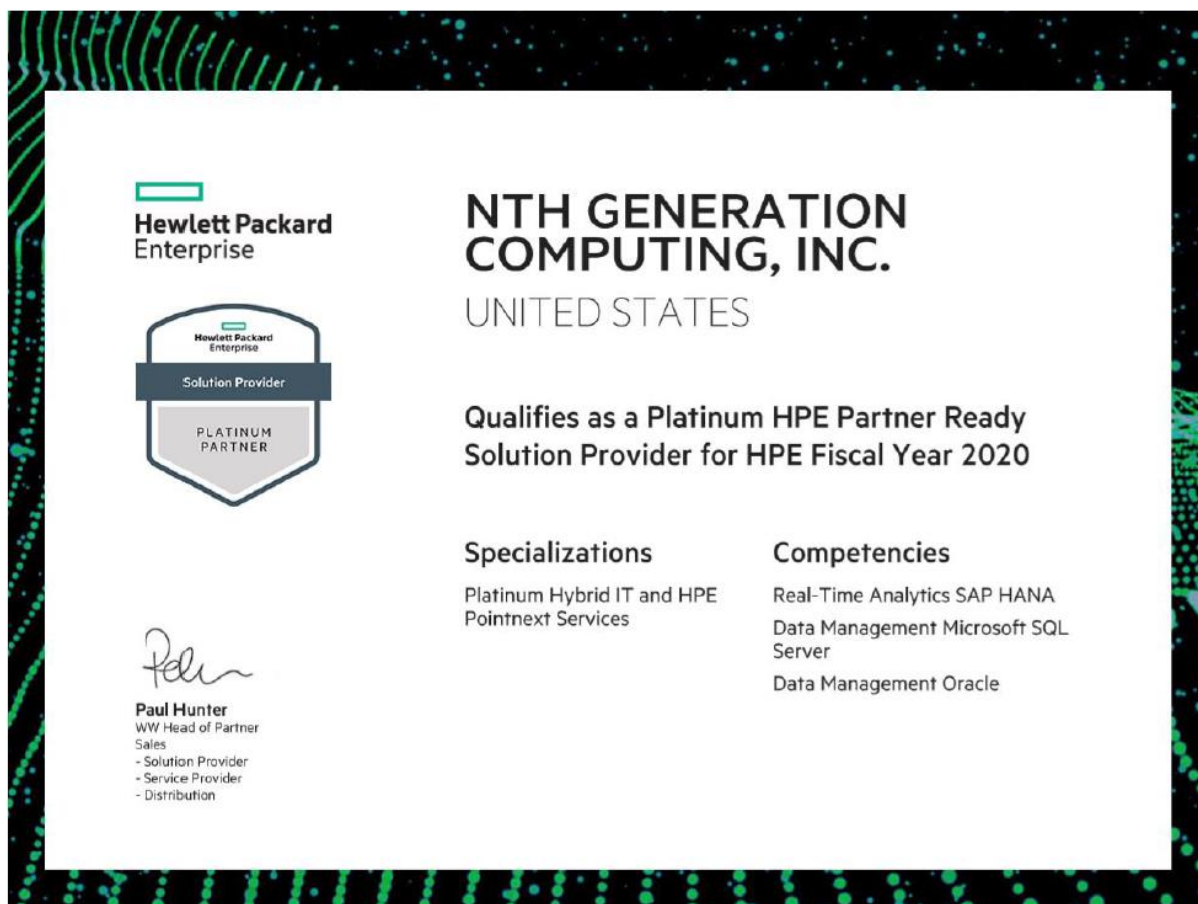
Other Notable certs

- ITIL Certified
- Project Management Certified Professionals (PMP)

- Brocade Elite Fabric and BCNE Partner
- Fortinet- Platinum
- Silver Peak- Silver

F. Does your firm maintain current license or partnership and is this described in your response to the RFP? Please list page of proposal response where this information is provided.

Nth is a PointNext (formerly Service One/Partner Ready) Platinum HPE Partner, authorized to deliver services on behalf of HPE and maintains current licenses and partnership certifications as required. Our HPE Partner Certification is below for reference.



G. Does your firm have the necessary resources, knowledge, skills, experience, and the like to provide the required services, and is this described in your response to the RFP? Please list page of proposal response where this information is provided.

Yes, Nth has all of the necessary resources, knowledge, skills and experience to provide the required and proposed services.

- List of all Partner and Technical certifications: Section III, F., page 26-28
- Team Qualifications and Certifications: Section II, C., pages 19

H. Does your firm have financial stability and the necessary financial resources to provide the required services?

Yes, Nth has been providing Solutions and Services since 1991. Nth is financially stable; a call with Nth's CFO or other financial references can be coordinated upon request.

I. Does your firm demonstrate the requisite technical proficiency? Only Providers with verifiable (e.g., licensed partnerships with manufacturers, certified technical staff, etc.) experience will be considered for award. Please list page of proposal response where this information is provided.

Yes, please see the pages listed below for licenses and certifications that demonstrates that Nth exceeds the requisite technical proficiency needed to execute the proposed solution.

- HPE Partner Certification: Section III, F., page 29
- List of all Partner and Technical certifications: Section III, F., page 26-28
- Team Qualifications and Certifications: Section II, C., pages 19

IV. Required Services: Meeting or Bettering these Requirements

Provide the following information relative to required services:

A. Ability to perform specific tasks as outlined in the RFP.

Nth Generation is proposing an All-in-One Disaggregated hyper-converged infrastructure (dHCI) Solution and HPE StoreOnce refresh for your production and disaster recovery sites.

HPE Nimble dHCI Combines Benefits of both CI and HCI with the flexibility of converged and the simplicity of HCI. Grow compute and storage independently, extended across a hybrid cloud, with industry-leading data efficiency.

We will leverage your existing Commvault platform and updated your backup physical backup repository with HPE StoreOnce to help minimize training on new products and services.

Nth is certified and able to deliver directly, all of the services proposed in this solution and is certified and authorized by HPE to deliver said services directly or on behalf of HPE.

- B. Reasonableness of your fee to do the work.

Nth's fees comply with the City's request for a fixed price contract and will deliver the solution as requested in the RFP per the work plan and scope created herein. The fees are inclusive of the stated scope including planning, installation, design and analysis, P2V migrations, and knowledge transfer.

If the City would like to implement any of these services on their own Nth is open to discussion.

- C. Current resources to meet or better all task and timeline requirements herein.

Nth has the depth of resources needed to complete the tasks stated in the work plan. Work will be planned by the Nth Project Manager in collaboration with the City and the technical team to ensure efficiency and thoroughness. We will utilize multiple resources in parallel as much as we are able in order to compress the schedule and ensure a timely implementation.

The timeline and resources listed in Section II, D., pages 20 - 21 will be confirmed upon award during the initial kick-off and planning meeting.

- D. Additional resources that might be needed to meet or better all task and timeline requirements of this request.

Nth will utilize multiple resources in parallel as much as we are able in order to compress the schedule and ensure a timely implementation; however, City resource availability, especially during planning and P2V migrations, will be essential to complete timelines.

- E. How quickly could you provide additional, extra trained staff if requested by City for additional work beyond the original scope of services?

Depending on the type of resource needed to fulfill the additional scope, Nth may provide additional staff immediately. Standard lead time to assign a new resource in a non-emergency situation is three (3) days to two (2) weeks.

- F. How quickly can you begin providing services if awarded the contract?

The Project Kick-off Call can be scheduled within days (generally 2-5 days) of the contract being awarded and executed. Nth will begin providing the planning and execution of prerequisites at that time. Implementation services will begin as soon as equipment is available and prerequisites have been met.

- G. Details of any improvement or upgrades your firm has designed or implemented.

Nth has been working with the City to improve the City's IT infrastructure since 2007, and deployed the first c-class blade, EVA, and 3PAR for the City. Nth has subsequently provided VMware and CommVault professional services to the City. We have also been known to be available to the City for urgent, mission critical situations during non-business hours in years past.

For this project Nth recommends the following additional improvements (budget and convenience):

- Existing Commvault Media Agent servers will not be migrated to new storage, two new Media Agent VMs will be configured to back up to new StoreOnce appliances. Old Media agents will be retired within Commvault and retained until backup data has aged off and can be deleted.
- Recommend preserving QUIMBY as physical domain controller for best system resilience. A new domain controller can be created as a backup and secondary DNS server.

V. Demonstrated and Technical Experience

Please describe your company's:

- A. Demonstrated record of success on work previously performed.

Nth Generation has been serving the public sector of Southern California for nearly 30 years and are known in the marketplace for exceptional technical capabilities and Client service. We have numerous satisfied Clients in all segments of the public sector including local government, special districts, higher education, K-12 and healthcare. Nth employs local solution architects and engineers who are certified in the latest HCI, compute and storage technologies and provides pre- and post-sales consultative services. We have also been known to be available to the City of Moreno Valley for urgent, mission critical situations during non-business hours in years past. Nth has achieved the highest level of certification for HPE. In addition, our very own Nth Labs has your proposed solution of HPE Synergy, Nimble, ProLiant, StoreOnce and Commvault available for demonstration or interoperability testing purposes!

Finally, Nth has been named HPE Partner of the Year for Public Sector the past two consecutive years!

- B. Specific methodology to be employed on the project.

In approaching this project, Nth will follow PMI guidelines based on the project management body of knowledge (PMBOK). The project will be implemented by our project management team. The method of implementing a project is based on the project's scope stability as well as clarity the sponsors have for its vision or end result. The Nth Project Manager will systematically plan and execute against project milestones and forecast a stable timeline.

Having a project management team of experienced project managers will ensure the appropriate tools and methods are used to reduce risk while increasing efficiency. Five

processes will be taken to ensure the project is successful; project initiation, planning, executing, monitoring and controlling, and closing.

Project Initiation – This is an important process as it sets the tone as the project moves forward. We take the time to spend with our clients to learn their objectives and goals to provide the best solution which will ultimately meet the clients overall strategic road map. This will involve identifying key stakeholders, critical dates, and setting client expectations early in the project.

Planning – By evaluating the project and its level of overall complexity we will implement various management plans to ensure the scope, schedule, and cost remain on track. This will also cover the resource load plan, quality, and procurement plans. These plans will provide the project team with processes and policies of how to handle various pieces of the project.

Executing – The execution process will involve the actual directing of the project. This will include quality assurance, any procurement, and the engagement of stakeholders. We will also secure and build the team during this phase.

Monitoring – The Project Manager will monitor the project scope and expectations, identify and manage project risks, track and process change requests as well as status and overall project progress. The Project Manager will provide status updates along with documentation and meeting minutes as applicable. The Project Manager will act as the main point of contact and ensure timelines, dependencies and communication requirements are met within the project lifecycle.

Closing – The Project Manager ensure all contracts/SOWs have been fully executed and will create a final close out package including all meeting notes, project plan, risk register, and all other applicable documentation from the project. The closure document will be distributed for sign off from stakeholders.

The processes outlined above will serve as guidelines and allow our project management team to apply the best methods as needed. Nth will also use its organization process assets, such as the PMO, to leverage its ability to provide timely metrics and ensure communications are consistently open to capture stakeholder concerns and potential risk. As with any project, this project will be approached by providing the best project management practices based on our expertise and experience to deliver a successful solution.

VI. Work Plan:

Taking all circumstances, current conditions, and required preparations into consideration, describe in detail, your proposed work plan for delivering the services required by this RFP, including, but not limited to;

As stated in our cover letter, Nth is proposing an All-in-One *Disaggregated hyper-converged infrastructure* (dHCI) Solution and HPE StoreOnce refresh for your production and disaster recovery sites.

HPE dHCI solution with Nimble Storage

HPE Nimble dHCI Combines Benefits of both CI and HCI with the flexibility of converged and the simplicity of HCI. Grow compute and storage independently, extended across a hybrid cloud, with industry-leading data efficiency. It disaggregates compute and storage and integrates hyperconverged control to give enterprises simple infrastructure management on a flexible architecture. HPE Nimble Storage dHCI extends the hyperconverged experience to workloads with unpredictable growth, allowing independent scaling of compute and storage. This gives enterprises the flexibility of converged and the simplicity of HCI, delivering sub-milliseconds data response times for latency-sensitive databases and up to 21X data reduction – Accelerating time-to market on an architecture built for the unpredictable with a resilient design center for 99.9999% availability (With Nimble Storage) with no single point of failure and advanced data integrity to tolerate three simultaneous drive failures. Built with the world’s Best-Selling server, HPE ProLiant, and the Self-Managing. HPE dHCI automation software, advanced data services, and HPE InfoSight are all free. Although HCI is an aggregated architecture that combines storage, compute and virtualization in the same chassis and software stack; HPE Nimble dHCI consists of HPE ProLiant servers packaged with Nimble Storage arrays, with automation software to help with deployment. The simplicity of hyper-converged experience with the flexibility of converged. A converged infrastructure packages servers and storage separately, as in a traditional infrastructure, but does not combine them into a single chassis and integrated stack, as HCI does.

HPE StoreOnce Appliance with Catalyst technology

We will leverage your existing Commvault platform and updated your backup physical backup repository with HPE StoreOnce to help minimize training on new products and services. Ransomware cannot affect what it cannot see and the best backup solution is useless if ransomware can access your backup repositories. HPE StoreOnce is a purpose-built backup appliance (or virtual machine) that includes HPE StoreOnce Catalyst stores to effectively isolate critical data where attackers cannot have impact on it without resorting to direct physical interactions that ultimately would result in the destruction of some or all of the hardware itself. Even when physical destruction is achieved at a single location, whether from malware or a natural disaster, the more advanced implementation of HPE StoreOnce Federated Catalyst stores (distributed implementation) would effectively protect mission critical data by effectively isolating it from traditional lines of communication and command sets leveraged by ransomware attackers. The single most important feature of HPE StoreOnce Catalyst is its ability to completely isolate data from being tampered with unintentionally. HPE has hidden the Catalyst store from attackers in plain sight but behind an application programming interface (API) that both enhances and simplifies the process of backing up and deduplicating data while making it practically impossible for ransomware to attack it directly. HPE StoreOnce Catalyst was designed for use as a disk-based solution capable of leveraging deduplication, compression, encryption, and data isolation for backup and archiving processes. HPE StoreOnce Catalyst prevents ransomware from accessing or encrypting backup data stores, thereby ensuring data integrity. The HPE Cloud Bank Storage feature of HPE StoreOnce lets you leverage the economics, agility, and flexibility of the cloud for modernized data protection. You can natively, securely, and cost-effectively move backup data to the public, private, or hybrid cloud—providing long-term retention and reliable disaster recovery that is simple and efficient.

Below are the tasks, deliverables, assumptions and limitations to execute the proposed solution:

Tasks and Deliverables

- Kick-off and Pre-installation review

- PM to coordinate kick-off call
- Installation Planning
- Install new systems in racks, two locations
- Network Startup and Configuration Services
 - Gather existing configuration of existing Catalyst 6509.
 - All equipment will be sent to Nth Generation in Rancho Bernardo for staging.
 - Configure the C9500 switches in a Stack-Wise Virtual via 40 or 100Gbps DAC cables. (these are not on the BOM so they will need to be acquired).
 - The 2 C9500's will be configured as the Core/Aggregation switches utilizing existing VLAN's, IP Addressing, OSPF routing, and all VLAN trunking to replace the C6509.
 - The 4 C9300's will be stacked via backplane ports in a physical stack. These will be Access Ports replacing the C6509 Access Ports.
 - The C9300 stack will be connected to the C9500 Core Switches via 10Gbps DAC cables. Multiple ports can be used in an Etherchannel/LAG for increased available bandwidth (these cables are not on the BOM so they will need to be acquired).
 - Equipment will be brought on site and installed in the rack.
 - Mellanox switches will be brought up into production since it is not service impacting.
 - The new Cisco Core can be brought up all at once or it can be done in phases in non-production hours. 3 phases would look like the below or modified to fit customer requirements.
 - Migrate all routing to the new C9500, connect C6509 and keep it as an Access Switch. Migrate 1/3 of the less critical departments/buildings over.
 - Migrate another 1/3 of the departments/buildings over and all access ports on C6509
 - Migrate final 1/3 departments/buildings over and decommission the C6509.
 - Document all Configurations

Networking Specific Customer requirements:

- Available rack space for all equipment
- Power available for all equipment
- All Copper and Fiber patch cords.
- Ability to access all departments to test
- Customer is required to provide 4x 10G SFP+ SR connections in both datacenters on existing switches
- DAC cables for the new Cisco C9500 switches in a Stack-Wise Virtual via 40 or 100Gbps and uplink to the C9300 stack are not on the BOM so they will need to be acquired by the City.

- dHCI startup services 2 locations
 - Review your installation and planning the key steps with you using our pre-installation workbook to be completed by Client prior to installation. Please note this workbook will be used as the as-built documentation
 - Review network IP and SAN connectivity as well storage volume design for up to 8 volumes on up to hosts
 - Rack and cable 2 HPE Nimble Storage dHCI array and up to six storage shelves (per array) purchased on the same order
 - Rack and cable HPE Proliant dHCI configured servers purchased on the same order
 - Upgrade the arrays, servers, network switches to latest supported versions
 - Deploy servers into the dHCI configuration
 - Configure the array to communicate on a single VLAN IP network and/or SAN as defined in pre-installation workbook
 - Configure HPE Infosight for basic management, monitoring and reporting with remote HPE Nimble storage support team if needed
 - Perform basic volume overview with creation of up to 8 test volumes per array
 - Demonstrate configuration replication between Nimble arrays and also assist in initial replication schedule on a new non-production array
 - Verify if the array's HA capabilities by disconnecting the cable in the active controller to test failover capability
 - Test outbound path by pinging your gateway, DNS server, and hpe.com if possible
 - Verify HPE Infosight support account is created, configured and functional.
 - Present an overview of GUI and VMware vCenter Plug-in
 - Demonstrate how to monitor array and engaging HPE support through Infosight
 - Perform Knowledge transfer around creating volumes and snapshot schedules, monitoring, engaging HPE support and HPE Nimble Storage best practices including OS upgrades and adding users.
 - Deliver as-built documentation
- Migrations and Backups
 - Operation readiness review
 - vCenter remediation
 - vSphere virtual network topology review and design
 - P2V detailed planning and server identification
 - virtualized DMZ network design
 - Commvault design review

- Gap analysis & remediation
 - Set up required DvSwitches including virtual DMZ
 - Set up Shared-nothing vMotion from production cluster
 - Demonstrate VM migration
 - Verify Shared nothing vMotion from DMZ host and demonstrate
 - Deploy two CommVault MA VMs as new installs
 - Start up StoreOnce and configure Catalyst to Mas (2 locations)
 - Configure CVLT to use new Media Agent(s) and StoreOnce appliances
 - P2V non-Commvault hosts
 - Wait for data on physical Media Agents to age off
 - Remove old MAs from CVLT and retire hardware
 - Create As-Built documentation
- Project review, Knowledge review, Best Practices
 - Overall Project Management

Service eligibility and Client responsibilities

- Client will be responsible for all data backup and restore operations
- Client to provide current network design, topology diagram and all network IP address
- Client is responsible providing all required data network cables and an appropriate cabling map that identifies each pre-labeled cable for each port
- Ensure that all service prerequisites as defined in the pre-installation workbook has been met prior to installation.
- Provide a suitable Client-supplied rack that the dHCI can be installed into if not included in the hardware order. Recommend 600mm X 1200mm rack for both sites. If rack is shorter than 1200mm, Nimble storage requires 35 inches full width clearance; as measured from the front rail. This may require repositioning side mounted PDUs in 1075mm deep rack.
- Perform final interconnection of any configured switches onto the Client's existing network
- Be responsible for cable runs under the floorboards or in above rack mounted cable trays
- If the Client wishes to have volumes as part of the full HPE Nimble Storage dHCI Installation and Startup Service created by the HPE authorized installation specialist (up to 8), then the design must be supplied; customized storage volume design is not part of this service.
- Eligible hosts that can be connected to the HPE Nimble Storage dHCI array environment is limited to VMware vSphere® only as defined in the product QuickSpecs at:
<https://h20195.www2.hpe.com/V2/getdocument.aspx?docname=a00067739enw>

- A suitable physical operating environment must be provided, including implementation of any environmental recommendations made by Hewlett Packard Enterprise prior to installation.
- An Ethernet/IP infrastructure must be installed, configured, and operating normally as defined in the pre-installation workbook and/or the pre-deployment checklist prior to installation.
- The network topology must be supported by the HPE Nimble Storage dHCI from a design and performance perspective as defined in the product manual.
- Where hosts at remote sites are being configured, gateways must be operating between the locations at time of installation.

Service Limitation

Activities such as, but not limited to, the following are excluded from this service:

- Any services not clearly specified in this document.
- The Nth Nimble Storage dHCI Installation and Startup Service is only available for new “Base” dHCI configuration and cannot be ordered and used for configuration using Client pre-owned equipment or “Expansion” purchased through Hewlett Packard Enterprise.
- Application integration or integration of third-party products or peripherals not included with the system.
- Backup, recovery, and support of the operating system, other software, and data.
- Installation and configuration of any third-party storage hardware, servers, switches, routers, network gateways, and so on.
- Laying of any cable beyond the required internal array interconnect cables and external cables to interconnect the solution components as needed and indicated by the deliverables in Table 1.
- Installation, configuration, or provisioning of any external SANs the Client is using for data storage.
- Analysis and/or reconfiguration of the existing environment to improve performance or robustness.
- Operational testing of applications or additional tests requested or required by the Client.
- Planning, design, or implementation of the Client’s overall SAN Fabric, or IP Ethernet architecture.
- Implementation of hardware and software products other than those specified in this document, including the Client’s pre-owned servers, applications, database, storage, SAN, and network not attached to the dHCI solution.
- Nth’s ability to deliver this service is dependent upon the Client’s full and timely cooperation with HPE, as well as the accuracy and completeness of any information and data the Client provides to HPE.
- Services that, in the opinion of Hewlett Packard Enterprise, are required due to unauthorized attempts by third-party personnel to install, repair, maintain, or modify hardware, firmware, or software.

- Any upgrade of the HPE NimbleOS is conditioned upon the Client having a valid HPE support contract and HPE NimbleOS release appropriate for the upgrade.
- Activities are delivered during HPE local standard business hours unless additional services have been purchased or special arrangements have been made.

Assumptions

- City will provide required LC-LC cables for 10GbE network connections
- Customer will provide 4x 10G SFP+ SR connections in both datacenters on existing switches
- DAC cables for the new Cisco C9500 switches in a Stack-Wise Virtual via 40 or 100Gbps and uplink to the C9300 stack are not on the BOM so they will need to be acquired by the City.

Recommendations

- Existing Commvault Media Agent servers will not be migrated to new storage, two new Media Agent VMs will be configured to back up to new StoreOnce appliances. Old Media agents will be retired within Commvault and retained until backup data has aged off and can be deleted.
- Recommend preserving QUIMBY as physical domain controller for best system resilience. A new domain controller can be created as a backup and secondary DNS server.

Limitations

- Project schedule assumes a 4 day (M-Thurs) work week for on-site efforts
- After hours work shall be scheduled 5 business days in advance

A. How you will schedule professional and staff to ensure milestones and deadlines are met?

Nth will assign a Project Manager that will collaborate with City stakeholders to ensure that milestones and deadlines are met. We have a depth of resources to complete the tasks stated in the work plan. Work will be planned by the Nth Project Manager in coordination with the City and the technical team to ensure efficiency and thoroughness. We will utilize multiple resources in parallel as much as we are able in order to compress the schedule and ensure a timely implementation.

The timeline and resources listed in Section II, D., Page 20 – 21 will be confirmed upon award during the initial kick-off and planning meeting.

While Nth will utilize multiple resources in parallel as much as we are able in order to compress the schedule and ensure a timely implementation; City resource availability, especially during planning and P2V migrations, will be essential to complete timelines.

B. Provide required response time to the urgent service requests.

Manufacturer's warranty and service agreement are included in Nth's proposal. Nth resources are available during the implementation and can act as a point of escalation as required.

C. How you will make up for work-hours lost (and resulting backlog that may occur) due to various unforeseen situations that may prohibit work on a specific day.


Nth will utilize multiple resources in parallel as much as we are able in order to compress the schedule and ensure a timely implementation. This method can also be used should unforeseen situations prohibit work. Any work that can be accomplished remotely will also be considered in order to increase the amount of actual work time. Off hours and weekend work may be required for the P2V migrations when work is prohibited on specific week days.

D. Provide any other relevant information that you believe would benefit City for the requested services.

Nth has a proven track record with the City and will provide services that will make the transition to this new solution easier and more manageable by City Staff.

Nth appreciates the opportunity to continue to serve the City.

Submitted by:

Company Name **Nth Generation Computing, Inc.**
Contact Name **Joyce Russell**
Title **EVP/CFO**
Signature 
Email **bjds@nth.com**
Phone **858-451-2383**
Date **09/18/2020**

G. Section 7: Required Forms and Samples

Requirement:

1. Special Provisions Form*
2. Client Reference List*
3. Non-Collusion Affidavit*

*Note these forms are provided by City in the submittal forms section.

Nth's Response:

Please see the executed required forms on the following pages.

RFP #2021-020 Professional Services for Server and Storage Replacement August 2020

Attachment B: Special Provisions

All items below apply to this bid proposal:

Hold Harmless and Indemnification: The successful bidder hereby agrees to indemnify, defend, and hold harmless City (including its officials, officers, agents, employees, and representatives) from and against any and all claims of any kind or nature presented against City arising out of vendor's (including vendor's employees, representatives, and subcontractors) performance under this agreement, excepting only such claims, costs or liability which may arise out of the sole negligence or willful misconduct of City.

Insurance Provisions: Unless otherwise stated in the RFP specifications, the following insurance requirements apply:

1. Comprehensive General Liability Insurance: (include products liability) \$1,000,000 per occurrence.
2. Auto Liability Insurance: \$1,000,000 per occurrence, combined single limit (CSL).
3. Workers' Compensation Insurance: as required by State statutes.
4. Employer's Liability Insurance: \$1,000,000 per accident; \$1,000,000 policy limit for disease.
5. All policies of insurance must provide for a minimum of thirty (30) days written notice of any change or cancellation of the policy.
6. Insurance policies to be in a form ad written through companies acceptable to City; and must include those endorsements which are necessary to extend coverage which is appropriate to the nature of the agreement.

Affirmative Action: In support of Affirmative Action, City requires all suppliers to comply with Title VII of the Civil Right Act of 1964, as amended, the Civil Rights Act of 1992, and all federal, state, and municipal laws and regulations pertaining thereto. In addition, successful bidders must certify prior to award of contracts in excess of \$50,000 that they have a written affirmative action plan in compliance with the above laws and regulations, and such plan may be reviewed by City. Said Certification may be made by signing below:

Certified to above - FIRM: Nth Generation Computing, Inc.
 SIGNATURE: Joyce F Russell
 PRINT NAME: Joyce Russell
 TITLE: EVP/CFO

Exceptions: If your company is taking exception to any of the specifications, terms or conditions (including insurance, indemnification and/or proposed contract language) stated in this Request for Proposal, please indicate below and describe details: (check any that apply).

- No exceptions taken
- Exception taken to the scope of work or specifications
- Exception taken to indemnification and insurance requirements
- Exception to proposed contract language
- Other

RFP #2021-020 Professional Services for Server and Storage Replacement August 2020

Please explain any of the checked items:

Note: Taking exception to City's requirements without approval of City prior to submission of your proposal may be cause for rejection of the proposal.

PROPOSING FIRM: Nth Generation Computing, Inc. DATE: 09/18/2020

BUSINESS ADDRESS: 17055 Camino San Bernardo, San Diego, CA 92127

SIGNATURE OF REPRESENTATIVE: 

BY: Joyce Russell TITLE: EVP/CFO (Corporation -Authorized Officer)

INSTRUCTION REGARDING SIGNATURE: If bidder is an individual, state "Sole Owner" after signature. If bidder is a partnership, signature must be by a general partner, so stated after "Title". Names of all other partners and their business addresses must be shown below. If bidder is a corporation, signature must be by an authorized officer, so stated after "Title", and the names of the President and Secretary and their business addresses must be shown below:

<u>Janis Baldwin, CEO</u>	<u>17055 Camino San Bernardo, San Diego, CA 92127</u>
<u>Richard Baldwin, CSO/Secretary</u>	<u>17055 Camino San Bernardo, San Diego, CA 92127</u>
<u> </u>	<u> </u>
<u> </u>	<u> </u>

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RFP #2021-020 Professional Services for Server and Storage Replacement August 2020

Attachment C: Client References

Nth Generation Computing, Inc.

(Bidder's Company Name)

1. Client's Company Name:	[] City of Los Angeles Sanitation
Client Address:	1149 S. Broadway St., 9th Floor, Los Angeles, CA 90015
Contact's Name:	Samra Varesanovic
Contact's Title:	Systems Programmer II
Contact's Telephone & FAX:	Phone: 310-648-5764 FAX: N/A
Contact's Email:	samra.varesanovic@lacity.org
Scope of Services/Products Provided:	[] HPE 3PAR, MSA, BladeSystem, ProLiant
Project Completion Date & Value:	2012-2017. Approximately \$1 - 1.3M.
2. Client's Company Name:	[] Orange County Transportation Authority
Client Address:	550 South Main Street, Orange, CA 92863
Contact's Name:	Michael Beerer
Contact's Title:	Sr. Section Manager
Contact's Telephone & FAX:	Phone: 714-560-5352 FAX: N/A
Contact's Email:	mbeerer@octa.net
Scope of Services/Products Provided:	[] HPE 3PAR, Nimble, ProLiant
Project Completion Date & Value:	2018-2020. Approximately \$750K - 1M.
3. Client's Company Name:	[] Orange County Sanitation District
Client Address:	10844 Ellis Avenue, Fountain Valley, CA 92708
Contact's Name:	Loc Trang
Contact's Title:	IT Analyst
Contact's Telephone & FAX:	Phone: 714-593-7031 FAX: N/A
Contact's Email:	Ltrang@ocsd.com
Scope of Services/Products Provided:	[] HPE Nimble, ProLiant, BladeSystem, StoreOnce
Project Completion Date & Value:	2016 - 2020. Approximately \$2.5 - 3M.
4. Client's Company Name:	[] County of Riverside
Client Address:	3450 14th Street, 4th Floor, Riverside, CA 92501
Contact's Name:	Gary Fortune
Contact's Title:	IT Manager
Contact's Telephone & FAX:	951-358-3859
Contact's Email:	GFortune@RIVCO.ORG
Scope of Services/Products Provided:	[] HPE Nimble, Synergy, and Install Services
Project Completion Date & Value:	Feb. 2020 Approximately \$900k

Duplicate this form as necessary to complete list.

RFP #2021-020 Professional Services for Server and Storage Replacement August 2020

Attachment D: Non-Collusion Affidavit

Note: To be executed by Proposer and submitted with proposal.

State of California
(the State of the place of business)

County of San Diego
(the County of the place of business)

Joyce Russell, being first duly sworn, deposes and
(name of the person signing this form)
says that he/she is EVP/CFO of
(title of the person signing this form)
Nth Generation Computing, Inc., the party making the foregoing bid
(name of bidding company)

that such bid is not made in the interest of or on the behalf of any undisclosed person, partnership, company, association, organization or corporation; that such bid is genuine and not collusive or sham; that said bidder has not directly or indirectly induced or solicited any other bidder to put in a false or sham bid and has not directly or indirectly colluded, conspired, connived, or agreed with any bidder or anyone else to put in a sham bid, or that anyone shall refrain from bidding; that said bidder has not in any manner directly or indirectly sought by agreement, communication, or conference with anyone to fix the bid price of said bidder or of any other bidder or to fix any overhead profit, or cost element of such bid price, or of that of any other bidder, or to secure any advantage against the public body awarding the contract of anyone interested in the proposed contract; that all statements contained in such bid are true, and further, that said bidder has not directly or indirectly submitted his bid price or any breakdown thereof, or the contents thereof, or divulged information or data relative thereto, or paid and will not pay any fee in connection therewith, to any corporation, partnership, company, association, organization, bid depository, or to any member or agent thereof, or to any other individual except to any person or persons as have a partnership or other financial interest with said bidder in the general business.

By: Joyce Russell
(signature)
Printed Name: Joyce Russell
(name of the person signing this form)
Title: EVP/CFO
(title of the person signing this form)

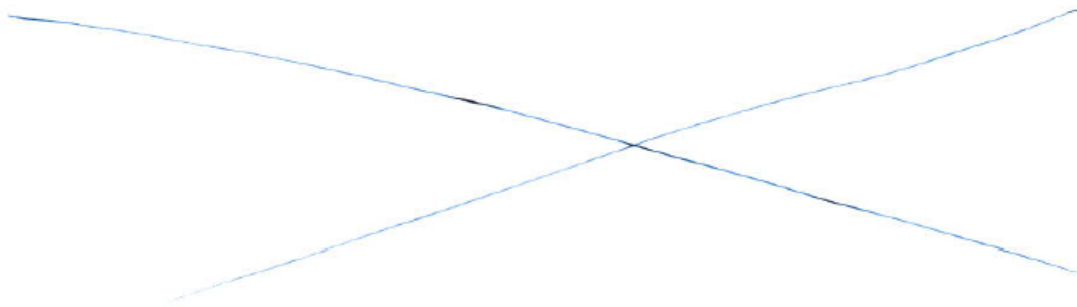
Notary is required for this bid.
see Attached

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

CALIFORNIA JURAT WITH AFFIANT STATEMENT

GOVERNMENT CODE § 8202

- See Attached Document (Notary to cross out lines 1-6 below)
- See Statement Below (Lines 1-6 to be completed only by document signer[s], *not* Notary)



Signature of Document Signer No. 1 *Signature of Document Signer No. 2 (if any)*

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

State of California
 County of San Diego

Subscribed and sworn to (or affirmed) before me
 on this 14th day of September, 2020,
 by *Date* *Month* *Year*

(1) Joyce Russell
 (and (2) _____),
Name(s) of Signer(s)



proved to me on the basis of satisfactory evidence
 to be the person(s) who appeared before me.

Signature *[Signature]*
Signature of Notary Public

Seal
 Place Notary Seal Above

OPTIONAL

Though this section is optional, completing this information can deter alteration of the document or fraudulent reattachment of this form to an unintended document.

Description of Attached Document RFP 2021-020
 Title or Type of Document: Don Collusion Affidavit Document Date: 9/14/2020
 Number of Pages: 1 Signer(s) Other Than Named Above: _____

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

H. Section 8: Work Samples

Requirement:

1. Samples of work, queries, reports, and forms**
2. Sample of ongoing support and services agreements**

** Note that these documents will not be returned to proposer.

1.

Nth's Response:

1. Nth's proposal is based upon information and data provided by the City therefore we are not providing samples of those queries.

Please see samples of work, reports, and forms on the following pages. These are examples of project related documents, not samples of solution system reports.

nimblestorage Tab - Physical Readiness Version 3.6 LX

Goal
The goal of this section is to ensure all of the *physical resources* (space, power, cooling, switches, cables) needed for the installation are available.

Next Steps
* Fill in/answer all white boxes under red, numbered circles 1-8 and return the form to the Nimble Coordinator

Key	Color
Automatically Calculated	(Green)
Nimble Input	(Blue)
Customer Input	(White/Grey)
Use for later documentation	(Orange)

Customer Info

Sales Order # _____
 Company Name _____
 Contact Name _____
 Location _____
 Project Name (opt'l) _____

Project Info 1

General Use Case _____
 Target Date to start Data Movement _____
 Target Date for General Use _____
 Other Partners(Need to Coordinate With) _____

Array Order Info

Array Model _____
 Sales Order Info _____
 Array Networking Type 4G
 # of Additional Shelves 3
 Data Type iSCSI
 Notes: Ex: AF5000
 Count: AFS2-...*

Networking Consideration 2

Are you using VLAN Tagging? Select Option
 Are you using Jumbo Frames? Select Option
 Will be using Nimble Replication? Select Option
 Using separate data and mgmt networks? Select Topology

HPE Array Order Info

	Sales Order Info	Description
HPE Nimble SKU 1		
HPE Nimble SKU 2		
HPE Nimble SKU 3		
HPE Nimble SKU 4		
HPE Nimble SKU 5		

Rackspace and Power 3a

# of Nimble Devices	4	Ready?	Select Option	Notes
Rack Units(U) Needed	16	Ready?	Select Option	
Power Sockets Needed	8	Ready?	Select Option	

Select the Power Cable Type Needed: Hooded(C13/C14) 3b
 Is the space together in 1 rack? Select Option

Hosts (Used for Testing at Install Time) 4

Note: Information on how to get Nimble Host Software is on the "Prepping Host" Tab

# of Systems	System Hypervisor or OS	Make / Model	Already Running	Will You be Using Nimble Connection Mgr / Host	Needs a Maintenance Window to Work on This Host?	Notes
1			Select Option	Select Option	Select Option	
2			Select Option	Select Option	Select Option	
3			Select Option	Select Option	Select Option	

Network/FC Cables 5

Note: Cable specs are listed under the "Cables" tab on your Quick Reference Packet. Customer MUST supply the cables.

Cable Type	Maximum Needed	Minimum Needed	# You Will Use	Ready?	Notes
Ethernet Twisted Pair	12	4		Ready? Select Option	
Ethernet SFP+ / Twin-Ax	0	0		Ready? Select Option	
Fibre Channel (LC)	0	0		Ready? Select Option	

Network (IP) Switches 6

Switch Brand/Model	New or In Production?	# of Ports Ready (SFPs? / Licensed)?	NTUs?	VLANs Needed? Set?	Notes
	Select Option				
	Select Option				

Vmware vSwitches 7

Are you using Vmware?	Are vSwitches Already Set?	Already using iSCSI?
Select Option	Select Option	Select Option


FC Switches 8

Switch Brand/Model	New or In Production?	# of Ports Ready (SFPs? / Licensed)?	Notes
	Select Option		
	Select Option		

Continue

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

Figure 1 - Nimble Pre-installation Checklist


Tab - IP Networks

Goal
The goal of this section is to ensure all of the configuration information (e.g., IP addresses) needed to set up the management network.

Next Steps

- Fill in/answer all white boxes under red, numbered circles #
- Be sure to read the comments (hover over the red triangle) when filling out a line
- Note: steps that are blacked out do not apply to your configuration

Key	Color
Automatically Calculated	(Green)
Nimble Input	(Blue)
Customer Input	(White/Grey)
Use for later documentation	(Orange)

Array Grouping

Adding this Array to Existing Nimble Group? 1

If "Yes", Group's Name?

(Optional) If "Yes", Group's OS Version?

Management IPs

Nimble Array Host Name 2a

Nimble Group "Host" Name

Management IP These (*) IP Addresses need to be routable

Diagnostics IP (Controller A)

Diagnostics IP (Controller B)

Subnet Mask

Default Gateway

Ethernet Connector Type for Mgmt Network 2b

4

Domain Admin Planning 3a

DNS	Time	Mail Alerts
DNS Domain Name		
DNS Servers (specify up to 5)		
NTP (Time Server)		
Time Zone		
SMTP Server		
Alert Emails Sent *AS* This User		
Alert Emails Sent *TO* Users (>1 OK)		

Using an HTTP Proxy Server? 3b

HTTP proxy host name and IP:

HTTP proxy server port:

Proxy server user name:

Proxy server password:

Data Subnets (iSCSI/ Replication)

Subnet Name	Netmask	Interface IP	Discovery IP	VLAN ID (optional)	MTU	Notes (optional)

Figure 2 - Nimble Pre-Installation Checklist pg. 2

Status Reporting Sample:



PROJECT STATUS REPORT

City of Moreno Valley – Project # Project Name

Date

PROJECT OVERVIEW

0%	01/01/0001
Project Completion	Project Closure Date

STATUS UPDATE [PREVIOUS UPDATE: DATE]

Date	Resource	Work Performed / Event

PROJECT TIMELINE

Milestone Description	Resource[s]	Date
Launch		
Send Intro Letter to Client		
Internal Meeting		
Conduct Client KO Meeting		
Discovery & Design		
Implementation		
Onsite Installation Phase		

Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

Completion Phase		
------------------	--	--

ACTION ITEMS

Date Due	Action	Owner	Status	Notes

MEETING RECAPS & MINUTES

Meeting Name				
Date	Meeting Time	Meeting Location	Meeting Minutes	Meeting Minutes Total

Nth Generation		Customer		Third Party
Attendees	Attendees	Attendees	Attendees	Attendees
Attendees	Attendees	Attendees	Attendees	Attendees

Discussion	
Agenda Topic 1	
Discussion	
Conclusions	
Agenda Topic 1	
Discussion	

Conclusions		
Agenda Topic 1		
Discussion		
Conclusions		
Action Items	Person Responsible	Deadline
Action item 1		
Action item 2		

IMPORTANT DOCUMENTS

Document	Document Purpose	Link	Version
Network Requirement Doc			

PROJECT TEAM & CONTACT

Name	Project Role	Office Phone	Mobile Phone	Email

PROJECT GOAL

- Input from SOW

PROJECT OBJECTIVES & DELIVERABLES

- Input from SOW

CLIENT RESPONSIBILITIES

- Input from SOW

RISK MANAGEMENT

Risk Score: Low 0-25, Moderate 26-70, High 71-90, Critical 91-100

Risk		Qualitative Rating		
Description	Date Opened	Probability	Impact	Risk Score

HISTORICAL TIMELINE

Date	Resource	Work Performed / Event

2. The following pages include the Data Sheet for the services agreement for the proposed solution.



Data sheet



HPE FOUNDATION CARE SERVICE

Support Services

SERVICE OVERVIEW

HPE Foundation Care Service is composed of comprehensive hardware and software services aimed to help increase the availability of your IT infrastructure. Hewlett Packard Enterprise (HPE) technical resources provide support and work with your IT team to help you resolve hardware and software problems with HPE and selected third-party products.

For hardware products covered by HPE Foundation Care, the service includes remote diagnosis and support, as well as on-site hardware repair if it is required to resolve an issue. For eligible HPE hardware products, this service may also include Basic Software Support and Collaborative Call Management for selected non-HPE software. Contact HPE for more information and determination regarding which eligible software products may be included as part of your hardware product coverage. For software products covered by HPE Foundation Care, HPE provides remote technical support and access to software updates and patches. Updates for selected HPE-supported third-party software products are included, as they are made available from the original software manufacturer.

In addition, HPE Foundation Care provides electronic access to related product and support information, enabling any member of your IT staff to locate this commercially available essential information. For third-party products, access is subject to availability of information from the original manufacturer.

You can choose from a set of reactive support levels to meet your business and operational needs.

HPE FOUNDATION CARE SERVICE-LEVEL OPTIONS

The HPE Foundation Care options noted in the following are product dependent. HPE will provide the hardware support features for covered hardware products and the software support features for covered software products.

Hardware support coverage windows and response times will apply to covered hardware products, and software support coverage windows and response times will apply to covered software products.

All coverage windows are subject to local availability. Product eligibility may vary. Contact a local HPE sales office for detailed information on service availability and product eligibility.

Regardless of your coverage window, incidents with covered hardware or software can be reported to HPE via telephone or web portal, as locally available, or as an automated equipment reporting event via the HPE electronic remote support solution 24 hours a day, 7 days a week.

For products covered by HPE Foundation Care, HPE offers three distinct service levels:

- HPE Foundation Care NBD Service
- HPE Foundation Care 24x7 Service
- HPE Foundation Care CTR Service

TABLE 1. Service-level options

Service-level options	Coverage window	Hardware response time ¹	Software response time
Next business day (NBD)	Standard business hours, standard business days: Service is available 9 hours per day between 8:00 a.m. and 5:00 p.m. local time, Monday through Friday excluding HPE holidays.	Next-business-day on-site response for covered hardware. Cases received outside the coverage window will be acknowledged the next coverage day and serviced within the following coverage day.	
24x7	24x7. Service is available 24 hours per day, 7 days per week including HPE holidays.	4-hour on-site response for covered hardware.	
Call to repair (CTR)	24x7. Service is available 24 hours per day, 7 days per week including HPE holidays.	<p>6-hour call-to-repair time: For critical incidents (Severity 1), HPE will return the covered hardware to operating condition within six hours.²</p> <p>Availability of call-to-repair times is dependent on the proximity of your site to an HPE-designated support hub, as described in the Travel zones section. For noncritical incidents (Severity 2 or 3), or at your request, HPE will work with you to schedule an agreed-upon time for the remedial action to begin, and the call-to-repair time will then start at that time. Incident severity levels are defined in the General provisions section.</p>	Once a software case is logged, an HPE Solution Center engineer will respond to the case within two hours during the coverage window.

¹ On-site response time for hardware issues begins when the initial case has been received and acknowledged by HPE. The on-site response time ends when the HPE authorized representative arrives at your site, or when the reported event is closed with the explanation that HPE has determined that no on-site intervention is required.

² Call-to-repair time for hardware issues begins when the initial case has been received and acknowledged by HPE or at the start time for work scheduled in agreement with you, as specified in the [Hardware call-to-repair](#) section. Call-to-repair time ends with HPE's determination that the hardware is repaired, or when the incident is closed with the explanation that HPE has determined that no on-site intervention is required. For HPE ConvergedSystem, call-to-repair time begins when the initial case has been received and acknowledged by HPE as a hardware problem.

TRAVEL ZONES

All hardware on-site response times apply only to sites located within 160 km (100 miles) of an HPE designated support hub. Travel to sites located within 320 km (200 miles) of an HPE designated support hub is provided at no additional charge. If the site is located more than 320 km (200 miles) from the HPE designated support hub, there will be an additional travel charge.

Travel zones and charges, if applicable, may vary in some geographic locations.

Response times to sites located more than 160 km (100 miles) from an HPE designated support hub will have modified response times for extended travel, as shown in the following table.

TABLE 2. Travel zones

Distance from HPE-designated support hub	4-hour hardware on-site response time	Next-day hardware on-site response time
0–80 km (0–50 miles)	4 hours	Next coverage day
81–160 km (51–100 miles)	4 hours	Next coverage day
161–320 km (101–200 miles)	8 hours	1 additional coverage day
321–480 km (201–300 miles)	Established at time of order and subject to availability	2 additional coverage days
More than 480+ km (300 miles)	Established at time of order and subject to availability	Established at time of order and subject to availability



A call-to-repair time commitment is available for sites located within 80 km (50 miles) of an HPE designated support hub.

For sites that are located within 81 to 160 km (51 to 100 miles) of an HPE designated support hub, an adjusted hardware call-to-repair time commitment applies, as shown in the following table.

The hardware call-to-repair time commitment is not available for sites located more than 160 km (100 miles) from an HPE designated support hub.

TABLE 3. CTR travel zones

Distance from HPE-designated support hub	6-hour hardware call-to-repair time
0–80 km (0–50 miles)	6 hours
81–160 km (51–100 miles)	8 hours
More than 160+ km (100 miles)	Not available

GENERAL PROVISIONS

HPE will acknowledge a case by logging a case, communicating the case ID to the Customer, and confirming the Customer's incident severity and time requirements for commencement of remedial action.

Hardware support on-site response time and call-to-repair time commitment, as well as software support remote response time, may differ depending on incident severity. The Customer determines the incident severity level.

Incident severity levels are defined as follows:

- Severity 1—Critical or Severe Business Impact: For example, production environment down; production system or production application down/at severe risk; data corruption/loss or risk; business severely affected; safety and security issues
- Severity 2—Limited Business Impact or Business Risk: For example, critical nonproduction environment or system issue; production environment available but some functions limited or degraded; severely restricted use
- Severity 3—No Business Impact: For example, nonproduction system (e.g., test system) or noncritical issue; work around in place, installations



SPECIFICATIONS

TABLE 4. Service features

Feature	Delivery specifications
Hardware support	
Remote problem diagnosis and support	<p>Once the Customer has placed and HPE has acknowledged the receipt of a case as described in the General provisions section, HPE will work during the coverage window to isolate the hardware incident and to remotely troubleshoot, remedy, and resolve the incident with the Customer. Prior to any on-site assistance, HPE may initiate and perform remote diagnostics using electronic remote support solution to access covered products, or HPE may use other means available to facilitate remote incident resolution.</p> <p>HPE will provide telephone assistance during the service coverage window for installation of Customer-installable firmware or parts classified by HPE as Customer Self Repair (CSR) parts.</p>
On-site hardware support	<p>For hardware incidents that cannot, in HPE's judgment, be resolved remotely, an HPE authorized representative will provide on-site technical support on covered hardware products to return them to operating condition. Once an HPE authorized representative arrives at your site, the representative will continue to deliver the service, either on-site or remotely, at the discretion of HPE, until the products are repaired. Work may be temporarily suspended if parts or additional resources are required, but work will resume when they become available.</p> <p>Repair is considered complete upon HPE verification that the hardware malfunction has been corrected or that the hardware has been replaced.</p> <p>In addition, at the time of on-site technical support delivery, HPE may:</p> <ul style="list-style-type: none"> • Install available engineering improvements for covered hardware products to help you ensure proper operation of the hardware products and maintain compatibility with HPE-supplied hardware replacement parts • Install available firmware updates defined by HPE as noncustomer installable for covered hardware products, that, in the opinion of HPE, are required to return the covered product to operating condition or to maintain supportability by HPE <p>At your request, HPE will install during coverage hours critical firmware updates defined by HPE as noncustomer installable for covered hardware products. Critical firmware updates are firmware updates recommended by the HPE product division for immediate installation.</p>
Replacement parts and materials	<p>HPE will provide HPE-supported replacement parts and materials necessary to maintain the covered hardware product in operating condition, including parts and materials for available engineering improvements required by HPE to assure supportability of the product.</p>
Firmware updates for selected products	<p>As part of this service, you will have the right to download, install, and use firmware updates for hardware products covered by this service, subject to all applicable license restrictions in HPE's current standard sales terms.</p> <p>For Customers with licenses to firmware-based software products (features implemented in firmware activated by the purchase of a separate software license product), you must also have, if available, active HPE Foundation Care support coverage or an active HPE Software Support agreement on the firmware-based software products to receive, download, install, and use related firmware updates.</p> <p>HPE will provide, install, or assist you with installation of firmware updates as previously described in this document only if you have the license to use the related software updates for each system, socket, processor, processor core, or end-user software license as allowed by the original HPE or original manufacturer software license terms.</p>
Basic Software Support and Collaborative Call Management for non-HPE software on eligible HPE hardware products	<p>Basic Software Support provides 24 hours per day, 7 days per week phone support for selected independent software vendor (ISV) software that resides on hardware covered by HPE Foundation Care. For Basic Software Support, HPE will investigate and attempt to resolve problems by asking you to apply fixes that have been made available or known to HPE. In some cases, support may be limited to communication of a known fix available through the installation of a software update or patch, and you will be directed to available sources for the applicable updates or patches because access to the known fix requires additional service contracts with the respective software vendor. If the case is still not resolved, then Collaborative Call Management can be initiated at your request.</p> <p>If HPE determines that a problem is caused by a selected ISV product and the problem is not resolved by you applying known available fixes, HPE will, at your request, initiate Collaborative Call Management with the ISV. Collaborative Call Management can be provided only in cases where you have appropriate active support agreements in place with selected ISVs and you have taken the steps necessary to ensure that HPE can submit cases on your behalf for the limited purpose of placing a support case with the vendor. HPE will engage the ISV and provide information about your issue, as obtained during the Basic Software Support service case. Once the case has transitioned to the ISV, it is then the responsibility of the ISV to resolve your issue, which will be subject to the support levels of the agreement between you and that ISV. Once the ISV is engaged, HPE will close the HPE case, but you or ISV can resume the service issue with HPE if needed by referencing the original case identification number.</p> <p>Basic Software Support and Collaborative Call Management apply only to select ISV software when that software is not under HPE support. When ISV software is covered by HPE Software Support, support is provided as described in the Software support section of this table. Note: For a list of the non-HPE software products eligible for Basic Software Support and Collaborative Call Management, please refer to hpe.com/services/collaborativesupport.</p>



TABLE 4. Service features (continued)

Feature	Delivery specifications
Periodic maintenance	<p>For more information on eligible products that will receive periodic maintenance services as part of this service, please contact your HPE sales representative.</p> <p>If periodic maintenance is included, an HPE authorized representative will contact you, to arrange for the periodic maintenance to be performed at a mutually agreed-upon time, during local HPE standard business hours excluding HPE holidays, and within the required scheduled interval as defined in the product maintenance schedule, unless otherwise agreed by HPE in writing. Any services provided outside of HPE standard business hours may be subject to additional charges.</p> <p>HPE will plan the necessary periodic maintenance activities, and identify and communicate any prerequisites to you when contacting you to schedule the service. You must provide access to the product, ensure that the prerequisites have been met, and supply any consumables such as filters and chemicals required at the time of product maintenance.</p>
Software support	
Access to technical resources	<p>You can access HPE technical resources via telephone, electronic communication, or fax (where locally available) for assistance in resolving software implementation or operations problems.</p>
License to use software updates	<p>You receive the license to use software updates to HPE or HPE-supported third-party software for each system, socket, processor, processor core, or end-user software license covered by this service, as allowed by the original HPE or original manufacturer software license terms, provided you have rightfully acquired the original software license.</p> <p>The license terms shall be as described in the HPE software licensing terms corresponding to your prerequisite underlying software license, or in accordance with the current licensing terms of the third-party software manufacturer, if applicable, including any additional software licensing terms that may accompany or otherwise be made available for such software updates provided under this service.</p> <p>Distribution of certain third-party software updates, license agreements, and license keys may be made directly from the third-party vendor to you, as applicable.</p>
Software support	<p>HPE provides corrective support to resolve identifiable and Customer-reproducible software product problems, support to help you identify problems that are difficult to reproduce, and assistance in troubleshooting problems and determining configuration parameters for supported configurations.</p> <p>Once a software case is logged, an HPE Solution Center engineer will respond to the case within two hours. Cases received and answered outside the service coverage window will be logged the next day for which you have a service coverage window (may vary by geographic location).</p>
Installation advisory support	<p>Limited advisory support is provided and is restricted to basic advisory assistance if you encounter difficulties while performing a software product installation or advice on proper installation methods and updating of standalone applications. The scope of such advisory support is at HPE's discretion.</p> <p>Exclusions to this advisory support include, but are not limited to, the following: any downloading of complete software packages or walking you through an installation from start to finish. These services are available for an additional charge and can be purchased separately from HPE.</p>
Software features and operational support	<p>HPE provides information, as commercially available, on current product features, known problems and available solutions, and operational advice and assistance.</p>
Software product and documentation updates	<p>As HPE releases updates to HPE software, the latest revisions of the software and reference manuals are made available to you. For selected third-party software, HPE will provide software updates as such updates are made available from the third party, or HPE may provide instructions on how you can obtain any software updates directly from the third party. A license key or access code, or instructions for obtaining a license key or access code, will also be provided to you when required to download, install, or run the latest software revision.</p> <p>For most HPE software and selected HPE-supported third-party software, updates will be made available through the Software Updates and Licensing portal via the HPE Support Center. The Software Updates and Licensing portal provides you with electronic access to receive and proactively manage software product and documentation updates.</p> <p>For other HPE-supported third-party software, you may be required to download updates directly from the vendor's website.</p> <p>When this service is provided for a solution that is composed of multiple HPE and/or third-party products, software support will be offered only on updates that are made available for the solution by HPE.</p>



TABLE 4. Service features (continued)

Feature	Delivery specifications
Other features	
Access to electronic support information and services	<p>As part of this service, HPE provides you with access to certain commercially available electronic and web-based tools. You have access to:</p> <ul style="list-style-type: none"> Certain capabilities made available to registered users with linked entitlements, such as downloading selected HPE software patches, subscribing to hardware-related proactive service notifications, and participating in support forums for solving problems and sharing best practices with other registered users. Expanded web-based searches of technical support documents to facilitate faster problem solving and certain HPE proprietary service diagnostic tools with password access. A web-based tool for submitting questions directly to HPE; the tool helps to resolve problems quickly with a prequalification process that routes the case to the resource qualified to answer the question, it also allows the status of each case submitted to be viewed, including cases submitted by telephone. HPE and third-party hosted knowledge databases for certain third-party products, where you can search for and retrieve product information, find answers to support questions, participate in support forums, and download software updates; this service may be limited by third-party access restrictions. The Software Updates and Licensing portal, which provides you with electronic access to receive, proactively manage, and plan for software product updates; access to the portal is through the HPE Support Center.
Escalation management	<p>HPE has established formal escalation procedures to facilitate the resolution of complex incidents. Local HPE management coordinates incident escalation, enlisting the skills of appropriate HPE resources to assist you with problem solving. For selected third-party software products for which HPE is providing software support and update services, HPE will follow the agreed-upon escalation processes established between HPE and the third-party vendor to assist with case resolution.</p>
HPE electronic remote support solution	<p>For eligible products, the HPE electronic remote support solution provides robust troubleshooting and repair capabilities. It can include remote system access solutions and may offer a convenient central point of administration and an enterprise view of open incidents and history. An HPE support specialist will only use the remote system access with your authorization. The remote system access may enable the HPE support specialist to provide more efficient troubleshooting and faster case resolution.</p>

SPECIFICATIONS (OPTIONAL)

TABLE 5. Optional service features

Feature	Delivery specifications
Defective media retention	<p>For eligible products, the defective media retention (DMR) service feature option allows you to retain defective hard disk or eligible SSD/Flash drive components that you do not want to relinquish due to sensitive data contained within the disk ("Disk or SSD/Flash Drive") covered under this service. All Disk or eligible SSD/Flash Drives on a covered system must participate in the defective media retention.</p>
Comprehensive defective material retention	<p>In addition to defective media retention, the comprehensive defective material retention service feature option allows you to retain additional components that have been designated by HPE as having data retentive capabilities, such as memory modules. All eligible data retentive components on a covered system must participate in the comprehensive defective material retention. The components that can be retained under this service feature are outlined in the document located at hpe.com/services/cdmr.</p>
Optional features available only with HPE Contractual Services	
Preventive maintenance	<p>An HPE authorized representative will visit your site at regularly scheduled intervals. You shall call HPE to request and schedule a preventive maintenance visit at the agreed-upon intervals. Availability and deliverables may vary by region.</p> <p>Preventive maintenance services will be delivered between 8:00 a.m. and 5:00 p.m. local time, Monday through Friday excluding HPE holidays, regardless of the selected coverage window.</p>



COVERAGE

For HPE ProLiant servers and storage systems, the service on the main product covers HPE-branded hardware options not designated by HPE as requiring separate coverage, that are qualified for the server, are purchased at the same time or afterward, and are internal to the enclosure, as well as HPE-supported and -supplied tower UPS products. These items will be covered at the same service level as the main product.

For HPE BladeSystem enclosures, this service covers the enclosure, power supplies, fans, enclosure devices, and options not designated by HPE as requiring separate coverage.

For HPE Moonshot systems, this service covers the chassis, power supplies, fans, chassis devices, and servers not designated by HPE as requiring separate coverage.

For HPE ProLiant servers, storage, or HPE BladeSystem enclosures installed within a rack, the service also covers all HPE qualified rack options not designated by HPE as requiring separate coverage. Coverage includes HPE-supported and -supplied UPS products not exceeding 12 kVA, KVM switch, console, and PDU installed within the same rack. The UPS battery is covered separately under its own warranty terms and conditions, limited to the term of the applicable warranty period. For a complete list of the HPE ProLiant and HPE BladeSystem options and Moonshot servers that require separate service coverage, please visit h20195.www2.hp.com/v2/getdocument.aspx?docname=4AA4-6535ENW.

For HPE networking systems, the service on the main product covers HPE-branded hardware options not designated by HPE as requiring separate coverage, that are qualified for the system, are purchased at the same time or afterward, and are internal to the system (for example, connectivity modules, transceivers, and internal power supplies).

Supplies and consumable parts including, but not limited to removable media, maintenance kits and other supplies, as well as user maintenance are not supported and will not be provided as part of this service; standard warranty terms and conditions apply to supplies and consumable parts. The repair or replacement of any supplies or consumables is the responsibility of the Customer. Some exceptions may apply; contact HPE for more information. If a consumable part is eligible for coverage, as determined by HPE, call-to-repair time commitments and on-site response times do not apply to repair or replacement of the covered consumable part.

Notwithstanding anything to the contrary in this document or HPE's current standard sales terms, HPE will, for select enterprise storage arrays and enterprise tape products, cover and replace defective or depleted batteries that are critical to the proper operation of the covered product.

Maximum supported lifetime/maximum usage: Parts and components that have reached their maximum supported lifetime and/or the maximum usage limit as set forth in the manufacturer's operating manual, product QuickSpecs, or the technical product data sheet will not be provided, repaired, or replaced as part of this service.

PREREQUISITES, CUSTOMER RESPONSIBILITIES, AND SERVICE LIMITATIONS

HARDWARE ON-SITE SUPPORT

An on-site response time will not apply if the service can be delivered using remote diagnosis, remote support, or other service delivery methods described herein.

For technical hardware issues that cannot, in HPE's judgment, be resolved remotely, an HPE authorized representative will provide on-site technical support on covered hardware products to return them to operating condition. For certain products, HPE may, at its sole discretion, elect to replace such products in lieu of repairing them. Replacement products are new or functionally equivalent to new in performance. Replaced products become the property of HPE.

In cases where CSR parts or replacement products are shipped to resolve a case, the Customer is responsible for returning the defective part or product within a time period designated by HPE. In the event that HPE does not receive the defective part or product within the designated time period or if the part or product is degaussed or otherwise physically damaged upon receipt, the Customer will be required to pay the HPE list price for the defective part or product, as determined by HPE.

If you agree to the recommended CSR and a CSR part is provided to return the system to operating condition, the on-site service level shall not apply. In such cases, it is HPE's practice to express ship to your location the CSR parts that are critical to the product's operation. For more information about CSR, visit hpe.com and search for the HPE Product User and Maintenance Guide for your product.



For replacement parts and components that are discontinued, an upgrade path may be required. Upgrades for discontinued parts or components may in some cases result in additional charges to the Customer. HPE will work with the Customer to recommend a replacement. Not all components will have available replacements in all countries due to local support capabilities.

The Customer is responsible for installing, in a timely manner, critical Customer-installable firmware updates, as well as CSR parts and replacement products delivered to the Customer.

The Customer agrees to pay additional charges if the Customer requests that HPE install Customer-installable firmware or software updates or patches. Any additional charges to the Customer will be on a time and materials basis, unless otherwise previously agreed to in writing by HPE and the Customer.

The Customer is responsible for the security of the Customer's proprietary and confidential information. The Customer is responsible for properly sanitizing or removing data from products that may be replaced and returned to HPE as part of the repair process to ensure the safeguarding of the Customer's data. For more information on Customer responsibilities, including those outlined in the HPE Media Sanitization Policy and Media Handling Policy for Healthcare Customers, go to hpe.com/mediahandling.

HARDWARE CALL-TO-REPAIR

For hardware call-to-repair time commitments, HPE requires that you install and operate the appropriate HPE remote support solution, with a secure connection to HPE, in order to enable the delivery of the service. Please contact a local HPE representative for further details on requirements, specifications, and exclusions. If you do not deploy the appropriate HPE remote support solution, HPE may not be able to provide the service as defined and is not obligated to do so. Additional charges will be applied for on-site installation of noncustomer-installable firmware if you do not deploy the appropriate HPE remote support solution in cases where recommended and available. Installation of Customer-installable firmware is your responsibility.

HPE, at its sole discretion, may require an audit on the covered products. If such an audit is required, an HPE authorized representative will contact you, and you will agree to arrange for an audit to be performed within the initial 30-day time frame. At the sole discretion of HPE, the audit may be performed on-site, via remote system access, via remote audit tools, or over the phone. If an audit is required by HPE, it will take 30 days from the time this service is purchased to set up and perform the audits and processes that must be completed before the hardware call-to-repair time commitment can be put into effect. The hardware call-to-repair time commitment will not take effect until five (5) business days after the audit has been completed. Until such time, service for the covered hardware will be delivered at a 4-hour on-site response time service level.

In addition, HPE reserves the right to downgrade service to an on-site response time or cancel the service contract if critical audit suggestions are not followed or the audit is not performed within the specified time frame, unless the delay is caused by HPE.

The hardware repair time may vary for specific products.

HPE reserves the right to modify the call-to-repair time commitment as it applies to your specific product configuration, location, and environment. This is established at the time of the support agreement order and is subject to resource availability.

The hardware call-to-repair time commitment is subject to you providing immediate and unrestricted access to the system, as requested by HPE. The call-to-repair time commitment does not apply when system access, including physical, remote troubleshooting, and hardware diagnostic assessments, is delayed or denied. If you request scheduled service, the call-to-repair time period begins at the agreed-upon scheduled time.

A hardware call-to-repair time commitment does not apply to software products or when you choose to have HPE prolong diagnosis rather than execute recommended recovery procedures.

The hardware call-to-repair time commitments and on-site response times do not apply to the repair or replacement of defective or depleted batteries for selected enterprise storage arrays and enterprise tape products.

The following activities or situations will suspend the hardware call-to-repair time calculation (if applicable) until they are completed or resolved:

- Any Customer or third-party action or inaction impacting the repair process
- Any automated or manual recovery processes triggered by a hardware malfunction, such as disk mechanism rebuild, sparing procedures, or data integrity protection measures
- Any other activities not specific to the hardware repair but which are required to verify that the hardware malfunction has been corrected, such as rebooting the operating system



DEFECTIVE MEDIA RETENTION AND COMPREHENSIVE DEFECTIVE MATERIAL RETENTION

The defective media retention and comprehensive defective material retention service feature options apply only to eligible data retentive components replaced by HPE due to malfunction. They do not apply to any exchange of data retentive components that have not failed.

Data retentive components that are specified by HPE as consumable parts and/or have reached the maximum supported lifetime and/or the maximum usage limit as set forth in the manufacturer's operating manual, the product QuickSpecs, or the technical data sheet are not covered by this service.

Defective media retention service and comprehensive defective material retention service coverage for options designated by HPE as requiring separate coverage, if available, must be configured and purchased separately.

Failure rates on these components are constantly monitored, and HPE reserves the right to cancel this service with 30 days' notice if HPE reasonably believes that you are overusing the defective media retention or comprehensive defective material retention service feature option (such as when replacement of defective data retentive components materially exceeds the standard failure rates for the system involved).

If you choose to retain repair parts covered under the defective media retention and/or comprehensive defective material retention service feature options, it is your responsibility to:

- Retain covered data retentive components that are replaced during support delivery by HPE
- Ensure that any Customer sensitive data on the retained component is destroyed or remains secure
- Have an authorized representative present to retain the defective data retentive component, accept the replacement component, provide HPE with identification information such as the serial number for each component retained hereunder, and, upon HPE request, execute a document provided by HPE acknowledging the retention of the data retentive component
- Destroy the retained data retentive component and/or ensure that it is not put into use again
- Dispose of all retained data retentive components in compliance with applicable environmental laws and regulations

For data retentive components supplied by HPE to you as loaned, rented, or leased products, you will promptly return the replacement components at the expiration or termination of support with HPE. You will be solely responsible for removing all sensitive data before returning any such loaned, rented, or leased components or products to HPE, and HPE shall not be responsible for maintaining the confidentiality or privacy of any sensitive data that remains on such components.

COLLABORATIVE CALL MANAGEMENT FOR NON-HPE SOFTWARE

In order for HPE to provide Collaborative Call Management, you must have an active support agreement with the software vendor that includes the required service level and features that allow you to place cases and receive support from the vendor. If the vendor requires it, you will take any steps necessary to ensure that HPE can submit cases on your behalf. In addition, you must provide HPE with the appropriate information needed for HPE to initiate a service case with the software vendor on behalf of you. Without these steps, HPE will not be able to transfer cases to the vendor and assumes no responsibility for failure to do so. HPE's obligations are limited to the placement of support cases only. Purchase of Collaborative Call Management does not assign the support agreement between you and vendor to HPE. You remain responsible for the performance of its obligations under such agreements, which include payment of all applicable fees, including any fees that may apply as a result of logging cases with the vendor. HPE is not liable for the performance or nonperformance of third-party vendors, their products, or their support services.

SOFTWARE SUPPORT

Software updates are not available for all software products. For some products, software updates include only minor improved features. New software versions must be purchased separately.

When Software support is purchased, it must be for the same service coverage (or higher) as the base product and for each system, processor, processor core, or end user in the Customer's environment that will require support.



The Customer will:

- Take responsibility for registering to use the HPE or third-party vendor's electronic facility in order to access knowledge databases or to obtain product information. HPE will provide registration information to the Customer, as required; additionally, for certain products, the Customer may be required to accept vendor-specific terms for use of the electronic facility.
- Retain, and provide to HPE upon request, all original software licenses, license agreements, license keys, and subscription service registration information.
- Take responsibility for acting upon software product updates and obsolescence notifications received from the HPE Support Center.
- Use all software products in accordance with current HPE software licensing terms corresponding to the Customer's prerequisite underlying software license, or in accordance with the current licensing terms of the third-party software manufacturer, if applicable, including any additional software licensing terms that may accompany or made available for such software updates provided under this service.

GENERAL PROVISIONS AND EXCLUSIONS

The Customer acknowledges and agrees that HPE may use resources outside the country of purchase for delivery of these services unless otherwise specified as part of a service feature description.

At the discretion of HPE, service will be provided using a combination of remote diagnosis and support, services delivered on-site, and other service delivery methods. Other service delivery methods may include the delivery via a courier of Customer-replaceable parts such as certain hard disk drives, and other parts classified by HPE as CSR parts, or an entire replacement product. HPE will determine the appropriate delivery method required to provide effective and timely Customer support and meet the call-to-repair time commitment, if applicable.

You must have rightfully acquired the license for any underlying firmware that will be covered under these services.

To be eligible to purchase this service, you must be properly licensed to use a currently supported revision of the software at the time the support agreement coverage begins; otherwise, additional charges may be applied to bring you into service eligibility.

If you do not act upon your specified Customer responsibilities, at HPE's discretion, HPE or the HPE authorized service provider will i) not be obligated to deliver the services as described, or ii) perform such service at your expense at the prevailing time and material rates.

HPE retains the right to determine the final resolution of all incidents.

Activities such as, but not limited to, the following are excluded from this service:

- Services required due to your failure to incorporate any system fix, repair, patch, or modification provided to you by HPE
- Services that, in the opinion of HPE, are required due to unauthorized attempts by non-HPE personnel to install, repair, maintain, or modify hardware, firmware, or software
- Operational testing of applications, or additional tests requested or required by you
- Services that, in the opinion of HPE, are required due to improper treatment or use of the products or equipment
- Services required due to failure of you to take avoidance action previously advised by HPE
- Backup and recovery of the operating system, other software, and data
- Installation of any Customer-installable firmware and/or software updates
- Troubleshooting for interconnectivity or compatibility problems
- Support for network-related problems

HPE may utilize authorized service delivery partners in certain countries where HPE does not have a direct, local presence. Any specified on-site response times or provision of CSR parts is subject to local parts availability at the country level.

Coverage for eligible multivendor systems is limited to all standard vendor-supplied internal components.



CUSTOMER RESPONSIBILITIES

If required by HPE, the Customer or HPE authorized representative must activate the hardware product to be supported within 10 days of purchase of this service, using the registration instructions within the documentation provided by HPE, or as otherwise directed by HPE. In the event that a covered product changes location, activation and registration (or proper adjustment to existing HPE registration) is to occur within 10 days of the change.

The Customer is responsible for removing devices that are blocked from physical access and ensuring any covered devices are directly accessible without the use of additional tools or equipment and do not expose the HPE authorized representative to a potential health or safety hazard in order to perform the services. The Customer must ensure the covered device(s) are fully and freely accessible to the HPE authorized representative without any hindrance whatsoever prior to the delivery of the service.

Remote Support: Upon HPE request, you will be required to support HPE's remote case resolution efforts. You will:

- Start self-tests and install and run other diagnostic tools and programs
- Install Customer-installable software and firmware updates and patches
- Run data collection "scripts" on behalf of HPE when they cannot be initiated from HPE Remote Support Technology
- Provide all information necessary for HPE to deliver timely and professional remote support and to enable HPE to determine the level of support eligibility
- Perform other reasonable activities to help HPE identify or resolve cases, as requested by HPE

SMART SPARES BOX

The following terms apply only if Customer has an HPE owned and provided Smart Spares Box installed at the Customer's site. The Smart Spares Box is configured to store a remotely manageable inventory of product spare parts ("Smart Spares Box Content") in connection with the qualifying Support Services described in this data sheet.

Customer responsibilities

1. Allow HPE to deliver and timely install (including securing permission for placement and installation at Customer sites leased from or owned by a third party) the Smart Spares Box on Customer's premises at a location mutually determined by the parties for purpose described previously
2. Allow HPE to have timely unrestricted access to the Smart Spares Box to drop off and pick up Smart Spares Box Content, inspect, maintain, repair and de-install or replace the Smart Spares Box as requested by HPE
3. To not transfer the care or custody or remove the Smart Spares Box
4. Notify HPE as soon as reasonably possible of any unauthorized use or damage to or malfunction of the Smart Spares Box, as soon as reasonably possible following discovery by Customer
5. Take reasonable care (including taking reasonable measures to prevent loss or damage) of the Smart Spares Box while installed at Customer's site
6. Upon termination or expiration of support coverage with HPE, or at HPE's sole discretion, allow HPE to remove the Smart Spares Box and Smart Spares Box Content. Customer can request removal of the Smart Spares Box by providing HPE with sixty (60) days advance notice

Additional limitations

The Smart Spares Box is considered a Proprietary Service Tool as defined in HPE's support terms, including any Supporting Material.

The Customer expressly acknowledges that neither this provision nor the provisioning of the Smart Spares Box constitutes a sale or rental of the Smart Spares Box or the Smart Spares Box Content. **The Smart Spares Box is provided "as is," without express or implied warranty of any kind, and to the extent permitted by law, HPE disclaims all warranties.**



Attachment: Contract with Nth Generation Computing [Revision 1] (4182 : APPROVE THE USE OF TECHNOLOGY REPLACEMENT FUNDS TO

Data sheet

ORDERING INFORMATION

For products containing individually sold and supported units or options, all individually sold and supported units or options must be on contract and at the same service level as the base product if that service level is available on those units or options.

When Software support is purchased, it must be for the same service coverage (or higher) as the base product and for each system, processor, processor core, or end user in the Customer's environment that will require support.

Availability of service features and service levels may vary according to local resources and may be restricted to eligible products and geographic locations.

To obtain further information or to order HPE Foundation Care Service, contact a local HPE sales representative and reference the following product numbers (x denotes the service length in years; options are 3, 4, or 5 years, or "C" for HPE Contractual Services).

- HPE Foundation Care NBD SVC (H7J32Ax)
- HPE Foundation Care NBD wDMR SVC (H7J33Ax)
- HPE Foundation Care 24x7 SVC (H7J34Ax)
- HPE Foundation Care 24x7 wDMR SVC (H7J35Ax)
- HPE Foundation Care CTR SVC (H7J36Ax)
- HPE Foundation Care CTR wDMR SVC (H7J37Ax)

CDMR requires DMR and can either be selected as a configurable option within the DMR service levels, or the CDMR service level will be preconfigured with both DMR and CDMR features included.

Depending on the point of purchase and the requested service-level option, other product numbers may apply. Please consult a local HPE representative or HPE reseller regarding which product number will best meet your specific needs.

FOR MORE INFORMATION

For more information on HPE Foundation Care Service or other Support Services, contact any of our worldwide sales offices or visit the following website: hpe.com/services/support.

LEARN MORE AT

hpe.com/us/en/services/foundation-care-services

Make the right purchase decision.
Contact our presales specialists.



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HPE support



Get updates



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This data sheet is governed by the Hewlett Packard Enterprise current standard sales terms, which include the supplemental data sheet, or, if applicable, the Customer's purchase agreement with Hewlett Packard Enterprise.

4AA4-8876ENW, June 2020, Rev. 6

EXHIBIT B**CITY'S RESPONSIBILITIES**

1. Provide timely access to the City Hall Data Center, the Conference and Recreation Center Disaster Recovery site, and any other City sites associated with delivering the contracted services.
2. Furnish the Contractor all in-house data which is pertinent to services to be performed by the Contractor and which is within the custody or control of the City, including, but not limited to, blueprints, server performance and capacity reports, storage performance and capacity reports, server and network diagrams, network design information, GIS data, and such other pertinent data which may become available to the City.
3. Provide timely review, processing, and reasonably expeditious approval of all submittals by the Contractor.
4. Provide timely access to the Contractor for City staff when requested and when reasonably needed.

EXHIBIT C

TERMS OF PAYMENT

1. The Contractor's compensation shall not exceed **\$833,766**.
2. The Contractor will obtain, and keep current during the term of this Agreement, the required City of Moreno Valley business license. Proof of a current City of Moreno Valley business license will be required prior to any payments by the City. Any invoice not paid because the proof of a current City of Moreno Valley business license has not been provided will not incur any fees, late charges, or other penalties. Complete instructions for obtaining a City of Moreno Valley business license are located at: http://www.moval.org/do_biz/biz-license.shtml
3. The Contractor will electronically submit an invoice to the City on a monthly basis for progress payments along with documentation evidencing services completed to date. The progress payment is based on actual time and materials expended in furnishing authorized professional services since the last invoice. At no time will the City pay for more services than have been satisfactorily completed and the City's determination of the amount due for any progress payment shall be final. The Contractor will submit all original invoices to Accounts Payable staff at AccountsPayable@moval.org

Accounts Payable questions can be directed to (951) 413-3073.

Copies of invoices may be submitted to the Technology Services Division at

steveh@moval.org or calls directed to (951) 413-3410.
3. The Contractor agrees that City payments will be received via Automated Clearing House (ACH) Direct Deposit and that the required ACH Authorization form will be completed prior to any payments by the City. Any invoice not paid because the completed ACH Authorization Form has not been provided will not incur any fees, late charges, or other penalties. The ACH Authorization Form is located at:
http://www.moval.org/city_hall/forms.shtml#bf
4. The minimum information required on all invoices is:
 - A. Vendor Name, Mailing Address, and Phone Number
 - B. Invoice Date
 - C. Vendor Invoice Number
 - D. City-provided Reference Number (e.g. Project, Activity)

- E. Detailed work hours by class title (e.g. Manager, Technician, or Specialist), services performed and rates, explicit portion of a contract amount, or detailed billing information that is sufficient to justify the invoice amount; single, lump amounts without detail are not acceptable.
6. The City shall pay the Contractor for all invoiced, authorized professional services within thirty (30) days of receipt of the invoice for same.
7. Reimbursement for Expenses. Contractor shall not be reimbursed for any expenses unless authorized in writing by City.
8. Maintenance and Inspection. Contractor shall maintain complete and accurate records with respect to all costs and expenses incurred under this Agreement. All such records shall be clearly identifiable. Contractor shall allow a representative of City during normal business hours to examine, audit, and make transcripts or copies of such records and any other documents created pursuant to this Agreement. Contractor shall allow inspection of all work, data, documents, proceedings, and activities related to the Agreement for a period of three (3) years from the date of final payment under this Agreement.



Report to City Council

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: PAYMENT REGISTER- OCTOBER 2020

RECOMMENDED ACTION

Recommendation:

1. Receive and file the Payment Register.

SUMMARY

The Payment Register is an important report providing transparency of financial transactions and payments for City activity for review by the City Council and the residents and businesses in Moreno Valley. The report is posted to the City's website as soon as it is available. The report is included in the City Council agenda as an additional means of distributing the report.

The payment register lists in alphabetical order all checks and wires in the amount of \$25,000 or greater, followed by a listing in alphabetical order of all checks and wires less than \$25,000. The payment register also includes the fiscal year-to-date (FYTD) amount paid to each vendor.

PREPARATION OF STAFF REPORT

Prepared By:
Dena Heald
Deputy Finance Director

Department Head Approval:
Marshall Eyerman
Assistant City Manager
Chief Financial Officer/City Treasurer

CITY COUNCIL GOALS

None

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. October 2020 Payment Register

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	11/23/20 6:39 AM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	11/23/20 8:56 AM



City of Moreno Valley
Payment Register
For Period 10/1/2020 through 10/31/2020

CHECKS IN THE AMOUNT OF \$25,000 OR GREATER

Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
AGATI INC	28953	10/12/2020	17886	IRIS PLAZA LIBRARY-FURNITURE	\$109,481.79
Remit to: CHICAGO, IL					FYTD: \$109,481.79
CALPINE CORPORATION DBA CALPINE ENERGY SERVICES	28958	10/12/2020	65189	RESOURCE ADEQUACY-SEP 2020/MV UTILITY	\$105,400.00
Remit to: HOUSTON, TX					FYTD: \$421,600.00
COMPULINK MANAGEMENT CENTER, INC. - DBA LASERFICHE	28913	10/05/2020	4376	ANNUAL SUBSCRIPTION 9/30/20-9/29/21	\$79,378.36
Remit to: LONG BEACH, CA					FYTD: \$79,378.36
COUNTY OF RIVERSIDE SHERIFF	29028	10/19/2020	SH0000038005	CONTRACT LAW ENFORCEMENT BILLING #2 (07/30-8/26/20)	\$3,113,260.32
		10/19/2020	SH0000038006	SPECIAL BILLING #2 (07/30-8/26/20)-LOT RESTORATION PROJECT	
Remit to: RIVERSIDE, CA					FYTD: \$15,870,630.69
COVINGTON & BURLING LLP	28915	10/05/2020	60907615	LEGAL SERVICES	\$91,318.50
		10/05/2020	60912621	LEGAL SERVICES	
Remit to: WASHINGTON, DC					FYTD: \$129,692.50
COWBOY MOTOR CO. DBA MOORE CHRYSLER DODGE JEEP RAM	29067	10/26/2020	G210909	2020 RAM 3500 CREW CAB-VIN#3C6JR6AG6LG210909	\$123,209.70
		10/26/2020	G210908	2020 RAM 3500 CREW CAB-VIN#3C6JR6AG4LG210908	
		10/26/2020	S155090	2020 RAM 3500 CREW CAB-VIN#1C6RR6KT0LS155090	
		10/26/2020	G210910	2020 RAM 3500 CREW CAB-VIN#3C6JR6AG2LG210910	
		10/26/2020	G210906	2020 RAM 3500 CREW CAB-VIN#3C6JR6AG0LG210906	
Remit to: SILSBEE, TX					FYTD: \$312,883.32

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



City of Moreno Valley
Payment Register
For Period 10/1/2020 through 10/31/2020

CHECKS IN THE AMOUNT OF \$25,000 OR GREATER

<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
DECKERS OUTDOOR CORPORATION	28966	10/12/2020	QTR ENDING JUN20	SALES TAX PAYMENT PER OPERATING COVENANT AGREEMENT	\$77,571.00
Remit to: GOLETA, CA					<u>FYTD:</u> \$77,571.00
DYETT & BHATIA URBAN AND REGIONAL PLANNERS	28920	10/05/2020	19-572-09	GENERAL PLAN UPDATE AND EIR CONSULTING 7/1-7/31/20	\$45,471.50
Remit to: OAKLAND, CA					<u>FYTD:</u> \$143,100.00
EASTERN MUNICIPAL WATER DISTRICT	240434	10/19/2020	AUG-20 10/19/20	WATER CHARGES	\$52,005.33
		10/19/2020	SEPT-20 10/19/20	WATER CHARGES	
	10/19/2020	OCT-20 10/19/20	WATER CHARGES		
	240472	10/26/2020	SEPT-20 10/26/20	WATER CHARGES	\$177,400.13
		10/26/2020	OCT-20 10/26/20	WATER CHARGES	
Remit to: LOS ANGELES, CA					<u>FYTD:</u> \$999,517.36

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



City of Moreno Valley
Payment Register
For Period 10/1/2020 through 10/31/2020

CHECKS IN THE AMOUNT OF \$25,000 OR GREATER

<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
ENCO UTILITY SERVICES MORENO VALLEY LLC	28922	10/05/2020	C20-15	LASELLE ST.-VEHICLE HIT STREETLIGHT	\$626,797.24
		10/05/2020	C20-14	LASELLE ST.-VEHICLE HIT STREETLIGHT	
		10/05/2020	0402-MF-02417	SOLAR SYSTEM INSPECTION	
		10/05/2020	0402-MF-02418	SOLAR SYSTEM INSPECTION	
		10/05/2020	0402-MF-02419	SOLAR SYSTEM INSPECTION	
		10/05/2020	C20-11	PIGEON PASS RD.-VEHICLE HIT STREETLIGHT	
		10/05/2020	C20-13B-02	LASELLE ST.-VEHICLE HIT SWITCH S-61	
		10/05/2020	MVU- 2020-44102	DISTRIBUTION CHARGES 8/17-9/14/20	

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



City of Moreno Valley
Payment Register
For Period 10/1/2020 through 10/31/2020

CHECKS IN THE AMOUNT OF \$25,000 OR GREATER

<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
ENCO UTILITY SERVICES MORENO VALLEY LLC	28967	10/12/2020	40-408A-13	RANCHO BELAGO PHASE 2	\$600,430.16
		10/12/2020	40-448B-01	DAVIS ST.-6SL'S	
		10/12/2020	40-374B-15	CONTINENTAL VILLAGES APTS.	
		10/12/2020	40-438B-01	CENTURY COMMUNITIES	
		10/12/2020	40-438A-11	CENTURY COMMUNITIES	
		10/12/2020	40-449A-06	MERITAGE HOMES LEGACY PARK-221 HOMES	
		10/12/2020	40-450A-05	HIGHLAND FAIRVIEW CORPORATE PARK:PHASE 2	
		10/12/2020	40-454A-03	MORENO VALLEY LOGISTICS CENTER BUILDING 4	
		10/12/2020	40-405A-14	MORENO BEACH DR BRIDGE CROSSING	
		10/12/2020	40-378B-03	VERIZON MONOPALM NEW CELL TOWER	
		10/12/2020	0402-MF-02421	SOLAR SYSTEM INSPECTION	
		10/12/2020	40-452A-03	FIRST NANDINA II LOGISTICS CENTER	
		10/12/2020	40-447B-04	KIA DEALERSHIP	
		10/12/2020	40-443B-04	MVU ANNEX BUILDING	
		10/12/2020	40-431B-09	PHELAN DEVELOPMENT	
		10/12/2020	40-446B-04	ALERE PROPERTY GROUP	
		10/12/2020	40-401B-11	DAY STREET LINE EXTENSION	
		10/12/2020	40-459A-02	S61 PME GS RECONFIGURATION	
		10/12/2020	40-457-01	HEMLOCK STREETLIGHTS	
		10/12/2020	40-444B-02	AT&T CELL TOWER-IRIS PLAZA	
	10/12/2020	40-413B-01	CONTINENTAL VILLAGES STREET LIGHTS AND CONDUIT		
	10/12/2020	40-410B-06	VILLA ANNETTE APARTMENT HOMES		
	10/12/2020	MVU-2020- 44075	DISTRIBUTION CHARGES 7/20-8/17/20		
	10/12/2020	40-442B-03	BEAZER HOMES-PHASE 4-79 HOMES		

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Remit to: ANAHEIM, CA

FYTD: \$2,377,553.02



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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
EXELON GENERATION COMPANY, LLC	28968	10/12/2020	MVEU-00096A	POWER PURCHASE 9/1-9/30/20	\$538,637.60
Remit to: BALTIMORE, MD					<u>FYTD:</u> \$2,399,595.04
G. HURTADO CONSTRUCTION INC.	29034	10/19/2020	5955	MORENO MDP LINE H-2 STORM DRAIN	\$129,176.60
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$181,759.10
GRAYBAR ELECTRIC CO INC	28970	10/12/2020	9318058964	LIGHT POLES & FIXTURES-CIVIC CENTER	\$37,966.25
Remit to: DIAMOND BAR, CA					<u>FYTD:</u> \$162,408.35
HARDY & HARPER, INC.	29074	10/26/2020	46804	CITYWIDE PAVEMENT REHAB (FY 19/20)	\$497,077.71
Remit to: SANTA ANA, CA					<u>FYTD:</u> \$497,077.71
HORIZONS CONSTRUCTION COMPANY INTERNATIONAL INC.	29036	10/19/2020	5	MV AMPHITHEATER-PROGRESS PAYMENT #5	\$562,044.61
Remit to: ORANGE, CA					<u>FYTD:</u> \$1,904,078.65
HOT LINE CONSTRUCTION, INC	29077	10/26/2020	DAY STREET-RET	RETENTION RELEASE-DAY STREET	\$137,203.79
Remit to: IRVING, TX					<u>FYTD:</u> \$840,037.81
HYUNDAI OF MORENO VALLEY	240357	10/05/2020	SEPTEMBER 2020	YEAR 3-QTR 2 TAX PAYMENT PER OPERATING COVENANT AGREEMENT	\$32,223.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$32,223.00
ITERIS, INC.	28925	10/05/2020	126326	ADVANCED DILEMMA ZONE DETECTION	\$608,503.90
Remit to: SANTA ANA, CA					<u>FYTD:</u> \$1,185,712.24

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KTU+A	29083	10/26/2020	32433	DRACAEA NEIGHBORHOOD GREENWAY	\$40,223.75
		10/26/2020	32299	DRACAEA NEIGHBORHOOD GREENWAY	
Remit to: SAN DIEGO, CA					<u>FYTD:</u> \$64,744.17
LIBRARY SYSTEMS & SERVICES, LLC	28930	10/05/2020	INV3447	LIBRARY CONTRACT SVCS & MATERIALS-MAIN & MALL-OCT 2020	\$190,400.13
Remit to: ROCKVILLE, MD					<u>FYTD:</u> \$920,817.17

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MARIPOSA LANDSCAPES, INC.	29042	10/19/2020	90592	LANDSCAPE MAINT.-ANIMAL SHELTER-SEP. 2020	\$36,858.88
		10/19/2020	90590	LANDSCAPE MAINT.-SOUTH AQUEDUCT B-SEP. 2020	
		10/19/2020	90605	LANDSCAPE MAINT.-CITY YARD SANTIAGO OFFICE-SEP. 2020	
		10/19/2020	90600	LANDSCAPE MAINT.-UTILITY FIELD OFFICE-SEP. 2020	
		10/19/2020	90588	LANDSCAPE MAINT.-PAN AM SECTION AQUEDUCT-SEP. 2020	
		10/19/2020	90593	LANDSCAPE MAINT.-MARCH ANNEX BUILDING-SEP. 2020	
		10/19/2020	90582	LANDSCAPE MAINT.-TOWNGATE COMMUNITY CENTER-SEP. 2020	
		10/19/2020	90584	LANDSCAPE MAINT.-AQUEDUCT BIKEWAY/BAY AVE. TO GRAHAM-SEP. 2020	
		10/19/2020	90585	LANDSCAPE MAINT.-AQUEDUCT BIKEWAY-DEPHINIUM/PERHAM TO JFK-SEP20	
		10/19/2020	90586	LANDSCAPE MAINT.-AQUEDUCT BIKEWAY/VANDENBERG TO FAY-SEP. 2020	
		10/19/2020	90581	LANDSCAPE MAINT.-SD LMD ZONE 02-SEP. 2020	
		10/19/2020	90587	LANDSCAPE MAINT.-NORTH AQUEDUCT-SEP. 2020	
		10/19/2020	90583	LANDSCAPE MAINT.-TOWNGATE AQUEDUCT BIKEWAY-SEP. 2020	
		10/19/2020	90598	LANDSCAPE MAINT.-PUBLIC SAFETY BUILDING-SEP. 2020	
		10/19/2020	90606	LANDSCAPE MAINT.-KITCHING ELECTRIC SUBSTATION-SEP. 2020	
		10/19/2020	90599	LANDSCAPE MAINT.-SENIOR CENTER-SEP. 2020	
		10/19/2020	90571	LANDSCAPE MAINT.-NPDES WQB-SEP. 2020	
		10/19/2020	90594	LANDSCAPE MAINT.-CITY YARD-SEP. 2020	
		10/19/2020	90595	LANDSCAPE MAINT.-CONFERENCE & REC. CENTER-SEP. 2020	
		10/19/2020	90596	LANDSCAPE MAINT.-MORENO BEACH ELECTRIC SUBSTATION-SEP. 2020	
		10/19/2020	90607	LANDSCAPE MAINT.-AQUEDUCT BIKEWAY/FAY TO GENTIAN-SEP. 2020	
		10/19/2020	90589	LANDSCAPE MAINT.-SOUTH AQUEDUCT A-SEP. 2020	
		10/19/2020	90591	LANDSCAPE MAINT.-AQUEDUCT/SCE AND OLD LAKE DRIVE-SEP. 2020	

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MARIPOSA LANDSCAPES, INC.		10/19/2020	90601	LANDSCAPE MAINT.-FIRE STATIONS 2, 6, 48, 58, 65, 91, & 99-SEP20	
		10/19/2020	90602	LANDSCAPE MAINT.-CITY HALL-SEP. 2020	
		10/19/2020	90603	LANDSCAPE MAINT.-CITY HALL ANNEX-SEP. 2020	
		10/19/2020	90604	LANDSCAPE MAINT.-VETERANS MEMORIAL-SEP. 2020	
		10/19/2020	90597	LANDSCAPE MAINT.-LIBRARY-SEP. 2020	
Remit to: IRWINDALE, CA					FYTD: \$174,781.53
MERCHANTS BUILDING MAINTENANCE, LLC.	28982	10/12/2020	597766	JANITORIAL SERVICES-AUG. 2020	\$79,855.50
		10/12/2020	597768	JANITORIAL SERVICES-JUL. 2020	
		10/12/2020	600536	DAY PORTER SERVICES FOR ENHANCED COVID-19 CLEANING-SEP. 2020	
		10/12/2020	598565	INITIAL DEEP CLEANING AT IRIS PLAZA LIBRARY BRANCH	
		10/12/2020	599457	REFILLS OF COVID-19 DISINFECTANT - VIREX	
		10/12/2020	599458	SPECIAL CLEANING SERVICES - STORAGE ROOM HAND SOAP SPILL AT CRC	
		10/12/2020	598542	CARPET & RECLINER CLEANING SERVICES AT ALL FIRE STATIONS-AUG20	
Remit to: MONTEREY PARK, CA					FYTD: \$136,127.88
MORENO VALLEY UTILITY	240360	10/05/2020	OCT-20 10/5/20	ELECTRICITY CHARGES	\$94,649.23
Remit to: HEMET, CA					FYTD: \$349,429.53
NEXTERA ENERGY CAPITAL HOLDINGS INC.	240392	10/12/2020	593721	RENEWABLE ENERGY-MV UTILITY-SEP. 2020	\$28,856.82
Remit to: JUNO BEACH, FL					FYTD: \$162,145.92

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NPG CORPORATION	28934	10/05/2020	1119096	ADA PARKING LOT IMPROVEMENT PROJECT-PUBLIC SAFETY BUILDING	\$46,496.80
Remit to: PERRIS, CA					<u>FYTD:</u> \$46,496.80
RE ASTORIA 2 LLC	29090	10/26/2020	00049	RENEWABLE ENERGY-MV UTILITY-SEP. 2020	\$30,531.35
Remit to: SAN FRANCISCO, CA					<u>FYTD:</u> \$154,710.24
ROGERS, ANDERSON, MALODY & SCOTT, LLP	29093	10/26/2020	64305	AUDIT SERVICES FOR FY ENDING 6/30/20 - SEPT. 2020 BILLING	\$39,000.00
Remit to: SAN BERNARDINO, CA					<u>FYTD:</u> \$47,000.00
SIDRA GROUP, INC.	29048	10/19/2020	1	PEDESTRIAN HYBRID BEACON ON CACTUS AVE	\$119,722.61
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$119,722.61
SILSBEE FORD INC.	29097	10/26/2020	25042F	2020 FORD TRANSIT VAN HIGH ROOF-VIN# 1FBAX2X88LKB25042	\$39,258.00
Remit to: SILSBEE, TX					<u>FYTD:</u> \$39,258.00



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SOUTHERN CALIFORNIA EDISON	240397	10/12/2020	721-3449/SEP-20	IFA CHARGES-SUBSTATION	\$30,148.21
		10/12/2020	026-1608/SEP-20	IFA & DISTRIBUTION UPGRADE CHARGES-KITCHING SUBSTATION	
		10/12/2020	SEP-20 10/12/20	ELECTRICITY CHARGES	
	240446	10/19/2020	729-6522/SEP-20	ELECTRICITY CHARGES FOR ACQUIRED STREETLIGHTS	\$96,521.70
		10/19/2020	SEP-20 10/19/20	ELECTRICITY CHARGES	
		10/19/2020	717-8027/SEP-20	ELECTRICITY CHARGES FOR ADDED STREETLIGHTS	
		10/19/2020	707-6081/SEP-20	ELECTRICITY CHARGES	
		10/19/2020	717-7516/SEP-20	ELECTRICITY CHARGES FOR ACQUIRED STREETLIGHTS	
		10/19/2020	717-8456/SEP-20	ELECTRICITY CHARGES FOR ADDED STREETLIGHTS	
	10/19/2020	587-9520/SEP-20	ELECTRICITY-FERC CHARGES/MVU		
Remit to: ROSEMEAD, CA					<u>FYTD:</u> \$797,765.48
SYNERGY COMPANIES	240450	10/19/2020	MVU RES DI 09-20	ENERGY AUDITS & INSTALLATION OF ENERGY EFFICIENCY MEASURES-SEP20	\$38,934.07
Remit to: HAYWARD, CA					<u>FYTD:</u> \$247,681.13
TENASKA ENERGY, INC	29101	10/26/2020	MOREN00202010210	ELECTRICITY POWER PURCHASE-MV UTILITY	\$905,142.40
Remit to: ARLINGTON, TX					<u>FYTD:</u> \$3,539,048.54
THE ADVANTAGE GROUP/ FLEX ADVANTAGE	28941	10/05/2020	202010	OCTOBER 2020 RETIREE MEDICAL BENEFIT BILLING	\$45,858.48
Remit to: TEMECULA, CA					<u>FYTD:</u> \$185,029.70

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THINK TOGETHER, INC	29103	10/26/2020	111-20/21-3	ASES EXPANDED LEARNING PROGRAM MGMT. SERVICES-INSTALLMENT #3	\$1,789,737.72
		10/26/2020	111-20/21-1	ASES EXPANDED LEARNING PROGRAM MGMT. SERVICES-INSTALLMENT #1	
		10/26/2020	111-20/21-2	ASES EXPANDED LEARNING PROGRAM MGMT. SERVICES-INSTALLMENT #2	
Remit to: SANTA ANA, CA					<u>FYTD:</u> \$1,789,737.72
U.S. BANK/CALCARDS	28945	10/05/2020	09-28-20	SEPTEMBER 2020 CALCARD ACTIVITY	\$140,493.14
Remit to: ST. LOUIS, MO					<u>FYTD:</u> \$609,711.06
WELLS FARGO CORPORATE TRUST	29133	10/27/2020	W201001	DEBT SERVICE-2013 REFUNDING OF 05 LRB	\$1,585,870.83
	29134	10/27/2020	W201002	DEBT SERVICE-2014 REFUNDING OF 05 LRB	\$564,106.42
	29135	10/27/2020	W201003	DEBT SERVICE-2015 TAXABLE LRB	\$437,777.00
	29136	10/27/2020	W201004	DEBT SERVICE-2016 REFUNDING OF 2007 LRB	\$412,633.36
	29137	10/27/2020	W201005	DEBT SERVICE-2019 TAXABLE LRB	\$316,600.00
Remit to: LOS ANGELES, CA					<u>FYTD:</u> \$4,642,850.16
WILLDAN ENGINEERING	29112	10/26/2020	002-23206	PLAN CHECK & INSPECTION SERVICES FOR BLDG. & SAFETY DEPT.-AUG20	\$48,694.04
		10/26/2020	002-23081	PLAN CHECK & INSPECTION SERVICES FOR BLDG. & SAFETY DEPT.-JUL20	
Remit to: ANAHEIM, CA					<u>FYTD:</u> \$122,419.32
TOTAL AMOUNTS OF \$25,000 OR GREATER					\$14,902,927.91

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AAC UTILITY PARTNERS, LLC	28949	10/12/2020	MVUS201007	CONSULTING SVCS-MV UTILITIES	\$18,700.00
Remit to: COLUMBIA, SC					<u>FYTD:</u> \$56,100.00
ABILITY COUNTS, INC	28950	10/12/2020	ACI115835	LANDSCAPE MAINT-CFD #1-SEPT 2020	\$1,835.10
Remit to: CORONA, CA					<u>FYTD:</u> \$9,609.30
ADLERHORST INTERNATIONAL LLC	28951	10/12/2020	105427	MONTHLY K-9 TRAINING (RICO/ARKAN) SEPT 2020	\$350.00
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$1,992.88
ADMINSURE	29061	10/26/2020	13646	WORKERS' COMP CLAIM ADMIN-NOV 2020	\$1,150.00
Remit to: ONTARIO, CA					<u>FYTD:</u> \$10,350.00
ADVANCED ELECTRIC INC.	28952	10/12/2020	12738	ELECTRICAL REPAIRS-PARKS	\$2,896.47
		10/12/2020	12737	ELECTRICAL REPAIRS-PARKS	
	29062	10/26/2020	12735	ELECTRICAL REPAIRS-SENIOR CENTER	\$6,490.00
		10/26/2020	12736	ELECTRICAL REPAIRS-SENIOR CENTER	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$12,604.67
AIR EXCHANGE INC	29023	10/19/2020	91603210	PLYMOVENT MAINT & REPAIR-FIRE STATIONS	\$1,488.19
Remit to: FAIRFIELD, CA					<u>FYTD:</u> \$8,284.09
AIRESPRING INC.	28954	10/12/2020	1359638/OCT20	INTERNET SVCS 10/01-10/31/20	\$2,821.91
Remit to: VAN NUYS, CA					<u>FYTD:</u> \$6,467.78
ALATORRE, REYMUNDO	240501	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00

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ALFONSO LOZANO DBA OUR PLACE RESTAURANT	240351	10/05/2020	SEPT. 25, 2020	SENIOR EATS PROGRAM 9/21-9/25/20	\$3,825.00
	240379	10/12/2020	OCTOBER 2, 2020	SENIOR EATS PROGRAM 9/28-10/2/20	\$3,825.00
	240433	10/19/2020	OCTOBER 9, 2020	SENIOR EATS PROGRAM 10/05-10/09/20	\$3,825.00
	240465	10/26/2020	OCTOBER 16, 2020	SENIOR EATS PROGRAM 10/12-10/16/20	\$3,825.00
Remit to: MORENO VALLEY, CA					FYTD: \$64,665.00
ALLIANT INSURANCE SERVICES, INC.	29024	10/19/2020	2ND QTR-CY2020	SPECIAL EVENT INSURANCE (JULY-SEPT 2020 PREMIUMS)	\$113.00
Remit to: SAN DIEGO, CA					FYTD: \$4,166.00
ALLIED STORAGE CONTAINERS	28955	10/12/2020	S20095966	20' STORAGE CONTAINER CONEX BOXES-PD	\$10,214.70
Remit to: FRESNO, CA					FYTD: \$10,354.78
ALMANZA, ANGIE MARINAJ	240417	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
ALVAREZ, HUMBERTO ANTONIO	240500	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
AMERICAN FORENSIC NURSES	28956	10/12/2020	73835	PHLEBOTOMY SVCS	\$995.00
Remit to: LA QUINTA, CA					FYTD: \$7,895.00
ANDRAWIS, AKMIL	240371	10/05/2020	R20-150067	ANIMAL SERVICES REFUND-SPAY/NEUTER AND RABIES DEPOSITS	\$95.00
Remit to: MORENO VALLEY, CA					FYTD: \$95.00

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ANIMAL PEST MANAGEMENT SERVICES, INC.	29063	10/26/2020	635400	PEST MANAGMENT SERVICE-SEPT 2020	\$2,947.80
		10/26/2020	635399	PEST MANAGMENT SERVICE-AUG 2020	
Remit to: CHINO, CA					FYTD: \$2,947.80
AROCHE, MIGUEL	240420	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
AUTOMATIC STOREFRONT SERVICE/E-Z AUTOMATED SYSTEMS	240466	10/26/2020	0031147	SLIDING GLASS DOOR REPAIR- CONF. & REC CTR	\$528.00
		10/26/2020	0031148	SLIDING GLASS DOOR PREV MAINT-LIBRARY	
Remit to: CHINO, CA					FYTD: \$7,538.46
AVANT GARDE	28911	10/05/2020	6411	HOME PROGRAM MANAGEMENT-AUG 2020	\$2,358.75
		10/05/2020	6412	HOME HABITAT FOR HUMANITY-AUG 2020	
		10/05/2020	6431	CDBG HABITAT FOR HUMANITY-AUG 2020	
	29064	10/26/2020	6474	HOME PROGRAM MANAGEMENT-SEPT 2020	\$743.75
		10/26/2020	6475	HOME HABITAT FOR HUMANITY-SEPT 2020	
Remit to: POMONA, CA					FYTD: \$5,185.00
BARSHA, JOHN	240506	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
BELMAN, HERIBERTO	240429	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00

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BIO-TOX LABORATORIES	240352	10/05/2020	40106	FORENSIC TOXICOLOGY TESTING SVCS FOR PD	\$7,921.00
		10/05/2020	40162	FORENSIC TOXICOLOGY TESTING SVCS FOR PD	
		10/05/2020	40175	FORENSIC TOXICOLOGY TESTING SVCS FOR PD	
		10/05/2020	40105	FORENSIC TOXICOLOGY TESTING SVCS FOR PD	
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$28,411.00
BMW MOTORCYCLES OF RIVERSIDE	28957	10/12/2020	6025389	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	\$2,732.93
		10/12/2020	6025522	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
		10/12/2020	6025477	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
		10/12/2020	6025500	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
		10/12/2020	6025510	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
		10/12/2020	6025521	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
	29025	10/19/2020	6025623	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
		10/19/2020	6025639	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	\$2,725.46
		10/19/2020	6025620	MAINT & REPAIRS-TRAFFIC MOTORCYCLE	
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$42,396.64

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
BOX SPRINGS MUTUAL WATER COMPANY	240353	10/05/2020	1087-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	\$675.50
		10/05/2020	195-5 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	1085-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	1086-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	1088-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	189-13 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	331-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	36-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	1084-1 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	45-4 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	721-1 9/24/20	WATER USAGE-TOWNGATE-SEPT 2020	
		10/05/2020	80-4 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
		10/05/2020	204-9 9/24/20	WATER ASSESSMENT ON VACANT LOTS OWNED BY THE HOUSING AUTHORITY	
Remit to: MORENO VALLEY, CA					FYTD: \$1,899.77
BRAUN BLAISING SMITH WYNNE, P.C.	28912	10/05/2020	19124	LEGAL SERVICES-MV UTILITY-AUGUST	\$926.29
Remit to: SACRAMENTO, CA					FYTD: \$1,408.09

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Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
BRIGHTVIEW LANDSCAPE SERVICES, INC.	29065	10/26/2020	7044137	LANDSCAPE MAINT-ZONE 09	\$2,343.94
		10/26/2020	7005317	LANDSCAPE MAINT-ZONES D, M & S	
Remit to: PASADENA, CA					FYTD: \$179,898.83
CALIFORNIA DEPT. OF TAX AND FEE ADMINISTRATION	240467	10/26/2020	3RD QTR 2020	ACCT# 31-000177 ELECTRICAL ENERGY SURCHARGE RETURN/JUL-SEP 2020	\$17,401.33
Remit to: SACRAMENTO, CA					FYTD: \$30,551.39
CALIFORNIA MUNICIPAL UTILITIES ASSOC.	240380	10/12/2020	19-0689	GDS-ENERGY EFFICIENCY POTENTIAL FORECASTING SVC-SEPT 20	\$1,974.03
Remit to: SACRAMENTO, CA					FYTD: \$13,422.09
CAMERON-DANIEL, P.C.	28959	10/12/2020	1170	LEGAL SERVICES-MV UTILITY	\$550.00
Remit to: SEBASTOPOL, CA					FYTD: \$4,427.50
CARINA BALLOUT	240464	10/19/2020	OCTOBER 2020	FORWARD MOVAL SMALL BUSINESS GRANT-COVID-19	\$7,500.00
Remit to: MORENO VALLEY, CA					FYTD: \$7,500.00
CARR-SUTTON, ROXANNE	240508	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
CASTANON, IRMA	240418	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
CHACON, TANYA	240454	10/19/2020	R20-151261	ANIMAL SERVICES REFUND-OVERPAYMENT ON WEB LICENSE	\$20.00
Remit to: MORENO VALLEY, CA					FYTD: \$20.00

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
CHANDLER ASSET MANAGEMENT, INC	28960	10/12/2020	2009MORENOVA	INVESTMENT MANAGEMENT SVCS-SEPT 2020	\$7,439.70
Remit to: SAN DIEGO, CA					<u>FYTD:</u> \$22,303.77
CINTAS FIRST AID & SAFETY	240381	10/12/2020	5033094026	FIRST AID KIT SUPPLIES-CORPORATE YARD MECHANIC SHOP	\$355.76
		10/12/2020	5033094092	FIRST AID KIT SUPPLIES-CITY HALL	
		10/12/2020	5033094098	FIRST AID KIT SUPPLIES-CORPORATE YARD WAREHOUSE	
		10/12/2020	5033200584	FIRST AID KIT SUPPLIES-SENIOR CENTER	
		10/12/2020	5033094013	FIRST AID KIT SUPPLIES-CONFERENCE AND RECREATION CENTER	
		10/12/2020	5033094071	FIRST AID KIT SUPPLIES-CORPORATE YARD SIGNS SIGNAL	
		10/12/2020	5033094020	FIRST AID KIT SUPPLIES-ANIMAL SHELTER	
		10/12/2020	5033094057	FIRST AID KIT SUPPLIES-CORPORATE YARD OFFICES	
		10/12/2020	5033200555	FIRST AID KIT SUPPLIES-RESOURCE CENTER	
		10/12/2020	5033094001	FIRST AID KIT SUPPLIES-COTTONWOOD GOLF COURSE	
	240468	10/26/2020	5033094080	FIRST AID KIT SUPPLIES-ANNEX	\$19.75
Remit to: CINCINNATI, OH					<u>FYTD:</u> \$818.84
CIVIC SOLUTIONS, INC	29026	10/19/2020	093020	PLANNING ENTITLEMENT AND PLAN CHECK SVCS	\$1,045.53
Remit to: MISSION VIEJO, CA					<u>FYTD:</u> \$3,566.79
COLONIAL SUPPLEMENTAL INSURANCE	240354	10/05/2020	7133069-1001289	EMPLOYEE SUPPLEMENTAL INSURANCE	\$7,598.50
	240469	10/26/2020	7133069-1101417	EMPLOYEE SUPPLEMENTAL INSURANCE	\$7,598.50
Remit to: COLUMBIA, SC					<u>FYTD:</u> \$38,135.90
CORODATA MEDIA STORAGE INC.	29027	10/19/2020	DS1295018	OFF-SITE MEDIA STORAGE-SEPTEMBER 2020	\$423.26
Remit to: LOS ANGELES, CA					<u>FYTD:</u> \$1,628.18

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
CORODATA RECORDS MANAGEMENT, INC.	28961	10/12/2020	RS4633920	RECORDS STORAGE-SEPT 2020	\$1,357.83
Remit to: POWAY, CA					<u>FYTD:</u> \$5,220.65
COSTAR REALTY INFORMATION, INC	28962	10/12/2020	112507126-1	COMMERCIAL REAL ESTATE DATABASE SVC-OCT 2020	\$1,500.63
Remit to: CHICAGO, IL					<u>FYTD:</u> \$6,002.52
COUNSELING TEAM, THE	240382	10/12/2020	77351	EMPLOYEE ASSISTANCE PROGRAM-SEPT 2020	\$1,667.00
Remit to: SAN BERNARDINO, CA					<u>FYTD:</u> \$6,668.00
COUNTS UNLIMITED, INC.	28963	10/12/2020	20317	TRAFFIC DATA COLLECTION	\$346.50
		10/12/2020	20308	TRAFFIC DATA COLLECTION	
		10/12/2020	20301	TRAFFIC DATA COLLECTION	
Remit to: CORONA, CA					<u>FYTD:</u> \$2,371.50

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
COUNTY OF RIVERSIDE	28914	10/05/2020	IT0000004243	APX 7500M DUAL BAND, HPD MODEM MAINT	\$2,494.75
	240383	10/12/2020	20-339834	RECORDATION DOCUMENT	\$42.50
	240384	10/12/2020	3068	REGISTERED VOTERS CONFIRMATION-CFD NO. 2014-01/AMEND NO. 49	\$175.00
			3072	REGISTERED VOTERS CONFIRMATION-CFD NO. 2014-01/AMEND NO. 52	
			3069	REGISTERED VOTERS CONFIRMATION-CFD NO. 1/ANNEX NO. 2020-54	
			3070	REGISTERED VOTERS CONFIRMATION-CFD NO. 2014-01/AMEND NO. 51	
	240470	10/26/2020	3071	REGISTERED VOTERS CONFIRMATION-CFD NO. 1/ANNEX NO. 2020-58	
			3086	REGISTERED VOTERS CONFIRMATION-CFD NO. 01/ANNEX NO. 2020-59	\$140.00
			3088	REGISTERED VOTERS CONFIRMATION-CFD NO. 2014/AMEND NO. 53	
	240492	10/26/2020	3087	REGISTERED VOTERS CONFIRMATION-CFD NO. 1/ANNEX NO. 2020-60	
3090			REGISTERED VOTERS CONFIRMATION-CFD NO. 1/ANNEX NO. 2020-61		
Remit to: RIVERSIDE, CA					FYTD: \$30,056.87
COVINGTON & BURLING LLP	29066	10/26/2020	60914212	LEGAL SERVICES	\$17,769.50
Remit to: WASHINGTON, DC					FYTD: \$129,692.50

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
CRIME SCENE STERI-CLEAN, LLC	28916	10/05/2020	41223	BIO HAZARD REMOVAL SERVICE	\$850.00
	28964	10/12/2020	41259	BIO HAZARD REMOVAL SERVICE	\$200.00
	29029	10/19/2020	41085B	BIO HAZARD REMOVAL SERVICE	\$850.00
Remit to: RANCHO CUCAMONGA, CA					<u>FYTD:</u> \$5,200.00
CROSSROAD MEGAWASH, INC DBA WASH BANK EXPRESS	29030	10/19/2020	001	CAR WASH SVC-PD	\$675.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$675.00
CSAC EIA	240412	10/15/2020	21990002	ADDITIONAL EWC 2020-21 CHARGES	\$985.79
Remit to: FOLSOM, CA					<u>FYTD:</u> \$171,675.79
DAILEY & WELLS COMMUNICATIONS, INC.	240385	10/12/2020	20BP090903	HANDHELD PSEC CONNECTED RADIOS-FIRE	\$9,348.00
Remit to: SAN ANTONIO, TX					<u>FYTD:</u> \$9,348.00
DATA TICKET, INC.	28917	10/05/2020	116310	ADMINISTRATIVE CITATIONS-CODE-AUG 2020	\$12,049.64
		10/05/2020	116863HH	PARKING CITATION PROCESSING-CODE-AUG 2020	
		10/05/2020	116863	PARKING CITATION PROCESSING-CODE-AUG 2020	
	28965	10/12/2020	117383	ADMIN CITATION-PD-SEPT 2020	\$205.50
	29068	10/26/2020	118013	PARKING CITATION PROCESSING-CODE-SEPT 2020	\$11,891.04
		10/26/2020	118013HH	PARKING HANDHELD DEVICES LEASE-AIR TIME-CODE-SEPT 2020	
		10/26/2020	117382	ADMINISTRATIVE CITATIONS-CODE-SEPT 2020	
		10/26/2020	117380	ADMIN CITATION PROCESSING-ANIMAL SVC-SEPT 2020	
Remit to: IRVINE, CA					<u>FYTD:</u> \$49,790.39
DELTA DENTAL OF CALIFORNIA	28918	10/05/2020	BE004104051	EMPLOYEE DENTAL INSURANCE-PPO	\$15,111.53
	29069	10/26/2020	BE004145424	EMPLOYEE DENTAL INSURANCE-PPO	\$14,996.93
Remit to: SAN FRANCISCO, CA					<u>FYTD:</u> \$76,096.04

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Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
DELTACARE USA	28919	10/05/2020	BE004104878	EMPLOYEE DENTAL INSURANCE-HMO	\$4,439.57
	29070	10/26/2020	BE004146253	EMPLOYEE DENTAL INSURANCE-HMO	\$4,439.57
Remit to: DALLAS, TX					FYTD: \$22,621.81
DEPARTMENT OF CONSERVATION	240471	10/26/2020	3RD QTR 2020	SMI FEES REPORT-3RD QTR ENDING 9/30/20	\$23,947.74
Remit to: SACRAMENTO, CA					FYTD: \$29,783.48
DISH DBS CORPORATION	240386	10/12/2020	86557282/OCT20	SATELLITE TV-FIRE STATION 99-10/01-10/30/20	\$135.36
Remit to: PALATINE, IL					FYTD: \$512.13
DIVISION OF THE STATE ARCHITECT	240387	10/12/2020	3RD QTR 2020-796	STATE PORTION-DISABILITY ACCESS & EDUCATION FEE REPORT 796	\$196.10
		10/12/2020	3RD QTR 2020-786	STATE PORTION-DISABILITY ACCESS & EDUCATION FEE REPORT 786	
Remit to: SACRAMENTO, CA					FYTD: \$379.70
DIXON, ERIC	240426	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
DIXON, JEFFERY	240503	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
DOBBS, KELLEEE	240372	10/05/2020	R20-147230	ANIMAL SERVICES REFUND-SPAY/NEUTER DEPOSIT	\$75.00
Remit to: MORENO VALLEY, CA					FYTD: \$75.00

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Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
E.R. BLOCK PLUMBING & HEATING, INC.	28921	10/05/2020	132489	BACKFLOW DEVICE TEST-ZONE D	\$1,183.83
Remit to: RIVERSIDE, CA					FYTD: \$2,812.59
EASTER SEALS OF SOUTHERN CA, INC	240405	10/12/2020	2002024.047	CONFERENCE & REC. CTR. RENTAL REFUND	\$443.90
Remit to: IRVINE, CA					FYTD: \$557.50
EASTERN MUNICIPAL WATER DISTRICT	240355	10/05/2020	SEPT-20 10/05/20	WATER CHARGES	\$6,203.21
		10/05/2020	AUG-20 10/05/20	WATER CHARGES	
Remit to: LOS ANGELES, CA					FYTD: \$999,517.36
ENCO UTILITY SERVICES MORENO VALLEY LLC	29031	10/19/2020	MFT-2020-44112	METER FEES-TEMPORARY	\$14,427.00
		10/19/2020	0402-MF-02405	METER FEES-REGULAR	
		10/19/2020	MFP-2020-44112	METER FEES-REGULAR	
		10/26/2020	40-501-2009	ACQUIRED SCE STREETLIGHTS MAINT SOLAR	
	29071	10/26/2020	0402-MF-02423	SYSTEM INSPECTION	\$7,279.08
Remit to: ANAHEIM, CA					FYTD: \$2,377,553.02
ENERGY SERVICE PARTNERS INC	240494	10/26/2020	BON20-0772	REFUND FEES-VOIDED BUILDING PERMIT-15745 AVENIDA DE CALMA	\$436.70
Remit to: TORRANCE, CA					FYTD: \$436.70
ESCALANTE, SHARI	240455	10/19/2020	R20-151345	ANIMAL SERVICES REFUND-DUPLICATE WEB LICENSE PAYMENT	\$15.00
Remit to: MORENO VALLEY, CA					FYTD: \$15.00

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ESPINOZA ROBLES, XITLALI J	240505	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
EUGENE RUIZ	240456	10/19/2020	OCTOBER 2020	FORWARD MOVAL SMALL BUSINESS GRANT-COVID-19	\$7,500.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$7,500.00
EXCLUSIVE TOWING	240356	10/05/2020	20-09539	EVIDENCE VEHICLE TOWING	\$225.00
	240388	10/12/2020	20-09681	EVIDENCE VEHICLE TOWING	\$281.25
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$1,856.25
FAIR HOUSING COUNCIL OF RIVERSIDE COUNTY, INC.	29072	10/26/2020	JULY-20 (LT)	LANDLORD/TENANT MEDIATION SVCS-CDBG	\$8,699.43
		10/26/2020	AUG-20 (LT)	LANDLORD/TENANT MEDIATION SVCS-CDBG	
		10/26/2020	JULY-20 (FH)	FAIR HOUSING DISCRIMINATION SVCS-CDBG	
		10/26/2020	AUG-20 (FH)	FAIR HOUSING DISCRIMINATION SVCS-CDBG	
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$14,358.95
FERRELLGAS LP	240435	10/19/2020	1112515743	PROPANE-CITY YARD	\$1,794.83
		10/19/2020	1112514874	PROPANE-FIRE STATION 6	
Remit to: DENVER, CO					<u>FYTD:</u> \$4,646.74
FIRST AMERICAN DATA TREE, LLC	240389	10/12/2020	20027760920	ONLINE SOFTWARE SUBSCRIPTION-SEPT 20	\$99.00
Remit to: PASADENA, CA					<u>FYTD:</u> \$396.00

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FIRST CHOICE SERVICES	29032	10/19/2020	679066	WATER PURIF UNIT RENTAL-FIRE STATION 91	\$619.65
		10/19/2020	679064	WATER PURIF UNIT RENTAL-FIRE STATION 58	
		10/19/2020	679060	WATER PURIF UNIT RENTAL-EMERGENCY OP'S CTR	
		10/19/2020	679065	WATER PURIF UNIT RENTAL-FIRE STATION 65	
		10/19/2020	679062	WATER PURIF UNIT RENTAL-FIRE STATION 6	
		10/19/2020	679067	WATER PURIF UNIT RENTAL-FIRE STATION 99	
		10/19/2020	679068	WATER PURIF UNIT RENTAL-LIBRARY	
		10/19/2020	679061	WATER PURIF UNIT RENTAL-FIRE STATION 2	
		10/19/2020	679232	WATER PURIF UNIT RENTAL-RAINBOW RIDGE	
		10/19/2020	679069	WATER PURIF UNIT RENTAL-PUBLIC SAFETY BUILDING	
		10/19/2020	679071	WATER PURIF UNIT RENTAL-TRANS TRAILER	
		10/19/2020	679059	WATER PURIF UNIT RENTAL-CITY YARD	
		10/19/2020	679063	WATER PURIF UNIT RENTAL-FIRE STATION 48	
		10/19/2020	679057	WATER PURIF UNIT RENTAL-CITY HALL 2ND FLOOR	
		10/19/2020	679058	WATER PURIF UNIT RENTAL-CONF & REC CTR	
		10/19/2020	679055	WATER PURIF UNIT RENTAL-ANNEX 1	
		10/19/2020	679230	WATER PURIF UNIT RENTAL-VAL VERDE CHILD CARE SITE	
		10/19/2020	679056	WATER PURIF UNIT RENTAL-CITY HALL 1ST FLOOR	
		10/19/2020	677626	WATER PURIF UNIT RENTAL-VAL VERDE CHILD CARE SITE	
		10/19/2020	679054	WATER PURIF UNIT RENTAL-ANIMAL SHELTER	
		10/19/2020	679070	WATER PURIF UNIT RENTAL-SENIOR CENTER	
Remit to: ONTARIO, CA					FYTD: \$2,822.85
FLINN, KATELYNN	240413	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00

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FOWLER, JOLEEN	240424	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
FRED'S GLASS & MIRROR, INC.	240473	10/26/2020	0159275	WINDOW SEALING REPAIR-CITY HALL	\$3,929.30
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$6,745.53
FRONTIER COMMUNICATIONS/FORMERLY VERIZON	29033	10/19/2020	7002Z183-S-20279	BACKBONE COMMUNICATIONS SERVICE 10/5/20-11/4/20	\$3,797.28
Remit to: ROCHESTER, NY					<u>FYTD:</u> \$15,135.30
FRONTIER COMMUNICATIONS/FORMERLY VERIZON CALIF.	240474	10/26/2020	081095-5/OCT20	FOREIGN EXCHANGE BUS LISTING-MV UTILITY	\$7.61
Remit to: CINCINNATI, OH					<u>FYTD:</u> \$30.41
FUENTES, MONICA M	240516	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
GALLOWAY, VERA	240427	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
GARCIA, YVONNE	240510	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00

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Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount	
GARDAWORLD	28969	10/12/2020	20449529	AMORED CAR SERVICES-CONF & REC CTR. AND ANIMAL SVCS- AUG 20	\$692.45	
		10/12/2020	10595137	AMORED CAR SRV-CONF & REC CTR., ANIMAL SVCS & CITY HALL- OCT 20		
Remit to: CHICAGO, IL					FYTD: \$2,648.10	
GLOBAL AUTOMATION SERVICES, INC.	240475	10/26/2020	36393	AC REPAIR-EMERGENCY OPS CENTER	\$411.44	
Remit to: YUCAIPA, CA					FYTD: \$10,150.51	
GONZALEZ, STEPHANIE	240428	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00	
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020		
Remit to: MORENO VALLEY, CA					FYTD: \$500.00	
GRACIANO, KRISTY	240373	10/05/2020	R20-150584	ANIMAL SERVICES REFUND-OVERPAYMENT ON WEB LICENSE	\$13.00	
Remit to: MORENO VALLEY, CA					FYTD: \$13.00	
GREEN, DEAZJAH	240419	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00	
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020		
Remit to: MORENO VALLEY, CA					FYTD: \$500.00	
GREENTECH LANDSCAPE, INC.	28971	10/12/2020	49682	LANDSCAPE MAINT-PARKS	\$11,593.00	
		29073	10/26/2020	49681	LANDSCAPE MAINT-ZONES 01, 01A, 8 & E7	\$9,910.41
		10/26/2020	49740	LANDSCAPE MAINT-ZONE 01 & E-7		
Remit to: LOS ANGELES, CA					FYTD: \$86,531.24	
GUIDA SURVEYING, INC.	240436	10/19/2020	46224	MORENO MDP LINE H-2 STORM DRAIN	\$15,732.76	
		10/19/2020	46428	MORENO MDP LINE H-2 STORM DRAIN		
Remit to: IRVINE, CA					FYTD: \$15,732.76	

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GUZMAN, MAHRYA	240425	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
HASCO HEATING AIR CONDITIONING SERVICE COMPANY	29035	10/19/2020	95260	HVAC REPAIR - FIRE STATION 58	\$4,192.20
		10/19/2020	95281	HVAC REPAIR - FIRE STATION 2	
		10/19/2020	95278	HVAC REPAIR - FIRE STATION 48	
		10/19/2020	95310	HVAC REPAIR - FIRE STATION 99	
	29075	10/26/2020	95849	HVAC REPAIR-SENIOR CENTER	\$16,014.56
		10/26/2020	95888	HVAC REPAIRS-CONFERENCE & REC. CENTER	
		10/26/2020	95599	HVAC REPAIR-FIRE STATION 2	
Remit to: RIVERSIDE, CA					FYTD: \$78,139.63
HITACHI VANTARA LLC	28923	10/05/2020	7282019	SOFTWARE LICENSES-AMPHITHEATER	\$3,303.72
Remit to: SANTA CLARA, CA					FYTD: \$3,303.72
HLP, INC.	29076	10/26/2020	18716	WEB LICENSE MONTHLY SVC FEE	\$126.00
Remit to: LITTLETON, CO					FYTD: \$32,321.10
HR GREEN PACIFIC INC.	28973	10/12/2020	138382	ON-CALL TRAFFIC ENGINEERING SERVICES	\$5,478.24
Remit to: DES MOINES, IA					FYTD: \$62,778.69
HUGHES NETWORK SYSTEMS, LLC	240437	10/19/2020	B1-366827834	INTERNET SVCS 9/30-10/30/20	\$92.34
Remit to: CHICAGO, IL					FYTD: \$369.36

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HUNSAKER & ASSOCIATES IRVINE, INC	29078	10/26/2020	20080364	PLAN CHECK SVCS-PEN19-0107	\$432.00
		10/26/2020	20080363	PLAN CHECK SVCS-PEN18-0232	
Remit to: IRVINE, CA					<u>FYTD:</u> \$432.00
IMEG CORP./FORMERLY TTG ENGINEERS	28974	10/12/2020	20001409.00-4	HVAC REPLACEMENT CONSULTATION-PUBLIC SAFETY BLDG.	\$11,765.00
Remit to: ROCK ISLAND, IL					<u>FYTD:</u> \$48,125.00
INGRAM, ROBIN	240374	10/05/2020	R20-150811	ANIMAL SERVICES REFUND-OVERPAYMENT ON WEB LICENSE	\$13.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$13.00

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>		
INLAND EMPIRE PROPERTY SERVICE, INC	28924	10/05/2020	20131	NUISANCE ABATEMENT SVCS-264-236-001	\$6,374.90		
		10/05/2020	201162	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 256-182-055			
		10/05/2020	201185	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 297-150-031			
		10/05/2020	201186	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 478-272-003			
		10/05/2020	20124	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 478-120-027			
		10/05/2020	20130	NUISANCE ABATEMENT SVCS-292-032-011			
		10/05/2020	201184	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 260-460-007			
		10/05/2020	201017	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 481-120-020			
		28975	10/12/2020	20132		NUISANCE ABATEMENT SVCS-12348 BREWSTER DR	\$506.06
		29037	10/19/2020	201194		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 474-721-004	\$7,838.64
10/19/2020	201200		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 297-130-058				
10/19/2020	201197		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 474-180-030				
10/19/2020	201196		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 474-721-002				
10/19/2020	201195		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 474-721-003				
10/19/2020	201199		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 297-170-083				
10/19/2020	201198		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 474-130-014				
10/19/2020	201190		WEED ABATEMENT SVCS-FIRE PREVENTION-APN 478-230-017				
10/19/2020	201201	WEED ABATEMENT SVCS-FIRE PREVENTION-APN 292-032-011					
Remit to: MORENO VALLEY, CA				FYTD:	\$140,363.79		

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INLAND OVERHEAD DOOR COMPANY	28976	10/12/2020	45507	ROLL UP DOOR REPAIR-FIRE STATION 2	\$2,129.50
	29038	10/12/2020	45743	ROLL UP DOOR REPAIR-FIRE STATION 2	
		10/19/2020	45589	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 2	\$3,864.00
		10/19/2020	45895	GATE REPAIR - FIRE STATION 2	
		10/19/2020	45591	PREVENTATIVE MAINTENANCE DOORS & GATES-UTILITY FIELD OFFICE	
		10/19/2020	45590	PREVENTATIVE MAINTENANCE DOORS & GATES-PUBLIC SAFETY BLDG.	
		10/19/2020	45583	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 99	
		10/19/2020	45582	PREVENTATIVE MAINTENANCE DOORS & GATES-CITY YARD	
		10/19/2020	45593	PREVENTATIVE MAINTENANCE DOORS & GATES-ANNEX 1	
		10/19/2020	45588	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 6	
		10/19/2020	45584	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 91	
		10/19/2020	45585	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 65	
		10/19/2020	45586	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 58	
		10/19/2020	45587	PREVENTATIVE MAINTENANCE DOORS & GATES-FIRE STATION 48	
10/19/2020	45594	PREVENTATIVE MAINTENANCE DOORS & GATES-ANIMAL SHELTER			
Remit to: COLTON, CA					<u>FYTD:</u> \$8,345.50
INTERWEST CONSULTING GROUP	29079	10/26/2020	62039	PLAN CHECK SVCS-VIA DEL LAGO	\$16,346.25
		10/26/2020	62040	CONSTRUCTION INSPECTION SVCS-JULY 2020	
Remit to: BOULDER, CO					<u>FYTD:</u> \$107,491.25
IRIS PARTNERS, LLC	29080	10/26/2020	NOVEMBER 2020	LEASE PAYMENT-LIBRARY-NOVEMBER 2020	\$11,666.67
Remit to: UPLAND, CA					<u>FYTD:</u> \$46,666.68

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JITTERZ GOURMET COFFEE	28926	10/05/2020	SEPT 25, 2020	SENIOR EATS PROGRAM-9/21-9/25/20	\$4,275.00
	28977	10/12/2020	OCT 2, 2020	SENIOR EATS PROGRAM-9/28-10/02/20	\$4,275.00
	29039	10/19/2020	OCT 9, 2020	SENIOR EATS PROGRAM-10/05-10/09/20	\$4,275.00
	29081	10/26/2020	OCT 16, 2020	SENIOR EATS PROGRAM-10/12-10/16/20	\$4,275.00
Remit to: REDLANDS, CA					FYTD: \$71,550.00
JTB SUPPLY CO., INC.	28978	10/12/2020	107959	TRAFFIC SIGNAL MAINT SUPPLIES	\$10,470.30
		10/12/2020	107901	TRAFFIC SIGNAL MAINT SUPPLIES	
Remit to: ORANGE, CA					FYTD: \$26,535.75
JVS RESTAURANTS DBA LOS ZAPATAS MEXICAN RESTAURANT	28927	10/05/2020	SEPT 25, 2020	SENIOR EATS PROGRAM-9/21-9/25/20	\$3,825.00
	28979	10/12/2020	OCT 2, 2020	SENIOR EATS PROGRAM-9/28-10/02/20	\$3,825.00
	29040	10/19/2020	OCT 9, 2020	SENIOR EATS PROGRAM-10/05-10/09/20	\$3,825.00
	29082	10/26/2020	OCT 16, 2020	SENIOR EATS PROGRAM-10/12-10/16/20	\$3,825.00
Remit to: MORENO VALLEY, CA					FYTD: \$65,025.00
KHAN, TAHREEM	240416	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
KONICA MINOLTA BUSINESS SOLUTIONS, USA	28928	10/05/2020	36055075	COPIER LEASE-CITY WIDE-SEPT 2020	\$11,064.12
		10/05/2020	36213343	COPIER LEASE-CITY CLERK	
Remit to: PASADENA, CA					FYTD: \$33,320.56
KOONER, GURJOT	240423	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00

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LANDREY, FELICIA	240504	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
LETNER ROOFING COMPANY	240476	10/26/2020	58587	ROOF REPAIRS-FIRE STATION 99	\$22,680.00
Remit to: ORANGE, CA					FYTD: \$22,680.00
LEVEL 3 COMMUNICATIONS/FORMERLY TW TELCOM	29084	10/26/2020	161087435	LOCAL/LONG DISTANCE CALLS 10/17-11/12/20	\$3,168.56
		10/26/2020	161087435(a)	INTERNET & DATA SVCS 10/17-11/12/20	
Remit to: BROOMFIELD, CO					FYTD: \$24,600.08
LEXISNEXIS PRACTICE MANAGEMENT	28929	10/05/2020	3092779445	LEGAL RESEARCH TOOLS-JULY 2020	\$2,208.00
		10/05/2020	3092809685	LEGAL RESEARCH TOOLS-AUGUST 2020	
	29041	10/19/2020	3092894917	LEGAL RESEARCH TOOLS-SEPT 2020	\$1,104.00
Remit to: CHICAGO, IL					FYTD: \$4,416.00
LOPEZ, MARIA	240457	10/19/2020	R20-150725	ANIMAL SERVICES REFUND-RABIES DEPOSIT	\$20.00
Remit to: WHITTIER, CA					FYTD: \$20.00
LYONS SECURITY SERVICE, INC.	28980	10/12/2020	28269	SECURITY GUARD SVCS-CITY HALL-SEPT 20-COVID-19	\$16,122.08
		10/12/2020	28270	SECURITY GUARD SVCS-CONF & REC CTR-SEPT 2020	
		10/12/2020	28272	SECURITY GUARD SVCS-LIBRARY-SEPT 2020	
		10/12/2020	28268	SECURITY GUARD SVCS-CITY HALL-SEPT 20	
		10/12/2020	28271	SECURITY GUARD SVCS-ERC-SEPT 20-COVID-19	
Remit to: ANAHEIM, CA					FYTD: \$78,033.23

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Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
MADRIGAL , JERONIMO	240411	10/12/2020	OCTOBER 2020	FORWARD MOVAL SMALL BUSINESS GRANT-COVID-19	\$7,500.00
Remit to: MORENO VALLEY, CA					FYTD: \$7,500.00
MARCH JOINT POWERS AUTHORITY	240390	10/12/2020	52162	GAS CHARGES-M.A.R.B. BUILDING 823-AUG. 2020	\$50.74
		10/12/2020	52165	GAS CHARGES-M.A.R.B. BUILDING 938-AUG. 2020	
Remit to: RIVERSIDE, CA					FYTD: \$199.15
MARGARITAS GRILL RESTAURANT & CATERING, LLC	240358	10/05/2020	SEPT. 25, 2020	SENIOR EATS PROGRAM 9/21-9/25/20	\$4,050.00
	240391	10/12/2020	OCTOBER 2, 2020	SENIOR EATS PROGRAM 9/28-10/2/20	
	240438	10/19/2020	OCTOBER 9, 2020	SENIOR EATS PROGRAM 10/05-10/09/20	\$4,050.00
	240477	10/26/2020	OCTOBER 16, 2020	SENIOR EATS PROGRAM 10/12-10/16/20	\$4,050.00
Remit to: MORENO VALLEY, CA					FYTD: \$68,175.00
MBIA MUNISERVICES COMPANY	28981	10/12/2020	INV06-009982	CAFR STATISTICAL REPORTS - SEPT. 2020	\$1,900.00
Remit to: BIRMINGHAM, AL					FYTD: \$1,900.00
MCKINLEY ELEVATOR CORPORATION	240439	10/19/2020	A132351-IN	MAINTENANCE FOR WHEELCHAIR LIFT AT SENIOR CENTER	\$500.00
		10/19/2020	A132350-IN	MAINTENANCE FOR WHEELCHAIR LIFT AT CONFERENCE & REC. CENTER	
Remit to: IRVINE, CA					FYTD: \$500.00
MENCHACA, CEASER OMAR	240515	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00

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MERCHANTS BUILDING MAINTENANCE, LLC.	28931	10/05/2020	598550	COVID-19 DISINFECTANT CLEANING SERVICES-CITY YARD/VEHICLES	\$500.00
	29043	10/19/2020	599473	CARPET EXTRACTION-CITY HALL	\$220.00
Remit to: MONTEREY PARK, CA					<u>FYTD:</u> \$136,127.88
MGT OF AMERICA, LLC	29005	10/15/2020	38217	FULL COST ALLOCATION PLAN - FINAL DRAFT	\$2,062.50
Remit to: TAMPA, FL					<u>FYTD:</u> \$18,007.50
MIRANDA, JONATHAN	240517	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
MONTGOMERY PLUMBING INC	240478	10/26/2020	091520	EMERGENCY PLUMBING REPAIRS-CITY HALL	\$1,250.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$29,450.00
MORENO BEACH DENTAL	240375	10/05/2020	SEPTEMBER 2020	FORWARD MOVAL SMALL BUSINESS GRANT-COVID-19	\$7,500.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$7,500.00
MORENO VALLEY HISTORICAL SOCIETY	240406	10/12/2020	CK NO. 238947	REISSUE UNCLAIMED CK/REFUND SECURITY DEPOSIT	\$500.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
MORENO VALLEY MALL HOLDING, LLC	29085	10/26/2020	NOV. 2020 RENT	NOVEMBER 2020 RENT PAYMENT FOR SP. 2078-M.V. LIBRARY BRANCH	\$6,874.54
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$27,498.16

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MORENO VALLEY TOW & RADIATOR	240359	10/05/2020	11485	EVIDENCE TOWING FOR PD	\$225.00
	240440	10/19/2020	11595	EVIDENCE TOWING FOR PD	\$225.00
	240479	10/26/2020	11677	EVIDENCE TOWING FOR PD	\$225.00
Remit to: MORENO VALLEY, CA					FYTD: \$2,443.75
MORENO, MICHAEL DOMINICK	240512	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
MORUA, MICHELLE	240431	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
MOVAL ENTERPRISES INC. DBA MARINAJ CATERING	28932	10/05/2020	SEPT. 25, 2020	SENIOR EATS PROGRAM 9/21-9/25/20	\$3,285.00
				SENIOR EATS PROGRAM 9/28-10/2/20	
				SENIOR EATS PROGRAM 10/05-10/09/20	
				SENIOR EATS PROGRAM 10/12-10/16/20	
Remit to: MORENO VALLEY, CA					FYTD: \$58,275.00
MPULSE MAINTENANCE SOFTWARE, INC.	29087	10/26/2020	20093069	MSP SOFTWARE MAINTENANCE AGREEMENT 12/16/20-12/15/21	\$6,226.95
Remit to: EUGENE, OR					FYTD: \$6,226.95
MURILLO, KATHERINE	240507	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00

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NARANJO, ANGELA CELESTE	240415	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
NBS GOVERNMENT FINANCE GROUP	28933	10/05/2020	720000051	CONSULTING SERVICES-CFD BOUNDARY MAP PREPARATION	\$1,200.00
		10/05/2020	720000052	CONSULTING SERVICES-CFD BOUNDARY MAP PREPARATION	
		10/05/2020	720000053	CONSULTING SERVICES-CFD BOUNDARY MAP PREPARATION	
Remit to: TEMECULA, CA					<u>FYTD:</u> \$2,800.00
NKWOCHA, NKEIRU CHELSEA	240513	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
NORCAL PERMIT SERVICES	240376	10/05/2020	130891	REFUND-DUPLICATE PAYMENT/FEE CODE 5003	\$104.00
Remit to: CHULA VISTA, CA					<u>FYTD:</u> \$104.00
PACIFIC PRODUCTS & SERVICES, INC	240361	10/05/2020	27497	SIGN POSTS, ANCHORS, SLEEVES, & DRIVE RIVETS	\$11,141.35
Remit to: ANAHEIM, CA					<u>FYTD:</u> \$11,141.35
PACIFIC RESTORATION GROUP, INC.	240362	10/05/2020	1702905	LANDSCAPE MAINT.- SR-60/NASON INTERCHANGE	\$4,342.00
Remit to: PERRIS, CA					<u>FYTD:</u> \$37,750.00
PACIFIC TELEMAGEMENT SERVICES	28984	10/12/2020	2054366	PAY PHONE SERVICES-NOV. 2020	\$128.28
Remit to: SAN RAMON, CA					<u>FYTD:</u> \$638.40

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City of Moreno Valley
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Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
PACIFIC UTILITY INSTALLATION, INC	28985	10/12/2020	22011R	OFFLOAD STREETLIGHTS PURCHASED BY MV UTILITY	\$759.00
Remit to: ANAHEIM, CA					FYTD: \$10,374.90
PADILLA, GLORIA YOULANDA	240518	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
PAINTING BY ZEB BODE	28986	10/12/2020	20202909	PAINTING SERVICES-FIRE STATION 6	\$5,200.00
Remit to: NORCO, CA					FYTD: \$13,115.00
PARSONS TRANSPORTATION GROUP, INC.	29045	10/19/2020	2010A010	SR-60/MORENO BEACH IC PHASE 2	\$13,057.84
Remit to: IRVINE, CA					FYTD: \$112,805.12
PECHANGA BAND OF LUISENO MISSION INDIANS	240441	10/19/2020	13661	PECHANGA 804 0016 MONITORING	\$963.70
Remit to: TEMECULA, CA					FYTD: \$963.70
PEDLEY SQUARE VETERINARY CLINIC	29088	10/26/2020	SEP-2020	VETERINARY SERVICES-MV ANIMAL SHELTER	\$6,402.50
Remit to: RIVERSIDE, CA					FYTD: \$25,567.30

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
PEPE'S TOWING	240393	10/12/2020	94720	EVIDENCE TOWING FOR PD	\$550.00
		10/12/2020	94724	EVIDENCE TOWING FOR PD	
		10/12/2020	94719	EVIDENCE TOWING FOR PD	
	240480	10/26/2020	95239	EVIDENCE TOWING FOR PD	\$787.50
		10/26/2020	95489	EVIDENCE TOWING FOR PD	
		10/26/2020	94314	EVIDENCE TOWING FOR PD	
Remit to: MORENO VALLEY, CA					FYTD: \$2,518.75
PERCEPTIVE ENTERPRISES, INC.	28987	10/12/2020	3648	PROFESSIONAL DBE CONSULTING SERVICES	\$7,482.00
Remit to: LOS ANGELES, CA					FYTD: \$22,902.00
PLACEWORKS, INC	28935	10/05/2020	3180	MISCELLANEOUS SERVICES	\$24.00
Remit to: SANTA ANA, CA					FYTD: \$24.00
PLANETBIDS, INC.	240363	10/05/2020	0320101	VENDOR & BID MANAGEMENT SYSTEM SERVICES AND FEES 8/31/18-8/31/19	\$14,557.00
		10/05/2020	820289	VENDOR & BID MANAGEMENT SYSTEM SERVICES AND FEES 8/31/19-8/31/20	
Remit to: STUDIO CITY, CA					FYTD: \$14,557.00
POSADAS, MARIBEL	240458	10/19/2020	R20-151198	ANIMAL SERVICES REFUND-OVERPAYMENT ON WEB LICENSE	\$20.00
Remit to: MORENO VALLEY, CA					FYTD: \$20.00
PRESLEY, LORAINÉ	240459	10/19/2020	R20-149079	ANIMAL SERVICES REFUND-SPAY/NEUTER DEPOSIT	\$75.00
Remit to: MORENO VALLEY, CA					FYTD: \$75.00
PRIME ACTUARIAL CONSULTING DBA BICKMORE ACTUARIAL	28936	10/05/2020	27915	WORKERS COMP ACTUARIAL REVIEW SERVICE	\$6,500.00
Remit to: MEDFORD, OR					FYTD: \$6,500.00

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PROFESSIONAL COMMUNICATIONS NETWORK PCN	240481	10/26/2020	157600250	LIVE ANSWERING SERVICE FOR ROTATIONAL TOW PROGRAM	\$534.31
Remit to: RIVERSIDE, CA					FYTD: \$3,158.36
PROMENTORY PARK HOMEOWNERS ASSOCIATION	240460	10/19/2020	2002028.047	CONFERENCE & REC. CTR. RENTAL REFUND	\$225.00
Remit to: CORONA, CA					FYTD: \$350.00
PROMENTORY POINTE HOMEOWNERS ASSOCIATION	240461	10/19/2020	2002027.047	CONFERENCE & REC. CTR. RENTAL REFUND	\$350.00
Remit to: CORONA, CA					FYTD: \$475.00

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
PRUDENTIAL OVERALL SUPPLY	28937	10/05/2020	23053212	UNIFORM RENTAL & LAUNDERING SVC.-CONCRETE MAINT. STAFF	\$738.57
		10/05/2020	23027806	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23053209	UNIFORM RENTAL & LAUNDERING SVC.-TRAFFIC SIGNAL MAINT. STAFF	
		10/05/2020	23053211	UNIFORM RENTAL & LAUNDERING SVC.-STREET SWEEPING STAFF	
		10/05/2020	23020917	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23053207	UNIFORM RENTAL & LAUNDERING SVC.-STREET MAINT. STAFF	
		10/05/2020	23050103	UNIFORM RENTAL & LAUNDERING SVC.-TRAFFIC SIGNAL MAINT. STAFF	
		10/05/2020	23024370	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23017466	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23037615	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23053206	UNIFORM RENTAL & LAUNDERING SVC.-VEHICLE/EQUIPMENT MAINT. STAFF	
		10/05/2020	23024369	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23053203	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23017467	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23050104	UNIFORM RENTAL & LAUNDERING SVC.-CFD #1 STAFF	
		10/05/2020	23050102	UNIFORM RENTAL & LAUNDERING SVC.-SIGNS & STRIPING STAFF	
		10/05/2020	23050097	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23046993	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23046992	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23050098	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23050108	UNIFORM RENTAL & LAUNDERING SVC.-PARKS MAINT. STAFF	

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
PRUDENTIAL OVERALL SUPPLY		10/05/2020	23044093	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23044092	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23040973	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23034611	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23053213	UNIFORM RENTAL & LAUNDERING SVC.-GRAFFITI REMOVAL STAFF	
		10/05/2020	23053215	UNIFORM RENTAL & LAUNDERING SVC.-TREE MAINT. STAFF	
		10/05/2020	23027807	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23040972	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
		10/05/2020	23037616	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23034612	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23053208	UNIFORM RENTAL & LAUNDERING SVC.-SIGNS & STRIPING STAFF	
		10/05/2020	23053204	UNIFORM RENTAL & LAUNDERING SVC.-CITY YARD SECURITY GUARD	
		10/05/2020	23020916	UNIFORM RENTAL & LAUNDERING SVC.-FACILITIES MAINT. STAFF	
	29046	10/19/2020	23059398	UNIFORM RENTAL & LAUNDERING SVC.-PARKS MAINT. STAFF	\$122.10
		10/19/2020	23053210	UNIFORM RENTAL & LAUNDERING SVC.-CFD #1 STAFF	
		10/19/2020	23053214	UNIFORM RENTAL & LAUNDERING SVC.-PARKS MAINT. STAFF	
		10/19/2020	23059394	UNIFORM RENTAL & LAUNDERING SVC.-CFD #1 STAFF	

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
PRUDENTIAL OVERALL SUPPLY	29089	10/26/2020	23059395	UNIFORM RENTAL & LAUNDERING SVC.-STREET SWEEPING STAFF	\$391.80
		10/26/2020	23059392	UNIFORM RENTAL & LAUNDERING SVC.-SIGNS & STRIPING STAFF	
		10/26/2020	23059396	UNIFORM RENTAL & LAUNDERING SVC.-CONCRETE MAINT. STAFF	
		10/26/2020	23059399	UNIFORM RENTAL & LAUNDERING SVC.-TREE MAINT. STAFF	
		10/26/2020	23062511	UNIFORM RENTAL & LAUNDERING SVC.-VEHICLE/EQUIPMENT MAINT. STAFF	
		10/26/2020	23059393	UNIFORM RENTAL & LAUNDERING SVC.-TRAFFIC SIGNAL MAINT. STAFF	
		10/26/2020	23059390	UNIFORM RENTAL & LAUNDERING SVC.-VEHICLE/EQUIPMENT MAINT. STAFF	
		10/26/2020	23062513	UNIFORM RENTAL & LAUNDERING SVC.-SIGNS & STRIPING STAFF	
		10/26/2020	23062514	UNIFORM RENTAL & LAUNDERING SVC.-TRAFFIC SIGNAL MAINT. STAFF	
		10/26/2020	23059391	UNIFORM RENTAL & LAUNDERING SVC.-STREET MAINT. STAFF	
		10/26/2020	23062516	UNIFORM RENTAL & LAUNDERING SVC.-STREET SWEEPING STAFF	
		10/26/2020	23062517	UNIFORM RENTAL & LAUNDERING SVC.-CONCRETE MAINT. STAFF	
		10/26/2020	23062518	UNIFORM RENTAL & LAUNDERING SVC.-GRAFFITI REMOVAL STAFF	
		10/26/2020	23062520	UNIFORM RENTAL & LAUNDERING SVC.-TREE MAINT. STAFF	
		10/26/2020	23059397	UNIFORM RENTAL & LAUNDERING SVC.-GRAFFITI REMOVAL STAFF	
10/26/2020	23062512	UNIFORM RENTAL & LAUNDERING SVC.-STREET MAINT. STAFF			
Remit to: RIVERSIDE, CA					FYTD: \$5,160.99
RADAR SHOP, THE	240394	10/12/2020	13126	LASER/LIDAR UNITS RECERTIFICATION	\$2,180.00
Remit to: WICHITA, KS					FYTD: \$2,180.00

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RANCHO PACIFIC GLOBAL PARTNERS INC, ASH ETEMADIAN	240407	10/12/2020	127569	40% REFUND-PEN 20-0048	\$4,370.40
Remit to: RIVERSIDE, CA					FYTD: \$4,370.40
RAVSTEN, JULIE	240462	10/19/2020	R20-151060	ANIMAL SERVICES REFUND-OVERPAYMENT ON WEB LICENSE	\$20.00
Remit to: MORENO VALLEY, CA					FYTD: \$20.00
RC STORE MAINTENANCE, INC	240442	10/19/2020	417640	LIBRARY COUNTER TOP REPAIR - IRIS LIBRARY	\$240.26
Remit to: CORONA, CA					FYTD: \$240.26
READY REFRESH BY NESTLE	29091	10/26/2020	00J0035449180	BOTTLED WATER COOLER RENTAL-ARMADA ELEMENTARY/CHILD CARE	\$4.29
Remit to: LOUISVILLE, KY					FYTD: \$34.13
RICK ENGINEERING COMPANY	28988	10/12/2020	77143	CITYWIDE PAVEMENT REHAB (FY 18/19)	\$985.50
Remit to: RIVERSIDE, CA					FYTD: \$7,389.00
RIGHTWAY SITE SERVICES, INC.	240395	10/12/2020	269266	PORTABLE RESTROOM AND WASH STATIONS RENTALS AT POLICE STATION	\$689.45
	240443	10/19/2020	271372	PORTABLE RESTROOMS/WASH STATIONS FOR FARMERS MARKET AT CRC	\$1,205.91
		10/19/2020	269956	PORTABLE RESTROOMS RENTAL-EQUESTRIAN CENTER	
		10/19/2020	270023	PORTABLE RESTROOMS RENTAL/SERVICE-MAINT. & OPS. DIVISION	
		10/19/2020	269955	PORTABLE RESTROOM RENTAL-COTTONWOOD GOLF COURSE	
Remit to: LAKE ELSINORE, CA					FYTD: \$10,458.78

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
RIOS, DANA	240493	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
RIVERSIDE COUNTY HABITAT CONSERVATION	240396	10/12/2020	3RD QTR 2020	STEPHEN'S KANGAROO RAT MITIGATION FEES FOR QTR ENDING 9/30/2020	\$18,514.00
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$45,224.00
RIVERSIDE COUNTY OFFICE OF EDUCATION	240482	10/26/2020	2020-1305/1003	TRANSLATION SVCS-PW/TRAFFIC	\$348.40
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$348.40
RIVERSIDE MEDICAL CLINIC	28989	10/12/2020	700000183 10/02	EMPLOYMENT PHYSICALS/DRUG SCREENINGS	\$960.00
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$1,788.00
RIVERSIDE UNIVERSITY HEALTH SYSTEMS - MEDICAL CTR	29047	10/19/2020	1156	SART EXAMS BILLING FOR PD - SEP. 2020	\$2,400.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$15,600.00
ROADPOST USA INC DBA BLUECOSMO	29092	10/26/2020	BU01259148	SATELLITE PHONE SERVICE PLAN-FIRE	\$548.00
Remit to: SEATTLE, WA					<u>FYTD:</u> \$2,192.00
ROBERT HALF INTERNATIONAL	28990	10/12/2020	56414333	APPLICATIONS ANALYST TEMP-W.E. 9/25/20 (J. PERLAS)	\$1,907.40
Remit to: SAN RAMON, CA					<u>FYTD:</u> \$1,907.40
ROMAN TINT, INC	28991	10/12/2020	2750	ROLLER SHADE INSTALLATION-EMPLOYMENT RESOURCE CTR.	\$3,365.03
Remit to: RIALTO, CA					<u>FYTD:</u> \$10,457.20

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RSG, INC	29094	10/26/2020	I006556	AFFORDABLE HOUSING COMPLIANCE MONITORING SERVICES-SEP. 2020	\$6,175.00
Remit to: IRVINE, CA					<u>FYTD:</u> \$19,738.75
RUIZ, RAMON	240495	10/26/2020	R20-151170	ANIMAL SERVICES REFUND-DUPLICATE WEB LICENSE PAYMENT	\$15.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$15.00
SAFEWAY SIGN CO.	28992	10/12/2020	50555	TRAFFIC SIGNS/HARDWARE	\$15,661.87
		10/12/2020	50556	TRAFFIC SIGNS/HARDWARE	
Remit to: ADELANTO, CA					<u>FYTD:</u> \$24,303.83
SANCHEZ, ESMERALDA	240432	10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
SAVE, JOSEPHINE NINA	240511	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
SEARLE CREATIVE GROUP, LLC	29095	10/26/2020	20355	WEBSITE HOSTING & MAINTENANCE-SEP. 2020	\$5,791.25
		10/26/2020	20328	WEBSITE HOSTING & MAINTENANCE-AUG. 2020	
		10/26/2020	20292	WEBSITE HOSTING & MAINTENANCE-JUL. 2020	
Remit to: VENTURA, CA					<u>FYTD:</u> \$6,391.25

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SECURITY LOCK & KEY	29096	10/26/2020	D30322	EXTRA KEYS FOR IRIS PLAZA LIBRARY BRANCH	\$884.40
		10/26/2020	D30313	LOCK INSTALLATIONS & RE-KEYING FOR IRIS PLAZA LIBRARY BRANCH	
		10/26/2020	30351	ADDITIONAL KEYS FOR IRIS PLAZA LIBRARY BRANCH	
		10/26/2020	30378	LOCK REPAIRS & ADDITIONAL KEYS FOR IRIS PLAZA LIBRARY BRANCH	
Remit to: RIVERSIDE, CA					FYTD: \$1,017.96
SEMPER SOLARIS	240496	10/26/2020	BON20-0775	REFUND OVER-ASSESSED SB1473 FEE-BUILDING PERMIT-11540 SEAPORT	\$331.00
Remit to: RIVERSIDE, CA					FYTD: \$331.00
SIGNS BY TOMORROW	28938	10/05/2020	25544	UPDATE & INSTALLATION OF PUBLIC HEARING SIGN	\$1,115.70
		10/05/2020	25545	PRODUCE & INSTALLATION OF PUBLIC HEARING SIGNS	
Remit to: MURRIETA, CA					FYTD: \$1,115.70
SILVA RICO, BRENDA DENISSE	240422	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
SITEIMPROVE, INC.	28993	10/12/2020	67736 / CM 69788	SUBSCRIPTION-ANNUAL FEE 7/25/20 TO 7/24/21 (CREDIT MEMO APPLIED)	\$4,980.00
Remit to: MINNEAPOLIS, MN					FYTD: \$4,980.00
SKY PUBLISHING	29098	10/26/2020	20-6_016	FULL PAGE MAGAZINE AD-FLOOD PLAIN MGMT PUBLIC SVC MSG/2020 ISS 6	\$2,800.00
		10/26/2020	20-5_125	FULL PAGE MAGAZINE AD-FLOOD PLAIN MGMT PUBLIC SVC MSG/2020 ISS 5	
Remit to: MORENO VALLEY, CA					FYTD: \$2,800.00

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SNST ENTERPRISE INC. DBA OISHII SUSHI AND TERIYAKI	28939	10/05/2020	SEPT. 25, 2020	SENIOR EATS PROGRAM 9/21-9/25/20	\$3,600.00
	28994	10/12/2020	OCTOBER 2, 2020	SENIOR EATS PROGRAM 9/28-10/2/20	\$3,600.00
	29049	10/19/2020	OCTOBER 9, 2020	SENIOR EATS PROGRAM 10/05-10/09/20	\$3,600.00
	29099	10/26/2020	OCTOBER 16, 2020	SENIOR EATS PROGRAM 10/12-10/16/20	\$3,600.00
Remit to: MORENO VALLEY, CA					FYTD: \$61,200.00
SOBOBA BAND OF LUISENO INDIANS	240444	10/19/2020	002	SOBOBA 804 0016 MONITORING	\$1,837.50
Remit to: SAN JACINTO, CA					FYTD: \$4,950.00
SOUTH COAST AIR QUALITY MANAGEMENT DISTRICT	240445	10/19/2020	3717671	EMISSIONS FEES INVOICE-ANIMAL SHELTER	\$557.42
		10/19/2020	3715045	ANNUAL RENEWAL FEES-ANIMAL SHELTER	
Remit to: DIAMOND BAR, CA					FYTD: \$1,636.56
SOUTHERN CALIFORNIA EDISON	240364	10/05/2020	SEP-20 10/5/20	ELECTRICITY CHARGES	\$6,356.23
	240483	10/26/2020	SEP-20 10/26/20	ELECTRICITY CHARGES	\$22,096.80
Remit to: ROSEMEAD, CA					FYTD: \$797,765.48
SOUTHERN CALIFORNIA GAS CO.	240398	10/12/2020	SEP-2020	GAS CHARGES	\$2,431.42
Remit to: MONTEREY PARK, CA					FYTD: \$12,819.16
SOUTHWEST TRAFFIC SYSTEMS	240365	10/05/2020	4864	EMERGENCY LIGHTING EQUIPMENT INSTALLATIONS FOR 5 CITY VEHICLES	\$3,875.00
Remit to: PHOENIX, AZ					FYTD: \$41,400.95
SPRUCE GROVE, INC.	240497	10/26/2020	128672	REFUND-DUPLICATE PAYMENT/PEN18-0225 (TR 35931)	\$483.00
Remit to: SANTA ANA, CA					FYTD: \$483.00

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SSD ALARM FORMERLY PACIFIC ALARM SERVICE, INC	240447	10/19/2020	CK NO. 238794	REISSUE UNCLAIMED CHECK/ALARM SYSTEM RENT/SVC./MONITORING-MAR'20	\$516.50
Remit to: BEAUMONT, CA					FYTD: \$2,427.55
STANDARD INSURANCE CO	240366	10/05/2020	201001	EMPLOYEE SUPPLEMENTAL INSURANCE	\$1,167.46
	240484	10/26/2020	201101	EMPLOYEE SUPPLEMENTAL INSURANCE	\$1,167.46
Remit to: PORTLAND, OR					FYTD: \$5,933.00

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<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
STANLEY CONVERGENT SECURITY SOLUTIONS, INC	28940	10/05/2020	17528719	ALARM SYSTEM MONITORING-CONFERENCE & REC. CENTER/JUL-SEP 2020	\$3,966.46
		10/05/2020	17608393	ALARM SYSTEM MONITORING-LASSELLE SPORTS PARK/AUG-OCT 2020	
		10/05/2020	17592982	ALARM SYSTEM MONITORING-CITY HALL/AUG-OCT 2020	
		10/05/2020	17592530	ALARM SYSTEM MONITORING-COTTONWOOD GOLF CENTER/AUG-OCT 2020	
		10/05/2020	17591395	ALARM SYSTEM MONITORING-ANNEX 1 FIRE ALARM/AUG-OCT 2020	
		10/05/2020	17585737	ALARM SYSTEM MONITORING-MORRISON PARK SNACK BAR/AUG-OCT 2020	
		10/05/2020	17582680	ALARM SYSTEM MONITORING-VAL VERDE RED MAPLE-CHILD CARE/AUG-OCT20	
		10/05/2020	17532790	ALARM SYSTEM MONITORING-MARCH FIELD PARK COMMUNITY CTR/JUL-SEP20	
		10/05/2020	17531226	ALARM SYSTEM MONITORING-SUNNYMEAD MID. SCHOOL-THINK/JUL-SEP 2020	
		10/05/2020	17581566	ALARM SYSTEM MONITORING-EOC/AUG. 2020	
		10/05/2020	17520073	ALARM SYSTEM MONITORING-CY SANTIAGO OFFICE FIRE ALARM/JUL-SEP 20	
		10/05/2020	17533295	ALARM SYSTEM MONITORING-ANNEX 1 BURGLAR ALARM/JUL-SEP 2020	
		10/05/2020	17501222	ALARM SYSTEM MONITORING-EOC/JUL. 2020	
		10/05/2020	17508564	ALARM SYSTEM MONITORING-PUBLIC SAFETY BUILDING/JUL-SEP 2020	
		10/05/2020	17511755	ALARM SYSTEM MONITORING-TOWNGATE COMMUNITY CENTER/JUL-SEP 2020	

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STANLEY CONVERGENT SECURITY SOLUTIONS, INC	29100	10/26/2020	17607163	ALARM SYSTEM MONITORING-FIRE STATIONS/AUG. 2020	\$10,709.71
		10/26/2020	17531237	ALARM SYSTEM MONITORING-FIRE STATIONS/JUL. 2020	
		10/26/2020	17751221	ALARM INSTALLATION FOR IRIS PLAZA LIBRARY	
		10/26/2020	17671418	ALARM SYSTEM MONITORING-FIRE STATIONS/SEP. 2020	
		10/26/2020	17730244	ALARM SYSTEM MONITORING-FIRE STATIONS/OCT. 2020	
Remit to: PALATINE, IL					<u>FYTD:</u> \$16,457.15
STARLITE RECLAMATION ENVIRONMENTAL SERVICES, INC.	240448	10/19/2020	118073	HAZARDOUS MATERIAL WASTE AT CITY YARD	\$3,524.75
Remit to: FONTANA, CA					<u>FYTD:</u> \$3,524.75
STATE BOARD OF EQUALIZATION 1	29176	10/31/2020	3RD QTR 2020	SALES & USE TAX REPORT FOR THE QUARTER ENDING 9/30/20	\$14,237.00
Remit to: SACRAMENTO, CA					<u>FYTD:</u> \$16,535.00
STATE OF CALIFORNIA DEPT. OF JUSTICE	240449	10/19/2020	471583	LIVE SCAN FINGERPRINTING APPS FOR PD-SEP. 2020	\$49.00
Remit to: SACRAMENTO, CA					<u>FYTD:</u> \$1,583.00
STENO SOLUTIONS TRANSCRIPTION SVCS., INC.	29050	10/19/2020	43391	TRANSCRIPTION SERVICES FOR PD-SEP. 2020	\$118.58
Remit to: CORONA, CA					<u>FYTD:</u> \$729.66
STEPHEN H BADGETT CONSULTING LLC	28995	10/12/2020	MVU-020	CONSULTING SERVICES-REVIEW SCOPE OF WORK ON RFI'S/SEP. 2020	\$900.00
Remit to: MURRIETA, CA					<u>FYTD:</u> \$16,381.25

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STILES ANIMAL REMOVAL, INC.	240367	10/05/2020	1055	DECEASED ANIMAL REMOVAL SVCS.-DROP OFF BARRELS 7/21-7/24/20	\$920.00
	240485	10/26/2020	110505	DECEASED LARGE ANIMAL REMOVAL SERVICES-SEP. 2020	\$1,635.00
Remit to: GUSTI, CA					<u>FYTD:</u> \$5,470.00
STRADLING, YOCCA, CARLSON & RAUTH	28996	10/12/2020	367404-0044	LEGAL SERVICES-SPECIAL DIST. CFD-AUG. 2020	\$3,197.20
Remit to: NEWPORT BEACH, CA					<u>FYTD:</u> \$25,158.37
SUCCESSORIES, INC. DBA SUCCESSORIES AWARDS	29051	10/19/2020	IN-5692807	CUSTOM 3D ACRYLIC REPLICAS OF AMPHITHEATER	\$6,888.00
Remit to: BOCA RATON, FL					<u>FYTD:</u> \$6,888.00
SUNNYMEAD ACE HARDWARE	240399	10/12/2020	88820	MISC. SUPPLIES FOR PD	\$116.01
		10/12/2020	88784	MISC SUPPLIES FOR FIRE STATION 91	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$495.10
TAITT, SELWYN	240463	10/19/2020	R20-151399	ANIMAL SERVICES REFUND-OVERPAYMENT ON WEB LICENSE	\$7.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$7.00
TELEPHONE TOWN HALL MEETING, INC.	28997	10/12/2020	8811	ROBO CALLS 9/29/20	\$1,157.00
Remit to: GOLDEN, CO					<u>FYTD:</u> \$1,157.00
TESLA ENERGY OPERATIONS INC	240408	10/12/2020	BON20-0646	REFUND CANCELLED BUILDING PERMIT 10835 FOXLOVE LN	\$262.16
	240498	10/26/2020	BON20-0695	REFUND FEES-VOIDED BUILDING PERMIT-25131 EUCALYPTUS	\$435.70
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$2,673.54

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THE ADVANTAGE GROUP/ FLEX ADVANTAGE	28998	10/12/2020	123895	FLEX AND COBRA ADMIN FEES-SEPT 2020	\$1,475.25
Remit to: TEMECULA, CA					FYTD: \$185,029.70
THE ALTUM GROUP	29052	10/19/2020	5919	PEDESTRIAN HYBRID BEACON ON CACTUS AVE	\$329.84
Remit to: PALM DESERT, CA					FYTD: \$7,748.84
THE CUPCAKE & ESPRESSO BAR	240369	10/05/2020	SEPT 25, 2020	SENIOR EATS PROGRAM-9/21-9/25/20	\$3,825.00
	240401	10/12/2020	OCT 2, 2020	SENIOR EATS PROGRAM-9/28-10/02/20	\$3,825.00
	240451	10/19/2020	OCT 9, 2020	SENIOR EATS PROGRAM-10/05-10/09/20	\$3,825.00
	240486	10/26/2020	OCT 16, 2020	SENIOR EATS PROGRAM-10/12-10/16/20	\$3,825.00
Remit to: MORENO VALLEY, CA					FYTD: \$65,025.00
THE GROWTH CENTER MINISTRIES	240409	10/12/2020	2002023.047	COTTONWOOD GOLF CTR. RENTAL REFUND	\$29.00
Remit to: MORENO VALLEY, CA					FYTD: \$29.00
THE PALM HOUSE LLC DBA WOODY'S BREWHOUSE	28942	10/05/2020	SEPT. 25, 2020	SENIOR EATS PROGRAM 9/21-9/25/20	\$3,420.00
	28999	10/12/2020	OCTOBER 2, 2020	SENIOR EATS PROGRAM 9/28-10/2/20	\$3,330.00
	29053	10/19/2020	OCTOBER 9, 2020	SENIOR EATS PROGRAM 10/05-10/09/20	\$3,375.00
	29102	10/26/2020	OCTOBER 16, 2020	SENIOR EATS PROGRAM 10/12-10/16/20	\$3,285.00
Remit to: MORENO VALLEY, CA					FYTD: \$59,895.00
THEODORE, TONYA L	240414	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00

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THOMPSON COBURN LLP	29000	10/12/2020	3442677	LEGAL SERVICES-MVU/RELIABILITY STANDARD COMPLIANCE-AUG. 2020	\$62.23
Remit to: WASHINGTON, DC					FYTD: \$165.11
THOMSON REUTERS-WEST PUBLISHING CORP.	29001	10/12/2020	843106206	AUTO TRACK SERVICES FOR PD INVESTIGATIONS-SEP. 2020	\$1,175.16
Remit to: CAROL STREAM, IL					FYTD: \$4,700.64
TIMARONG, JU-ANN ANNA	240502	10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	\$500.00
		10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	
Remit to: MORENO VALLEY, CA					FYTD: \$500.00
TIME WARNER CABLE	240402	10/12/2020	2622388091320	SPECTRUM 1G ISP LINE FOR EOC 9/13-10/12/20	\$6,300.00
		10/12/2020	2622388081320	SPECTRUM 1G ISP LINE FOR EOC 7/13-9/12/20	
	240487	10/26/2020	2622388101320	SPECTRUM 1G ISP LINE FOR EOC 10/13-11/12/20	\$2,104.75
	240488	10/26/2020	091922301100120	FIBER INTERNET ACCESS SERVICES - OCT. 2020	\$844.00
Remit to: PASADENA, CA					FYTD: \$11,780.75
TKE ENGINEERING INC	240489	10/26/2020	2020-564	CONSULTANT PLAN CHECK SERVICES-TR 36760 MERITAGE HOME/PEN20-0016	\$1,127.50
		10/26/2020	2020-565	CONSULTANT PLAN CHECK SERVICES-TR 36708 CENTURY COMM./PEN18-0032	
Remit to: RIVERSIDE, CA					FYTD: \$126,266.01
T-MOBILE USA	240368	10/05/2020	9412215599	CELLULAR TECHNOLOGY EXTRACTION/LOCATOR SERVICES FOR PD	\$255.00
		10/05/2020	9412215598	CELLULAR TECHNOLOGY EXTRACTION/LOCATOR SERVICES FOR PD	
	240400	10/12/2020	9413281673	CELLULAR TECHNOLOGY EXTRACTION/LOCATOR SERVICES FOR PD	\$1,071.00
		10/12/2020	9413562049	CELLULAR TECHNOLOGY EXTRACTION/LOCATOR SERVICES FOR PD	
Remit to: SEATTLE, WA					FYTD: \$1,734.00

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TNPP RESTAURANT INC DBA BRAVO BURGERS	28943	10/05/2020	SEPT 25, 2020	SENIOR EATS PROGRAM-9/21-9/25/20	\$3,600.00
	29002	10/12/2020	OCT 2, 2020	SENIOR EATS PROGRAM-9/28-10/02/20	\$3,600.00
	29054	10/19/2020	OCT 9, 2020	SENIOR EATS PROGRAM-10/05-10/09/20	\$3,600.00
	29104	10/26/2020	OCT 16, 2020	SENIOR EATS PROGRAM-10/12-10/16/20	\$3,600.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$61,200.00
TORRES, RODOLFO	240377	10/05/2020	R20-150840	ANIMAL SERVICES REFUND-DUPLICATE WEB LICENSE PAYMENT	\$15.00
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$15.00
TOWNSEND PUBLIC AFFAIRS, INC.	29105	10/26/2020	16391	CONSULTING SERVICES-LOBBYIST/ADVOCATE & GRANT WRITING-OCT. 2020	\$4,000.00
Remit to: NEWPORT BEACH, CA					<u>FYTD:</u> \$20,000.00
TR DESIGN GROUP, INC.	29055	10/19/2020	4432	ARCHITECTURAL DESIGN SERVICES FOR CIVIC CENTER MARQUEE SIGN	\$1,376.02
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$78,868.37
TRUEPOINT SOLUTIONS, LLC	28944	10/05/2020	20-732R	SUPPORT SERVICES-AUG. 2020/SB2-ACP/ACA ONLINE ENHANCEMENTS	\$4,567.50
		10/05/2020	20-733R	SUPPORT SERVICES-AUG. 2020-ACP/ACA SUPPORT & ENHANCEMENTS	
		10/05/2020	20-410	SUPPORT SERVICES-MAY 2020-ACP/ACA SUPPORT & ENHANCEMENTS	
Remit to: LOOMIS, CA					<u>FYTD:</u> \$36,238.23
TUMON BAY RESORT & SPA	29106	10/26/2020	NOV. 2020 RENT	NOVEMBER 2020 RENT (INCL. CAM, ETC.) FOR EMPLOYMENT RESOURCE CTR	\$8,116.77
Remit to: TAMUNING, GU					<u>FYTD:</u> \$23,420.27

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ULTRASERV AUTOMATED SERVICES, LLC	29003	10/12/2020	240318	COFFEE SERVICE SUPPLIES-ANNEX 1	\$607.28
		10/12/2020	240317	COFFEE SERVICE SUPPLIES-CITY HALL/BREAK ROOM LOCATION	
		10/12/2020	237410	COFFEE SERVICE SUPPLIES-ANNEX 1	
		10/12/2020	237409	COFFEE SERVICE SUPPLIES-CITY YARD	
		10/12/2020	240319	COFFEE SERVICE SUPPLIES-CITY HALL/CITY CLERK LOCATION	
Remit to: COSTA MESA, CA					FYTD: \$4,292.48
ULTRASYSTEMS ENVIRONMENTAL, INC.	29056	10/19/2020	12038	MDP LINE K-1 AND K-4	\$2,689.00
Remit to: IRVINE, CA					FYTD: \$12,040.00
UNITED POWER GENERATION, INC.	29107	10/26/2020	5166	GENERATOR PREVENTATIVE MAINT./LOAD BANK TESTING-CITY YARD	\$4,389.16
		10/26/2020	5165	GENERATOR PREVENTATIVE MAINT./LOAD BANK TESTING-FIRE STATION 6	
		10/26/2020	5163	GENERATOR PREVENTATIVE MAINT./LOAD BANK TESTING-FIRE STATION 91	
		10/26/2020	5168	GENERATOR PREVENTATIVE MAINT./LOAD BANK TESTING-FIRE STATION 48	
		10/26/2020	5162	GENERATOR PREVENTATIVE MAINT./LOAD BANK TESTING-FIRE STATION 58	
Remit to: RIVERSIDE, CA					FYTD: \$5,238.56
UNITED ROTARY BRUSH CORP	29057	10/19/2020	CI256670	STREET SWEEPER BRUSHES & ACCESSORIES	\$2,249.12
		10/19/2020	CI256213	STREET SWEEPER BRUSHES & ACCESSORIES	
Remit to: KANSAS CITY, MO					FYTD: \$15,065.46

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UNITED SITE SERVICES OF CA, INC.	29108	10/26/2020	114-11036538	FENCE RENTAL AT ANIMAL SHELTER 9/24-10/21/20	\$106.40
Remit to: PHOENIX, AZ					<u>FYTD:</u> \$425.60
URBAN FUTURES, INC.	240452	10/19/2020	0820-006	BASELINE FORECAST MODEL-GEN. FUND/AUG. 2020 SERVICES	\$4,812.50
Remit to: ORANGE, CA					<u>FYTD:</u> \$6,050.00
VALENZUELA, MELISSA MARIE	240514	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
VALLEY WIDE TOWING, LLC	29004	10/12/2020	9412	EVIDENCE TOWING FOR PD	\$675.00
		10/12/2020	9413	EVIDENCE TOWING FOR PD	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$7,156.25
VARGAS, DIXIE	240430	10/15/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/15/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
VERIZON WIRELESS	240490	10/26/2020	9864639242	DATA CHARGES FOR CELLULAR SERVICE FOR PD DEVICES	\$376.45
Remit to: BELLEVUE, WA					<u>FYTD:</u> \$11,696.94
VIDONA, NATALIE	240509	10/26/2020	SEPTEMBER 2020	MOVAL LEARNS-SEPTEMBER 2020	\$500.00
		10/26/2020	OCTOBER 2020	MOVAL LEARNS-OCTOBER 2020	
Remit to: MORENO VALLEY, CA					<u>FYTD:</u> \$500.00
VISION SERVICE PLAN	28946	10/05/2020	810472262	EMPLOYEE VISION INSURANCE	\$3,652.52
	29109	10/26/2020	810704163	EMPLOYEE VISION INSURANCE	\$3,745.39
Remit to: SAN FRANCISCO, CA					<u>FYTD:</u> \$19,053.95

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VISTA PAINT CORPORATION	29110	10/26/2020	2020-663889-00	ON-LINE TRAFFIC PAINT, GLASS BEADS, ETC.	\$13,657.01
Remit to: FULLERTON, CA					<u>FYTD:</u> \$33,635.61
VIVINT SOLAR DEVELOPER LLC	240499	10/26/2020	BON20-0771	REFUND FEES-VOIDED BUILDING PERMIT-12361 EYRE CT	\$435.70
Remit to: RIVERSIDE, CA					<u>FYTD:</u> \$2,008.66
VOYAGER FLEET SYSTEM, INC.	29058	10/19/2020	869211615039	CNG FUEL PURCHASES	\$6,997.47
Remit to: HOUSTON, TX					<u>FYTD:</u> \$29,916.74

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VULCAN MATERIALS CO, INC.	29059	10/19/2020	72689519	ASPHALTIC MATERIALS	\$8,885.10
		10/19/2020	72712824	ASPHALTIC MATERIALS	
		10/19/2020	72639176	ASPHALTIC MATERIALS	
		10/19/2020	72644007	ASPHALTIC MATERIALS	
		10/19/2020	72636859	ASPHALTIC MATERIALS	
		10/19/2020	72634841	ASPHALTIC MATERIALS	
		10/19/2020	72634840	ASPHALTIC MATERIALS	
		10/19/2020	305456	ASPHALTIC MATERIALS-FINANCE CHARGE INVOICE	
		10/19/2020	302130	ASPHALTIC MATERIALS-FINANCE CHARGE INVOICE	
		10/19/2020	72649521	ASPHALTIC MATERIALS	
		10/19/2020	72694599	ASPHALTIC MATERIALS	
		10/19/2020	72712823	ASPHALTIC MATERIALS	
		10/19/2020	72710043	ASPHALTIC MATERIALS	
		10/19/2020	72700392	ASPHALTIC MATERIALS	
		10/19/2020	72659183	ASPHALTIC MATERIALS	
		10/19/2020	72665304	ASPHALTIC MATERIALS	
		10/19/2020	72677424	ASPHALTIC MATERIALS	
		10/19/2020	72641712	ASPHALTIC MATERIALS	
		10/19/2020	72682151	ASPHALTIC MATERIALS	
		10/19/2020	72700391	ASPHALTIC MATERIALS	
		10/19/2020	72679919	ASPHALTIC MATERIALS	
		10/19/2020	72685158	ASPHALTIC MATERIALS	
		10/19/2020	72685159	ASPHALTIC MATERIALS	
		10/19/2020	72687466	ASPHALTIC MATERIALS	
		10/19/2020	72697086	ASPHALTIC MATERIALS	
		10/19/2020	72717162	ASPHALTIC MATERIALS	
		10/19/2020	72639175	ASPHALTIC MATERIALS	
		10/19/2020	72662089	ASPHALTIC MATERIALS	
		10/19/2020	72646892	ASPHALTIC MATERIALS	

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VULCAN MATERIALS CO, INC.		10/19/2020	72646891	ASPHALTIC MATERIALS	
		10/19/2020	72669802	ASPHALTIC MATERIALS	
		10/19/2020	72669801	ASPHALTIC MATERIALS	
		10/19/2020	72715242	ASPHALTIC MATERIALS	
		10/19/2020	72667606	ASPHALTIC MATERIALS	
		10/19/2020	72657045	ASPHALTIC MATERIALS	
		10/19/2020	72654687	ASPHALTIC MATERIALS	
		10/19/2020	72662088	ASPHALTIC MATERIALS	
		10/19/2020	72672074	ASPHALTIC MATERIALS	
		10/19/2020	72651690	ASPHALTIC MATERIALS	
		10/19/2020	72654686	ASPHALTIC MATERIALS	
	29111	10/26/2020	72724119	ASPHALTIC/CRACK SEAL MATERIALS	\$2,018.67
		10/26/2020	72736654	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72734231	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72719805	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72731374	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72722147	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72726819	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72726820	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72729388	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72734232	ASPHALTIC/CRACK SEAL MATERIALS	
		10/26/2020	72719806	ASPHALTIC/CRACK SEAL MATERIALS	

Remit to: LOS ANGELES, CA					FYTD: \$15,262.11
WALK 'N ROLLERS	28947	10/05/2020	353	801 0063 CYCLE 1 ATP CITYWIDE SRFS PED FACILITY IMPROV	\$7,950.00
Remit to: LOS ANGELES, CA					FYTD: \$12,624.00

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



City of Moreno Valley
Payment Register
For Period 10/1/2020 through 10/31/2020

CHECKS UNDER \$25,000

<u>Vendor Name</u>	<u>Check/EFT Number</u>	<u>Payment Date</u>	<u>Inv Number</u>	<u>Invoice Description</u>	<u>Payment Amount</u>
WEST COAST ARBORISTS, INC.	29060	10/19/2020	163067	TREE TRIMMING SERVICES FOR MAINT. & OPERATIONS DIVISION	\$748.75
Remit to: ANAHEIM, CA					<u>FYTD:</u> \$38,261.43
WEST COAST SHOPPING CART SERVICE, INC.	240403	10/12/2020	20-094	SHOPPING CART RETRIEVAL SERVICES-SEP. 2020	\$3,153.50
Remit to: WEST COVINA, CA					<u>FYTD:</u> \$12,516.25
WESTERN MUNICIPAL WATER DISTRICT	240370	10/05/2020	24753-018620/AG0	WATER CHARGES-M.A.R.B. BALLFIELDS	\$5,184.93
		10/05/2020	23866-018292/AG0	WATER CHARGES-SKATE PARK	
		10/05/2020	23821-018258/AG0	WATER CHARGES-MARCH FIELD PARK COMMUNITY CTR.-BLDG. 938	
	240404	10/12/2020	23821-018257/AG0	WATER CHARGES-MARCH FIELD PARK COMMUNITY CTR. LANDSCAPE	\$2,261.41
	240491	10/26/2020	23821-018258/SP0	WATER CHARGES-MARCH FIELD PARK COMMUNITY CTR.-BLDG. 938	\$5,621.29
		10/26/2020	24753-018620/SP0	WATER CHARGES-M.A.R.B. BALLFIELDS	
		10/26/2020	23866-018292/SP0	WATER CHARGES-SKATE PARK	
		10/26/2020	23821-018257/SP0	WATER CHARGES-MARCH FIELD PARK COMMUNITY CTR. LANDSCAPE	
Remit to: ARTESIA, CA					<u>FYTD:</u> \$26,903.27
WFG NATIONAL TITLE COMPANY OF CALIF	240410	10/12/2020	CK NO. 238899	REISSUE UNCLAIMED CK - FILE#19-393470	\$1,000.00
Remit to: SAN BERNARDINO, CA					<u>FYTD:</u> \$1,000.00

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



City of Moreno Valley
Payment Register
For Period 10/1/2020 through 10/31/2020

CHECKS UNDER \$25,000

Vendor Name	Check/EFT Number	Payment Date	Inv Number	Invoice Description	Payment Amount
WILLDAN ENGINEERING	29113	10/26/2020	00713222	PROJECT MANAGEMENT SERVICES-AUG. 2020/NSP CLOSEOUT, ETC.	\$1,457.50
		10/26/2020	00713207	PROJECT MANAGEMENT SERVICES-JUL. 2020/NSP CLOSEOUT, ETC.	
Remit to: ANAHEIM, CA					FYTD: \$122,419.32
WRCRCA	240453	10/19/2020	SEP-2020 MSHCP	MSHCP FEES COLLECTED FOR SEP. 2020-RESIDENTIAL SINGLE FAMILY	\$4,468.00
Remit to: RIVERSIDE, CA					FYTD: \$208,486.06
WYSINGER, VALERIE	240378	10/05/2020	R20-151083	ANIMAL SERVICES REFUND-CHARGED MICROCHIP FEE BY MISTAKE	\$16.00
Remit to: MORENO VALLEY, CA					FYTD: \$16.00
XEROX CAPITAL SERVICES, LLC	28948	10/05/2020	011358575	COLOR COPIER LEASE/BILLABLE PRINTS-AUG. 2020-GRAPHICS DEPT.	\$927.84
		10/05/2020	011280531	COLOR COPIER EQUIPMENT LEASE-AUG. 2020-GRAPHICS DEPT.	
Remit to: PASADENA, CA					FYTD: \$7,240.00
TOTAL CHECKS UNDER \$25,000					\$1,051,970.81
GRAND TOTAL					\$15,954,898.74

Attachment: October 2020 Payment Register (4150 : PAYMENT REGISTER- OCTOBER 2020)



Report to City Council

TO: Mayor and City Council

FROM: Michael L. Wolfe P.E., Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: PROJECT SPECIFIC AGREEMENT FOR ON-CALL CONSTRUCTION INSPECTION SERVICES WITH NV5, INC.

RECOMMENDED ACTION

1. Approve the Project Specific Agreement for On-Call Professional Services for Construction Inspection Services with NV5, Inc. and authorize the City Manager to execute the Project Specific Agreement with NV5, Inc., subject to the approval of the City Attorney;
2. Authorize the issuance of a purchase order to NV5, Inc. in the amount of \$270,000.00 for Fiscal Year (FY) 2020/2021 and authorize the Chief Financial Officer to approve subsequent related purchase orders for annual extensions up to a maximum of two annual extensions with NV5, Inc. not to exceed \$270,000.00 annually, for a total not to exceed of \$810,000.00, in accordance with the approved terms of the Agreement, using funds collected from development and utility company projects; and
3. Authorize the Public Works Director/City Engineer to execute subsequent extensions and/or amendments to the Agreements, including the authority to authorize subsequent associated purchase orders in accordance with the terms of the Agreements, subject to the approval of the City Attorney.

SUMMARY

This report recommends approval of the Project Specific Agreement for On-Call Professional Consultant Services for Construction Inspection Services with NV5, Inc. to provide additional development related inspection services.

Development in residential, commercial, industrial and utility projects has continued to be busy despite the ongoing COVID-19 Pandemic. Due to the continued high demand

for inspection services, it is necessary to obtain additional consultant inspector services in the Land Development Division for the remainder of Fiscal Year (FY) 2020/2021.

DISCUSSION

Currently, the City has four full-time construction inspectors, one construction inspector supervisor, and one consultant inspector overseeing all private development and utility projects. The current agreement for the consultant inspector expires December 31, 2020. With the continued activity in development and utility projects, it is necessary to have available consultant inspectors to meet the inspection requests within contractors' aggressive schedules.

The Public Works Department maintains an On-Call list of consultants available to perform Construction Management and Inspection Services. Staff requested proposals from the consultants to provide construction inspection services. The City received proposals from five of the On-Call consultants. Pursuant to the Moreno Valley Municipal Code, staff reviewed and scored the proposals for project understanding, project deliverable plan, project resource allocation, and project deliverable schedule. Based upon this review, Staff determined NV5, Inc. to be the most qualified consultant to provide the construction inspection services. The proposed NV5, Inc. hourly rates were negotiated with Staff, being reasonable and competitive within industry standard.

Attached is the proposed Project Specific Agreement with NV5, Inc. The not to exceed amount will be \$270,000.00 per year, with the ability to extend the agreement an additional two years. Executing the inspection services contract will allow the City to respond to developer and utility inspection requests in a timely manner and meet their schedules.

ALTERNATIVES

1. Approve the recommended actions as presented in this staff report. *Staff recommends this alternative in order to accommodate all inspection requests by developers and utilities within the timeframe required.*
2. Do not approve the recommended actions as presented in this staff report. *Staff does not recommend this alternative, as it would result in prioritizing inspections and possible development project delays.*

FISCAL IMPACT

The expenses of this agreement are accounted for in the professional services account (GL Account Number 1010-70-29-20410-620299). These costs have an offsetting revenue as inspection fees (GL Account Number 1010-70-29-20410-523010) collected from developers and utility companies that will fund the professional services account. **There is no negative impact to the General Fund.**

NOTIFICATION

N/A

PREPARATION OF STAFF REPORT

Prepared By:
Michael Lloyd, P.E.
Engineering Division Manager

Department Head Approval:
Michael L. Wolfe, P.E.
Public Works Director/City Engineer

CITY COUNCIL GOALS

Public Facilities and Capital Projects. Ensure that needed public facilities, roadway improvements, and other infrastructure improvements are constructed and maintained.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

Objective 4.2: Develop and maintain a comprehensive Infrastructure Plan to invest in and deliver City infrastructure.

ATTACHMENTS

- 1. NV5, Inc. Project Specific Agreement for On-Call

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/07/20 4:40 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/07/20 6:38 PM

**PROJECT SPECIFIC AGREEMENT FOR ON-CALL
PROFESSIONAL CONSTRUCTION INSPECTION SERVICES**

This Agreement is made and entered into as of the date signed by the City of Moreno Valley, by and between the City of Moreno Valley, California, a municipal corporation, hereinafter described as "City," and **NV5, Inc.**, a California corporation, hereinafter described as "Consultant."

RECITALS

WHEREAS, the City has pre-qualified Consultant for On-call Consultant work in an Agreement ("On-Call Agreement") executed on **March 23, 2020** for **Construction Inspection Services** hereinafter described as "Project;" and

WHEREAS, the City wishes to engage the services of Consultant for the Project set forth in Exhibit "A".

THEREFORE, the City and the Consultant, for the consideration hereinafter described, mutually agree as follows:

1. The previously executed On-Call Agreement and subsequent Amendments between City and Consultant are incorporated herein by reference and made a part of this Agreement as if set forth in full and available for review in the City Engineer's office. Notwithstanding any expiration or termination of the On-Call Agreement or Amendment, all terms and provisions of the aforementioned On-Call Agreement and Amendments incorporated herein shall survive the expiration or termination of such Agreement or Amendment for the duration of this Project Specific Agreement. In the event of a conflict between this Agreement and the On-Call Agreement, the most current amendment shall prevail.

2. The City's Request for Proposal is for **Construction Inspection Services** and is described in detail in Exhibit "A" attached hereto and incorporated herein by this reference.

**PROJECT SPECIFIC AGREEMENT FOR ON-CALL
PROFESSIONAL CONSTRUCTION INSPECTION SERVICES**

Page 2

3. The Consultant's scope of service is described in detail in Exhibit "B" attached hereto and incorporated herein by this reference.

4. The City agrees to pay the Consultant and the Consultant agrees to receive a "Not-to-Exceed" fee of **\$270,000 per year** in accordance with the payment terms provided in Exhibit "C", attached hereto and incorporated herein by this reference.

5. The Consultant shall commence services upon receipt of written direction to proceed from the City.

6. This agreement will terminate on **December 31, 2021** unless the termination date is extended by an amendment to the agreement, subject to any earlier termination in accordance with this Agreement, or extensions in one year increments up to two additional years as further provided for in the City's Procurement Policy.

7. Consultant shall provide updated insurance documentation, certificates or endorsements unless specifically waived by the City Attorney.

SIGNATURE PAGE FOLLOWS

**PROJECT SPECIFIC AGREEMENT FOR ON-CALL
PROFESSIONAL CONSTRUCTION INSPECTION SERVICES**

Page 3

IN WITNESS HEREOF, the parties have each caused their authorized representative to execute this Agreement.

City of Moreno Valley

NV5, Inc.

BY: _____
Mike Lee, City Manager

Date

BY: _____
TITLE: _____
(President or Vice President)

Date

<u>INTERNAL USE ONLY</u>
APPROVED AS TO LEGAL FORM:

City Attorney

Date
RECOMMENDED FOR APPROVAL:

Public Works Director/City Engineer

Date

BY: _____
TITLE: _____
(Corporate Secretary)

Date

Enclosures: Exhibit "A" – City Scope of Services
Exhibit "B" – Consultant Proposal
Exhibit "C" – Terms of Payment/Consultant Cost Proposal

See Attached

Attachment: NV5, Inc. Project Specific Agreement for On-Call (4225 : AGREEMENT FOR ON-CALL CONSTRUCTION INSPECTION SERVICES

EXHIBIT "A"

TEL: 951.413.3120
 FAX: 951.413.3158
 WWW.MOVAL.ORG



14177 FREDERICK STREET
 P.O. BOX 88005
 MORENO VALLEY, CA 92552

October 29, 2020

Mr. Guillaume Gau
 NV5
 163 Technology Drive, Suite 100
 Irvine, CA 92618

**Subject: Request for Proposal for
 Construction Inspection Services for Land Development Division**

Dear Mr. Gau:

The City of Moreno Valley is requesting a proposal for construction inspection services for the Land Development Division, in accordance with the Agreement for Professional Consultant Services for Construction Management/Inspection Services. The Land Development Division (LDD) oversees private development activities such as grading, erosion control, drainage, and water quality BMPs. Furthermore, LDD oversees encroachment permits issued to utilities and private developers that typically require roadway and sidewalk/pedestrian ramp construction/repairs through the use of appropriate traffic control. This request is to augment existing City staff on an as needed basis as determined by the City, and the expected number of hours needed are between 20 and 50 hours per week. If a firm is selected to provide construction inspection services, the first agreement will be through the end of FY 20/21, with options to extend the agreement up to two years. The Proposal should include, but not be limited to:

- a. Identify staff member(s) that will provide the construction inspection services along with resume, certifications, and similar experience. Please note that the agreement will stipulate that the City has the right to accept/reject any changes in personnel during the term of the agreement
- b. List of references.
- c. List of similar projects/tasks.
- d. Number of hours per week for each staff member identified in (a) that the firm can commit to provide the services as well as the hourly rate.
- e. An acknowledgement that your firm will provide all necessary vehicles, equipment, uniforms, etc. for the staff member identified in (a) required to conduct inspections.

Three (3) copies of your Proposal should be submitted to the City before 5:00 p.m. on November 16, 2020, or a pdf can be sent to michaell@moval.org with Subject Line Consultant Inspector Proposal. Please limit your proposal to a maximum of 10 pages. Forward your Proposal to:

Michael Lloyd, P.E.

LAND DEVELOPMENT DIVISION

TEL: 951.413.3120
 FAX: 951.413.3158
 WWW.MOVAL.ORG



14177 FREDERICK STREET
 P.O. BOX 88005
 MORENO VALLEY, CA 92552

RFP for Construction Inspection Services
 October 29, 2020
 Page 2

Land Development Division
 14177 Frederick Street *(hand delivery)*
 P.O. Box 88005 *(mail delivery)*
 Moreno Valley, California 92552-0805

Per the on-call agreement, final consultant selection will be based on:

1. Project Understanding – does the Consultant understand the project's objectives and issues? (20 points)
2. Project Deliverable Plan – has Consultant submitted a sound, executable work plan? (30 points)
3. Project Resource Allocation – does the proposal reflect efficient utilization of experienced, dedicated project manager and staff? (30 points)
4. Project Deliverable Schedule – does the proposal reflect timely deliverables at appropriate milestones? (20 points).

Should you have any questions, please contact Michael Lloyd, P.E., at 951.413.3120.

Sincerely,

Michael Lloyd, P.E.
 Engineering Division Manager/Assistant City Engineer

c: Project File

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EXHIBIT "B"

PN: P27020-0006658.00



November 16, 2020

City of Moreno Valley
 Attn: Michael Lloyd, PE
 14177 Frederick Street
 Moreno Valley, CA 92552

SUBJECT: Construction Inspection Services for Land Development Division

Dear Mr. Lloyd,

NV5, Inc. is excited to have the opportunity to provide Construction Inspection Services to the City of Moreno Valley (City) Land Development Division. Our team has the knowledge and experience to assure your projects are completed on schedule and within budget.

We have been providing highly qualified and experienced professionals on staff augmentation contracts for several years. Public agencies have contracted with our firm when it is not cost effective to fill a full-time position; to relieve peak workload situations; and to fill vacant positions on an interim basis. We select, train and retain highly qualified employees to supplement contracting cities' staffing requirements. Our professional staff has expertise in a wide range of disciplines and extensive experience in many areas, including project management, construction management, and design engineering.

NV5 is committed to providing superior, high-level services to our clients. We do so by maintaining a core of highly qualified staff possessing a wide range of skills that we match with our clients' needs. Our professionals are committed to reliable performance, positive attitudes, and a mission focus that is essential to successful outcomes. NV5's strengths include the following:

Project Team: Our team will be led by Project Manager **Jonathan Smith, PE, QSD/QSP**. Mr. Smith has more than 30 years of experience and, as the former City Engineer for the City of Menifee, Mr. Smith is thoroughly familiar with the developers that are working in and around the City. In addition, he has a strong working relationship with most of the major builders in southwest Riverside County and understands how to work with these contractors to assure projects stay on schedule and budget.

NV5 is also offering highly experienced construction inspectors to provide as-needed services for the City. Each of our proposed inspectors has nearly 30 years or more of experience working for public and private clients. Our inspectors have experience working on projects that require inspections of grading, erosion control, drainage and water quality BMPs. In addition, our inspectors, along with our dedicated project manager, are experienced at reviewing traffic control plans and assuring that work is being completed safely in the public right-of-way.

Accessibility and Responsiveness: Our professionals take pride in providing immediate response and personal attention to our clients' needs. Additionally, each of our Construction Inspectors has been provided with an iPad and will be using our exclusive iPad application to prepare daily reports with photos. These reports are uploaded to our servers at the end of each day and can be made available to the City immediately, granting you convenient, full access to the current status and progress of your projects.

Inland Empire Office Location: In August 2017, we opened a Corona office to better serve our clients in San Bernardino and Riverside counties. Our Inland Empire office is located at 1101 California Avenue, Suite 209, Corona, CA 92881.

NV5 looks forward to the opportunity to work with the City and help you achieve your goals and objectives. If you need further information, we may be reached by phone at (949) 585-0477, or electronically at jonathan.smith@nv5.com and jeff.cooper@nv5.com. Thank you for your time and consideration.

Sincerely,
 NV5


 Jeffrey M. Cooper, PE
 Director of Infrastructure


 Jonathan Smith, PE, QSD/QSP
 Project Manager

COMPANY INFORMATION/CONTACT

Name: NV5, Inc.
 Address: 1101 California Avenue, Suite 209
 Corona, CA 92881
 Telephone Number: (858) 472-2212
 Email of Contact: jonathan.smith@nv5.com

WORK APPROACH + SCOPE OF SERVICES

WORK APPROACH

Detailed and accurate inspection of the work performed by a developer's contractor is extremely important. Not only does the inspector need to be sure that the work is performed in a safe manner but, he/she also needs to confirm that the improvements are constructed per the City approved grading and improvement plans. This becomes very important for when the City Council accepts the improvements. This process guarantees that the City will be accepting improvements that will not be a burden to the City to maintain or to the private property owner if the improvements are privately maintained.

Inspection of contractors working in the public right-of-way for a utility is critical in avoiding complaints from the public, or worse, from elected officials. Oftentimes the utility contractor overlooks the work items that are important to work safely in the public right-of-way. We have found from our inspection experience in southwest Riverside County that utility contractors often do not set up proper traffic control, do not vacate the public right-of-way per the conditions of the permit, and sometimes are in the right-of-way without a permit.

Utility contractors are not usually experienced in street, sidewalk and pedestrian ramp repairs. It is important that the contractor repairs the roadway per the City's utility trench repair standard or similar City-approved structural section. It is also important for the utility contractor to repair the sidewalk/pedestrian ramp work areas per ADA standards. Properly inspected utility roadway repairs will avoid future roadway hazards.

Project Manager **Jonathan Smith, PE, QSD/QSP**, is a hands-on manager who will monitor the City's inspection needs on a daily basis and will allocate the necessary resources to be sure that the land development projects are inspected according to the type of work being performed and the project schedule. Mr. Smith will also be certain that the inspection team monitors all utility work in the public right-of-way.

As the former City Engineer for the City of Menifee, Mr. Smith knows the developers that are working in the City of Moreno Valley. He has a good working relationship with most of the major builders in southwest Riverside County – for example, KB homes, Lennar, Pulte, Pardee and others. He also has a good working relationship with the utility companies that work within the City's public right-of-way. These include Eastern Municipal Water District (EMWD), Southern California Edison (SCE), and The Gas Company, to name a few.

The City will benefit from the relationships that Mr. Smith has with the development community and the various utility companies. When projects start to go wrong, due to his knowledge and relationships, he can quickly assist the inspection team by working with his contacts to quickly resolve any construction issues.

It is very important to have diligent inspections of land development projects. The inspector needs to be in front of the contractor to avoid any surprises. It is important for the inspector to be sure that the developer's contractor has the proper BMPs in place and sufficient BMP supplies on-site to avoid any stormwater issues. It is also important for the inspector to know the production rate of the developer – for example, when the developer expects to receive sign-off on pad certifications to get a building permit or when a lot needs to have a final sign-off for a certificate of occupancy.

Mr. Smith will provide daily management of the NV5 inspection team. He is also available to help mentor City inspection staff if needed.

SCOPE OF SERVICES

Our Construction Inspectors will discuss any appropriate revisions to the contractor's methods and coordinate with the City's Project Manager at all times that work is progressing. Our Inspection services can include:

- Review and become familiar with all development-contract- and construction-related documents, including City-approved plans and specifications, traffic control plans, construction schedules, construction sequences and permitting requirements, and present any concerns during the pre-construction meeting and weekly progress meetings. Attend subsequent progress meetings to maintain communication regarding issues or problems. For all projects, maintain correspondence file and copy all contractor communications to land development engineer.
- Conduct a pre-construction photo/video survey of the project site for verification that the site is returned to its original condition at the end of the construction period.
- Perform daily field and quality control inspections and measurements of the contractor's work, submittals, and construction materials to assure all conform to the City-approved plans, contract documents, codes and ordinances,

WORK APPROACH + SCOPE OF SERVICES

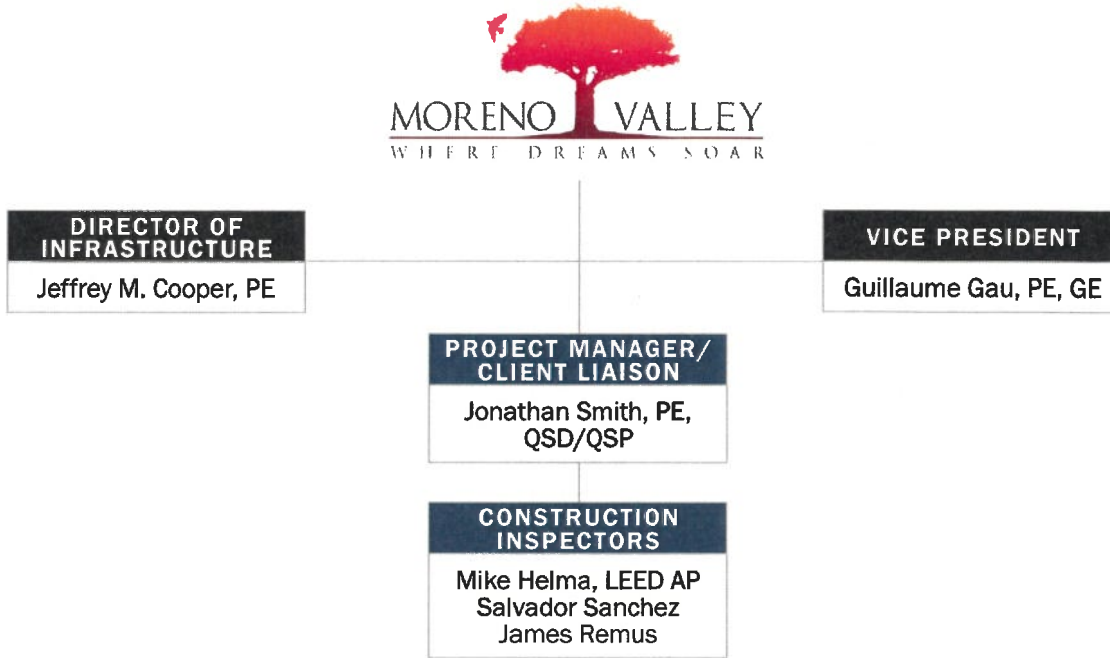
“Greenbook” Standard Specifications, Manual of Traffic Controls for Construction and Maintenance Work Zones, Caltrans Construction Manual and all other applicable codes and regulations. Health and safety measures will be strictly enforced to maintain a safe construction environment.

- Prepare daily reports, including daily progress photos, and detail work completed and/or other activities for each day the contractor performs work. Daily reports will be kept in a bound notebook and will state at a minimum the date, weather conditions, traffic control measures taken, progression of work, materials used, and subcontractors, equipment and employees on-site, and major incidents/safety violations. We will also prepare reports of deviations and non-conformance to specifications and provide responses in accordance with the specification requirements. Notices of Non-Compliance will be immediately delivered to the contractor for any and all deviations.
- We will monitor the establishment, maintenance and any requirement modification of the approved SWPPP. Should the provisions for developing the SWPPP in the specifications be inadequate to support development of a comprehensive and timely submitted stormwater and erosion control plan necessary to comply with current NPDES requirements, the NV5 team will work closely with the design consultants to incorporate the required language into the project specifications suitable to meet these goals.
- Monitor utility coordination as set forth in the pre-construction meeting, reporting conflicts to the land development engineer and recommending a course of action.
- Assure compliance to the coordination plan set forth in the project startup to mitigate construction impacts to businesses and residents, altering and revising the coordination plan as project demands require. As part of the coordination plan, monitor site safety, public safety and convenience, reporting problems to the land development engineer.
- Assure implementation of the City-approved traffic control plan or the latest edition of the Work Area Traffic Control Handbook (WATCH) requirements for utility contractors that are approved to use the WATCH manual.
- Compile detailed punch lists with the developer’s contractor, and/or utility’s contractor, and Consultants. NV5 will conduct a preliminary walk-through with the contractors to assure there are no blatant issues and to generate a preliminary punch list. After completion of the preliminary punch-list items, prepare the final punch list and coordinate with the contractors for item completion. All punch lists generated will include schedules for completion.
- Upon project completion, conduct final inspection, closeout permits, including encroachment and construction/excavation, and prepare and submit as-built plans to the land development engineer.

PROPOSED TEAM

ORGANIZATION CHART

Our team of experts brings direct, relevant, successful and current experience working on projects of similar scope and complexity. We are equipped with the resources to provide the City with the requested inspection services. Shown below are team roles and responsibilities. Any changes in personnel during the term of the agreement will be submitted to the City for approval. NV5 will provide all necessary vehicles, equipment, uniforms, etc., for the staff members listed below to conduct inspections.



KEY STAFF QUALIFICATIONS

Name/Role	Years	Education, Certifications & Licenses	Available Hours/Rate
Jonathan Smith, PE, QSD/QSP Project Manager/ Client Liaison	30	B.S. Civil Engineering Civil Engineer (CA) No. C61253 Qualified SWPPP Developer/Practitioner, No. 23727	As-Needed \$165/hr
Mike Helma, LEED AP Construction Inspector	30	B.S. Civil Engineering LEED Accredited Professional	20-50 hours \$130/hr (prevailing wage) \$120/hr (non-prevailing wage)
Salvador Sanchez Construction Inspector	30	CA Water Distribution Operator, D5 No. 08546 CA Water Treatment Operator, T4 No. 16793 Water Utilities Sciences Certificate	20-50 hours \$130/hr (prevailing wage) \$120/hr (non-prevailing wage)
James Remus Construction Inspector	28	Cross Connection Control and Backflow Prevention and Program Specialist C2 Workzone Traffic Control Certificate OSHA 10-Hour Construction Certificate	20-50 hours \$130/hr (prevailing wage) \$120/hr (non-prevailing wage)

Attachment: NV5, Inc. Project Specific Agreement for On-Call (4225 : AGREEMENT FOR ON-CALL CONSTRUCTION INSPECTION SERVICES

RESUMES

CONTACT INFO

mike.helma@nv5.com

EXPERIENCE

30 years

EDUCATION

B.S. Civil Engineering

LICENSES

LEED Accredited
Professional

MIKE HELMA, LEED AP

Construction Inspector

Mr. Helma has more than 30 years of experience in construction management and inspection with specific expertise in street improvements, sewer, water and storm drain systems, heavy grading, concrete structures, traffic signals, paving and landscaping. He is a LEED Accredited Professional with expertise in the management of large- and small-scale projects, in addition to complex projects. His experience includes serving as an extension of city staff and public works departments, where all assigned projects are completed in a timely and professional manner.

Project Experience

Public Works Inspection

CITY OF YORBA LINDA | YORBA LINDA, CA

Public Works Inspector. Mr. Helma is providing public works inspection services for various construction projects within the City. His duties included construction observation, public contract administration, project meeting attendance, utility coordination, minimizing traffic impacts, construction staking and surveying, material testing, project documentation, record drawings, and permit system record keeping.

Public Works Inspection (Staff Augmentation)

CITY OF NEWPORT BEACH | NEWPORT BEACH, CA

Public Works Inspector. Mr. Helma provided construction inspection services for various City projects as well as oversight inspection for Public Works Encroachment permits for work within the public right-of-way. His duties included construction observation, public contract administration, project meeting attendance, utility coordination, minimizing traffic impacts, construction staking and surveying, material testing, project documentation, record drawings, and permit system record keeping.

Public Works Inspection (Staff Augmentation)

CITY OF SAN CLEMENTE | SAN CLEMENTE, CA

Public Works Inspector. Mr. Helma inspected various City sidewalks, ramps, driveways, and curb and gutters for damage or tripping hazards as part of the City's yearly sidewalk repair program. He identified areas of encroachment and provided a written report for each street address, complete with repair quantities and locations. In a three-month period, Mr. Helma provided more than 1,500 reports.

Residential Street Rehabilitation

CITY OF WEST COVINA | WEST COVINA, CA

Construction Inspector. Mr. Helma provided construction inspection services for the rehabilitation of various streets throughout the City of West Covina. The scope of work included reconstruction of existing PCC sidewalks, driveways, curb and gutter, and ADA curb ramps; street reconstruction and resurfacing; utility adjustments; and traffic signage and striping.

Slurry Seal and Rehabilitation Project

CITY OF ALISO VIEJO | ALISO VIEJO, CA

Construction Inspector. Mr. Helma provided construction inspection services for the FY 2014-15 Slurry Seal and Rehabilitation Project throughout the City of Aliso Viejo. The scope of work consisted of crack sealing, slurry sealing, and traffic striping and signage.



RESUMES

CONTACT INFO

salvador.sanchez@nv5.com

EXPERIENCE

30 years

CERTIFICATIONS

CA Water Distribution
Operator, D5 No. 08546

CA Water Treatment
Operator, T4 No. 16793

Water Utilities Sciences
Certificate

SALVADOR SANCHEZ

Construction Inspector

Mr. Sanchez is adept at managing and delivering a variety of public works projects, including water, sewer, and storm drain improvements. His skills in the development of project control procedures and methodologies for cost and schedule control have been implemented on several large-scale projects. Mr. Sanchez has accrued more than 30 years of public works experience, demonstrating his expertise in a range of responsibilities, including six years with the City of Riverside, nine years with the City of Pico Rivera and 14 years with the City of Glendale Water Department.

Project Experience

Arantine Hills Sewer and Water Infrastructure Projects

CITY OF CORONA | CORONA, CA

Construction Inspector. Mr. Sanchez provided inspection services for the \$14.7M three-phase Arantine Hills sewer and water improvement projects to provide the necessary infrastructure for the Arantine Hills Development Project. The project involved the construction of a buried cast-in-place sewer lift station with above-grade controls. This was connected to a second project to construct the sewer force main with fused HDPE pipe via conventional open trench and trenchless jack-and-bore sewer construction along arterial streets and Caltrans ROW, forming 4 miles of pipeline. Relocation of 3,300 LF of potable water line also was required to install the new force mains. A third project managed simultaneously was the construction of the California Avenue Sewer at a depth of 25 feet below grade. All three projects were to be completed together for the final and finished sewer lift station to function. Coordination with developers was required for the construction of the sewer lift station within their property, and Caltrans was required along certain reaches of the pipeline construction for construction within their ROW.

Water Main Replacement and Street Improvements

CITY OF MANHATTAN BEACH | MANHATTAN BEACH, CA

Inspector. Mr. Sanchez provided inspection services for the 2013-2014 Water Main Replacement and Street Improvements project. The project replaced old and/or undersized water mains to reduce the risk of main breaks and enhance fire suppression flows. New 6-inch ductile iron pipes replaced a total of 10,261 LF of existing water mains. Work also included the installation of five new fire hydrants, reconnection of four existing fire hydrants and the installation of 2-inch water blowoff assemblies at four sewer pump stations.

Various Water and Wastewater Projects

CITY OF GLENDALE DEPARTMENT OF WATER AND POWER | GLENDALE, CA

Inspector/Water System Supervisor II. Mr. Sanchez was responsible for the management and inspection of several water and wastewater projects over the course of 14 years. Responsibilities included inspecting construction, repair, maintenance and operation of water systems and pumping facilities; supervising pump maintenance mechanics, water distribution operators, water treatment operators, SCADA technicians and water system apprentices; overseeing the operation and maintenance of a Diatomaceous Earth Filter Treatment Plant; preparing for water outages and coordination with City staff and contractors for smooth and efficient outages; implementing necessary changes or postponements of projects as the system dictated; and working with water quality staff and health departments on water system sanitary surveys.



RESUMES

CONTACT INFO

james.remus@nv5.com

EXPERIENCE

28 years

EDUCATION

Office Management

LICENSES/CERTIFICATES

Cross Connection Control and Backflow Prevention and Program Specialist

C2 Workzone Traffic Control Certificate

OSHA 10-Hour Construction Certificate

JAMES REMUS

Construction Inspector

Mr. Remus has spent more than 28 years as a construction inspector for a variety of pipeline and construction projects. He has worked closely with engineering firms, public agencies and specialists in the fields of biology and archaeology as related to respective projects. Mr. Remus is knowledgeable in a variety of construction activities, including heavy documentation of design/build structures, testing and trench safety. He has spent more than 13 years as a cross-connection specialist with the Walnut Valley Water District.

Project Experience

Butterfield Stage Road, Phase II, PW15-11

CITY OF TEMECULA | TEMECULA, CA

Construction Inspector. Mr. Remus is providing construction inspection services for the Butterfield Stage Road project. The ultimate goal of the project is to widen Butterfield Stage Road from La Serena Way to Rancho California Road, which includes the widening of Rancho California Road to four lanes through the intersection of Butterfield Stage Road. The project consists of grading, embankments/slopes utilities, significant storm drain improvements, retaining walls, access roads, curb and gutter, sidewalk and appurtenant items associated with the new roadway, new storm drain systems as well as terrace/inceptor/down and parkway drains, headwalls/wing walls, outlet structures, cut-off walls, concrete aprons and splash walls and pads, conduits and pullboxes and traffic signing and striping.

Staff Augmentation Inspection Services

IRVINE RANCH WATER DISTRICT | IRVINE, CA

Inspector. Mr. Remus provided inspection services for water, wastewater and sewer projects for the Irvine Ranch Water District. His duties included confirming that the Contractors are adhering to the Contract Documents, as well as generating daily reports for several ongoing projects. The types of projects he worked on included housing tracts, commercial sites, as well as capital and non-capital projects.

Storm Drain and Street Improvements on Amar Road

CITY OF LA PUENTE, CA | LA PUENTE, CA

Construction Manager. Mr. Remus provided construction management services for the Storm Drain and Street Improvements of Amar Road. Improvements consisted of complete grind and overlay, striping, RPMs, and signage.

Bike Lane Installation and Street Improvements

ROLLING HILLS ESTATES | ROLLING HILLS, CA

Construction Manager. Mr. Remus provided construction management services for bike lane installation and street improvements. Work consisted of street improvements on Palos Verdes Drive north from Crenshaw Boulevard to west city limits.

Recharge Pipeline Project

MOJAVE WATER AGENCY | MOJAVE, CA

Construction Inspector. Mr. Remus was responsible for a 15-mile segment of the 48-inch CMI & C steel force main, which included both welded and o-ring gasket joints. He was accountable for trench safety and proper placement of two-sack sand slurry mix in the pipe zone at specified locations.



REFERENCES

REFERENCES

Provided below are references of clients for whom inspection services have been performed in the last five (5) years that are comparable in quality and scope to the services requested by the City.

Relevant Project Experience/References		
Client	Services	Contact / Title
City of Yorba Linda	NV5 is providing public works inspection services for various construction projects within the City. Duties include construction observation, public contract administration, project meeting attendance, utility coordination, minimizing traffic impacts, construction staking and surveying, material testing, project documentation, record drawings, and permit system record keeping.	Fredy A. Castillo Assistant Engineer 714.961.7170 fcastillo@yorbalindaca.gov
City of Newport Beach	NV5 provided construction inspection services for City projects and private improvements/encroachments within the public right-of-way. NV5's duties include construction observation, public contract administration, project meeting attendance, utility coordination, minimizing traffic impacts, construction staking and surveying, material testing, project documentation, record drawings, and permit system record keeping.	Mike Sinacori Assistant City Engineer 949.644.3342 msinacori@newportbeachca.gov
City of San Clemente	NV5 inspector Mike Helma inspected various City sidewalks, ramps, driveways and curb and gutters for damage or tripping hazards as part of the City's yearly sidewalk repair program. He identified areas of encroachment and provided a written report for each street address, complete with repair quantities and locations. In a three-month period, Mr. Helma provided more than 1,500 reports.	Zak Ponsen City Engineer 949.361.6135 ponsenz@sanclemente.org
City of Temecula	As part of an as-needed agreement with the City of Temecula, our team is providing project/construction management and inspection services for the Butterfield Stage Road Improvements. The ultimate goal of the project is to widen Butterfield Stage Road from La Serena Way to Rancho California Road, which includes the widening of Rancho California Road to four lanes through the intersection of Butterfield Stage Road. The project consists of grading, embankments/slopes utilities, significant storm drain improvements, retaining walls, access roads, curb and gutter, sidewalk and appurtenant items associated with the new roadway, new storm drain systems, as well as terrace/inceptor/down and parkway drains, headwalls/wing walls, outlet structures, cut-off walls, concrete aprons and splash walls and pads, conduits and pull boxes and traffic signing and striping.	Kendra Hannah-Meistrell, Sr. Civil Engineer 951.693.3949, kendra.hannah-meistrell@TemeculaCA.gov
Irvine Ranch Water District	Our team currently provides the Irvine Ranch Water District with as-needed construction inspection services as part of a staff augmentation contract with the District. Our construction inspectors are responsible for performing inspections on capital and subdivision construction projects, including sewer, water and recycled water pipelines; pumping stations; pressure-reducing stations; sewage lift stations; treatment plants; reservoirs; and other facilities. Location of work includes housing tracts, commercial sites, as well as capital and non-capital projects.	Kelly Lew Engineering Manager, Development & Inspection Services 949.453.5586 lew@irwd.com

Attachment: NV5, Inc. Project Specific Agreement for On-Call (4225 : AGREEMENT FOR ON-CALL CONSTRUCTION INSPECTION SERVICES

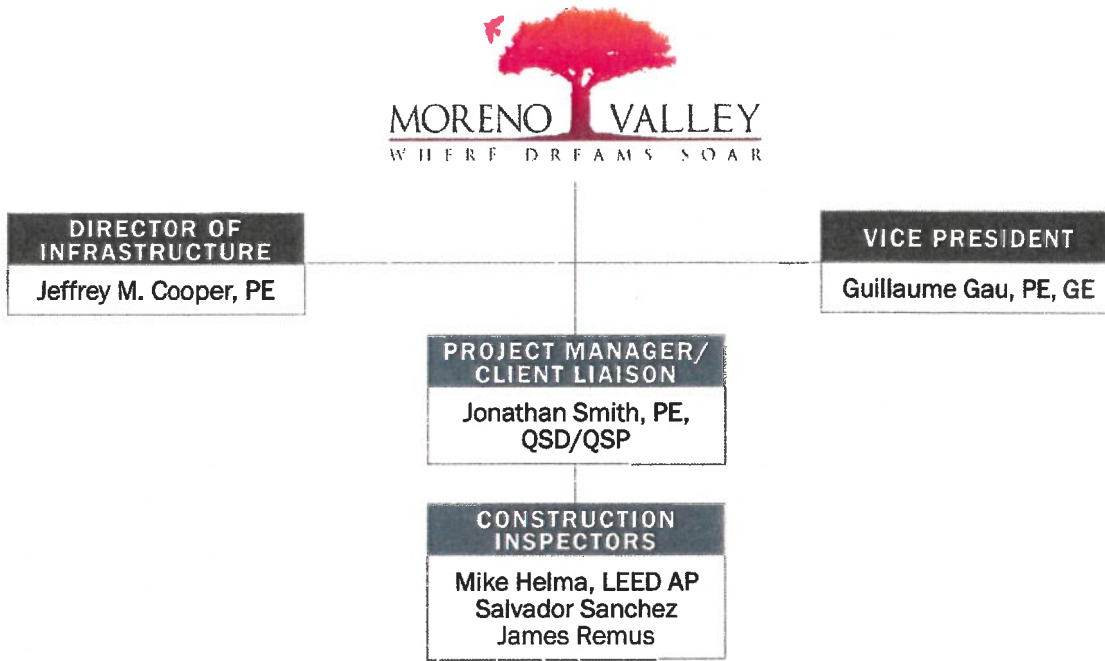
EXHIBIT "C"
TERMS OF PAYMENT

The Consultant's compensation shall not exceed **\$270,000 amount**.

PROPOSED TEAM

ORGANIZATION CHART

Our team of experts brings direct, relevant, successful and current experience working on projects of similar scope and complexity. We are equipped with the resources to provide the City with the requested inspection services. Shown below are team roles and responsibilities. Any changes in personnel during the term of the agreement will be submitted to the City for approval. NV5 will provide all necessary vehicles, equipment, uniforms, etc., for the staff members listed below to conduct inspections.



KEY STAFF QUALIFICATIONS			
Name/Role	Years	Education, Certifications & Licenses	Available Hours/Rate
Jonathan Smith, PE, QSD/QSP Project Manager/ Client Liaison	30	B.S. Civil Engineering Civil Engineer (CA) No. C61253 Qualified SWPPP Developer/Practitioner, No. 23727	As-Needed \$165/hr
Mike Helma, LEED AP Construction Inspector	30	B.S. Civil Engineering LEED Accredited Professional	20-50 hours \$130/hr (prevailing wage) \$120/hr (non-prevailing wage)
Salvador Sanchez Construction Inspector	30	CA Water Distribution Operator, D5 No. 08546 CA Water Treatment Operator, T4 No. 16793 Water Utilities Sciences Certificate	20-50 hours \$130/hr (prevailing wage) \$120/hr (non-prevailing wage)
James Remus Construction Inspector	28	Cross Connection Control and Backflow Prevention and Program Specialist C2 Workzone Traffic Control Certificate OSHA 10-Hour Construction Certificate	20-50 hours \$130/hr (prevailing wage) \$120/hr (non-prevailing wage)

Attachment: NV5, Inc. Project Specific Agreement for On-Call (4225 : AGREEMENT FOR ON-CALL CONSTRUCTION INSPECTION SERVICES



Report to City Council

TO: Mayor and City Council

FROM: Michael L. Wolfe P.E., Assistant City Manager
Marshall Eyerman, Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM FOR RESIDENTIAL AND SMALL BUSINESSES, PROVIDE FUNDING OF \$250,000 FOR THE COVID-19 UTILITY ASSISTANCE PROGRAM, APPROVE A SUSPENSION OF LATE FEES AND DISCONNECTS, AND APPROVE A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY TO CONFIRM THE ELECTRIC RATES FOR MORENO VALLEY UTILITY (MVU)

RECOMMENDED ACTION

Recommendations:

1. Approve a COVID-19 Utility Assistance Program for residential and small business customers;
2. Approve a budget allocation of \$250,000 to fund the new COVID-19 Utility Assistance Program;
3. Approve suspension of late fees and disconnects for non-payment until the Emergency Resolution is lifted by the City; and
4. Approve Resolution No. 2020-XX. A Resolution of the City Council of the City of Moreno Valley, California, to confirm the Electric Rates for Moreno Valley Utility (MVU) as currently required by increased energy costs, operating costs, operating agreements, and City resolution.

SUMMARY

To further aid the MVU residential and small business customers (businesses whose

monthly maximum demand for electricity is less than 20kW) during the current pandemic, this report recommends establishing a new COVID-19 Utility Assistance Program. The proposed program may be funded through the proposed utility rates, which can provide a budget allocation of \$250,000 to fund the COVID-19 Utility Assistance Program. In addition to the COVID-19 Utility Assistance Program, this report recommends suspending late fees and disconnections for non-payment until the Emergency Resolution is lifted by the City to parallel the State order for investor-owned electric, gas, water, and telecommunication utilities.

DISCUSSION

COVID-19 Utility Assistance Program:

During this current economic time, many households and small businesses have been significantly impacted by the pandemic, and may require some assistance with paying for their monthly MVU electrical bill. To provide financial relief, staff is proposing a COVID-19 Utility Assistance Program that can help residents and small businesses.

The new program, if approved, will include two components:

- The program will provide qualifying customers impacted by the pandemic with up to a 30% discount on their electric bills, which may be applied against balances accumulated since March 2020.
- The program will allow qualified customers to request an extended payment program for their charges. The extended payment arrangement (12 months for residential customers and 24 months for small businesses) can be applied for through an online application and can include outstanding balances since March 2020.

Discounts will be available on a first come basis until funding has been expended. A draft flyer with detailed information about the new program is attached. The flyer and application for the discount will be emailed to customers, distributed with the December and January bills, and placed on MVU's webpage along with the online application for the extended payment arrangement.

Staff recommends approval of funding of \$250,000 for the new COVID-19 Utility Assistance Program, which will provide a discount of up to 30% for qualifying residents and small businesses. This new program will be in addition to MVU's existing Energy Bill Assistance Program, which is funded in the amount of \$200,000 annually.

In addition to the new COVID-19 Utility Assistance Program, staff recommends a moratorium on disconnections for non-payment and the suspension of late MVU fees until the Emergency Resolution is lifted by the City as the Governor has implemented said order for investor owned utilities (e.g., SCE). If approved, MVU customers will be afforded the same benefits as SCE customers in the City.

Electric Service Area and Rates:

Two separate utility providers, Moreno Valley Utility (MVU) and Southern California Edison (SCE), currently serve the City of Moreno Valley. Between the two electric utilities, there are almost 61,000 accounts, of which SCE serves approximately 54,007 customers representing 88.8% total customers while the City's electric utility company MVU serves approximately 6,818 customers, representing 11.2% of total customers.

Total Active Accounts*	MVU	SCE	Total	MVU % of Total Accounts
Residential	6,094	50,174	56,268	10.8%
Commercial	724	3,833	4,557	15.9%
Total	6,818	54,007	60,825	11.2%

*Numbers based on the average from April – June 2020

While over 85% of MVU customers are residential, they account for only about 30% of revenue; the remaining 70% of revenue is generated by commercial and industrial customers. The 25 largest energy using business customers account for approximately 45% of the total revenue of MVU.

The requirement for utilities to purchase greater amounts of renewable energy has continuously increased, moving from an amount equal to 20% of retail sales by 2017 to 33% by 2020, to 60% by 2030, and 100% carbon-free energy by 2045. In addition, the focus on renewable, clean energy has impacted the capacity market by limiting the amount of generation capacity available to cover peak demand periods, thus reducing supply and driving up prices. **In calendar year 2018, the cost per kW-month for capacity products was approximately \$1.30. The calendar year 2021 cost per kW-month is approximately \$6.20.** MVU has seen a significant increase in power costs and require investments in technology to upgrade the system to maintain reliability to customers since its last rate adjustment.

On May 16, 2019, the California Public Utilities Commission (CPUC) approved the 2018 General Rate Case (GRC) for SCE, which covers the years 2018 - 2020. The approved SCE rate adjustment for calendar year 2020 is 13.2%, which is now in effect for SCE customers, and is related to capital investments for repair and replacement of equipment on its distribution system and investments in modernizing the system, as well as increasing power costs from State law that requires electric utilities to procure clean energy. Similarly, MVU has reviewed SCE's increased rates and has analyzed the current and upcoming operational and capital expenditures for MVU. Although MVU may require rates higher than SCE to establish reserves, pursuant to City Resolution 2006-112, staff recommends an adjustment to continue the policy of parity with SCE's electric rates.

To avoid the requirement for the General Fund to subsidize up to \$4.3 million annual to MVU, the proposed Resolution amends and confirms the electric rates for MVU to correspond with all four SCE rates increases in 2020. Staff recommends approval of the resolution that will amend and confirm the electric rates for Moreno Valley Utility

(MVU). Since the inception of MVU, City Council policy has been to maintain parity with SCE electric rates. Resolution 2006-112 approved implementing a schedule to adjust MVU’s rates to reflect the same rate schedule as SCE. This policy is also incorporated within the Professional Services Agreement with ENCO Utility Services Moreno Valley, LLC, which requires that MVU adjusts its electric rates to maintain approximate parity with those charged by SCE.

The table below shows the average change by customer class should the rate increase be approved.

Rate	Average Total Revenue per Month	Average %	Estimated Total (\$)
General Service	\$ 109,894	10.97%	\$ 12,050
Large General Service	\$ 541,713	15.78%	\$ 85,492
Large General Service/TOU	\$ 1,045,964	9.72%	\$ 101,639
Industrial	\$ 193,217	13.37%	\$ 25,825
Pumping	\$ 8,290	13.62%	\$ 1,129
Street Lighting	\$ 27,964	7.60%	\$ 2,124
Traffic Control	\$ 2,736	15.32%	\$ 419
Residential	\$ 859,606	15.20%	\$ 130,653
Total per Month	\$ 2,758,684		\$ 359,331
Total per year	\$ 33,104,207	Annual	\$ 4,311,968
			13.0%

The new COVID-19 Utility Assistance Program is developed to offset the potential impact to residential and general service customers.

ALTERNATIVES

1. Approve the Recommended Actions presented in this report. *Staff recommends this alternative as the proposed amended electric rates will allow the City’s utility to comply with established Council-adopted policies and practices and avoid adding a General Fund liability and provides a new assistance program for those impacted by the COVID-19 pandemic.*
2. Do not approve Recommended Actions presented in this report. *Staff does not recommend this alternative because the resolution is needed to keep the Utility in compliance with established Council-adopted policies and practices and will result in an annual General Fund liability of \$4,311,968*

FISCAL IMPACT

The new COVID-19 Utility Assistance Program will cost approximately \$250,000 annually. The proposed rate adjustment is estimated to increase revenue by an average of approximately \$360,000 per month. With approval of the rates, and no further energy cost increases, it is anticipated that net income will be positive until April 2021.

Without the rate increase as proposed, MVU is anticipated to operate in a structural deficit for fiscal year 2020/2021 and future years. This would produce an annual General Fund liability of at least \$4,311,968 and may require the use of General Fund reserves each fiscal year, without the reduction of other services provided by the General Fund. Future rate adjustments and cost increases will have additional future impacts, which will increase this General Fund liability.

The following table reflects the budget adjustments to increase funding for the Utility Payment Assistance Program, paid for with Public Purpose Funds. Public Purpose Program funds can only be utilized under a strict umbrella of programs, determined at the State level of government.

Description	Fund	GL Account No.	Type (Rev/Exp)	FY 20/21 Budget	Proposed Adjustments	FY 20/21 Amended Budget
Utility Assistance Programs	Public Purpose Program Fund	6012-70-80-45511-710152	Exp	\$200,000	\$250,000	\$450,000

As MVU is still growing, the utility has been able to break even for certain operational costs but has not yet fully established or maintained desired reserve fund levels. As such, the General Fund continues to be a backstop to the \$37 million of estimated reserves required for Operating Reserves, Emergency Reserves, Capital Reserves, and Rate Stabilization. These four reserve categories are necessary to maintain a financially strong utility for the future.

The City has also issued over \$62 million of bonds for utility infrastructure improvements, which are secured by the Base Rental Payments and other payments paid by the City and received by the Authority pursuant to the Facilities Sublease (as defined in the Trust Indenture).

NOTIFICATION

Publication of the Agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Jeannette Olko
Electric Utility Division Manager

Department Head Approval:
Michael L. Wolfe, P.E.
Assistant City Manager/Public Works Director/City Engineer

Concurred By:
Marshall Eyerman
Assistant City Manager/Chief Financial Officer

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

Objective 4.1: Develop a Moreno Valley Utility Strategic Plan to prepare for the 2022 expiration of the ENCO Utility Systems agreement.

ATTACHMENTS

- 1. Resolution Rate Adjustment 12152020
- 2. MVU Rates Final 12152020
- 3. MVU COVID-19 Energy Assistance Brochure Draft 12-03-20 V2 p1V2

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/07/20 5:00 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/07/20 6:21 PM

RESOLUTION NO. 2020-XX

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, TO CONFIRM THE ELECTRIC RATES AND RULES FOR MORENO VALLEY UTILITY

WHEREAS, the City of Moreno Valley (the "City"), a municipal corporation, is authorized pursuant to Article XI, Section 9(a) of the California Constitution to establish, purchase, and operate public works to furnish its inhabitants with light, water, power, heat, transportation, or means of communication; and

WHEREAS, on June 26, 2001, the City Council of the City of Moreno Valley approved Resolution No. 2001-33 and, as amended by Resolution 2002-46, authorized the formation of a municipally owned utility for the purpose of providing electrical power, storm water, telephone telecommunications, cable TV, water, natural gas, and sanitary sewer; and

WHEREAS, on July 8, 2003, the City Council approved Resolution No. 2003-58 adopting the Electric Service Rules, Fees and Charges document for Moreno Valley Utility which states, in part, that the rates to be charged by and paid to the City for electric service will be the rates legally in effect and on file with the City Council; and

WHEREAS, on January 13, 2004, the City Council approved Resolution No. 2004-05 establishing the electric rates for Moreno Valley Utility; and

WHEREAS, on September 26, 2006, the City Council approved Resolution No. 2006-112 implementing a schedule to adjust Moreno Valley Utility electric rates to reflect the same schedule as Southern California Edison; and

WHEREAS, there are sections of the Electric Service Rules, Fees and Charges document that contain rules which define the terms and conditions under which electric service will be provided to the customer; and

WHEREAS, there are rules, fees, charges, and rates associated with providing the services identified in these documents. These rules, fees, charges, and rates are deemed necessary and equitable for services rendered and are required to fund in whole or in part, all of the services required to facilitate the delivery of electric distribution pursuant to the rules; and

WHEREAS, Urgency Ordinance No. 651 was adopted by the City Council on December 9, 2003, allowing for the adoption of rates by resolution.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

1
Resolution No. 2020-XX
Date Adopted: December 15, 2020

- 1. The City Council hereby confirms the amended Moreno Valley Utility Rates, attached hereto as Exhibit A, incorporated herein, and on file in the Public Works Department.

APPROVED AND ADOPTED this 15th day of December 2020.

 Mayor of the City of Moreno Valley

ATTEST:

 City Clerk

APPROVED AS TO FORM:

 City Attorney

Attachment: Resolution Rate Adjustment 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

2
 Resolution No. 2020-XX
 Date Adopted: December 15, 2020

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Resolution No. 2020-XX was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the 15th day of December 2020 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

Attachment: Resolution Rate Adjustment 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Resolution No. 2020-XX³
Date Adopted: December 15, 2020

Moreno Valley Utility
Electric Rates

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

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Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

SCHEDULE A – RESIDENTIAL SERVICE

Applicability

Applicable to electric service for residential uses. Rate A is applicable to all residential customers with the exception of residential customers with solar generation installations that file an application on or after December 15, 2020. For these customers, Rate B Residential Time of Use (TOU) is applicable. All other residential customers may select Rate B Residential Time of Use (TOU) if desired.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Basic Charge - \$/Day:	Rate A – Non TOU
Single-Family Residence	\$ 0.031
Multi-Family Residence	\$ 0.024
 Energy Usage Charge - \$/kWh:	
Summer:	
Tier 1 -Baseline Quantities, all kWh, per kWh	\$ 0.20226
Tier 2 – 101% to 400% of Baseline	\$ 0.26368
 Tier 3 – All excess kWh, per kwh	 \$ 0.33291
 Winter:	
Tier 1 -Baseline Quantities, all kWh, per kWh	\$ 0.20226
Tier 2 – 101% to 400% of Baseline	\$ 0.26368
 Tier 3 – All excess kWh, per kWh	 \$ 0.33291
 Public Purpose Programs:	
All kWh per kWh	\$ 0.01323
 Monthly Minimum Charge:	
Monthly Minimum Charge	\$ 10.00

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Rates

Basic Charge - \$/Day:	Rate B - TOU
Single-Family Residence	\$ 0.031
Multi-Family Residence	\$ 0.024
 Energy Usage Charge - \$/kWh:	
Baseline Credit – Applies to 100% of baseline allocation regardless of time of use	-\$0.07848
 Summer	
On-Peak	\$ 0.40144
Mid-Peak	\$ 0.32324
Off-Peak	\$ 0.24623
 Winter	
Mid-Peak	\$ 0.34746
Off-Peak	\$ 0.26027
Super Off-Peak	\$ 0.23519
 Public Purpose Programs:	
All kWh per kWh	\$ 0.01323
 Monthly Minimum Charge:	
Monthly Minimum Charge	\$ 10.00

Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Special Conditions

1. Baseline Rates: Baseline rates are applicable only to separately metered residential use.

- 2. Baseline Quantities: The residential allocation shall be 18.9 kWhs per day in the Summer season and 12.5 kWhs per day in the Winter season.
- 3. Existing generating facilities currently under Schedule NEM or NEM 2.0 that are modified such that the generating capacity or output increases by 10% or more or if they have storage are required to be billed under Rate B.
- 4. Time periods are defined as follows:

TOU Period	Weekdays	Weekends & Holidays	Weekdays	Weekends & Holidays
	Summer	Summer	Winter	Winter
On-Peak	4 p.m. - 9 p.m.	N/A	N/A	N/A
Mid-Peak	N/A	4 p.m. - 9 p.m.	4 p.m. - 9 p.m.	4 p.m. - 9 p.m.
Off-Peak	All other hours	All other hours	9 p.m. - 8 a.m.	9 p.m. - 8 a.m.
Super-Off-Peak	N/A	N/A	8 a.m. - 4 p.m.	8 a.m. - 4 p.m.

- 5. Holidays are defined as New Year’s Day (January 1), Martin Luther King’s Birthday (third Monday in January), Washington’s Birthday (third Monday in February), Memorial Day (last Monday in May), Independence Day (July 4), Labor Day (first Monday in September), Veterans Day (November 11), Thanksgiving Day (fourth Thursday in November), and Christmas Day (December 25).

When any holiday listed above falls on Sunday, the following Monday will be recognized as an off-peak period. No change will be made for holidays falling on Saturday.

- 6. Summer and Winter Seasons are defined as follows: The Summer season begins at 12:00 a.m. on June 1 and will continue until 12:00 a.m. on October 1 each year. The Winter season begins at 12:00 a.m. on October 1 and continues until 12:00 a.m. on June 1 of the following year.
- 7. Voltage: Service will be supplied at one standard voltage.
- 8. For the purposes of applying the Basic Charge, the following definitions shall be used:

Single-Family Residence - A building of single occupancy which does not share common walls, floors, or ceilings with other residential dwelling units.

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Multi-Family Residence - Apartments, mobile homes, condominiums, townhouses, or a building of multiple occupancy which shares common walls and /or floors and ceilings with other residential dwelling units.

- 9. Medical Baseline Allocation: Upon application and acceptance of a certification from a medical doctor or osteopath licensed to practice medicine in California, eligible residential customers are provided a standard year-round medical baseline allocation of 16.5 kWh per day in addition to the applicable baseline allocation for the season.

	Regular Baseline Daily kWh Allocation	Additional Medical Baseline Daily kWh Allocation	Total Baseline Daily kWh Allocation
Summer	18.9	16.5	35.4
Winter	12.5	16.5	29.0

Medical Baseline Allocation Eligibility:

- a) Regular use in the customer's home of one or more medical life-support devices essential to maintain the life of a full-time resident of the household; and/or
- b) A full-time resident of the household is: a paraplegic, hemiplegic, quadriplegic, multiple sclerosis or scleroderma patient, being treated for life-threatening illness, and/or has a compromised immune system.

Life support devices are those devices or equipment that utilize mechanical or artificial means to sustain, restore or supplant a vital function, or mechanical equipment relied upon for mobility both within and outside of buildings.

Life-support devices include:

Aerosol Tent	Ultrasonic Nebulizer
Pressure Pad	Electrostatic Nebulizer
Apnea Monitor	Inhalation Pulmonary Pressure
Pressure Pump	Breather Machine (IPPB)
Compressor	Iron Lung
Concentrator	Dialysis Machine
Respirator (all types)	Hemodialysis Machine
Electronic Nerve Stimulator	Motorized Wheelchair
Suction Machine	Oxygen Generator

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Applying for the Medical Baseline Allocation:

1. Request application from Moreno Valley Utility by telephone, mail or in person
2. Complete application.
3. The patient's physician will need to fill out the required information on the application and sign it certifying the medical need.
4. The customer can mail or bring the application to Moreno Valley Utility's offices
5. Once the application is reviewed and approved, the Medical Baseline Allocation will be effective on the next regular electric billing.
6. Applications must be renewed every two years.
7. Low Income Program - A low-income assistance discount program is offered under this standard residential rate. To be considered for this discount, an application must be filed with Moreno Valley Utility. To be eligible for this discount, the income of the customer, including all members of the household, must meet the income levels of the program and can be no more than 200% of Federal Poverty Guidelines. Under this program a discount for qualified low-income residents of 30% is provided on monthly energy charges. Discount applies to energy charges only. The customer charge, public purpose charge, service fees and all taxes are calculated at the standard rates.
8. Family Electric Rate Assistance (FERA) Program: The FERA discount program is offered under the standard residential rate. To be considered for this discount, an application must be filed with Moreno Valley Utility. To be eligible for this discount the household must consist of three or more persons where the total gross income from all sources is no more than 250% of Federal Poverty Guidelines. Under this program a discount for qualified FERA households of 18% is provided on monthly energy charges. Discount applies to energy charges only. The customer charge, public purpose charge, service fees and all taxes are calculated at the standard rates.

SCHEDULE B – GENERAL SERVICE

Applicability

Applicable to nonresidential electric service for all types of uses including lighting and power. Customers whose monthly maximum demand is expected to exceed 20 kW or has exceeded 20 kW in any three months during the preceding 12 months, are ineligible for service under this schedule.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Customer Charge - \$/Day:

Single-Phase Service	\$ 0.397
Polyphase Service	\$ 0.428

Energy Usage Charge - \$/kWh:

Summer, all kWh, per kWh	\$ 0.21318
Winter, all kWh, per kWh	\$ 0.16520

Public Purpose Programs:

All kWh per kWh	\$ 0.00937
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Monthly Minimum Charge:

Monthly Minimum Charge	\$ 10.00
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Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Special Conditions

1. Summer and Winter Seasons are defined as follows: The Summer season begins at 12:00 a.m. on June 1 and will continue until 12:00 a.m. on October 1 each year. The Winter season begins at 12:00 a.m. on October 1 and continues until 12:00 a.m. on June 1 of the following year.

2. Voltage: Service will be supplied at one standard voltage.
3. Direct Current Fast Charger (DCFC) Charging Stations: DCFC station installations shall be billed on this rate regardless of the kilowatt demand until other rates are adopted for this use.

SCHEDULE C – LARGE GENERAL SERVICE

Applicability

Applicable to nonresidential electric service for all types of uses including lighting and power where the customer’s monthly maximum demand is expected to exceed 20 kW or has exceeded 20 kW in any of the 3 months during the preceding 12 months.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Customer Charge - \$/Meter/Month:

Single Phase	\$ 136.73
Polyphase	\$ 144.74

Energy Usage Charge - \$/kWh:

Summer, all kWh, per kWh	\$ 0.12471
Winter, all kWh, per kWh	\$ 0.10686

Demand Charge - \$/kW:

	<u>Summer</u>	<u>Winter</u>
Facilities Related Demand Charge, per kW	\$ 12.15	\$ 12.15
Time Related Demand Charge, per kW	\$ 16.23	\$ 0.00

Public Purpose Programs:

All kWh per kWh	\$ 0.00941
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Monthly Minimum Charge:

Monthly Minimum Charge	\$ 10.00
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Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Special Conditions

1. Summer and Winter Seasons are defined as follows:

The Summer season begins at 12:00 a.m. on June 1 and will continue until 12:00 a.m. on October 1 each year. The Winter season begins at 12:00 a.m. on October 1 and continues until 12:00 a.m. on June 1 of the following year.

2. Voltage: Service will be supplied at one standard voltage.

3. Billing Demand: The Billing Demand shall be the kilowatts of Maximum Demand, determined to the nearest kW. The Billing Demand shall be the greater of the kilowatts of Maximum Demand recorded (or established for) the monthly billing period or 50% of the highest Maximum Demand established in the preceding eleven months (Ratcheted Demand).

4. Maximum Demand: The maximum demand in any month shall be the measured maximum average kilowatt input, indicated or recorded by instruments to be supplied by the City, during any 15-minute metered interval in the month.

5. Voltage Discount: The monthly Facilities Related Demand Charge will be reduced by \$0.21 per kW for service delivered and metered at voltages of 4 kV through 12 kV. The energy charge will be reduced by \$.00101 per kWh for service delivered and metered at voltages of 2 kV through 12 kV.

6. Excess Transformer Capacity: Excess Transformer Capacity is the amount of transformer capacity requested by a customer in excess of that which the City would normally install to serve the customer's Maximum Demand. Excess Transformer Capacity shall be billed at the amount shown in the rates section above.

7. Power Factor Adjustment: When Maximum Demand has exceeded 200 kW for three consecutive months, kilovar metering will be installed as soon as practical, and thereafter, until the Maximum Demand has been less than 150 kW for twelve consecutive months, the billing will be adjusted each month for power factor.

a. Adjustment Rate:

i. For service delivered and metered at voltages 12 kV or less, the billing will be increased by \$0.60 per kilovar of maximum reactive demand.

b. Determining the Reactive Demand:

i. Service delivered and metered at voltages of 4 kV or greater:

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

1. The maximum reactive demand shall be the highest measured maximum average kilovar demand indicated or recorded by metering during any 15-minute metered interval in the month. The kilovars shall be determined to the nearest unit. A device will be installed on each kilovar meter to prevent reverse operation of the meter.
- ii. Services delivered and metered at voltages less than 4 kV:
1. For customers with metering used for billing that measures reactive demand, the maximum reactive demand shall be the highest measured maximum average kilovar demand indicated or recorded by metering during any 15-minute metered interval in the month. The kilovars shall be determined to the nearest unit. A device will be installed on each kilovar meter to prevent reverse operation of the meter.
 2. For customers with metering used for billing that measures kilovar-hours instead of reactive demand, the kilovars of reactive demand shall be calculated by multiplying the kilowatts of measured maximum demand by the ratio of the kilovar-hours to the kilowatt-hours. Demands in kilowatts and kilovars shall be determined to the nearest unit. A ratchet device will be installed on the kilovar-hour meter to prevent its reverse operation on leading power factors.

SCHEDULE P1 – PUMPING AND AGRICULTUAL SERVICE
(CONNECTED LOAD BASIS)

Applicability

Applicable to electric service for agricultural power service or for general water pumping or sewerage pumping based on connected load in horsepower. This schedule is not applicable to service for which a residential, commercial or industrial schedule is applicable. Customers whose monthly maximum demand is expected to or have exceeded 500 kW or 671 hp in any three months during the preceding 12 months, are ineligible for service under this schedule.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Customer Charge - \$/Day:	\$51.09
Energy Usage Charge - \$/kWh:	
Summer, all kWh, per kWh	\$ 0.12807
Winter, all kWh, per kWh	\$ 0.12807
Service Charge \$/HP/Month	\$4.36
Public Purpose Programs:	
All kWh per kWh	\$ 0.00858
Monthly Minimum Charge:	
Monthly Minimum Charge	\$ 10.00

Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Special Conditions

1. Summer and Winter Seasons are defined as follows: The Summer season begins at 12:00 a.m. on June 1 and will continue until 12:00 a.m. on October 1 each year. The Winter season begins at 12:00 a.m. on October 1 and continues until 12:00 a.m. on June 1 of the following year.
2. Voltage: Service will be supplied at one standard voltage.
3. Connected Load: Connected load is the sum of the rated capacities of all the customer's equipment that is possible to connect to the utility's lines at the same time, determine to the nearest 1/10th hp.

SCHEDULE SL – STREET LIGHTING SERVICE - MVU OWNED SYSTEM

Applicability

Applicable to un-metered service for the lighting of streets and highways where MVU owns and maintains the street lighting equipment and associated facilities included under this schedule.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Energy Usage Charge - High Pressure Sodium Vapor Lamps

Basic Charge:

<u>Initial Lumens</u>	<u>Wattage</u>	<u>All Night Service kWhs/Month</u>	<u>\$/Lamp/Month</u>	<u>\$/Lamp/Month Public Purpose Programs</u>
9,500	100	40	\$13.38	\$0.13
16,000	150	67	\$16.66	\$0.22
22,000	200	85	\$18.99	\$0.28
27,500	250	108	\$21.58	\$0.35

Energy Usage Charge – Light Emitting Diode (LED) Lamps

Basic Charge:

<u>Initial Lumens</u>	<u>Wattage</u>	<u>All Night Service kWhs/Month</u>	<u>\$/Lamp/Month</u>	<u>\$/Lamp/Month Public Purpose Programs</u>
14,700	173	75	\$17.73	\$0.25
11,500	98	47	\$14.23	\$0.16
3,800	31	15	\$ 10.22	\$0.05

Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Special Conditions

1. Maintenance shall include periodic inspection, renewal of lamps, cleaning of glassware, replacement of damaged glassware and lamps, and minor repairs to wiring and electrical appurtenances.
2. Hours of Service: Under MVU's standard all-night operating schedule, approximately 4,140 hours of service will be furnished.
3. The developer shall install streetlights that will be served from MVU's underground system. These streetlights must be installed in accordance with MVU's specifications and the developer will deed such facilities to MVU.
4. Requirements and Restrictions:
 - a. The applicant for street light service shall specify the lamp size and location of streetlights.
 - b. Service shall not be furnished under this schedule where location, mounting height, or other considerations are unacceptable to the MVU.
 - c. The installation of street lighting equipment and facilities hereunder is contingent upon the MVU obtaining easements, rights of way, and highway permits satisfactory to the MVU for the required poles, equipment, and facilities.
 - d. In accordance with Rule No. 4, a written contract for a term of not less than one year is required in order to receive street light service under the provisions of this schedule.
 - e. Should the applicant not commence using the street lighting in a bona fide manner within ninety (90) days after date of completion and installation of a street light or street lighting system requested by the applicant, the MVU will bill, and the applicant shall pay, the applicable lamp charge(s).
5. Liability of Utility: MVU shall not, by taking action pursuant to its tariffs, be liable for any loss, damage, or injury, established or alleged, which may result, or be claimed to result, therefrom.

SCHEDULE SL2 - STREET LIGHTING SERVICE
CUSTOMER OWNED AND MAINTAINED SYSTEM SCHEDULE
(UNMETERED)

Applicability

Applicable to service for un-metered lighting of streets, highways, and directional highway signs served in conjunction with street and highway lighting, and other publicly operated automobile parking lots which are open to the general public, where the customer owns and maintains the street lighting equipment operated within the period from dusk to dawn.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Energy Usage Charge - High Pressure Sodium Vapor Lamps

Basic Charge:

<u>Initial Lumens</u>	<u>Wattage</u>	<u>All Night Service kWhs/Month</u>	<u>\$/Lamp/Month</u>	<u>\$/Lamp/Month Public Purpose Programs</u>
9,500	100	40	\$ 6.87	\$0.13
16,000	150	67	\$ 9.75	\$0.22
22,000	200	85	\$ 11.75	\$0.28
27,500	250	108	\$14.28	\$0.35

Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Special Conditions

1. Voltage: Service will be supplied at one standard voltage.
2. Requirements and Restrictions:
 - a. The applicant for street light service shall specify the lamp size and location of streetlights.
 - b. Service shall not be furnished under this schedule where location, mounting height, or other considerations are unacceptable to the MVU.
 - c. The installation of street lighting equipment and facilities hereunder is contingent upon the MVU obtaining easements, rights of way, and highway permits satisfactory to the MVU for the required poles, equipment, and facilities.
3. Liability of Utility: MVU shall not, by taking action pursuant to its tariffs, be liable for any loss, damage, or injury, established or alleged, which may result, or be claimed to result, therefrom.

SCHEDULE SL3 – STREET LIGHTING SERVICE
CUSTOMER OWNED SYSTEM SCHEDULE
(METERED)

Applicability

Applicable to service for metered lighting service of streets, highways, and directional highway signs served in conjunction with street and highway lighting, and other publicly operated automobile parking lots which are open to the general public, where the customer owns the street lighting equipment operated within the period from dusk to dawn.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Customer Charge – Per Meter Per Month:	\$ 8.56
Energy Usage Charge - \$/kWh:	
All Year - all kWh, per kWh	\$ 0.08179
Public Purpose Programs:	
All kWh, per kWh	\$ 0.00327

Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Special Conditions

1. Voltage: Service will be supplied at one standard voltage.
2. The customer will furnish and maintain all equipment beyond the meter.

SCHEDULE TC-1 – TRAFFIC CONTROL SERVICE

Applicability

Applicable to service for traffic directional sign or signal lighting service owned by governmental agencies and located on streets, highways and other publicly dedicated outdoor ways and places.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates

Customer Charge – Per Meter Per Day:

Single-Phase Service	\$ 0.530
Polyphase Service	\$ 0.559

Energy Usage Charge - \$/kWh:

All kWh per kWh	\$ 0.15366
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Public Purpose Programs:

All kWh per kWh	\$ 0.00915
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Energy Cost Adjustment

1. The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Special Conditions

1. Voltage: Service will be supplied at one standard voltage.

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

SCHEDULE TOU-LGS – TIME OF USE – LARGE GENERAL SERVICE

Applicability

Applicable to nonresidential electric service for all types of uses including lighting and power where the customer’s monthly maximum demand is expected to exceed 500 kW or has exceeded 500 kW in any of the 3 months during the preceding 12 months.

Territory

Within the designated areas served by the Moreno Valley Utility.

Rates – Primary Voltage

Customer Charge:

\$/Meter/Month \$ 277.40

Energy Usage Charge - \$/kWh:

Summer

On-Peak \$ 0.10119
 Mid-Peak \$ 0.09337
 Off-Peak \$ 0.06807

Winter

Mid-Peak \$ 0.08208
 Off-Peak \$ 0.07266
 Super Off-Peak \$ 0.05500

Demand Charge - \$/kW:

	<u>Summer</u>	<u>Winter</u>
Facilities Related Demand Charge, per kW	\$13.10	\$13.10
Time Related Demand Charge, per kW		
On-Peak	\$32.939	\$0.00
Mid-Peak	\$0.00	\$8.16
Off-Peak	\$0.00	\$0.00

Public Purpose Programs:

All kWh per kWh \$0.01015

Minimum Monthly Charge See Conditions #4

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

Rates – Secondary Voltage

Customer Charge:

\$/Meter/Month \$ 521.25

Energy Usage Charge - \$/kWh:

Summer

On-Peak \$ 0.10758
 Mid-Peak \$ 0.09927
 Off-Peak \$ 0.07228

Winter

Mid-Peak \$ 0.08709
 Off-Peak \$ 0.07713
 Super Off-Peak \$ 0.05844

Demand Charge - \$/kW:

	<u>Summer</u>	<u>Winter</u>
Facilities Related Demand Charge, per kW	\$13.37	\$13.37
Time Related Demand Charge, per kW:		
On-Peak	\$33.89	\$0.00
Mid-Peak	\$ 0.00	\$8.17
Off-Peak	\$ 0.00	\$0.00

Public Purpose Programs:

All kWh per kWh \$ 0.01070

Minimum Monthly Charge:

Minimum Monthly Charge See Condition #4

Energy Cost Adjustment

- The energy charge may be adjusted each month based upon the percentage of the energy being provided by the Department of Water Resources to the investor owned utility on the billing date monthly. These adjustments could result in slight decreases or increases in the energy charge.

Special Conditions

- Time periods are defined as follows:

TOU Period	Weekdays	Weekends & Holidays	Weekdays	Weekends & Holidays
	Summer	Summer	Winter	Winter
On-Peak	4 p.m. - 9 p.m.	N/A	N/A	N/A

Mid-Peak	N/A	4 p.m. - 9 p.m.	4 p.m. - 9 p.m.	4 p.m. - 9 p.m.
Off-Peak	All other hours	All other hours	9 p.m. - 8 a.m.	9 p.m. - 8 a.m.
Super-Off-Peak	N/A	N/A	8 a.m. - 4 p.m.	8 a.m. - 4 p.m.

Holidays are defined as New Year’s Day (January 1), Martin Luther King’s Birthday (third Monday in January), Washington’s Birthday (third Monday in February), Memorial Day (last Monday in May), Independence Day (July 4), Labor Day (first Monday in September), Veterans Day (November 11), Thanksgiving Day (fourth Thursday in November), and Christmas Day (December 25).

When any holiday listed above falls on Sunday, the following Monday will be recognized as an off-peak period. No change will be made for holidays falling on Saturday.

2. Summer and Winter Seasons are defined as follows: The Summer season begins at 12:00 a.m. on June 1 and will continue until 12:00 a.m. on October 1 each year. The Winter season begins at 12:00 a.m. on October 1 and continues until 12:00 a.m. on June 1 of the following year.
3. Voltage: Service will be supplied at one standard voltage.
4. Billing Demand: The Billing Demand shall be the kilowatts of Maximum Demand, determined to the nearest kW. The Billing Demand shall be the greater of the kilowatts of Maximum Demand recorded (or established for) the monthly billing period or 50% of the highest Maximum Demand established in the preceding eleven months (Ratcheted Demand).
5. Maximum Demand: The maximum demand in any month shall be the measured maximum average kilowatt input, indicated or recorded by instruments to be supplied by the City, during any 15-minute metered interval in the month.
6. Excess Transformer Capacity: Transformer Capacity is the amount of transformer capacity requested by a customer in excess of that which the City would normally install to serve the customer’s Maximum Demand. Excess Transformer Capacity shall be billed at the amount shown in the rates section above.
7. Power Factor Adjustment: The billing will be adjusted each month for power factor.
 - a. Adjustment Rate: The customer’s bill will be increased each month for the power factor \$0.60 per kilovar of maximum reactive demand.
 - b. The maximum reactive demand shall be the highest measured maximum average kilovar demand indicated or recorded by metering during any 15-

minute metered interval in the month. For customers with metering used for billing that measures kilovar-hours instead of reactive demand, the kilovars of reactive demand shall be calculated by multiplying the kilowatts of measured maximum demand by the ratio of the kilovar-hours to the kilowatt-hours. Demands in kilowatts and kilovars shall be determined to the nearest unit. A device will be installed on the kilovar-hour meter to prevent its reverse operation on leading power factors.

Attachment: MVU Rates Final 12152020 (4174 : APPROVE A NEW COVID-19 UTILITY ASSISTANCE PROGRAM)

SCHEDULE SE - SERVICE ESTABLISHMENT CHARGE

Applicability

Applicable to general service and domestic service customers.

Territory

Within the entire territory served by Moreno Valley Utility.

Rate

For each establishment of electric service, a charge will apply.

Special Conditions

1. The service establishment charge is in addition to the charges calculated on the applicable rate schedule and will be made each time an account is established.
2. Establishment means each time an account is opened, including a turn on of electric service or a change of name that requires a meter reading.
3. If the customer requests electric service be established on the same day as his request or outside regular business hours, an additional charge will apply.

SCHEDULE NEM – NET ENERGY METERING

Applicability

Applicable to general service and domestic service customers who have eligible renewable energy generation systems connected to MVU’s system (interconnected) and meet program requirements. This schedule is closed to new applicants effective April 2018.

Territory

Within the entire territory served by Moreno Valley Utility.

Net Surplus Compensation Rate

The net surplus compensation rate shall be \$0.02490 per kWh applied to any net surplus energy remaining at the end of the customer’s twelve (12) monthly billing period (“relevant period”).

Special Conditions

1. NEM customers will receive a credit for the surplus electricity supplied to MVU’s system.
2. This credit will be applied to the customer’s energy bill, to offset all or part of the costs associated with the energy that is consumed each month.
3. Residential accounts are billed once a year for “net” energy consumed or generated over the previous 12 months, if any.
4. Small business accounts served under the General Service Rate also qualify for annual billing.
5. Large business NEM accounts under the Large General Service Rate are billed monthly for their energy usage.
6. Net surplus energy is the amount of generated kilowatt-hours (kWh) energy that is exported to MVU’s system that exceeds the amount that is received from MVU.
7. Any net surplus energy remaining at the end of the 12-month billing period (also called the “relevant period”) will be given a monetary value known as the Net Surplus Compensation Rate (NSCR).
8. The NSCR value is established by MVU to reflect the costs MVU avoids in procuring power during the time period net surplus generators are likely to produce excess power.

9. Customers may choose to either roll over the monetary value of any net surplus energy to the next billing cycle or receive payment for any net surplus energy at the end of your 12-month relevant period.
10. Customers will be billed monthly for nominal non-energy-related charges such as taxes.

SCHEDULE NEM 2.0 – NET ENERGY METERING SUCCESSOR RATE

Applicability

Applicable to Eligible Customer-Generators, as defined in Section 2827 of the California Public Utilities Code, operating a renewable electrical generation facility, as therein defined, located on the customer's owned, leased, or rented premises with a capacity of no more than one megawatt that is intended primarily to offset part or all of the customer's own electrical requirements and which is interconnected and operates in parallel with MVU's power system pursuant to Electric Rule 21 – Generating Facility Interconnections.

Territory

Within the entire territory served by Moreno Valley Utility.

Net Surplus Compensation Rate

The net surplus compensation rate shall be \$0.02490 per kWh applied to any net surplus energy remaining at the end of the customer's monthly billing period.

Special Conditions

1. As determined in each billing period, when a customer is a net consumer of energy, the resulting net consumed energy will be used in the calculation of all applicable energy charges.
2. As determined in each billing period, when a customer is a net producer of energy, the resulting net produced energy will be used in the calculation of a monetary value that shall only be applied to the customer's monthly bill, including any minimum charges and applicable taxes.
3. A customer is a net producer of energy when the amount of generated kilowatt-hours (kWh) of energy that is exported to MVU's system exceeds the amount that the customer receives from MVU.
4. The monetary value calculated is the product of the net kWh produced multiplied by the Net Surplus Compensation Rate (NSCR).
5. The NSCR value is established by MVU to reflect the costs MVU avoids in procuring power during the time period net surplus generators are likely to produce excess power.

6. MVU shall retain any net surplus energy generated by the NEM customer, including any associated environmental attributes or renewable energy credits (“REC”).
7. To be eligible for service under this Schedule, generating facilities must meet all applicable safety and performance standards established by the National Electrical Code, the Institute of Electrical and electronics Engineers, and accredited testing laboratories such as Underwriters Laboratories and, where applicable, rules regarding safety and reliability (i.e., MVU’s Electric Rule 21). All generating facilities must have a warranty of at least 10 years for all equipment and the associated installation from the system provider (not from MVU). All major solar system components (including PV panels and other generation equipment, inverters and meters) must be on the verified equipment list maintained by the CEC. Any other equipment, as determined by MVU, must be verified as having safety certification from a Nationally Recognized Testing Laboratory.
8. To be eligible for service under this Schedule, the customer’s generating facilities must be sized to offset part or all of the customer’s own electrical requirements and cannot be oversized. This means that the estimated output of the generating facility, using the CEC-AC nameplate rating for inverter-based generating facilities must not exceed the customer’s previous annual usage in kWh. In the event that there is less than 12 months of previous recorded usage data, the standard of 2 watts per square foot of the premises will apply.
9. Customers seeking to interconnect their generating facilities for the purpose of receiving service under this Schedule are subject to the interconnection requirements and interconnection cost responsibility provisions as established in MVU’s Electric Rule 21.
10. A new customer of record who owns, rents, or leases a premise that includes a generating facility that was approved by MVU for parallel operation prior to the new customer moving in and/or taking electric service with MVU will take service under this Schedule as long as the requirements of this Schedule are met. This provision also applies to premises where the developer/contractor establishes the interconnection.
11. Existing generating facilities currently under Schedule NEM that are modified such that: (1) the generating capacity or output increases by 10% or more; or (2) adding battery storage will be placed under Schedule NEM 2.0.

12. Existing customers under Schedule NEM will remain under Schedule NEM for a period of fifteen (15) years from the original year in which their generating facility was interconnected to MVU's grid as determined from the date the customer received the permission to operate (PTO), and then will be switched to Schedule NEM 2.0 or any otherwise applicable rate schedule. Existing customers under Schedule NEM can request to be placed under Schedule NEM 2.0 at any time; the customer's account will be trued up at the time of the request. This means that any outstanding balance due or credit due will be applied to the next regular billing.

SCHEDULE ED – ECONOMIC DEVELOPMENT (“ED”) RATE

Applicability

Commercial or industrial end-use customers that would otherwise receive service under Electric Rate Schedule TOU-LGS (Time of Use-Large General Service) and meet certain criteria as established and adopted by resolution of the City Council of the City of Moreno Valley may take advantage of the ED rate as a New Customer or Expanded Load Customer. This ED rate is applicable to all or part of the services provided to New Customers and Expanded Load Customers, as such terms are defined herein. Local Hiring Incentive applicable to certain other rate classes as described in Special Condition No. 6.

1. A New Customer shall be a customer seeking to locate a new business or relocate an existing business (not currently located within the territory served by Moreno Valley Utility) within Moreno Valley Utility’s service territory.

2. An Expanded Load Customer shall be an existing Moreno Valley Utility TOU-LGS customer that is adding new load to Moreno Valley by a minimum of 200 kW based upon the customer’s past electrical demand as determined by Moreno Valley Utility. The expanded load can be at the customer’s current site, or at a new site within the Moreno Valley Utility service territory. The ED rate will only be applied to the expanded load as determined in Section 5 below.

3. A New Customer shall meet the following criteria:
 - a. Targeted industries
 - i. Logistics/Distribution
 - ii. Medical/Healthcare
 - iii. Auto Dealerships

 - b. Job Creation

i. Tier 1 Discount Rate	150 – 499 jobs
ii. Tier 2 Discount Rate	500 – 999 jobs
iii. Tier 3 Discount Rate	greater than 1000 jobs
iv. Tier 4 Discount Rate	350 jobs minimum
v. Tier 5 Discount Rate	200 jobs minimum

 - c. City Revenue Producer – either sales tax or use tax generation
 - i. Tier 1a Discount Rate

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- ii. Tier 4 Discount Rate - minimum \$40,000 annual sales tax revenue to the City

Territory

Within the entire territory served by Moreno Valley Utility.

Character of Service

The service provided hereunder shall be alternating current with regulated frequency of 60 hertz, three-phase, or a combination single and three-phase served through one meter, at a standard voltage not to exceed 480 volts, or as may be specified by the Electric Division. To be eligible to participate all customers must have a demand meter.

Rates

Except as provided herein, or in the Economic Development Rate Agreement, all charges and provisions of the customer’s otherwise applicable rate schedule shall apply. The applicable Energy Charge and Demand Charge under the customer’s otherwise applicable rate schedule will be reduced as follows:

	Tier 1/Tier 1a	Tier 2	Tier 3	Tier 4
Year 1	19.00%	21.50%	24.00%	26.50%
Year 2	16.00%	18.50%	21.00%	23.50%
Year 3	13.00%	15.50%	18.00%	20.50%
Year 4	10.00%	12.50%	15.00%	17.50%
Year 5	7.00%	9.50%	12.00%	14.50%

	Tier 5
Years 1 – 4	20.00%
Years 5 – 8	15.00%
Years 9 – 12	10.00%
Years 13 - 16	5.00%

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Special Conditions

1. Term: Economic Development Rate Agreements entered into under this Schedule shall be for a single five-year term, except for Tier 5, which shall be for a single sixteen-year term.
2. Approval: Application of this Rate Schedule shall be subject to the approval of the City Manager or his designee, based on meeting the eligibility criteria outlined herein.
3. Agreement: The customer must sign a standard Moreno Valley Economic Development Rate Agreement in order for the rates under this Schedule to be applicable. In addition to the other terms of this Schedule, the Economic Development Rate Agreement shall require the customer to reimburse Moreno Valley for all rate reductions received under this Schedule, if the customer fails to maintain the required minimum load during the applicable term of the Agreement.
4. Minimum Load: Customers qualifying under this Schedule as a New Customer with a projected minimum monthly electric demand of at least 500 kW or as an Expanded Load Customer under Applicability Sections 1 and 2 above, respectively, must agree to maintain a minimum level of load for five years for Tiers 1 through 4 and sixteen years for Tier 5 from the date service is first rendered under this Schedule as set forth in the Economic Development Rate Agreement.
5. Jobs: Job as prescribed in Section 3c above is defined as Full Time Equivalent that is working at least 1750 hours per year. The Customer retains authority in making individual hiring decisions. This program does not require the Customer to hire any person who does not have the experience and ability to qualify such persons for a job.
6. Local Hiring Incentive: The Local Hiring Incentive is available for Tier 1 through Tier 5. Customers who qualify under Tiers 1 – 4 and voluntarily hire at least 20% of Full Time Equivalent (FTE) employees that are City of Moreno Valley residents will receive an additional discount of 2%; those Customers who hire at least 40% of Full Time Equivalent (FTE) employees that are City of Moreno Valley residents will receive an additional discount of 4%. For Customers eligible for the Tier 5 discount, the Local Hiring Incentive is an additional 1% discount for Customers who voluntarily hire at least 20% of FTE employees that are City of Moreno Valley residents. The additional 1% discount will be applied to the first five years of the sixteen-year term. Any additional discounts will apply to the Energy Charge and Demand Charge. Customers must certify the local hire percentage each year to remain eligible for the additional discount.
7. Base Period Usage: Base Period Usage shall be established and agreed to in the Economic Development Rate Agreement for Expanded Load

Customers. Base Period Usage shall be the average monthly energy use and demand for the customer during the last three years of service to the customer, from the date ending the last payment period before the date of the Agreement. Expanded Load qualifying for the rate under this Schedule shall be measured as the difference between the new monthly, meter documented energy use and demand, and the Base Period Usage.

8. State Mandated Public Purpose Program Charge: All bills rendered under this Schedule shall be subject to the Public Purpose Program Charge as established by the City Council.
9. Miscellaneous Fees and Charges: Rates charged pursuant to this Schedule shall be subject to any Energy Users Taxes, Utility Users Taxes, and any other governmental taxes, duties, or fees which are applicable to Electric Service provided to Customer by the City of Moreno Valley. Rates are also subject to adjustment, as established by the City of Moreno Valley City Council in response to federal or state climate change laws, renewable portfolio standard or other mandated legislation. These adjustments may include but are not limited to charges to mitigate the impacts of greenhouse gas emissions or “green power” premiums.
10. Expanded Load: Expanded Load customers applying for this rate must demonstrate to the satisfaction of the Utility that the expanded load is new to Moreno Valley.
11. Effective Date: The effective date of the Economic Development Rate Agreement shall commence within 12 months from the date of the City’s approval, or the Agreement becomes null and void. The Agreement becomes effective upon execution by the parties, and the Economic Development Rate commences upon written notice by customer and coincides with the customer’s normal billing cycle.
12. Reapplication: Customers who have received service under the Economic Development Rate are eligible to reapply for the rate as an Expanded Load Customer 12 months after their current Economic Development Rate Agreement has expired, if they meet the criteria therefore.
13. Restrictions: Residential customers and federal, state or local government agencies are not eligible to apply for service under this Schedule.
14. City Manager: The City Manager or his/her designee may offer to customers an Economic Development Rate and term based upon the actual cost to serve the customer. The customer must sign a Moreno Valley Economic Development Rate Agreement, and such Agreement shall be approved by the City Council. All other terms and conditions under this rate schedule shall apply.

SCHEDULE ED-BR - ECONOMIC DEVELOPMENT- BUSINESS RETENTION RATE

Applicability

This Schedule is applicable to the anchor stores at Stoneridge Towne Centre and Moreno Beach Plaza, whose building size is 25,000 square feet or larger and have 30 or more employees.

1. The Customer must demonstrate to the satisfaction of the City that relocation of its entire operation to a site outside of Moreno Valley Utility's service territory is a viable alternative or that the threat of closure of the Customer's existing facilities is otherwise imminent.
2. The Customer must provide:
 - a. An affidavit that "but for" the economic development retention rate incentives, in combination with other city-sponsored incentives, such customer would relocate outside of the City's electric service territory, and
 - b. Substantial evidence demonstrating the business has considered viable locations outside of Moreno Valley's service territory including but not limited to incentive offer letters from competing states, local jurisdictions and economic development organizations and/or real estate sale and lease agreements for competing sites, or
 - c. Substantial evidence documenting the imminent threat of facility closure, including but not limited to letters from business owners or appropriate corporate officers documenting the circumstances which have led to this imminent threat and why the Business Retention Rate is necessary to retain the business within Moreno Valley Utility's service territory.
3. The Customer must agree to maintain a minimum level of load for five years from the date service is first rendered as set forth in the Economic Development Rate Agreement for Business Retention.

Territory

Within the entire territory served by Moreno Valley Utility.

Rates

Except as provided herein, or in the Economic Development Business Retention Rate Agreement, all charges and provisions of the customer’s otherwise applicable rate schedule shall apply. The applicable Energy Charge and Demand Charge under the customer’s otherwise applicable rate schedule will be reduced as follows:

- Year 1 20%
- Year 2 20%
- Year 3 20%
- Year 4 0%
- Year 5 0%

Special Conditions

1. Term: Economic Development Rate Agreement for Business Retention entered into under this Schedule shall be for a single five-year term.
2. Approval: Application of this Rate Schedule shall be subject to the approval of the Public Works Director or his designee, based on meeting the eligibility criteria outlined herein.
3. Agreement: The customer must sign a standard Moreno Valley Economic Development Rate Agreement for Business Retention in order for the rates under this Schedule to be applicable. In addition to the terms of this Schedule, the Economic Development Rate Agreement for Business Retention shall require the customer to reimburse Moreno Valley for all rate reductions received under this Schedule, if the customer fails to maintain the required minimum load during the five-year term of the Agreement.
4. Minimum Load: All customers must agree to maintain a minimum level of load for five years from the date service is first rendered under this Schedule as set forth in the Economic Development Rate Agreement for Business Retention.
5. State Mandated Public Purpose Charge: All bills rendered under this Schedule shall be subject to the Public Purpose Charge as established by the City Council.
6. Miscellaneous Fees and Charges: Rates charged pursuant to this Schedule shall be subject to any Energy Users Taxes, Utility Users Taxes, and any other governmental taxes, duties, or fees which are applicable to Electric Service provided to Customer by the City of Moreno Valley. Rates are also subject to adjustment, as established by the City of Moreno Valley City Council in response to federal or state climate change laws, renewable portfolio standard or other mandated legislation. These adjustments may

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include but are not limited to charges to mitigate the impacts of greenhouse gas emissions or “green power” premiums.

7. Effective Date: The Agreement becomes effective upon execution by the parties, and the Economic Development Business Retention Rate commences with the customer’s normal billing cycle following execution of the Agreement by both parties.
8. Restrictions: Residential customers, small commercial customers, and federal, state or local government agencies are not eligible to apply for service under this Schedule.

SCHEDULE EV PUBLIC – ELECTRIC VEHICLE PUBLIC CHARGING

Applicability

This Schedule is applicable to electric vehicle charging stations owned and maintained by Moreno Valley Utility.

Charging type	Voltage
Level 2	240V
Level 3	480V

Territory

Within the entire territory served by Moreno Valley Utility.

Rates

Level 2 City Owned Charging Station	\$0.21 per kWh
Level 3 City Owned Charging Station	\$0.35 per kWh

Per Ordinance 942, there is a four-hour maximum for parking and charging of electric vehicles in a single charging session. Sessions will be given a 30-minute grace period and thereafter will be charged \$1.00 per hour up to a maximum of \$30.00.

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SCHEDULE WTR – WIRELESS TECHNOLOGY RATE

Applicability

This Schedule is applicable to single-phase service for wireless technology industries and utility customers deploying advanced metering infrastructure (AMI) that require electric service to operate wireless communication devices that are mounted on existing utility facilities, or other facilities approved by the utility and are unmetered.

The monthly kilowatt-hour (kWh) usage of each device shall not exceed 2,700 kWh. Effective with the date the customer becomes ineligible for service under this Schedule, the customer’s account shall be transferred to Schedule B - General Service or another applicable rate schedule.

Territory

Within the entire territory served by Moreno Valley Utility.

Rates

Customer Charge - \$/Month:

Single Phase	\$22.78
Polyphase	\$20.97

Inspection Charge - \$/Device/Inspection \$15.23

Initialization of Service Charge – One-Time Fee

Fixed Energy Charge - \$/Device/Month: \$7.31

Tier	Energy Use	Max Watts/ Connected Load	\$/Device/ Month
1	0-50 kWhs / Month	75	\$7.68
2	51-100 kWhs / Month	149	\$15.36
3	101-150 kWhs / Month	224	\$23.04
4	151-200 kWhs / Month	298	\$30.75
5	201-250 kWhs / Month	373	\$38.42
6	251-300 kWhs / Month	448	\$46.10
7	301-350 kWhs / Month	522	\$53.79
8	351-400 kWhs / Month	597	\$61.45
9	401-450 kWhs / Month	672	\$69.15

Tier	Energy Use	Max Watts/ Connected Load	\$/Device/ Month
10	451-500 kWhs / Month	746	\$76.84
11	501-900 kWhs / Month	1,343	\$138.30
12	901-1,350 kWhs / Month	2,014	\$207.45
13	1,351-1,800 kWhs / Month	2,686	\$276.57
14	1,801-2,250 kWhs / Month	3,357	\$345.74
15	2,251-2,700 kWhs / Month	4,028	\$414.88

Public Purpose Charge – Per Device per Month

Tiers	Energy Use	\$/Device/ Month
1	0-50 kWhs / Month	\$0.45
2	51-100 kWhs / Month	\$0.92
3	101-150 kWhs / Month	\$1.37
4	151-200 kWhs / Month	\$1.83
5	201-250 kWhs / Month	\$2.29
6	251-300 kWhs / Month	\$2.74
7	301-350 kWhs / Month	\$3.20
8	351-400 kWhs / Month	\$3.66
9	401-450 kWhs / Month	\$4.12
10	451-500 kWhs / Month	\$4.58
11	501-900 kWhs / Month	\$8.23
12	901-1,350 kWhs / Month	\$12.35
13	1,351-1,800 kWhs / Month	\$16.47
14	1,801-2,250 kWhs / Month	\$20.59
15	2,251-2,700 kWhs / Month	\$24.71

Special Conditions

1. Voltage: Service will be supplied at 120 volts (one fuse per 120-volt leg).
2. Three-Phase Service: Where the utility determines, it is impractical to provide single-phase service under this Schedule three-phase service will be provided.
3. Limited Availability: This Schedule is available only where MVU determines that an applicable agency having jurisdiction has an existing code, ordinance, formal

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policy statement or requirement that prohibits above ground electrical meter facilities in the public right-of-way.

4. Determination of Monthly usage: The customer must provide the utility information from which the utility can determine the level of kWh usage to be consumed and/or level of service to be provided, such as the manufacturers' equipment specifications, data sheets, etc., and the number of devices to be installed. The utility will place the customer in the appropriate usage tier and charge according to the maximum value of that tier. The utility retains the right to perform on- site inspections to verify the energy consumption of the device(s).

5. Maximum Wattage: The rate tiers must coincide with the maximum wattage ratings listed below. The wattage information shall be provided by the customer in order to assist SCE in determining the appropriate tier.

Tiers	Energy Use	Usage Fuse Size	Maximum Watts / Connected Load Name Plat
1	0-50 kWhs / Month	KTK-3/4	75 watts
2	51-100 kWhs / Month	KTK-1	149 watts
3	101-150 kWhs / Month	KTK-1-1/2	224 watts
4	151-200 kWhs / Month	KTK-2	298 watts
5	201-250 kWhs / Month	KTK-2-1/2	373 watts
6	251-300 kWhs / Month	KTK-3	448 watts
7	301-350 kWhs / Month	KTK-3-1/2	522 watts
8	351-400 kWhs / Month	KTK-4	597 watts
9	401-450 kWhs / Month	KTK-5	672 watts
10	451-500 kWhs / Month	KTK-6	746 watts
11	501-900 kWhs / Month	KTK-10	1,343 watts
12	901-1,350 kWhs / Month	KTK-15	2,014 watts
13	1,351-1,800 kWhs / Month	KTK-20	2,686 watts
14	1,801-2,250 kWhs / Month	KTK-25	3,357 watts
15	2,251-2,700 kWhs / Month	KTK-30	4,028 watts

6. Installation: The device(s) shall be installed on utility facilities, or other facilities approved by the utility. Utility customers taking service for AML-related devices attached to utility-owned facilities may attach only to underground-fed streetlight poles. When the devices are installed on utility facilities, the installation and

removal of such device(s) will be performed at the customer’s expense. Device installation shall not be performed under this Schedule where location, mounting height, and/or other considerations are not acceptable to the utility. Unless approved by the utility, all wireless communication devices must be visible to the utility.

- 7. Modification of Facilities: No modifications can be made to the customer-owned wireless communications devices or the AMI-related devices unless approved by MVU. Where the customer requests a modification of MVU-owned facilities, and such modifications are acceptable to MVU, MVU will perform the requested modifications at the customer’s expense.
- 8. Maintenance: Upon installation of the device(s), where the utility experiences, or expects to experience, maintenance costs exceeding its normal maintenance expense resulting from, but not limited to, vandalism, the utility may require the customer to pay the excess maintenance expense.
- 9. Discontinuance and Restoration of Service: Discontinuance and restoration of service to the customer shall be completed in accordance with Rule 11.
- 10. Liability of the Utility: The utility shall not, by taking action pursuant to its tariffs, be liable for any loss, damage, or injury, established or alleged, which may result, or be claimed to result, there from.
- 11. Distribution Line Extension: Distribution line extensions shall be installed in accordance with Rule 15.
- 12. Service Extension: Services shall be installed and maintained as provided in Rule 16.
- 13. Initialization of Service Charge: A one-time charge, as shown in the RATES section of this schedule, is applied to each service account provided service under this Schedule to recover the costs of a lock and spare fuse which are required with the initialization of service.

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New COVID-19 Utility Assistance Program

We want to assist you during the difficult times we are experiencing.

For Our Small Business Customers:

We understand that many businesses — and especially small businesses — are facing new and unforeseen challenges due to COVID-19. If you are on our General Service Rate (20kW or less) and are having trouble paying your bill, we are offering flexible payment arrangements of up to 24 months. To set this up, please call us at (844) 341-6469 or you can set it up online and receive instant confirmation at moval.org.

For our Residential Customers:

We have two offers to help our Residential customers. **First**, you can qualify for an extended payment plan with up to 12 months to pay your past due balance accrued since March 2020. For a confidential, instant confirmation, please apply online at moval.org or call us at (844) 341-6469.

Second, if your income meets the income listed below OR if it has changed significantly due to COVID-19, OR if someone in your home participates in at least one of the eligible public assistance programs listed on the application, YOU MAY QUALIFY FOR OUR NEW ONE-TIME BILL ASSISTANCE.

Moreno Valley Utility (MVU) has created a new COVID-19 Utility Assistance Program to provide qualified families with a discount on their electric energy charges. We will apply the discount to all bills accumulated since March 2020 and offer a deferred payment plan.

Maximum Household Income		
Number of Persons in Household	Total Combined Annual Income*	
	Tier 1 (CARE) 30% Discount	Tier 2 (FERA) 18% Discount
1 to 2	Up to \$34,480	Not Eligible
3	Up to \$43,440	\$43,441 to \$54,300
4	Up to \$52,400	\$52,401 to \$65,500
5	Up to \$61,360	\$61,361 to \$76,700
6	Up to \$70,320	\$70,321 to \$87,900
7	Up to \$79,280	\$79,281 to \$99,100
8	Up to \$88,240	\$88,241 to \$110,300
Each Additional Person	\$8,960	\$8,960 to \$11,200

*Current Gross (before taxes) household income from all sources.

Residential Bill Discount Details – It’s Easy

- This is a discount program for qualified residents, providing an **18% or 30% discount** on monthly energy charges.
- This is a limited offer, while funding lasts. We will apply the discount at the level for which you qualify to all MVU energy bills accumulated since March 2020.
- Discount excludes Public Purpose Charges and Taxes. These are calculated at the standard rates.
- To apply for the program, electric service must be provided in the name of the applicant.

How Do Residential Customers Qualify?

- Fill out the application on the back of this brochure
- You may provide an original Transcript of Tax Return for 2019 from the US Internal Revenue Service, a transcript is required for each member of the household. Transcripts are available free of charge by mailing a completed Form 4506-T to the IRS. Form and instructions are available at www.irs.gov. ... **or**
- You may provide other documentation that shows total gross monthly income for each family member over 18 is now at a significantly lower level. Documentation may be pay stubs, unemployment benefit confirmation or other income proof received for within the last 6 weeks. ... **or**
- If you are currently on a CARES for another utility or if you are receiving public assistance or welfare payments such as aid to families with dependent children, temporary assistance to needy families or general assistance, you can qualify. Just provide proof of income from these sources for a one-month period within 6 weeks of your application date.

How Residential Customers Apply?

Please do not email your application or personal documentation

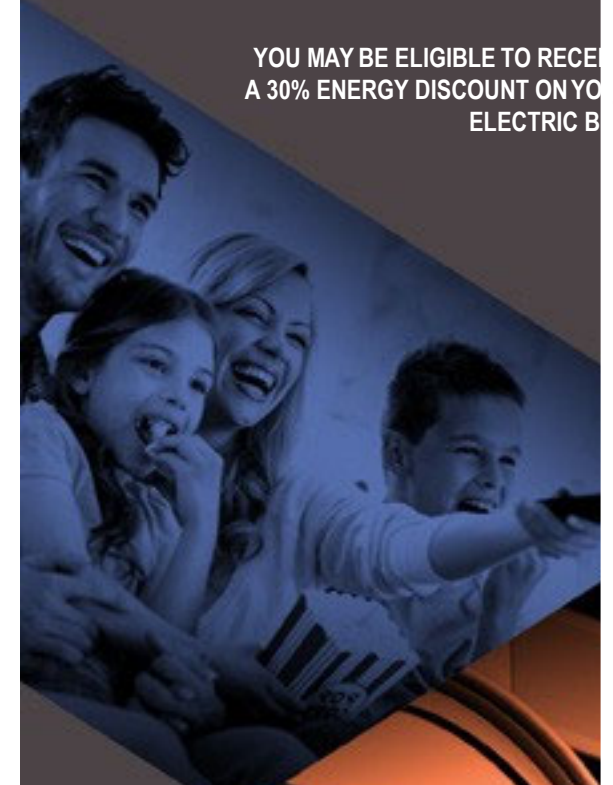
You can submit your completed application and documentation to us **by Fax: (877) 349-8870, by Mail: Moreno Valley Utility, Application Processing Center, 380 N. San Jacinto St., Hemet, CA 92543. Or at our local office, in a sealed envelope, in our drop box at: 14331 Frederick St Ste 2, Moreno Valley**

If you have questions, please call us at (844) 341-6469 or email questions to us at: mvutility@moval.org.

COVID-19 Assistance

A new discount savings program for income-qualified customers

YOU MAY BE ELIGIBLE TO RECEIVE A 30% ENERGY DISCOUNT ON YOUR ELECTRIC BILLS



Attachment: MVU COVID-19 Energy Assistance Brochure Draft 12-03-20 V2 p1V2 (4174) : APPROVE A



Report to City Council

TO: Mayor and City Council

FROM: Michael L. Wolfe P.E., Assistant City Manager

AGENDA DATE: December 15, 2020

TITLE: ADOPT RESOLUTION NO. 2020-XX, A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING A RENEWABLE ENERGY PROCUREMENT PLAN, COMPLIANCE PERIOD 3 (2017-2020) FOR MORENO VALLEY UTILITY

RECOMMENDED ACTION

Recommendation:

1. Adopt Resolution 2020-XX, a Resolution of the City of Moreno Valley adopting a Renewable Energy Resources Procurement Plan, Compliance Period 3 (2017-2020) pursuant to Public Utilities Code § 399.30(a)

SUMMARY

This report recommends approval of the Renewable Energy Resources Procurement Plan ("RPS Procurement Plan") for Compliance Period 3 via Resolution 2020-XX, as required by Public Utilities Code § 399.30(a).

DISCUSSION

California Public Utilities Code § 399.30(a) requires all publicly owned electric utilities (POUs), to "adopt and implement a renewable energy resources procurement plan" that requires the POU to procure a minimum quantity of electricity and/or renewable energy credits (RECs) from eligible renewable energy resources, as a percentage of total retail sales.

As directed by Public Utilities Code § 399.30(e), the City Council adopted the Renewable Energy Resources Enforcement Program (RPS Enforcement Program) on January 10, 2012. Through the RPS Enforcement Program, the City Council established the initial compliance periods, adopted RPS-eligible procurement targets, and described

the framework for how MVU would implement its RPS program. Section 10 of the RPS Enforcement Program directed the City Manager or his/her duly authorized designee to develop and present a Renewable Energy Resources Procurement Plan (RPS Procurement Plan) to the City Council.

Consistent with the RPS Enforcement Program and Public Utilities Code § 399.30(a), the City Council adopted an RPS Procurement Plan on June 13, 2013. The RPS Procurement Plan described MVU's intended strategy to comply with the RPS requirements in the first compliance period (2011-2013). The City Council adopted an updated RPS Procurement Plan on May 15, 2015 that described how MVU would comply with the RPS requirements during the second compliance period (2014-2016).

On October 7, 2015, Governor Brown signed SB 350 into law, adopting an RPS procurement target of 50 percent by 2030 and making various other changes to the RPS Program. On September 10, 2018, Governor Brown signed SB 100 into law, which increased the 2030 RPS target to 60 percent and set a statewide goal of meeting all electric load with carbon free resources by 2045. The specific RPS procurement targets set by SB 100 are the following: at least 44 percent by December 31, 2024, 52 percent by December 31, 2027, and 60 percent by December 31, 2030.

The State's demand of these unfunded mandates will continue to significantly increase renewable energy expenses for the utility. As an example, in fiscal year 2017/2018, renewable energy expenses totaled approximately \$790,000. Renewable energy projections for fiscal year 2020/2021 are approximately \$1.3 million. Unfortunately, non-compliance with the State's directives would result in a notice of violation from the California Energy Commission, referral to the California Air Resources Board, and potential penalty of \$50 per MWh of shortfall.

The attached MVU RPS Procurement Plan for compliance period 3 adopts procurement requirements consistent with SB 350 and SB 100, provides more recent forecast data, and updates the description of MVU's intended strategy to comply with the RPS Program requirements.

ALTERNATIVES

1. Approve the Recommended Action presented in this report. *Staff recommends this alternative as the City will be in compliance with Public Utilities Code § 399.30(a).*
2. Do not approve the Recommended Action presented in this report. *Staff does not recommend this alternative because the City will not be in compliance with Public Utilities Code § 399.30(a).*

FISCAL IMPACT

There is no fiscal impact associated with approval of the RPS Procurement Plan for Compliance Period 3.

NOTIFICATION

Publication of the Agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Jeannette Olko
Electric Utility Division Manager

Department Head Approval:
Michael L. Wolfe, P.E.
Assistant City Manager/Public Works Director/City
Engineer

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

Objective 4.1: Develop a Moreno Valley Utility Strategic Plan to prepare for the 2022 expiration of the ENCO Utility Systems agreement.

ATTACHMENTS

- 1. Resolution RPS Procurement Plan 12152020
- 2. Renewable Energy Procurement Plan Update 12152020 final

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/07/20 4:36 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/07/20 6:29 PM

RESOLUTION NO. 2020-XX

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING THE MORENO VALLEY UTILITY RENEWABLE ENERGY RESOURCES PROCUREMENT PLAN, COMPLIANCE PERIOD 3 (2017-2020) PURSUANT TO PUBLIC UTILITIES CODE § 399.30(a)

WHEREAS, the City of Moreno Valley (the "City"), a municipal corporation, is authorized pursuant to Article XI, Section 9(a) of the California Constitution to establish, purchase, and operate public works to furnish its inhabitants with light, water, power, heat, transportation, or means of communication; and

WHEREAS, on June 26, 2001, the City Council of the City of Moreno Valley approved Resolution No. 2001-33 and, as amended by Resolution 2002-46, authorized the formation of a municipally owned utility for the purpose of providing electrical power, storm water, telephone telecommunications, cable TV, water, natural gas, and sanitary sewer; and

WHEREAS, the City operates a municipal electric utility ("Moreno Valley Utility" or "MVU"); and

WHEREAS, the City Council of the City of Moreno Valley ("City Council") is the Governing Board for MVU; and

WHEREAS, as a municipal electric utility, MVU is generally subject to the legislative and regulatory requirements applicable to local publicly owned electric utilities ("POUs"); and

WHEREAS, the State of California passed Senate Bill 2 (1st Extraordinary Session) ("SBX1-2"), effective as of December 10, 2011, requiring governing boards of POU's to adopt and implement, among other things, a renewable energy resources enforcement program that requires POU's, like MVU, to procure a minimum quantity of eligible renewable energy resources over certain periods, subject to cost limitation and other flexible compliance measures adopted by the POU's' respective governing boards; and

WHEREAS, the California Energy Commission adopted regulations specifying procedures for the enforcement of the requirements of SBX1-2 on June 12, 2013, and which became effective on October 1, 2013; and

WHEREAS, on October 7, 2015, Governor Brown signed the Clean Energy and Pollution Reduction Act of 2015 (Senate Bill 350 (De Leon) ("SB 350")), with an effective date of January 1, 2016; and

WHEREAS, on September 10, 2018, Governor Brown signed Senate Bill 100 (De Leon) ("SB 100"), with an effective date of January 1, 2019; and

WHEREAS, SBX1-2, SB 350, and SB 100 require the City Council to ensure that the amount of eligible renewable energy resources that was procured by MVU for the period from January 1, 2011 to December 31, 2013 was equal to an average of 20 percent of retail sales; and that MVU must make reasonable progress to ensure that the procurement of eligible renewable energy resources achieves 25 percent of retail sales by December 31, 2016, 33 percent of retail sales by December 31, 2020, 44 percent by December 31, 2024, 52 percent by December 31, 2027, and 60 percent by December 31, 2030; and

WHEREAS, Public Utilities Code § 399.30(a) requires California's POU's to adopt a Renewable Energy Resources Procurement Plan ("RPS Procurement Plan"), and further, Public Utilities Code § 399.30(e) requires POU's to adopt and implement a Renewable Energy Resources Enforcement Program ("RPS Enforcement Program") to enforce the implementation of a POU's RPS Procurement Plan; and

WHEREAS, in accordance with Public Utilities Code § 399.30(e), the City Council, as the governing board of MVU, adopted Resolution No. 2012-02 on January 10, 2012, adopting and establishing an RPS Enforcement Program, which established compliance periods, adopted RPS procurement targets, authorized MVU to establish cost limitations on procurement expenditures, and directed the City Manager to develop and present to the City Council an RPS Procurement Plan; and

WHEREAS, in accordance with Public Utilities Code § 399.30(a), the City Council adopted Resolution No. 2013-37 on June 11, 2013, which adopted MVU's RPS Procurement Plan for the first compliance period (2011-2013), which established specific procurement targets for each of the compliance periods, specified portfolio balance requirements consistent with Public Utilities Code § 399.16, described MVU's existing and planned procurement activities, forecasted the MVU's retail sales and RPS obligations, adopted rules for excess procurement, and adopted a cost limitation methodology and cost limitation amount; and

WHEREAS, pursuant to Public Utilities Code § 399.30(a), MVU adopted an RPS Procurement Plan for the second compliance period (2014-2016) on May 15, 2015, which provided an updated description of MVU's procurement activities, forecast of MVU's retail sales and RPS obligations, and adopted a cost limitation methodology and cost limitation amount; and

WHEREAS, staff has prepared an RPS Procurement Plan for the third compliance period (2017-2020) updating the forecasted retail sales and RPS procurement requirements, providing an updated description of MVU's existing and planned RPS procurement activities, adopting new procurement targets for the fourth, fifth, and sixth compliance periods consistent with SB 350 and SB 100, and revising the

information and methodology used for establishing a cost limitation for the third compliance period; and

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY FINDS, DETERMINES, AND DECLARES AS FOLLOWS:

1. The Moreno Valley Utility Renewable Energy Resource Procurement Plan, Compliance Period 3 (2017-2020), which is attached hereto as Exhibit A and incorporated herein, is hereby approved and adopted.
2. As authorized by Public Utilities Code § 399.30(d)(2)(B), the following optional compliance mechanism is hereby approved and adopted:

A cost limitation for procurement expenditures by MVU is hereby approved and adopted (as described in Section 7 of the RPS Procurement Plan).

3. Except as to matters specifically reserved for MVU, the City Manager or their designee is authorized and directed to do and perform all acts required to implement the Moreno Valley Utility Renewable Energy Resource Procurement Plan, Compliance Period 3 (2017-2020), and to periodically report to the City Council.
4. The Resolution shall take effect immediately upon its adoption.

APPROVED AND ADOPTED this 15th day of December 2020.

Mayor of the City of Moreno Valley

ATTEST:

City Clerk

APPROVED AS TO FORM:

City Attorney

3
Resolution No. 2020-XX
Date Adopted: December 15, 2020

Attachment: Resolution RPS Procurement Plan 12152020 [Revision 1] (4160 : ADOPT RESOLUTION FOR UPDATED RENEWABLE ENERGY

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Resolution No. 2020-XX was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the 15th day of December 2020 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

Resolution No. 2020-XX⁴
Date Adopted: December 15, 2020

Attachment: Resolution RPS Procurement Plan 12152020 [Revision 1] (4160 : ADOPT RESOLUTION FOR UPDATED RENEWABLE ENERGY



City of Moreno Valley Utility
Updated Renewable Energy Resources
Procurement Plan
("RPS Procurement Plan")
Compliance Period 3
(2017-2020)

December 15, 2020

Attachment: Renewable Energy Procurement Plan Update 12152020 final (4160 : ADOPT RESOLUTION FOR UPDATED RENEWABLE ENERGY

Renewable Energy Resources Procurement Plan

Executive Summary

On September 18, 2018, the City Council approved Moreno Valley Utility’s (MVU) 2018 Integrated Resource Plan (“IRP”) Update. The IRP is a 20-year blueprint that describes MVU’s commitment to a sustainable energy future and endeavors to substantially conform to the requirements of the California Clean Energy and Pollution Reduction Act of 2015 (SB 350), which was signed into law October 2015, and the Publicly-Owned Utility IRP Guidelines issued by the California Energy Commission (CEC) on August 9, 2017. On September 10, 2018, SB 100 was signed into law, which increased the 2030 Renewable Portfolio Standard (“RPS”) target to 60 percent and set a statewide goal of meeting all electric load with carbon-free resources by 2045. The specific RPS procurement targets set by SB 100 are the following: at least 44 percent by December 31, 2024, 52 percent by December 31, 2027, and 60 percent by December 31, 2030.

The changes in the revised Renewable Energy Resources Procurement Plan (“RPS Procurement Plan”) include the following:

1. Pursuant to SB 350 and SB 100, MVU will incorporate the most recent RPS Procurement Plan into future iterations of the IRP;
2. Pursuant to SB 350, at least 65 percent of the procurement MVU counts toward the California RPS in each compliance period will be from contracts of ten years or more in duration for eligible renewable energy resources;
3. The Preferred Portfolio of renewable resources as described in the 2018 IRP Update shall serve as the intended strategy to comply with the RPS Program requirements.

This Renewable Energy Resources Procurement Plan (“RPS Procurement Plan”) shall apply to the City of Moreno Valley operating through its municipal electric utility (“City”).

Section 1: Definitions

The definitions set forth in Public Resources Code (“PRC”) § 25741 and Public Utilities Code (“PUC”) § 399.12 are incorporated herein. Capitalized terms in this RPS Procurement Plan, as first identified in parentheses, shall have the meaning given to such term in the body of this RPS Procurement Plan

Section 2: Previous Adoption of the RPS Enforcement Program

As required in Senate Bill 2 in the 2011-2012 First Extraordinary Session (“SB 2-1X”) with respect to California’s renewables portfolio standard (“RPS”), the City Council adopted the Renewable Energy Resources Enforcement Program (“RPS Enforcement Program”) on January 10, 2012. Through the RPS Enforcement Program, the City Council established three compliance periods consistent with SB 2-1X, adopted RPS-eligible procurement goals for each of the three compliance periods and described the framework for how the City would implement the requirements and measures in SB 2-1X with respect to the RPS program. Section 10 of the

RPS Enforcement Program directed the City Manager to develop and present an RPS Procurement Plan to the City Council.

Section 3: Purpose

This document comprises the City’s RPS Procurement Plan. This RPS Procurement Plan describes how the City will achieve its RPS procurement targets consistent with optional compliance measures specified in SB 2-1X and applicable to the City’s status as a small publicly owned utility (“POU”).

Section 4: Specified RPS Procurement Targets

SB 2-1X has the purpose of meeting the State’s RPS requirements of 33% for all utilities by 2020. Subsequently, SB 350 increased the RPS to 50% by 2030 and incorporated other electric utility resource planning obligations. SB 100 advances RPS even further by increasing the goal to 60% RPS by 2030 and 100% non-carbon emitting resources by 2045. In the RPS Enforcement Program, the City Council adopted general RPS procurement targets for each of the three Compliance Periods. Pursuant to PUC § 399.30(b) and (c), the City Council adopts and further specifies the RPS procurement targets, as follows:

Compliance Period	Years	SB 100 RPS Target (% of Retail Load)
3	January 1, 2017 through December 31, 2020	33%
4	January 1, 2021 through December 31, 2024	44%
5	January 1, 2025 through December 31, 2027	52%
6	January 1, 2028 through December 31, 2030+	60%

At least 65% of RPS contracts must have a duration 10+ years beginning Jan. 1, 2021.

Portfolio Content Category (“PCC”) Requirements

The following categories of the renewable resources may be used to meet statutory RPS procurement targets.

Portfolio Content Category (PCC)	Description	Requirement
0	Any contract or ownership agreement originally executed prior to June 1, 2010, shall “count in full” towards the RPS	N/A

	procurement requirements.	
1	Eligible renewable energy resource electricity products that: (A) Have a first point of interconnection with a California balancing authority; (B) Are scheduled from the eligible renewable energy resource into a California balancing authority without substituting electricity from another source; or (C) Have an agreement to dynamically transfer electricity to a California balancing authority.	=>75%
2	Firmed and shaped eligible renewable energy resource electricity products providing incremental electricity and scheduled into a California balancing authority.	Up to 15%
3	Eligible renewable energy resource electricity products, or any fraction of the electricity generated, including unbundled renewable energy credits, that do not qualify as PCC 1 or 2.	<=10%

Section 5: RPS Procurement Plan

This Plan is consistent with the renewable energy procurement guidelines recommended by the MVU 2018 IRP Update. The IRP Update was designed to identify a diverse and balanced portfolio of resources needed to ensure that MVU has reliable electricity supply that provides optimal integration of renewable energy in a cost-effective manner. The portfolio relies upon zero carbon-emitting resources to the maximum extent reasonable to achieve any statewide greenhouse gas emissions limit established pursuant to the California Global Warming Solutions Act of 2006 (Division 25.5 (commencing with Section 38500) of the Health and Safety Code) or any successor legislation. The proposed procurement plan includes a strategy for procuring best-fit and least-cost resources to satisfy these portfolio needs.

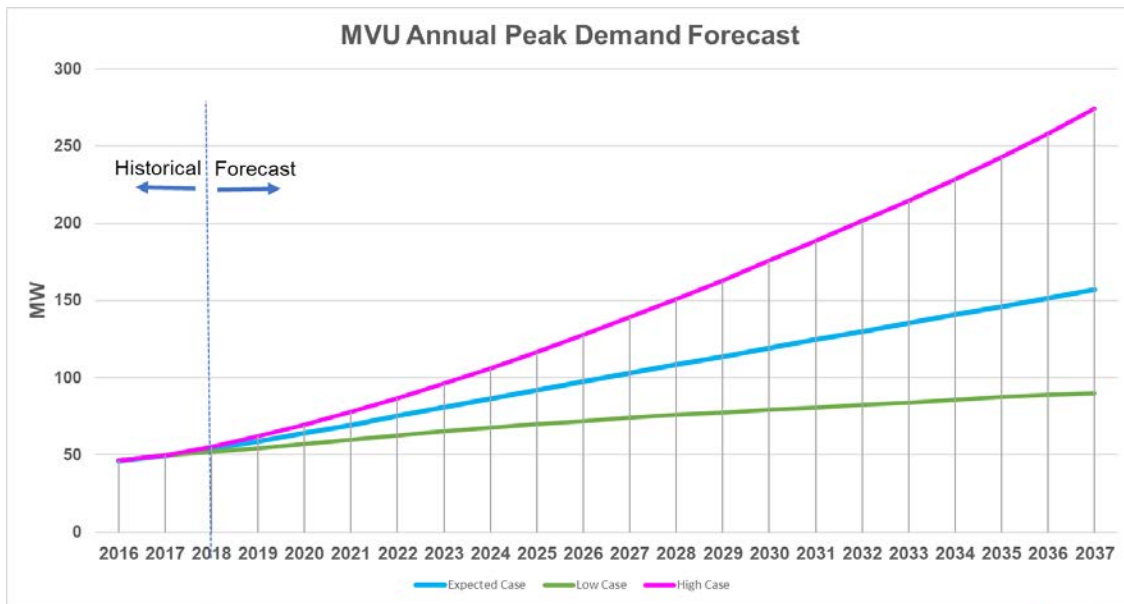
For RPS procurement purposes, MVU’s procurement plan includes a procurement compliance margin of 5% to address the risks of load variations, renewable resource performance and potential contract failure. The forecasted procurement targets for each compliance period may also be adjusted to reflect specific RPS provisions, such as voluntary green pricing programs.

Load Forecast

As described in the 2018 IRP Update, the annual load forecast is explicitly represented as a forecast of “Baseline Consumption” with a series of “demand-side modifiers.” These modifiers include:

- Electric vehicles;
- Behind-the-meter PV; and
- Energy efficiency.

The load forecast was developed by projecting MVU’s annual energy (MWh) and peak capacity demand (MW) using historical data and a simple annual projected growth rate. Historical data was also used to model MVU’s seasonal, monthly, daily and hourly load profiles. Using MVU’s proportion of the CEC’s state-wide load forecast, MVU-specific forecasts were derived for bulk transmission and distribution system losses, behind-the-meter solar PV installations, and electric vehicle charging load. The MVU peak demand forecast reflects anticipated shift of utility peaks occurring later in the day due to modifiers such as rooftop solar photovoltaic production.



Preferred Portfolio

The preferred portfolio of renewable resources identified in the 2018 IRP Update shall be used as the strategy to comply with the RPS Program requirements and is based on the following assumptions:

- 2030 GHG Target = 0.0481 MMT CO₂e (estimated proportionate share of 42 MMT statewide electricity sector target)
- RPS Goal of 60% by 2030; 100% clean energy by 2045
- Minimum RPS margin of procurement: 5% above goal
- At least 65% of RPS met through utility ownership or long-term (10+ year) power purchase agreements (Note: Utility ownership not practical until full capture of Investment Tax Credits for solar; not practical for wind due to Production Tax Credit)
- Assumes all RPS procurement is for Portfolio Content Category (PCC) 1 eligible resources. To reduce costs, some of these PCC 1 purchases could be replaced by PCC 2 and/or PCC 3 up to stated maximums.
- New renewable resource mix will be approximately 70% solar and 30% wind.

- Actual procurement will be based on market response and may differ from these initial targets.
- New renewable resources are assumed to have full capacity delivery status in order for the associated net qualifying capacity to count toward MVU’s resource adequacy requirements. If a new renewable resource has energy only deliverability status, it will not count for RA, and should be discounted from the price of fully deliverable products by at least the value of the RA capacity.

RPS Procurement Targets

The table below, taken from the 2018 IRP Update, shows the MVU RPS Procurement Targets for the preferred portfolio of renewable resources identified in the 2018 IRP Update. The targets include a 5% procurement margin in each compliance period. The last two compliance periods have not officially been established by the State but are assumed to follow the previously established structure of 3 to 4-year periods. MVU will act in good faith to procure resources pursuant to the preferred portfolio unless and until it becomes aware of circumstances that would dictate otherwise.

RPS Procurement Targets by Compliance Period (MWh)						
	2018-2020	2021-2024	2025-2027	2028-2030	2031-2033	2034-2037
Scenario B	CP 3	CP 4	CP 5	CP 6	CP [7]	CP [8]
Mid	0	66,707	195,623	303,720	382,412	622,645

RPS Procurement Process

Consistent with the procurement methods described in the Council approved 2018 IRP Update, MVU will follow certain procedures for the purchase of energy and capacity. For long term purchase commitments, MVU will typically use competitive solicitations which may take the form of a Request for Proposal (“[RFP](#)”) or a similar process where a comparative analysis of proposals is made at a single point in time. An RFP may be used where a specific resource need has been identified, the identified need is a priority, sufficient time exists to conduct an RFP, and management believes that an RFP would yield the most competitive outcome.

Bilaterally negotiated agreements in response to unsolicited proposals may be used for unique opportunities that are fleeting in nature such that timelines associated with an RFP would prevent MVU from engaging in beneficial procurement opportunities. Short- and medium-term power purchases will typically be negotiated on a bilateral basis or via independent energy brokers, particularly in markets with sufficient market price transparency to ensure competitive procurement outcomes. These markets include 1) system energy at a defined CAISO trading hub for peak, off-peak, or baseload products; 2) unbundled RECs; and 3) short term resource adequacy capacity. This process allows for maximum operational flexibility to manage supply and demand imbalances in an efficient manner.

Generally, the portfolio of renewable resources is met with longer term contracts, providing cost stability for the supply portfolio. MVU’s guidelines for long-term, bundled renewable energy purchases are shown in the table below. Note that such guidelines reflect the percentage of the Utility’s renewable energy requirements that may be placed under contract during each of the identified time horizons; such percentages may be adjusted in consideration of cost limitation principles referenced in MVU’s RPS Procurement Plan.

Time Horizon	Contracting Guideline (Contracts/Total Energy Need)
Current Year	90% to 105%
Year 2 – 3	70% to 90%
Year 4 – 5	50% to 75%
Beyond Year 5	40% to 60%

MVU’s supply preference is for a mix of renewable energy technologies that will deliver energy in a pattern that is generally consistent with MVU’s load shape. Preferred purchase volumes should be in rough proportion to the Utility’s load profile, subject to adjustments for market conditions and technology price differentials that exist at the time of purchase. Recent market data suggests that peaking renewable resources are likely to comprise a larger proportion of the renewable supply portfolio due to the recent rapid declines in prices for solar PV generation projects and the abundance of such projects in development. The actual renewable portfolio during the planning period will likely be more heavily weighted toward peaking energy production due to the prevalence of competitively priced solar projects. MVU may also engage in purchases from as-available renewable generation (e.g., wind) to the extent that energy prices reflect a lower value due to their intermittency.

MVU will continue to use a portfolio risk management approach in its renewable power purchasing program, seeking low cost supply as well as diversity among technologies, production profiles, generation project sizes, project locations, counterparty, length of contract, and timing of market purchases. Any existing renewable resources are considered in the determination of additional purchases of energy. The design of the resource portfolio will also consider the financial requirements of the utility and honor existing policies to maintain rate competitiveness with Southern California Edison and promote economic development in the City.

Section 6: Status of RPS Procurement Efforts

Long-term Power Purchase Agreements (“PPA”)

1. The City entered into a 20-year PPA with RE Astoria 2 LLC on July 23, 2014 along with the Cities of Lodi, Corona, and Rancho Cucamonga, the Southern California Public Power Authority (“SCPPA”), and the Power and Water Resources Pooling Authority (“PWRPA”) for the purchase of renewable energy from the RE Astoria 2 Solar Project. The output of the project is 75 MW, with 2 MW allocated to MVU. The project achieved commercial operation on December 9, 2016.

2. The City entered into a 20-year PPA with Whitney Point Solar, LLC along with PWRPA, Pittsburg Power Company, and Eastside Power Authority on April 15, 2015 for the purchase of renewable energy from the Whitney Point Solar Project. The output of the project is 20 MW, with 4.5 MW allocated to MVU. The project achieved commercial operation on April 13, 2017.
3. The City entered into a 20-year PPA with Antelope Expansion 3A, LLC on October 17, 2017 for the purchase of renewable energy from the Antelope Expansion 3A Solar Project. The proposed output of the project is 15 MW, and is fully dedicated to MVU. The projected commercial operation date is April 2021.
4. The City entered into a 20-year PPA with RE Greyshark, LLC for the purchase of renewable energy from the RE Greyshark Solar Project. The proposed output of the project is 16 MW, and is fully dedicated to MVU. The expected commercial operation date is December 31, 2022.

Short-term and Mid-term Agreements

1. The City entered into a 5-month agreement with Tenaska Power Services for the purchase of a maximum of 36,700 MWh of renewable energy from several geothermal facilities on August 21, 2019. The energy delivery term began on August 21, 2019, and expired on December 31, 2019.
2. The City entered into a 4-year agreement with TGP Energy Management, LLC for the purchase of 70,000 MWh of renewable energy from various wind facilities on August 30, 2016. The energy delivery term began on January 1, 2017, and expires on December 31, 2020.
3. The City entered into a 3-year agreement with Tenaska Power Services for the purchase of a maximum total of 90,000 MWh of renewable energy from the Rancho Penasquitos Pressure Control Hydro facility on December 12, 2017. The energy delivery term began on January 1, 2018, and expires on December 31, 2020.

City-owned solar generating facility

The construction of a 611kW solar carport facility with a 75kW battery storage unit began in January 2018, and was completed in November 2018. The solar carport is tied directly to MVU's distribution system. A second solar carport facility is under construction at the City's Annex 1 building, and is sized at 90kW. Expected completion for the solar carport is February 2021.

Section 7: Application of Banking Rules

Pursuant to PUC § 399.30(d)(1), and consistent with Section 8 of the RPS Enforcement Program, the City Council adopts the following rules for excess procurement:

- A. The City may apply excess procurement in one compliance period to a subsequent compliance period, subject to the following limitations: Electricity products that exceed the maximum limit for Portfolio Content Category 3, as specified in PUC § 399.16(c), must be subtracted from the calculation of excess procurement.
- B. The City may begin accruing excess procurement as of January 1, 2011.
- C. Excess procurement meeting these requirements may be applied to any future compliance period and shall not expire.

Section 7: Application of Flexible Compliance Mechanisms

A. Cost Limitation

1. *Relevant Factors.* The following factors are relevant to the City's adoption of cost limitations and potential application of cost limitations to reduce the amount of Eligible Renewable Energy Resources that the City may procure to meet its RPS obligation:
 - i. The City is one of the smallest POUs in California and has been in operation less than 20 years. The City incurred significant start up costs, including payment of exit fees to Southern California Edison, and the City may not be able to procure Eligible Renewable Energy Resources in volumes sufficient to satisfy its RPS targets without imposing disproportionate rate impacts on the City's customers.
 - ii. The City's rates cannot exceed those of Southern California Edison without creating a disproportionate rate impact on the City's customers.
 - iii. The City has conducted cost of service studies, and has adopted a resource adequacy policy. The City is beginning to establish financial reserves and rate stabilization funds consistent with prudent utility practices. Procuring Eligible Renewable Energy Resources in amounts which exceed cost limitations described below in Sections 7.A.3 and 7.A.4 could compromise the City's ability to accrue reserves and/or meet other procurement obligations focused on grid reliability and sufficiency of supply (such as Resource Adequacy).
2. *Information Reviewed* – Pursuant to PUC § 399.30(d)(3), and consistent with Section 9 of the RPS Enforcement Program, the City Council has relied on the following information to establish a limitation on the Incremental RPS Procurement Costs for all Eligible Renewable Energy Resources: (i) the information on cost and availability of Eligible Renewable Energy Resources which is

contained in this RPS Procurement Plan; (ii) the expected cost of building, owning and operating an Eligible Renewable Energy Resource, as described in Section 6; and (iii) the potential that some planned resource additions may be delayed or cancelled.

3. *Establishment of Cost Limitation for Compliance Period 3 and thereafter* – The City Council hereby establishes a cost limitation on the Incremental RPS Procurement Cost as follows:

- i. 4.5% of total annual revenues from electricity sales to retail customers – not including public goods charges and taxes.
- ii. In no event shall the purchase of renewable energy volumes required to ensure the City’s compliance with applicable RPS procurement guidelines result in customer rate impacts that would lead to the City’ electric rates exceeding those of Southern California Edison.

4. *Modifications to Cost Limitation* – The City will review the cost limitations biannually as part of its budget process to determine whether (i) the cost limitations continue to assure that there are no disproportionate rate impacts between the City’s customers and those of Southern California Edison in the City of Moreno Valley; (ii) the cost limitations are not unreasonably restricting the City in pursuing longer term, lower risk investment in Eligible Renewable Energy Resources, and (iii) the City is able to continue to demonstrate good faith in pursuing the objectives of SB 350 and SB 100 in meeting the State’s overall RPS goals. In approving contracts for the purchase of Eligible Renewable Energy Resources, the City reserves the right to exceed cost limitations if the contract provides offsetting benefits to the electric customers of the City.

B. Modification of the Portfolio Content Category Requirements

Consistent with PUC § 399.16(e), and as described in Section 6 of the RPS Enforcement Program, the City Council may modify the minimum and maximum requirements in PUC § 399.16(c) for the various portfolio content categories (“PCC Requirements”) if the City demonstrates that it cannot comply with the PCC Requirements because of: (a) conditions beyond the control of the City due to any of the conditions set forth in PUC § 399.15(b)(5), PUC § 399.15(c), or other conditions adopted by the City Council based on facts reasonably found by the City Council to be consistent with the intent of PUC § 399.15(b)(5), PUC § 399.15(c), or PUC § 399.18 and the unique circumstances facing the City as a small POU.

Section 7: Document Revision History

Version	Approval Date	Description of Changes
1.0	06/11/2013	Final Document
2.0	11/15/2015	Updated Document
3.0	12/15/2020	Updated Document



Report to City Council

TO: Mayor and City Council

FROM: Steve Quintanilla, Interim City Attorney

AGENDA DATE: December 15, 2020

TITLE: SECOND READING AND CONSIDERATION OF ADDING CHAPTER 2.65 "CAMPAIGN CONTRIBUTION REGULATIONS" TO TITLE 2 "ADMINISTRATION AND PERSONNEL" OF THE MORENO VALLEY MUNICIPAL CODE TO ESTABLISH REGULATIONS AND LIMITATIONS ON CAMPAIGN CONTRIBUTIONS FOR CITY ELECTED OFFICIALS (ORD. NO. 973)

RECOMMENDED ACTION

RECOMMENDATION:

That the City Council adopt the attached Ordinance No. 973 to add a new Chapter 2.65 ("Campaign Contribution Regulations") to Title 2 ("Administration and Personnel") of the Moreno Valley Municipal Code.

DISCUSSION

This report recommends adoption of Ordinance No. 973, introduced at the December 1, 2020 City Council meeting, approving the addition of a new Chapter 2.65 ("Campaign Contribution Regulations") to Title 2 ("Administration and Personnel") of the Moreno Valley Municipal Code.

ALTERNATIVES

The City Council may consider the following alternatives:

1. Conduct the second reading by title only and adopt Ordinance No. 973. Staff recommends this alternative.

- 2. Provide revisions to the draft Ordinance and have staff return with the revised draft for another adoption process.

FISCAL IMPACT

There are no fiscal impacts anticipated from the approval and adoption of this Ordinance.

NOTIFICATION

Agenda was posted in accordance with the Brown Act.

Prepared By:

Name Steven B. Quintanilla
Title Interim City Attorney

CITY COUNCIL GOALS

None

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Ordinance - Municipal Code Amendment - Chapter 2.65 Campaign Contributions Regulations

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/02/20 1:16 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/02/20 1:47 PM

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADDING CHAPTER 2.65 "CAMPAIGN CONTRIBUTION REGULATIONS" TO TITLE 2 "ADMINISTRATION AND PERSONNEL" OF THE MORENO VALLEY MUNICIPAL CODE TO ESTABLISH REGULATIONS AND LIMITATIONS ON CAMPAIGN CONTRIBUTIONS FOR CITY ELECTED OFFICIALS

WHEREAS, the City has authority to establish campaign contribution limits for elected positions within its jurisdiction under longstanding California law; and

WHEREAS, on October 8, 2019, the Governor signed Assembly Bill 571 which sets default campaign contribution limits for any local jurisdiction which does not specifically address campaign contributions in its own code; and

WHEREAS, AB 571 authorizes the City to set its own limits, either higher or lower than the default limits set forth in AB 571, and the City's ordinance will take precedence over the default limits.

NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY DOES ORDAIN AS FOLLOWS:

Section 1. RECITALS

That the above recitals are true and correct and are incorporated herein as though set forth at length herein.

Section 2. AMENDMENT TO TITLE 2 OF THE MORENO VALLEY MUNICIPAL CODE

That Title 2 (Administration and Personnel) of the Moreno Valley Municipal Code is hereby amended to add a new Chapter 2.65 (Campaign Contribution Regulations) to read as follows:

2.65.010 Purpose and Intent.

- A. It is the purpose and intent of this chapter to ensure a level of discussion of public issues adequate for a viable campaign by providing voters with the information necessary to make an assessment of each candidate before voting.
- B. By enacting this chapter, the city council does not intend to deprive or restrict any person of the exercise of rights guaranteed under the United States Constitution or the California Constitution.

2.65.020 Definitions.

Attachment: Ordinance - Municipal Code Amendment - Chapter 2.65 Campaign Contributions Regulations (4228 : SECOND READING AND

As used in this chapter, the following terms shall have the following meanings:

1. "City" means city of Moreno Valley, California.
2. "City candidate" means any person who is a candidate for an elected city office or who is an elected city official and who is the subject of a recall election.
3. "City official" includes:
 - (a) Any elected or appointed city officeholder, including any city officeholder elected but not yet sworn in; and
 - (b) Any "public official" of the city as the term "public official" is defined under Government Code section 82048.
4. "Campaign committee" means any person or combination of persons formed for the purpose of promoting or opposing the election or reelection of a person to city elected office who directly or indirectly,
 - (a) Receives contributions;
 - (b) Makes independent expenditures; or
 - (c) Makes contributions at the behest of any city candidate.

A campaign committee includes any "controlled committee" within the meaning of Government Code section 82016, any "general purpose committee" within the meaning of Government Code section 82027.5, any "primarily formed committee" within the meaning of Government Code section 82047.5, any "sponsored committee" within the meaning of Government Code section 82048.7, or political action committee.
5. "Contribution" shall have the same meaning as set forth under Government Code section 82015.
6. "Gift" shall have the same meaning as set forth under Government Code section 82028.
7. "Person" means any natural person; any corporation of any variety; any limited liability company; any partnership of any variety; any sole proprietorship; any joint venture or like commercial venture or partnership; any trust; any independent contractor; or any organization or association of persons of any variety and formed for any purpose, including, but not limited to, any collective bargaining group or labor association.

2.65.030 Campaign contribution limitations.

- A. Any funds received by any elected city official, candidate, or committee controlled by an elected city official or candidate shall be considered either a campaign contribution, income or a gift. All campaign contributions received by such persons shall be subject to the provisions of this chapter unless such campaign contributions are used exclusively for elections held outside the jurisdiction. All income and gifts shall be subject to the disqualification provisions of the California Political Reform Act.
- B. There shall be no limit on the campaign contributions a person may make to a city candidate, or his or her campaign committee.

2.65.040 City clerk responsibilities.

In addition to other duties required by law, the city clerk shall:

1. Furnish a copy of this chapter to all qualified city candidates during the nomination period for city office.
2. Determine whether required statements and declarations have been filed timely and, if so, whether they conform on their face with the requirements of this chapter.
3. Promptly notify city candidates, or his or her campaign committee, of any errors in their statements or of failure to file.
4. Report, in writing, apparent violations of this chapter to the city attorney.

2.65.050 Construction of provisions.

This chapter shall be in addition to all other city and state laws applicable to municipal elections. Unless the contrary is stated or clearly appears from the context, the definitions and terms set forth in the Government Code shall govern the interpretations of terms used in this chapter. This chapter shall be construed liberally in order to effectuate its purpose.

Section 3. REPEAL OF CONFLICTING PROVISIONS

That all the provisions of the Municipal Code as heretofore adopted by the City of Moreno Valley that are in conflict with the provisions of this Ordinance are hereby repealed.

Section 4. SEVERABILITY

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Ordinance be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this ordinance as hereby adopted shall remain in full force and effect.

Section 5. CEQA

The City Council finds that this Ordinance is not subject to the California Environmental Quality Act (CEQA) pursuant to Section 15061(b)(3) of the State CEQA Guidelines, California Code of Regulations, Title 14, Chapter 3, because it can be seen with certainty that there is no possibility that it will have a significant effect on the environment.

Section 6. EFFECTIVE DATE

That this Ordinance shall take effect thirty (30) days after its second reading.

Section 7. CERTIFICATION

That the City Clerk shall certify to the passage of this Ordinance and shall cause the same to be published according to law.

INTRODUCED at a regular meeting of the City Council on _____, 2020 and PASSED, APPROVED, and ADOPTED by the City Council on _____, 2020, by the following roll call vote, to wit:

Dr. Yxstian A. Gutierrez
Mayor
City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney



Report to City Council

TO: Mayor and City Council

FROM: Steve Quintanilla, Interim City Attorney

AGENDA DATE: December 15, 2020

TITLE: SECOND READING AND CONSIDERATION OF AMENDING CHAPTER 3.48 ("WESTERN RIVERSIDE COUNTY MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE PROGRAM") TO ELIMINATE LOW INCOME RESIDENTIAL HOUSING AS AN EXEMPTION TO THE MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE (ORD. NO. 972)

RECOMMENDED ACTION

RECOMMENDATION:

That the City Council adopt the attached Ordinance No. 972 amending Chapter 3.48 ("Western Riverside County Multiple Species Habitat Conservation Plan Fee Program") to Title 3 ("Revenue and Finance") of the Moreno Valley Municipal Code to eliminate "Low Income Residential Housing" as an exemption.

DISCUSSION

This report recommends adoption of Ordinance No. 972, introduced at the December 1, 2020 City Council meeting, approving amendments to Chapter 3.48 ("Western Riverside County Multiple Species Habitat Conservation Plan Fee Program") to Title 3 ("Revenue and Finance") of the Moreno Valley Municipal Code to eliminate "Low Income Residential Housing" as an exemption.

ALTERNATIVES

The City Council may consider the following alternatives:

1. Conduct the second reading by title only and adopt Ordinance No. 972. Staff recommends this alternative.
2. Provide revisions to the draft Ordinance and have staff return with the revised draft for another adoption process.

FISCAL IMPACT

There are no fiscal impacts anticipated from the approval and adoption of this Ordinance.

NOTIFICATION

Agenda was posted in accordance with the Brown Act.

Prepared By:
Name Steven B. Quintanilla
Title Interim City Attorney

CITY COUNCIL GOALS

None

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development
2. Public Safety
3. Library
4. Infrastructure
5. Beautification, Community Engagement, and Quality of Life
6. Youth Programs

ATTACHMENTS

1. Ordinance - Municipal Code Amendment - Chapter 3.48 Western Riverside County Multiple Species Conservation Plan Program Fee

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/02/20 1:17 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/02/20 1:48 PM

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, AMENDING CHAPTER 3.48 “WESTERN RIVERSIDE COUNTY MULTIPLE SPECIES HABITAT CONSERVATION PLAN FEE PROGRAM” TO TITLE 3 “REVENUE AND FINANCE” OF THE MORENO VALLEY MUNICIPAL CODE TO AMEND SECTION 3.48.030 “DEFINITIONS” AND REPEAL SECTION 3.48.060 (F) “EXEMPTIONS” TO REMOVE LOW INCOME RESIDENTIAL HOUSING CONSTRUCTION AS AN EXEMPTION TO THE LOCAL DEVELOPMENT MITIGATION FEE

WHEREAS, the City of Moreno Valley is a General Law city organized pursuant to Article XI of the California Constitution; and

WHEREAS, the County of Riverside (“County”) developed, the City of Moreno Valley (“City”) adopted, and the Western Riverside County Regional Conservation Authority (“RCA”) administers the Western Riverside County Multiple Species Habitat Conservation Plan (“MSHCP”) Local Development Mitigation Fee, a regional impact fee imposed on new development in Western Riverside County to mitigate the regional biological impacts of new development; and

WHEREAS, the City adopted Ordinance No. 654 (the City MSHCP Fee Ordinance) imposing the Local Development Mitigation Fee (“LDMF”) within the City; and

WHEREAS, the City MSHCP Fee Ordinance and RCA’s Fee Collection and Remittance Policy Resolution 07-04 sets forth the rules for the administration of LDMF, including without limitation, the City’s obligation to collect LDMF from non-exempt new development within the City and remit it to RCA; and

WHEREAS, the Measure A sales tax ordinance (“Measure A”) adopted by the Riverside County Transportation Commission (“RCTC”) requires the City to participate in the MSHCP as a condition of receiving Measure A local street and roads funds; and

WHEREAS, under Measure A, RCTC may not distribute Measure A local streets and roads funds to a city or county in Western Riverside County if such city or county is not certified by RCA as participating in the MSHCP; and

WHEREAS, Chapter 3.48 (“Western Riverside County Multiple Species Habitat Conservation Plan Fee Program”) of the Moreno Valley Municipal Code defines and exempts “low income residential housing” from the MSHCP Local Development Mitigation Fee; and

WHEREAS, section 3.48.030 of the Moreno Valley Municipal Code defines low income residential housing and section 3.48.060 (F) of the Moreno Valley Municipal Code exempts “low income residential housing”; and

WHEREAS, in 2017 there was a dispute between the City and RCA over the City’s obligation to collect the MSHCP Fee related to an “affordable housing project” and the City and RCA agreed to settle the issue by requiring the City to amend the City’s MSHCP Fee as set forth in Chapter 3.48, to remove the exemption for affordable housing projects.

NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY DOES ORDAIN AS FOLLOWS:

Section 1. RECITALS

That the above recitals are true and correct and are incorporated herein as though set forth at length herein.

Section 2. AMENDMENT TO CHAPTER 3.48 OF TITLE 3 OF THE MORENO VALLEY MUNICIPAL CODE

That the following sections of Chapter 3.48 (Western Riverside County Multiple Species Habitat Conservation Plan Fee Program) of Title 3 (Revenue and Finance) of the Moreno Valley Municipal Code are hereby amended as follows:

3.48.030 Definitions.

As used in this chapter, the following terms shall have the following meanings:

“City” means the city of Moreno Valley, California.

“City council” means the city council of the city of Moreno Valley, California.

“Certificate of occupancy” means a certificate of occupancy issued by the city in accordance with all applicable ordinances, regulations, and rules of the city and state law.

“Credit” means a credit allowed pursuant to Section 3.48.070 of this chapter, which may be applied against the development impact fee paid.

“Development project or project” means any project undertaken for the purpose of development pursuant to the issuance of a building permit by the city pursuant to all applicable ordinances, regulations, and rules of the city and state law.

“Final inspection” means a final inspection of a project as defined by the building codes of the city.

“Gross acreage” means the total property area as shown on a land division map of record, or described through a recorded legal description of the property. This area shall be bounded by road right-of-way and/or legal property lines.

“Local development mitigation fee or fee” means the development impact fee imposed pursuant to the provisions of this chapter.

~~“Low income residential housing” means residential units in projects constructed as housing for lower income households as such households are defined pursuant to Section 50079.5 of the Health and Safety Code.~~

“Multiple species habitat conservation plan or MSHCP” means the western Riverside County multiple species habitat conservation plan, adopted by the city council on January 13, 2004.

“MSHCP conservation area” has the same meaning and intent as such term is defined and utilized in the MSHCP.

“Ordinance” means this ordinance No. 742 of the city of Moreno Valley, California.

“Project area” means the area, measured in acres, from the adjacent road right-of-way line to the limits of project improvements. Project area includes all project improvements and areas that are disturbed as a result of project improvements on an owner’s gross acreage, including all areas depicted on the forms required to be submitted to the city pursuant to this chapter and/or other applicable development ordinance or regulation of the city. Previously improved shall be construed to mean all areas that have been improved with existing foundation, parking lot, hardscape, foundation, and/or building structures under city-approved permits prior to the effective date of the ordinance codified in this chapter. Except as otherwise provided herein, the project area is the area upon which the project will be assessed the local development mitigation fee.

“Residential unit” means a building or portion thereof used by one family and containing but one kitchen, which unit is designed or occupied for residential purposes, including single-family, multiple-family dwellings, and mobile homes on a permanent foundations, but not including hotels and motels.

“Revenue or revenues” means any funds received by the city pursuant to the provisions of this chapter for the purpose of defraying all or a portion of the cost of acquiring and preserving vegetation communities and natural areas within the city and the region which are known to support threatened, endangered or key sensitive populations of plant and wildlife species.

“Western Riverside County regional conservation authority” means the governing body established pursuant to the MSHCP that is delegated the authority to oversee and implement the provisions of the MSHCP. (Ord. 742 § 1.1, 2007; Ord. 654 § 2.2, 2004)

3.48.060 Exemptions

Any project exempt under this section may not be covered by the provisions of the MSHCP. The developer may elect to pay the appropriate MSHCP mitigation fee to ensure coverage by the MSHCP.

The following types of construction shall be exempt from the provisions of this chapter:

- A. Reconstruction or improvements that are damaged or destroyed by fire or other natural causes.
- B. Rehabilitation, remodeling, or minor additions to an existing development project.
- C. Second units, granny units, and residential accessory structures as defined in the city of Moreno Valley Municipal Code.
- D. Existing improvements that are converted from an existing permitted use to a different permitted use, provided that no additional area of the property is disturbed as a result of such conversion.
- E. Any further development within a previously improved project area as defined in Section 3.48.030 of this chapter.

~~**F. Low income residential housing as defined in Section 3.48.030 of this chapter.**~~

F. —~~G.~~ Industrial and commercial projects determined by the city to be submitted for the city development review process as evidenced by a completed application as of the date of January 13, 2004, and the building permit has been issued by the city by no later than July 1, 2004.

G.—~~H.~~ Development projects that are the subject of a public facilities or development agreement entered into pursuant to Government Code Section 65864 et seq., prior to the effective date of the ordinance codified in this chapter, wherein the imposition of new fees are expressly prohibited.

Section 3. SEVERABILITY

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Ordinance be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this ordinance as hereby adopted shall remain in full force and effect.

Section 4. REPEAL OF CONFLICTING PROVISIONS

That all the provisions of the Municipal Code as heretofore adopted by the City of Moreno Valley that conflict with the provisions of this Ordinance are hereby repealed.

Section 5. EFFECTIVE DATE

That this Ordinance shall take effect thirty (30) days after its second reading.

Section 6. CERTIFICATION

That the City Clerk shall certify to the passage of this Ordinance and shall cause the same to be published according to law.

INTRODUCED at a regular meeting of the City Council on _____, 2020 and PASSED, APPROVED, and ADOPTED by the City Council on _____, 2020, by the following roll call vote, to wit:

Dr. Yxstian A. Gutierrez
Mayor
City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney



Report to City Council

TO: Mayor and City Council

FROM: Manuel A. Mancha, Community Development Director

AGENDA DATE: December 15, 2020

TITLE: DRAFT HOUSING ELEMENT AND SITES INVENTORY OVERVIEW

RECOMMENDED ACTION

Recommendation:

1. Receive and File the staff report and supporting information

SUMMARY

The Housing Element is a component of the General Plan, which guides planning for housing to meet the current and projected needs of all households in the city. This Housing Element information is being provided to the City Council as an informational item prior to submitting a draft Housing Element to the State Department of Housing and Community Development (HCD) for its mandatory review. Staff anticipates finalizing and submitting the Housing Element to HCD this month.

DISCUSSION

BACKGROUND

The Housing Element guides planning for housing to meet the current and projected needs of all households in the city. It includes an assessment of housing needs and lays out goals, policies, and programs for the preservation, improvement, and development of housing to meet community needs. The State requires that local agencies update their Housing Elements every eight years; the City's Housing Element was last updated in 2014.

This Housing Element information is being provided to the City Council prior to submittal of the draft Housing Element to HCD for its mandatory review. The State's review is

expected to take approximately two months. The Housing Element is being submitted to the State now to ensure their review matches the Environmental Document timeline for the overall General Plan update.

HOUSING ELEMENT GOALS AND POLICIES

Moreno Valley has seen a steady growth in housing development over the past 10 years. Based on review and analysis by the consultant team, the goals of the General Plan are recommended to remain consistent with the prior Housing Element. There will be some minor modifications to the policies to address legislative changes that have occurred since the adoption of the last Housing Element. The following are the recommended goals:

- Goal 1: Availability of a wide range of housing by location, type of unit, and price to meet the existing and future needs of Moreno Valley residents.
- Goal 2: Promote and preserve suitable and affordable housing for persons with special needs, including lower income households, large families, single parent households, the disabled, senior citizens and shelter for the homeless.
- Goal 3: Removal or mitigation of constraints to the maintenance, improvement and development of affordable housing, where appropriate and legally possible.
- Goal 4: Provide increased opportunities for homeownership.
- Goal 5: Enhance the quality of existing residential neighborhoods in Moreno Valley, through maintenance and preservation, while minimizing displacement impacts.
- Goal 6: Encourage energy conservation activities in all neighborhoods.
- Goal 7: Equal housing opportunity for all residents of Moreno Valley, regardless of race, religion, sex, marital status, ancestry, national origin, color, or accessibility needs.

Policies and actions to support these goals will be included in the Housing Element that is submitted to the state.

REGIONAL HOUSING NEEDS ASSESSMENT (RHNA)

A critical part of the Housing Element is the inventory of housing opportunity sites and an analysis of the capacity of those sites to accommodate the City's RHNA allocation as determined by the Southern California Association of Governments (SCAG). The Housing Element must include the RHNA allocation per the State's mandate under Government Code section 65584.

Identification of Sites

The City of Moreno Valley's Sixth Cycle RHNA allocation is currently set at 13,595 units, broken down into four required affordability categories: Very Low Income, Low Income, Moderate Income, and Above Moderate Income. The City's previous RHNA allocation

was 6,169 units total (across all categories). An explanation of the current breakdown is attached to this staff report.

In the City's current General Plan Land Use map, there are many parcels throughout the City that are zoned for housing in the ranges from R1 (one dwelling to the acre) to R30 (30 units to the acre). Typically, properties with a density of 0-20 units/acre can accommodate Moderate Income or Above Moderate Income housing. Properties that are developable at densities of 20 units per acre or more are presumed to qualify as affordable to Low and Very Low income households for the purpose of demonstrating that the City has adequate opportunity sites to meet RHNA requirements for these two income categories. Since there are many properties in the City already zoned for housing, parcels that can accommodate RHNA allocations are identified as opportunities to ensure the State mandated amounts are achievable. The parcels are not restricted to solely address RHNA housing, but they have the capability should the owner elect to do so. Attached is a map showing the existing residential/housing designations.

State law requires that the City demonstrate that it has sufficient land available to accommodate its RHNA allocation for all income categories. To do this, the Housing Element contains an inventory of available sites that are appropriate for housing. Land considered suitable for residential development includes the following:

- Vacant sites zoned for residential use.
- Vacant sites zoned for nonresidential use that also allow residential development.
- Residentially zoned sites that are capable of being developed at a higher density (non-vacant sites, including underutilized sites).
- Sites owned or leased by a city and/or county.
- Sites zoned for nonresidential use that can be redeveloped for residential use if a program is included in the Housing Element to rezone the site to permit residential use within three years of adoption.

Further, State law stipulates criteria for determining the adequacy of sites that are included on the inventory, including that they be zoned to accommodate housing, that they have appropriate development standards, and that they be served by public facilities as needed to facilitate the development of a variety of housing products suitable for all income levels.

Concepts of residential density changes have been presented to the public and the General Plan Advisory Committee. Based upon these concepts and existing vacant residentially zoned parcels, suitable sites have been identified for all categories. The majority of RHNA sites are being accommodated within the major concept areas presented to the General Plan Advisory Committee. A small buffer is included in each RHNA category to account for any parcel that may not be developable to its full density or if the parcel owner elects to develop something different pursuant to City Code and policies. To address a deficit in the Moderate category, minor additions were needed to satisfy the State's required number.

NEXT STEPS

Staff anticipates finalizing and submitting the Housing Element to the State Department of Housing and Community Development Department this month. Any comments provided by the state will be reviewed and addressed.

ALTERNATIVES

1. Receive and file the staff report. *This alternative is recommended to ensure that the Housing Element is submitted to State Department of Housing and Community Development in a timely manner that will allow the General Plan update process to remain on schedule.*
2. Do not receive and file the staff report. *This alternative is not recommended by staff.*

FISCAL IMPACT

Completion of a Housing Element in 2021 is required to comply with State law. There is no direct impact to the General Fund.

NOTIFICATION

Notification was provided by posting of the agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Chris Ormsby
Senior Planner

Department Head Approval:
Manuel A. Mancha
Community Development Director

Concurred By:
Patty Nevins
Planning Official

CITY COUNCIL GOALS

Positive Environment. Create a positive environment for the development of Moreno Valley's future.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

Objective 1.9: Ensure the City’s General Plan articulates the vision for how Moreno Valley wants to evolve over time, and provides an orderly and predictable process through which this vision is developed and implemented, including new attention to economic development, sustainability, public health, and innovation.

ATTACHMENTS

- 1. Attachment 1 - RHNA description
- 2. Attachment 2 - Map A: Existing Residential Land Use and Vacant Sites
- 3. Attachment 3 - Map B: 2040 General Plan Concept Areas
- 4. Attachment 4 - Map C: 2021-29 Housing Element Opportunity Sites
- 5. Attachment 5 - Map D: 2021-29 Housing Element Opportunity Sites Outside Concept Areas

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/10/20 5:20 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/10/20 5:23 PM

REGIONAL HOUSING NEEDS ASSESSMENT CURRENT REQUIREMENTS FOR MORENO VALLEY (subject to finalization by the Southern California Association of Governments)

TABLE 1 - CITY OF MORENO VALLEY SIXTH CYCLE RHNA (2021 TO 2029)

Income Category	Units	Percent
Very Low (0-50% of AMI)	3,768	27.7%
Low (51-80% of AMI)	2,046	15.0%
Moderate (81-120% of AMI)	2,161	15.9%
Above Moderate (more than 120% of AMI)	5,620	41.3%
Total New Construction Need	13,595	100.0%

Source: Southern California Association of Governments, 2020

To identify sites for the 2021-29 Moreno Valley Housing Element inventory, a parcel-based analysis of properties within the City limits using County Assessor data and will be included in the state submittal.

Low and Very Low Income RHNA: Sites and Capacity

Low Income households are defined as those making 51-80% of area median income; Very Low Income households are those making 0-50% of area median income. HCD has established parameters for the size of sites for lower income RHNA in view of feasibility considerations. Parcels that are less than 0.5 acres in size are generally not considered suitable for lower income housing development as smaller parcels may not allow development of a sufficient number of units for proposed affordable housing projects to compete effectively for limited funding resources. Parcels larger than 10 acres in size are also not considered suitable by HCD as development of very large projects may lead to an over concentration of affordable housing in one location or may render proposed affordable housing projects ineligible for funding. Therefore, all sites attributed to Low/Very Low Income RHNA on the Draft inventory are between 0.5 and 10 acres in size. Additionally, consistent with eligibility criteria established by the State, analysis was conducted to ensure that sites for Lower Income RHNA are not constrained by environmental factors such as susceptibility to landslides, fault rupture or flooding, among other eligibility criteria. Consistent with guidance from the State, Lower Income RHNA sites are generally located in areas of the city with good access to transit, jobs, parks, schools, and other public services.

Moderate Income RHNA: Sites and Capacity

Moderate Income households are those making between 80 and 120 percent of the local area median income, which for Moreno Valley is \$63,572 annually or \$5,298 monthly according to the US Census. With rent that ranges from \$962 for a studio unit to \$1,840 for a 4-bedroom unit, many moderate income households may be able to afford market rate housing, assuming that moderate income households will devote up to 30 percent of their gross monthly income to housing cost. The Draft Inventory includes 58 sites totaling 332 acres of vacant and underutilized land within the City limit that can accommodate housing suited to moderate income households.

Above Moderate Income Households RHNA: Sites and Capacity

Above Moderate Income households are those making over 120 percent of the local area median income. The Draft Inventory identifies a range of sites that can accommodate market rate housing, including vacant land zoned for low density residential development, vacant land in the COMU designation, and underutilized commercial properties with mixed use land use designations (CEMU and COMU). The sites vary in size from as large as 78.5 acres to as small as 5,600 square feet. The size parameters applicable to lower income RHNA sites do not apply to Above Moderate RHNA sites.

Summary

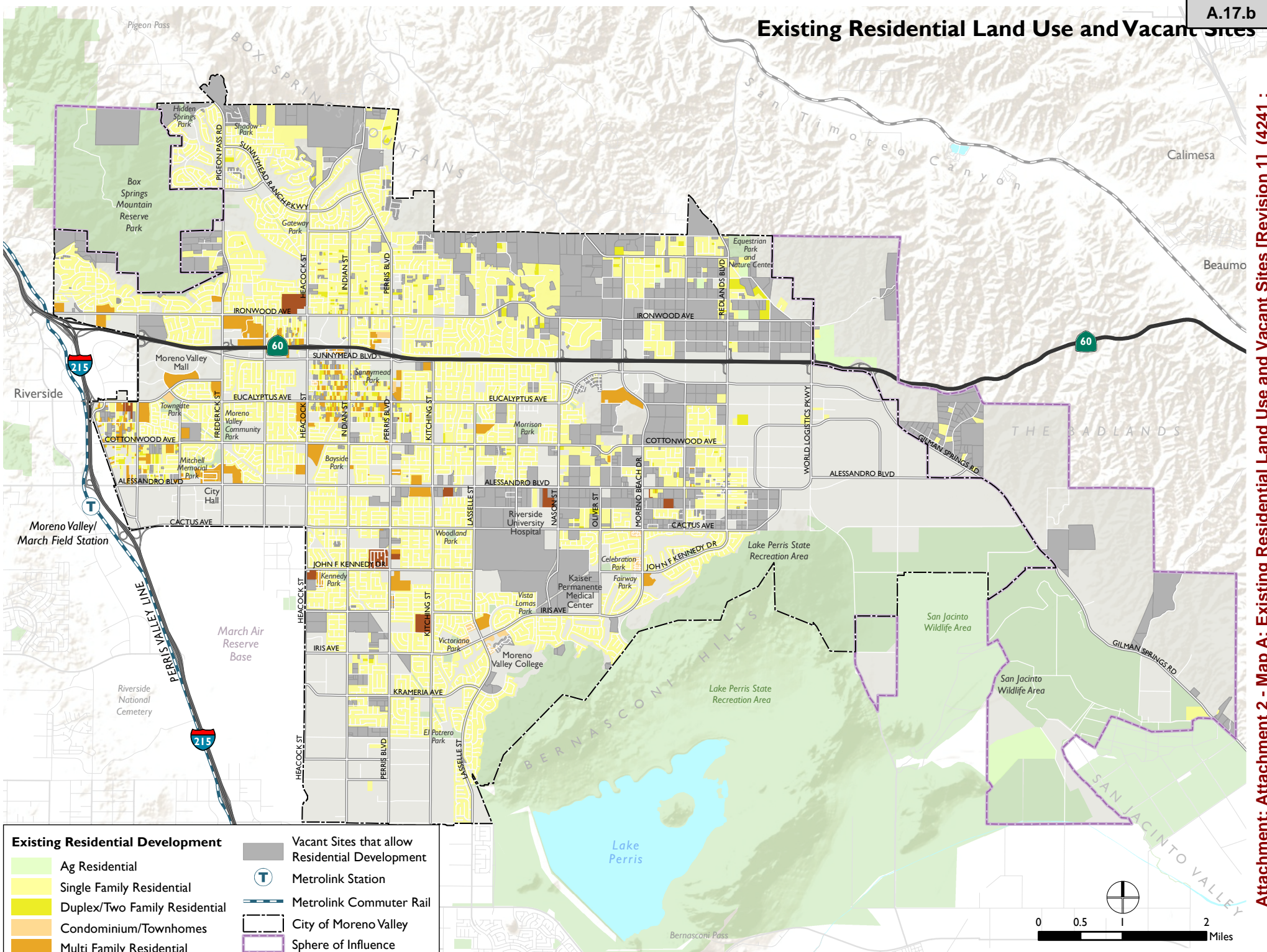
The Table below summarizes the realistic capacity of sites in all proposed land use designations that allow for residential development, including capacity on vacant and non-vacant sites. As shown, the Draft Inventory can accommodate 16,381 units which is sufficient to satisfy the City's Sixth Cycle RHNA for all income categories with at least a five percent buffer to ensure the City can meet the RHNA by income group throughout the planning period. If the remaining sites in the inventory cannot accommodate the unmet RHNA by income category, the City must be prepared to rezone other sites where residential development is allowed regardless of any growth management restriction, open space or agricultural preservation policies.

City of Moreno Valley - Sixth Cycle Housing Element Update Sites Inventory Capacity Analysis
Vacant and Non-Vacant Sites and Pipeline Projects
Revision Date: December 9, 2020

	Low and Very Low Income				Moderate Income				Above Moderate Income			
	Vacant	Non-Vacant	Pipeline	Subtotal	Vacant	Non-Vacant	Pipeline	Subtotal	Vacant	Non-Vacant	Pipeline	Subtotal
COMU	1,656	2,383		4,039			490	490	632	317	52	1,001
CEMU		759		759				0		227		227
R30	335			335			266	266				0
R20	838			838	216		517	733		13		13
R15				0	198		318	516			66	66
R10			80	80	290			290	756		377	1,133
R5				0				0	719		1,302	2,021
R3				0				0	348	6	204	558
R2				0				0	366		91	457
R1				0				0	197		34	231
HR				0				0	14		179	193
R/O				0			18	18			16	16
Subtotal	2,829	3,142	80	6,051	704	0	1,609	2,313	3,032	563	2,321	5,916
RHNA	5,814	5,814	5,814	5,814	2,161	2,161	2,161	2,161	5,620	5,620	5,620	5,620
% RHNA	48.7%	54.0%	1.4%	104.1%	32.6%	0.0%	74.5%	107.0%	54.0%	10.0%	41.3%	105.3%
Difference	-2,985	-2,672	-5,734	237	-1,457	-2,161	-552	152	-2,588	-5,057	-3,299	296
GRAND TOTAL												14,280

Attachment: Attachment 1 - RHNA description [Revision 2] (4241 : Draft Housing Element and Sites

Existing Residential Land Use and Vacant Sites

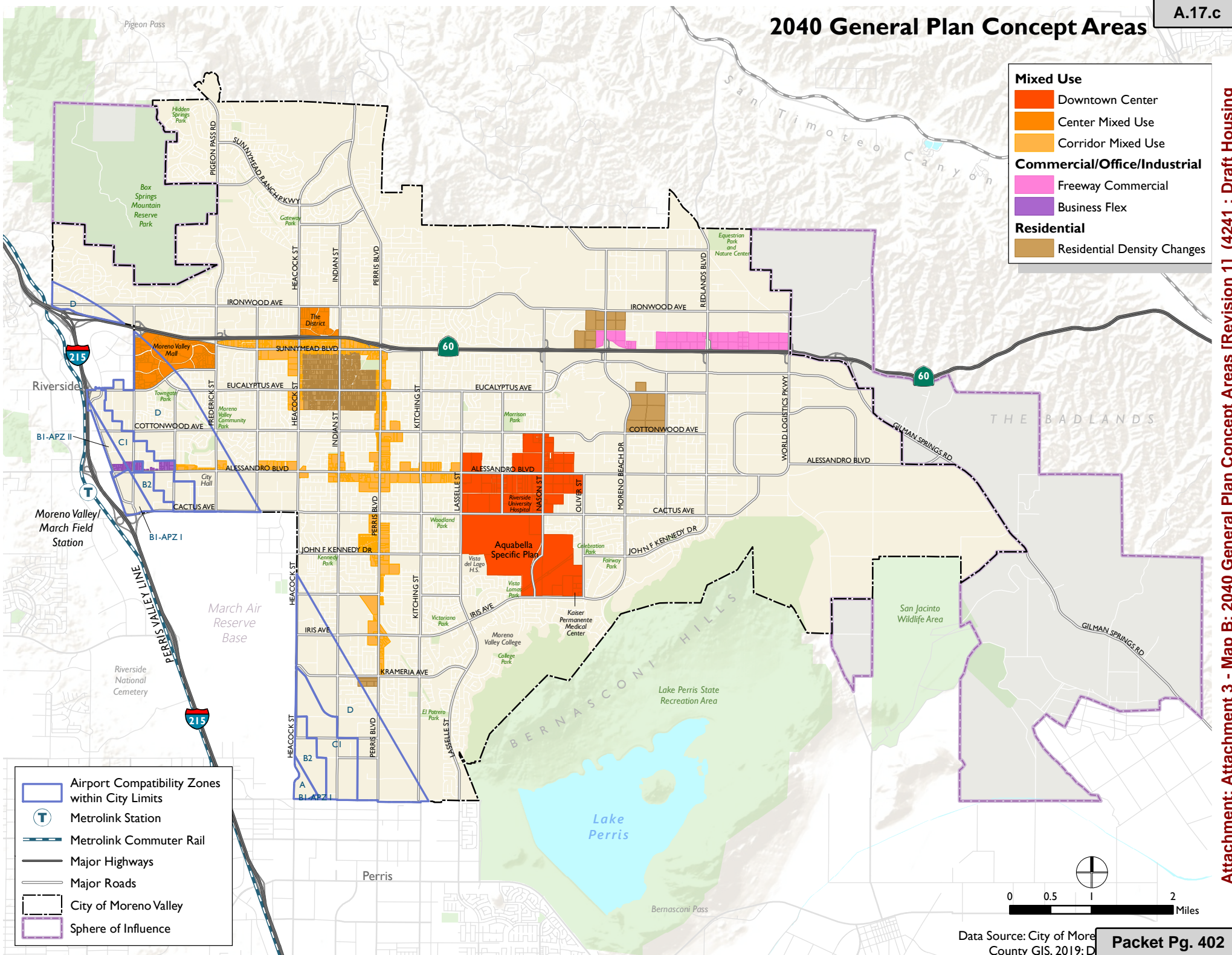


Existing Residential Development	
	Ag Residential
	Single Family Residential
	Duplex/Two Family Residential
	Condominium/Townhomes
	Multi Family Residential
	Mobile Home Parks
	Vacant Sites that allow Residential Development
T	Metrolink Station
	Metrolink Commuter Rail
	City of Moreno Valley
	Sphere of Influence

*Note: Some of the vacant sites from 2019 may have since been developed.

Attachment: Attachment 2 - Map A: Existing Residential Land Use and Vacant Sites [Revision 1] (4241 :

2040 General Plan Concept Areas



Mixed Use

- Downtown Center
- Center Mixed Use
- Corridor Mixed Use

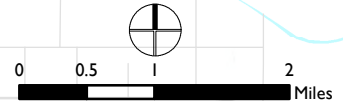
Commercial/Office/Industrial

- Freeway Commercial
- Business Flex

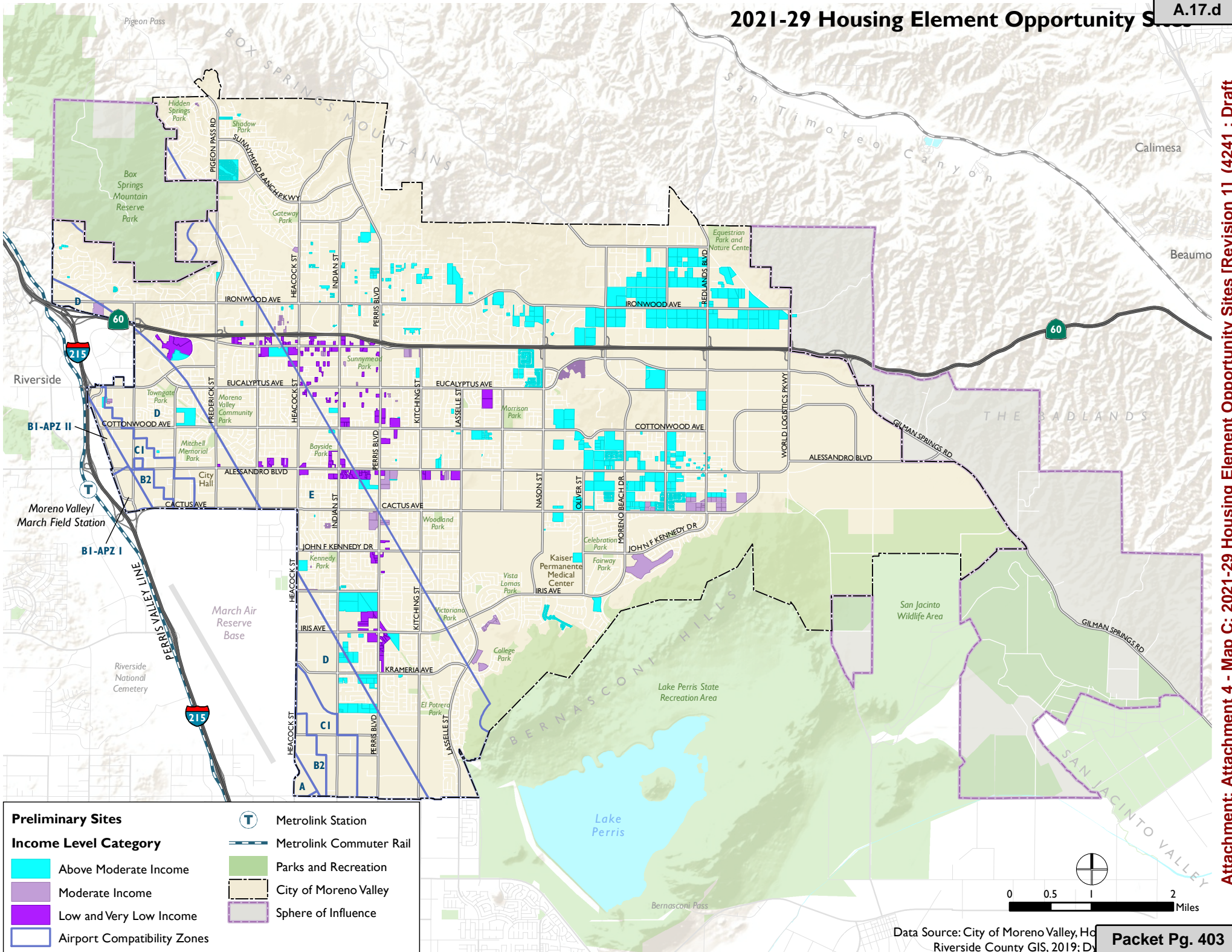
Residential

- Residential Density Changes

- Airport Compatibility Zones within City Limits
- Metrolink Station
- Metrolink Commuter Rail
- Major Highways
- Major Roads
- City of Moreno Valley
- Sphere of Influence



Attachment: Attachment 3 - Map B: 2040 General Plan Concept Areas [Revision 1] (4241 : Draft Housing

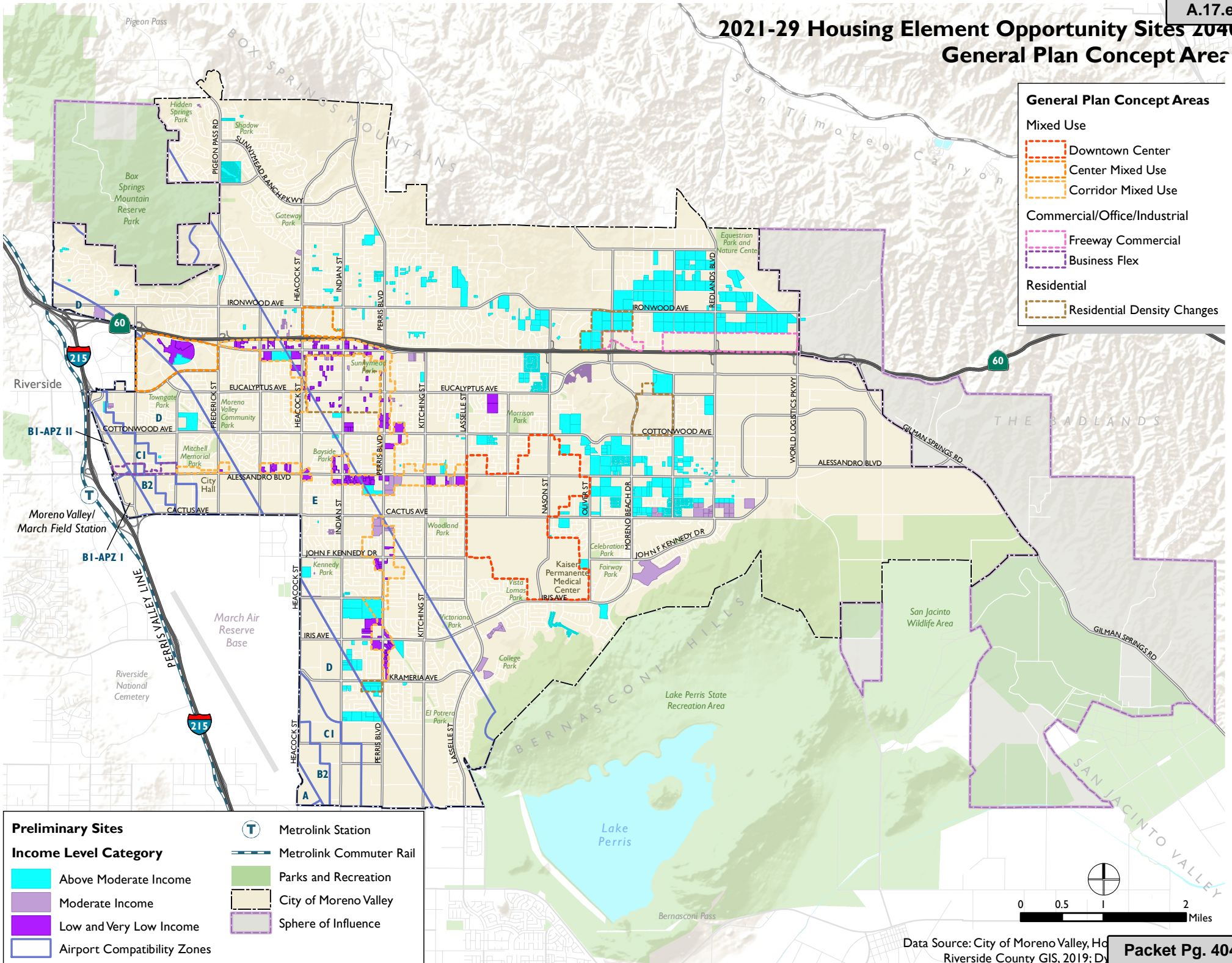


Preliminary Sites	Metrolink Station
Income Level Category	Metrolink Commuter Rail
Above Moderate Income	Parks and Recreation
Moderate Income	City of Moreno Valley
Low and Very Low Income	Sphere of Influence
Airport Compatibility Zones	



Attachment: Attachment 4 - Map C: 2021-29 Housing Element Opportunity Sites [Revision 1] (4241 : Draft

2021-29 Housing Element Opportunity Sites 2040 General Plan Concept Area



General Plan Concept Areas

- Mixed Use
 - Downtown Center
 - Center Mixed Use
 - Corridor Mixed Use
- Commercial/Office/Industrial
 - Freeway Commercial
 - Business Flex
- Residential
 - Residential Density Changes

Preliminary Sites

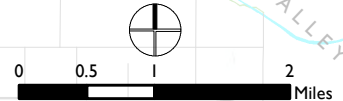
- Metrolink Station
- Metrolink Commuter Rail

Income Level Category

- Above Moderate Income
- Moderate Income
- Low and Very Low Income
- Airport Compatibility Zones

Other Features

- Parks and Recreation
- City of Moreno Valley
- Sphere of Influence



Attachment: Attachment 5 - Map D: 2021-29 Housing Element Opportunity Sites Outside Concept Areas



Report to City Council

TO: Mayor and City Council

FROM: Patti Solano, Parks & Community Services Director

AGENDA DATE: December 15, 2020

TITLE: ADOPT A RESOLUTION OF THE CITY OF MORENO VALLEY, CALIFORNIA, APPROVING AN ANNUAL ADJUSTMENT TO QUIMBY IN-LIEU FEES PURSUANT TO CHAPTER 3.40.110 OF THE MORENO VALLEY MUNICIPAL CODE

RECOMMENDED ACTION

Recommendation:

1. Adopt Resolution No. 2020-____, authorizing an adjustment to Quimby In-Lieu fees.

SUMMARY

This report recommends updating Quimby In-Lieu fees based on the Consumer Price Index (CPI) for the Riverside-San Bernardino-Ontario Metropolitan Area for the period of September 2019 – September 2020, as published by the U.S. Department of Labor Statistics in October 2020.

Recommended updates for Quimby In-Lieu fees were reviewed by members of the Parks and Community Services Subcommittee and the Finance Subcommittee.

DISCUSSION

In 1975, the State of California passed the Quimby Act, allowing cities and counties to adopt local ordinances requiring developers to set aside land, donate conservation easements, or pay fees for park improvements. In-Lieu fees are required for residential developments of 50 or fewer lots or parcels. Dedication of parkland is required for residential developments of 51 or more lots or parcels. The City may elect to condition Quimby In-Lieu fees for residential developments of 51 or more lots or parcels if the location is not conducive to development of parks and recreation facilities. Quimby In-

Lieu fees are not conditioned for non-residential land uses.

As established in the Quimby Act, for residential subdivisions the Moreno Valley General Plan specifies a parkland standard of 3 acres per 1,000 residents. As new residential development occurs, maintenance of this minimum level of service standard is essential to provide adequate parks and recreational opportunities.

In 2000, the City Council adopted Ordinance 581 establishing the Quimby In-Lieu fee to be paid when new residential development does not dedicate land for parks or recreation facilities. The Quimby In-Lieu fee was developed to mitigate the additional requirement for park resources resulting from population increases due to new residential development. Staff is recommending approval of an adjustment to Quimby Fees as set forth in the attached resolution. This adjustment is based on the Consumer Price Index (CPI) for the Riverside-San Bernardino-Ontario Metropolitan Area for the period of September 2019 – September 2020, which is 1.68%.

Per Moreno Valley Municipal Code Section 3.40.110, Quimby In-Lieu fees shall be adopted annually by resolution of the City Council. Additionally, the City Council may authorize annual adjustments to Quimby fees to account for escalating land costs based on current market conditions. The following fee table, based on current CPI, is recommended for adoption and implementation.

Land Use	Current Fee (per Dwelling Unit)	Proposed Fee (per Dwelling Unit)
Single Family Unit	\$1,962	\$1,995
Multi Family Unit	\$1,457	\$1,481
Senior Housing Unit	\$1,022	\$1,039

Recommended updates for Quimby In-Lieu fees were reviewed by members of the Parks and Community Services Subcommittee and the Finance Subcommittee.

ALTERNATIVES

1. Adopt the recommended action as presented in this staff report. ***This alternative is recommended by staff as this will update the fee for needs currently projected to meet population growth.***
2. Do not approve the recommended action as presented in this staff report. ***This alternative is not recommended as this will result in a fee structure that is insufficient to meet requirements for projected growth levels.***

FISCAL IMPACT

Revenue that will be generated by the Quimby In-Lieu fee is dependent on levels of new residential development in the City.

NOTIFICATION

Posting of the Agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Patti Solano
Parks & Community Services Director

Department Head Approval:
Patti Solano
Parks & Community Services Director

Concurred By:
Marshall Eyerman
Assistant City Manager/Chief Financial Officer/City Treasurer

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

Objective 5.5: Promote a healthy community and lifestyle.

Objective 6.2: Improve health, wellness and fitness for Moreno Valley youth through recreation and sports programs.

ATTACHMENTS

- 1. Quimby Fee Schedule Resolution No. 2020-

APPROVALS

Budget Officer Approval	<u> ✓ Approved </u>	11/04/20 5:56 PM
City Attorney Approval	<u> ✓ Approved </u>	
City Manager Approval	<u> ✓ Approved </u>	11/05/20 8:53 AM

RESOLUTION NO. 2020-_____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADJUSTING QUIMBY IN-LIEU FEES

WHEREAS, in accordance with the provisions of Government Code Section 66477 (the "Quimby Act"), the City Council of the City of Moreno Valley ("City Council") adopted Ordinance 581 establishing the authority to require the dedication of land for park and recreation facilities or a payment in-lieu; and

WHEREAS, the City Council has determined that these fees should be established to provide for certain fees in-lieu of dedicating parkland required of certain types of new subdivisions located in or to be located within the City of Moreno Valley; and

WHEREAS, the City Council has determined that Quimby In-Lieu fees shall be adopted annually by resolution of the City Council; and

WHEREAS, the City Council may authorize annual adjustments to Quimby fees to account for escalating land costs based on current market conditions; and

WHEREAS, the proposed Quimby In-lieu fees to be charged are appropriate and do not exceed the actual cost of parkland acquisition in the City of Moreno Valley.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

APPROVES THE ADJUSTMENT OF QUIMBY IN-LIEU FEES:

Single Family Unit	\$1,989
Multi Family Unit	\$1,477
Senior Housing Unit	\$1,036

The Quimby In-lieu Fee Schedule as set forth above shall go into effect on the date that this resolution is approved and adopted.

Resolution No. 2020-_____ 1
Date Adopted: December 1, 2020

Attachment: Quimby Fee Schedule Resolution No. 2020- (4201 : QUIMBY-ADOPT A RESOLUTION OF THE CITY OF MORENO VALLEY,

APPROVED AND ADOPTED this 1st day of December, 2020.

Dr. Yxstian A. Gutierrez
Mayor
City of Moreno Valley,

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Attachment: Quimby Fee Schedule Resolution No. 2020- (4201 : QUIMBY-ADOPT A RESOLUTION OF THE CITY OF MORENO VALLEY,

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California do hereby certify that Resolution No. 2020-___ was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting held on the 1st day of December, 2020, by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor, Mayor Pro Tem)

PAT JACQUEZ-NARES, CITY CLERK

(SEAL)

Resolution No. 2020-_____
Date Adopted: December 1, 2020



Report to City Council

TO: Mayor and City Council

FROM: Patti Solano, Parks & Community Services Director

AGENDA DATE: December 15, 2020

TITLE: ADOPTION OF RESOLUTION FOR UPDATE OF THE CITY USER FEE SCHEDULE – PARKS AND COMMUNITY SERVICES USER FEES FOR FY 2020/2021

RECOMMENDED ACTION

Recommendation:

1. Adopt Resolution 2020-XX approving an annual CPI adjustment to the Parks and Community Services User Fees.

SUMMARY

This report recommends adoption of Resolution 2020-XX approving a 1.68% Consumer Price Index (CPI) adjustment to the existing Parks and Community Services Department's user fees and the addition of new facility fees and miscellaneous fees, to take effect December 15, 2020.

The City's Fee Schedule sets the rates or fees for services. The adjustment of fees for FY 2020/2021 reflects a fee increase of 1.68%, which was published in the CPI for the Riverside, San Bernardino, Ontario area for the period of September 2019 to September 2020 in October 2020 by the U.S. Department of Labor, Bureau of Labor Statistics. In addition to the CPI adjustment, staff is requesting the inclusion of fees for the rental of the amphitheater parking lot, fax and copy fees at the Senior Community Center, which are consistent with other departments in the City's fee schedule, and a fee for coffee at both the Cottonwood Golf Course and the Senior Community Center.

In evaluating the proposed user fee adjustment, it is important to note that the Department no longer generates adequate revenue to cover its expenditures and service levels and in Fiscal Years 2019/2020 and 2020/2021, the General Fund supplement of \$528,237, needed to maintain a balanced budget, was removed from the Department's allocations. The Department's primary revenue source, the Zone A parcel

tax, has been fixed since June 30, 1992, while other costs have risen over the last 28 years. The annual Zone A parcel tax of \$87.50 can be increased only by two-thirds (2/3) voter-approval.

The proposed fee schedule update has been reviewed by the Parks and Community Services and Finance Subcommittee members.

DISCUSSION

The City of Moreno Valley is allowed to recover costs for certain services provided at the request of the public. Based on California statutes, cities are able to recover up to the full cost of services, which are provided based on voluntary requests from the public. The statutes require that any revenues collected related to the provision of these services can only be used to offset costs resulting from the performance of the requested services or activities. These fees may not exceed the estimated reasonable cost of providing that service.

The Parks and Community Services User Fees include fees related to programs (sports, contract classes and seniors), park reservations (athletic fields), and facility room rentals (Conference and Recreation Center, Senior Community Center, and other community centers) managed by the Parks and Community Services Department. Another fee study will be conducted in 2022, which follows the routine five-year review period.

In keeping with best practices, fees will be adjusted annually to reflect the impact of inflation. The proposed fee increase reflects the increase in the CPI of 1.68% for the Riverside, San Bernardino, Ontario area for the period of September 2019 to September 2020, which was published in October 2020 by the U.S. Department of Labor, Bureau of Labor Statistics. CPI measures the rate of inflation experienced by consumer goods during a specific period. This ensures that the fees are comparable to the surrounding businesses that provide similar services to the Parks and Community Services Department.

The draft fee schedule refers to the previous maximum City Council approved fees, proposed fee and applied fees. The applied fees were determined to ensure cashiering efficiency (fees were rounded down to the nearest whole dollar, .10 or .25 cents, not to exceed the CPI) as well as customer convenience. Fees that did not meet these dollar amount thresholds will remain unaffected and will be compounded annually with each CPI increase until one of the thresholds is met.

ALTERNATIVES

1. Adopt the proposed resolution adjusting Parks and Community Services Fees. Staff recommends this alternative since this establishes a fee schedule that is able to meet the expected needs of the community by offering park programs, activities, and facility rentals.

2. Do not adopt the proposed resolution and provide staff with additional direction. Staff does not recommend this alternative since this will result in a fee structure that is not sufficient to offer the needed programs, activities, and facility rentals to the community.

FISCAL IMPACT

The fiscal impact of the adoption of these user fees will be dependent on the demand for these services from the community.

NOTIFICATION

Posting of the agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Erica Green
Parks & Community Services Deputy Director

Department Head Approval:
Patti Solano
Parks & Community Services Director

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development
2. Public Safety
3. Library
4. Infrastructure
5. Beautification, Community Engagement, and Quality of Life
6. Youth Programs

Objective 5.5: Promote a healthy community and lifestyle.

Objective 6.2: Improve health, wellness and fitness for Moreno Valley youth through recreation and sports programs.

ATTACHMENTS

1. RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/08/20 4:48 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/08/20 5:55 PM

RESOLUTION NO. 2020-__

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ESTABLISHING USER FEES FOR THE PARKS AND COMMUNITY SERVICES DEPARTMENT FOR FISCAL YEAR (FY) 2020-2021 AND REPEALING PRIOR RESOLUTIONS THAT MAY CONFLICT WITH APPROVING THE FY 2020-2021 FEE SCHEDULE.

WHEREAS, the City Council of the City of Moreno Valley, California, has in the past approved and adopted fee schedules for various services provided by the City for the benefit of a limited number of persons; and

WHEREAS, some portion of the cost of rendering such services should be borne by the beneficiaries of such service; and

WHEREAS, data supporting the estimated cost of providing said services has been made available to the City Council and to the public, and the source of revenues required to defray such costs; and

WHEREAS, the fees set forth in the proposed fee schedule do not exceed the actual or estimated costs to the City of providing the services to which the fees relate; and

WHEREAS, the City Council has duly considered at a duly noticed regularly scheduled meeting, the question of whether or not to modify existing fees and establish new fees for such services, and

WHEREAS, at said meeting, the City Council duly considered all public comments which were made with respect to said question;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

1. The fee schedule attached hereto as Exhibit A, "Section 6 – Parks & Community Services", is hereby approved, adopted and incorporated herein by this reference, amending the City of Moreno Valley Schedule of City Fees, Charges and Rates Fiscal Year 2020-2021.
2. The approved fees shall be applied to all uses set forth in said schedule.
3. There is a reasonable relationship between the fees to be collected for the provision of various City services and the City's costs in providing those services as identified in the data made available.

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION FOR UPDATE OF THE CITY USER FEE

4. The fees set forth in Exhibit A are necessary to enable the City to provide the services to which they relate.

5. The approved fee schedule established herein shall become effective December 15, 2020.

6. All prior enactments of the City Council establishing fees for services and materials are hereby repealed to the extent that such enactments establish fees for services and materials which are different than the fees established therefore by this Resolution, but shall otherwise remain in full force and effect.

7. If any provision, clause, sentence or paragraph of this Resolution or the application thereof to any person or circumstance shall be held invalid, such invalidity shall not affect any other provision or application of the provisions of this Resolution which can be given effect without the invalid provision or application, and to this end the provisions of this Resolution are hereby declared to be severable.

APPROVED AND ADOPTED this 15th day of December, 2020.

Mayor

ATTEST:

City Clerk

APPROVED AS TO FORM:

City Attorney

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Resolution No. 2020- was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the 15th day of December, 2020 by the following vote:

AYES:

NOES:

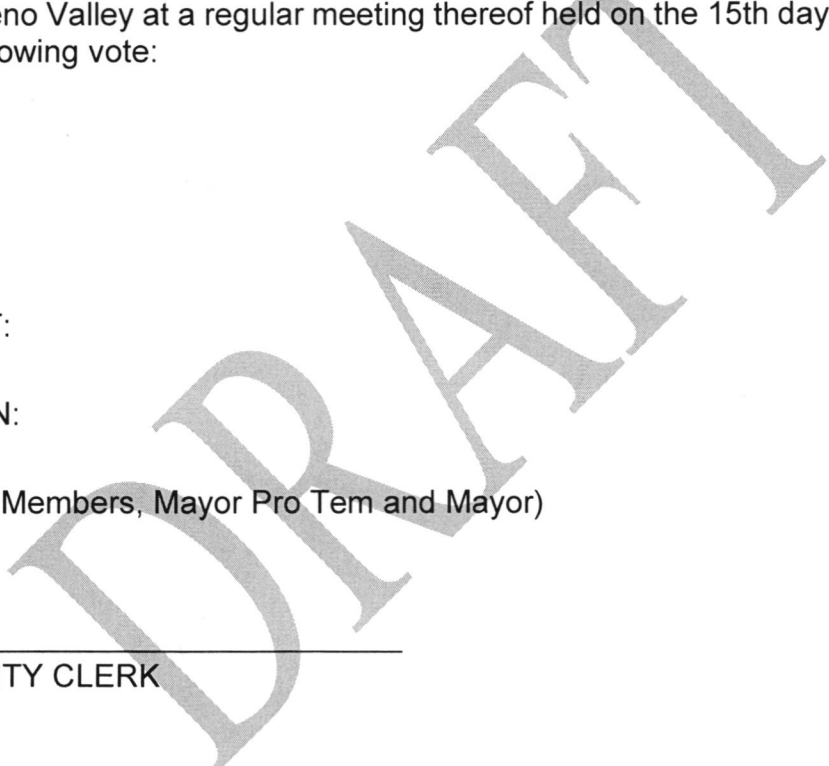
ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)



Section 6 - Parks and Community Services

Fee Description	Unit	CURRENT FEE	Proposed Fee w/CPI	Applied Fee	Comments on Proposed Changes to Fees
Recreation Programs					
<i>Non-Resident Fees: Fees for Non-Residents to participate in Recreation Programs and Services will be charged the Resident Fee plus 30%. Where fee ranges are used, actual fees are set based on specific program elements and posted in the Soaring Guide, as determined by Parks & Community Services staff.</i>					
Adult Sports (18 Years or Older)					
Adult Sports League	Per Team	\$ 324.75-433	\$ 330.20-440.27	\$ 330-440.25	
Adult Sports Tournament	Per Team	\$ 200-346.40	\$ 203.36-352.21	\$ 203.30-352.20	
Open Play	Per Person	\$ 4.10	\$ 4.1689	\$ 4.10	
Protest and Forfeit Fee	Per Person	\$ 26.80	\$ 27.2502	\$ 27.25	
New Program	Per Person	Cost Recovery	Cost Recovery	Cost Recovery	
Cancellation/Transfer Fee (before start of program)	Per Person	20%	20%	20%	
Adult Activities and Programs					
Contract Classes	Per Person	\$ 16.50-324.75	\$ 16.78-330.21	\$ 16.75-330.20	
Programs	Per Person	\$ 11.30-91.75	\$ 11.49-93.29	\$ 11.40-93.25	
Cancellation/Transfer Fee (before start of program)	Per Person	20%	20%	20%	
New Contract Classes	Per Person	Cost Recovery	Cost Recovery	Cost Recovery	
Youth Sports					
Youth Sports Leagues #	Per Person	\$ 54.60-113.40	\$ 55.52-115.30	\$ 55.50-115.30	
Youth Sports Tournaments	Per Team	\$ 162.90-324.75	\$ 165.64-330.21	\$ 165.60-330.20	
Sports Camps/Clinics #	Per Person	\$ 32.90-119.60	\$ 33.45-121.61	\$ 33.40-121.60	
Open Play Sports	Per Person	\$ 2.00	\$ 2.0336	\$ 2.00	
New Youth Sports Programs #	Per Person	Cost Recovery	Cost Recovery	Cost Recovery	
Cancellation/Transfer Fee (before start of program)	Per Person	20%	20%	20%	
# Denotes Family Discount available. See note below.					
Youth Activities and Programs					
Contract Classes	Per Person	\$ 15.40-328.80	\$ 15.65-334.32	\$ 15.60-334.30	
Camp (Weekly) #	Per Person	\$ 55.60-129.90	\$ 56.53-132.08	\$ 56.50-132	
Time for Tots - 4 Days (2 Weeks) #	Per Person	\$ 84.50	\$ 85.92	\$ 85.90	
T-Shirts	Each	\$ 11.30	\$ 11.49	\$ 11.40	
Late Pick-Up Fee	Per 15 min	\$ 5.10	\$ 5.19	\$ 5.10	
New Activity/Program #	Per Person	Cost Recovery	Cost Recovery	Cost Recovery	
New Contract Class	Per Person	Cost Recovery	Cost Recovery	Cost Recovery	
Cancellation/Transfer Fee (before start of program)	Per Person	20%	20%	0.20	
# Denotes Family Discount available. See note below.					

-FAMILY DISCOUNT

A family discount applies when siblings are registered in the same program. The family discount only applies to non-contract programs as identified with an #. The following family discount rates apply:

- 1st Child = Full rate
- 2nd Child = Receives a 15% discount
- 3rd Child + = Receives a 10% discount off the 2nd child rate

Licensed Child Care

Fees for "A Child's Place" Child Care Program are established by the California Department of Education Family Fee Schedule, pursuant to the Education Code [EC 8273 and EC 8447(e)].

Section 6 - Parks and Community Services

Fee Description	Unit	CURRENT FEE	Proposed Fee w/CPI	Applied Fee	Comments on Proposed Changes to Fees
Senior Programs					
Bingo	Per Person	1.00	\$ 1.02	\$ 1.00	
	Addtl. Card	0.50	\$ 0.51	\$ 0.50	
Breakfast, Lunch, Pizza, Picnic	Per Person	Cost Recovery	Cost Recovery	Cost Recovery	
Bunco	Per Person	2.00	\$ 2.03	\$ 2.00	
Dances	Per Person	8.20	\$ 8.34	\$ 8.30	
Line Dancing	Per Person	Contracted	Contracted	Contracted	
Swap Meet	Per Table	5.10	\$ 5.26	\$ 5.25	
New Senior Contract Program - Administered by Contract	Per Person	Contracted	Contracted	Contracted	
New Senior Program Provided by Outside Agencies	Per Person		\$	\$	
New Senior Program Assisted by City Staff	Per Person	Contracted	Contracted	Contracted	
New Senior Programs Assisted By Volunteers	Per Person		\$	\$	
Trips/Tours - Administered by Contract	Per Person	Contracted	Contracted	Contracted	
Community Events					
Entry & Activity Fee	Per Person	\$ 0-11.30	\$ 0-11.49	\$ 0-11.40	
Parade/Exhibitor Entry*	Per Entry	\$ 32.90-54.60	\$ 33.45-55.52	\$ 33.40-55.50	
Info Booth (Commercial)	Per Booth	\$ 108.25-271.10	\$ 110.07-275.65	\$ 110.00-275.60	
Info Booth (Non-Profit)	Per Booth	\$ 26.80-54.60	\$ 27.25-55.52	\$ 27.25-55.50	
Food Booth (Commercial)	Per Booth	\$ 81.40-324.75	\$ 82.77-330.21	\$ 82.75-330.20	
Food Booth (Non-Profit)	Per Booth	\$ 81.40-189.70	\$ 82.77-192.87	\$ 82.75-192.80	
Vendor Booth Deposit	Per Booth	\$ 24.75-103.10	\$ 25.17-104.83	\$ 25.10-104.80	
Arts & Craft Vendor	Per Booth	\$ 26.80-108.25	\$ 27.25-110.07	\$ 27.25-110	

*Denotes early registration fee

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Fee Description	Unit	CURRENT FEE	Proposed Fee w/CPI	Applied Fee	Comments on Proposed Changes to Fees
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Park Reservations

*Non-Resident Fees: Fees for Non-Residents to participate in Recreation Programs and Services will be charged the Resident Fee plus 30%.
Where fee ranges are used, actual fees are set based on specific program elements and posted in the Soaring Guide, as determined by Parks & Community Services staff.*

Athletic Facility Reservation

Non-Profit Youth Groups (per field)	Per Hour	\$ 3.00	\$ 3.05	\$ 3.00	
Adult Non-Profit Groups and Unorganized Public Play (per field)	Per Hour	\$ 7.70	\$ 7.83	\$ 7.80	
Adult & Youth Sports play by Private Groups	Per Hour	\$ 15.40	\$ 15.66	\$ 15.60	
Organized Commercial (For Profit) Groups (per field)	Per Hour	\$ 77.30	\$ 78.60	\$ 78.50	
Baseball Field Preparation	Per Field	\$ Cost Recovery	\$ Cost Recovery	\$ Cost Recovery	
Multi-Use Field Preparation	Per Field	\$ Cost Recovery	\$ Cost Recovery	\$ Cost Recovery	
Basketball Court	Per Hour	\$ 5.10	\$ 5.19	\$ 5.10	
Tennis Court	Per Hour	\$ 5.10	\$ 5.19	\$ 5.10	
Cancellation/Non-field use (per hour, per field)		50%	50%	50%	
Refundable Security Deposit	Org. per year	\$ 257.75	\$ 262.08	\$ 262.00	

Athletic Field Lighting

Youth Groups	Per Hour	\$ 18.50	\$ 18.81	\$ 18.80	
Adult Groups	Per Hour	\$ 21.60	\$ 21.96	\$ 21.90	

Snack Bar

Non-Profit Youth Groups	Per Day	\$ 25.75	\$ 26.18	\$ 26.10	
Non-Profit Youth Groups	Per Evening	\$ 15.40	\$ 15.66	\$ 15.60	
Youth and Non-Profit Groups (over 4 hour play)	Per Day	\$ 61.80	\$ 62.84	\$ 62.80	
Refundable Security Deposit (required for all groups)	Per Rental	\$ 206.20	\$ 209.66	\$ 209.60	
Key Deposit (required for all groups)	Per Group	\$ 26.80	\$ 27.25	\$ 27.25	

Storage Unit Rental

Storage Units 0 - 80 ft ²	Per Month	\$ 38.10	\$ 38.74	\$ 38.70	
Storage Units 81 - 160 ft ²	Per Month	\$ 76.25	\$ 77.53	\$ 77.50	

Over 4 Hour Play

Field Reservation/Non-Profit (First field preparation included)*	Per Field	\$ 128.80	\$ 130.96	\$ 130.90	
Field Reservation/Private Group (First field preparation included)	Per Field	\$ 206.20	\$ 209.66	\$ 209.60	
Mound Drop and Removal	Per Event	\$ 189.70-324.75	\$ 192.80-330.21	\$ 192.80-330.20	
Refundable Security Deposit	Per Field	\$ 103.10	\$ 104.83	\$ 104.80	
Cancellation/Non-field use (per hour, per field)		50%	50%	50%	

Gate Admission and Camp/Clinic Fees Must be Pre-Approved by Director Per Event

**Up to 3 days @ 4 fields maximum before reverting to athletic facility reservation fees*

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Fee Description	Unit	CURRENT FEE	Proposed Fee w/CPI	Applied Fee	Comments on Proposed Changes to Fees
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Park Fees

Park Reservation for Special Events	Per Day	\$ 54.60	\$ 55.52	\$ 55.50	
Park Reservation for Park Area	Per Day	\$ 26.80	\$ 27.25	\$ 27.25	
Picnic Shelter Small (1-2 Tables)	Per Day	\$ 43.30	\$ 44.03	\$ 44.00	
Picnic Shelter Medium (3-6 Tables)	Per Day	\$ 86.60	\$ 88.05	\$ 88.00	
Picnic Shelter Large (6 or More Tables)	Per Day	\$ 173.20	\$ 176.11	\$ 176.10	
Refundable Security Deposit	Per Day	\$ 77.30-515.50	\$ 78.60-524.16	\$ 78.60-524.10	
Special Event Permit	Per Day	\$ 223.70	\$ 227.46	\$ 227.40	
Cancellation/Transfer Fee		20%	0.20	20%	
Electrical Use	Per Day	\$ 38.10	\$ 38.74	\$ 38.70	

Valley Skate Park and Soccer Arena

Soccer Arena Rental	Per Hour	\$ 5.10-54.60	\$ 5.19 - 55.52	\$ 5.10-55.50	
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Golf Course

Adult 9 Holes	Weekday	\$ 9.25	\$ 9.41	\$ 9.40	
	Weekend	\$ 11.30	\$ 11.49	\$ 11.40	
Senior (55+) 9 Holes	Weekday	\$ 6.10	\$ 6.20	\$ 6.20	
	Weekend	\$ 9.25	\$ 9.41	\$ 9.40	
Youth* (17 & under) 9 Holes	Weekday	\$ 5.10	\$ 5.19	\$ 5.10	
	Weekend	\$ 7.20	\$ 7.32	\$ 7.30	
Twilight Fees		\$ 6.10	\$ 6.20	\$ 6.20	
Tournament Play		\$ 12.30	\$ 12.51	\$ 12.50	
Additional 9 Holes		\$ 5.10	\$ 5.19	\$ 5.10	
Cart and Club Rentals	Weekday	\$ 3.00	\$ 3.05	\$ 3.00	
	Weekend	\$ 5.10	\$ 5.19	\$ 5.10	
Adult Punch Card**	10 Rounds	\$ 77.30	\$ 78.60	\$ 78.60	
Senior Punch Card**	10 Rounds	\$ 51.50	\$ 52.37	\$ 52.30	
Youth Punch Card**	10 Rounds	\$ 41.20	\$ 41.89	\$ 41.80	

*All Youth must be accompanied by a paying adult.
 ** All Punch cards only valid for 9 hole rounds on Monday through Friday.

Miscellaneous

Coffee (Senior Center and Golf Course)	Per Cup	\$ 0.75	\$ 0.76	\$ 0.75	
Photocopies (Black and White) CRC and Senior Center	Per Page	\$ 0.15	\$ 0.15	\$ 0.15	
Faxes (Senior Center and CRC)	Per Page	\$ 1.00	\$ 1.02	\$ 1.00	

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Facility Rental Fees

Conference and Recreation Center Hourly Rental Rates

Group 1	City events
Group 2	City of Moreno Valley residents, non-profit resident organizations, Moreno Valley commercial groups for social activities, government agencies (Federal, State, County) and educational institutions. Proof of residency may be required.
Group 3	Non-Resident and non-resident non-profit agencies. Proof of non-profit status may be required. Commercial use.

DAYS 4 HOUR MINIMUM	Grand Valley Ballroom* (includes small patio)			DAYS 4 HOUR MINIMUM	Garden Room		
	Group 3 Applied Proposed w/CPI Current	Group 2 Applied Proposed w/CPI Current	Group 1 Applied Proposed w/CPI Current		Group 3 Applied Proposed w/CPI Current	Group 2 Applied Proposed w/CPI Current	Group 1 Applied Proposed w/CPI Current
M - FRI	\$220.10	\$176.10	\$55.50	M - TH	\$79.90	\$58.50	\$33.90
	\$220.14	\$176.11	\$55.52		\$79.92	\$58.57	\$33.96
	\$216.50	\$173.20	\$54.60		\$78.60	\$57.60	\$33.40
SAT	\$385.75	\$330.20	\$330.20	FRI - SUN	\$99.50	\$78.60	\$33.40
	\$385.77	\$192.80 after 6 hrs	\$192.80 after 6 hrs				
	\$379.40	\$330.22	\$330.22				
SUN	\$330.20	\$275.60	\$275.60		\$99.54	\$78.60	\$33.45
	\$330.21	\$192.80 after 6 hrs	\$192.80 after 6 hrs	\$97.90	\$77.30	\$32.90	
	\$324.75	\$275.65	\$275.65				
LONG TERM	\$176.10	\$176.10	n/a	LONG TERM	\$52.30	\$52.30	n/a
	\$176.11	\$176.11	n/a		\$52.37	\$52.37	n/a
	\$173.20	\$173.20	n/a		\$51.50	\$51.50	n/a

*Use of Large Patio is \$52.30/hour for Groups 2 & 3 (no cost to Group 1)

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

DAYS 2 HOUR MINIMUM	Alessandro Room (A&B)**			Frank E. Brown Room			DAYS 2 HOUR MINIMUM	Gymnasium**			Aerobics Studio		
	Group 3 Applied	Group 2 Applied	Group 1 Applied	Group 3 Applied	Group 2 Applied	Group 1 Applied		Group 3 Applied	Group 2 Applied	Group 1 Applied	Group 3 Applied	Group 2 Applied	Group 1 Applied
	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI		Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI
	Current	Current	Current	Current	Current	Current		Current	Current	Current	Current	Current	Current
M - SUN	\$94.25	\$62.80	\$33.40	\$47.00	\$31.40	\$16.60	M - SUN						
	\$94.26	\$62.84	\$33.45	\$47.08	\$31.42	\$16.68							
	\$92.70	\$61.80	\$32.90	\$46.30	\$30.90	\$16.40							
	\$54.50	\$54.50	n/a	\$27.25	\$27.25	n/a			\$110.00	\$55.50	\$27.25	\$82.75	\$33.40
LONG TERM	\$54.50	\$54.50	n/a	\$27.25	\$27.25	n/a		\$110.07	\$55.52	\$27.25	\$82.77	\$33.45	\$16.68
	\$53.60	\$53.60	n/a	\$26.80	\$26.80	n/a		\$108.25	\$54.60	\$26.80	\$81.40	\$32.90	\$16.40

**Rates reduced 50% for half of Alessandro Room or Gymnasium rental
 Little Rascals Room rented at \$44/hour as part of large event rental

Conference and Recreation Center Service/Facility Fees

Refundable Security Deposits
 CURRENT

Grand Valley Ballroom	Garden Room	Alessandro Rooms	Frank E. Brown Room	Gymnasium	Aerobics Studio
\$206.20-\$773.25	\$206.20	\$103.10	\$103.10	\$257.75-\$515.50	\$257.75-\$515.50

PROPOSED W/CPI

Grand Valley Ballroom	Garden Room	Alessandro Rooms	Frank E. Brown Room	Gymnasium	Aerobics Studio
\$209.66-\$786.24	\$209.66	\$104.83	\$104.83	\$262.08-\$524.16	\$262.08-\$524.16

APPLIED

Grand Valley Ballroom	Garden Room	Alessandro Rooms	Frank E. Brown Room	Gymnasium	Aerobics Studio
\$209.60-\$786.20	\$209.60	\$104.80	\$104.80	\$262-\$524.10	\$262-\$524.10

Fee	Current	Proposed w/CPI	Applied	Fee	Rate
AV Projector	\$54.60-81.40	\$55.52-82.77	\$55.50-82.75	Audio/Visual Tech	Cost Recovery
Microphone	\$26.80	\$27.25	\$27.25	Staff Time/Overtime	
Deluxe Sound System	\$108.25	\$110.07	\$110.00	Cleaning	
Dance Floor (500-1000 sq ft)	\$216.50	\$220.14	\$220.10	Security	
Platform Lights	\$108.25-216.50	\$110.07-\$220.14	\$110-\$220.10	Linens	
Scoreboard	\$25.75	\$26.18	\$26.10	Insurance	
Gym Floor Covering	\$1,407.30	\$1,430.94	\$1,430.90	False Alarms (Police/Fire)	
Transfer Fee, each	\$43.30	\$44.03	\$44.00	Cancellation Fees	
Extended Facility Use	150% of regular rate, plus security	150% of regular rate, plus security		5-31 days prior to event 0-4 days prior to event	
				Loss of Deposit, plus: 50% of Rental Fees 100% of Rental Fees	

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Community Center Hourly Rental Rates

Group 1	City sponsored/conducted events, governmental agencies with reciprocal agreements.
Group 2	City of Moreno Valley residents, non-profit resident organizations, Moreno Valley commercial groups for social activities, government agencies (Federal, State, County) and educational institutions. Proof of residency and non-profit status may be required.
Group 3	Non-Resident and non-resident non-profit agencies. Proof of non-profit status may be required. Commercial use.

DAYS MIN. HRS	Senior Community Center						Cottonwood Golf Center					
	Banquet Room (w/ North Patio/Kitchen)			Meeting Rooms*			Banquet Room (w/ Kitchen)			Meeting Room		
	Group 3 Applied	Group 2 Applied	Group 1 Applied	Group 3 Applied	Group 2 Applied	Group 1 Applied	Group 3 Applied	Group 2 Applied	Group 1 Applied	Group 3 Applied	Group 2 Applied	Group 1 Applied
	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed	Proposed
M - TH 2 HOUR MIN.	\$130.90 \$130.96 \$128.80	\$110.00 \$110.07 \$108.25	\$41.80 \$41.89 \$41.20	\$52.30 \$52.37 \$51.50	\$31.40 \$31.42 \$30.90	\$27.25 \$27.25 \$26.80	\$110.00 \$110.07 \$108.25	\$89.00 \$89.07 \$87.60	\$41.80 \$41.89 \$41.20	\$62.80 \$62.84 \$61.80	\$41.80 \$41.89 \$41.20	\$33.40 \$33.45 \$32.90
FRI - SUN 4 HOUR MIN.	\$151.90 \$151.91 \$149.40	\$130.90 \$130.96 \$128.80		\$62.80 \$62.84 \$61.80	\$41.80 \$41.89 \$41.20		\$130.90 \$130.96 \$128.80	\$110.00 \$110.07 \$108.25		\$73.30 \$73.31 \$72.10	\$52.30 \$52.37 \$51.50	
LONG TERM 4 HOUR MIN.	\$125.75 \$125.78 \$123.70	\$104.80 \$104.83 \$103.10	n/a n/a n/a	\$48.20 \$48.20 \$47.40	\$27.25 \$27.25 \$26.80	n/a n/a n/a	\$104.80 \$104.83 \$103.10	\$83.75 \$83.78 \$82.40	n/a n/a n/a	\$57.60 \$57.65 \$56.70	\$36.60 \$36.60 \$36.00	n/a n/a n/a

*Meeting rooms are Classroom 1 & 2 or Arts & Crafts 1 & 2

DAYS MIN. HRS	TownGate Community Center**			March Field Park Community Center		
	Banquet Room			Banquet Room		
	Group 3 Applied	Group 2 Applied	Group 1 Applied	Group 3 Proposed	Group 2 Proposed	Group 1 Proposed
	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI
M - TH 2 HOUR MIN.	\$99.50 \$99.54 \$97.90	\$78.60 \$78.60 \$77.30	\$41.80 \$41.89 \$41.20	\$99.50 \$99.54 \$97.90	\$78.60 \$78.60 \$77.30	\$33.40 \$33.45 \$32.90
FRI - SUN 4 HOUR MIN.	\$120.40 \$120.49 \$118.50	\$99.50 \$99.54 \$97.90		\$120.40 \$120.49 \$118.50	\$99.50 \$99.54 \$97.90	
LONG TERM 4 HOUR MIN.	\$94.25 \$94.26 \$92.70	\$73.30 \$73.31 \$72.10	n/a n/a n/a	\$78.60 \$78.60 \$77.30	\$57.60 \$57.65 \$56.70	n/a n/a n/a

**Renaissance Park discount of \$30.50 on a 4 hour rental

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Community Centers Service/Facility Fees

Refundable Security Deposits

CURRENT

Senior Community Center	Cottonwood Golf Center	TownGate Community Center	March Community Center
\$309.30	\$206.20	\$206.20	\$206.20

PROPOSED W/CPI

Senior Community Center	Cottonwood Golf Center	TownGate Community Center	March Community Center
\$314.50	\$209.66	\$209.66	\$209.66

APPLIED

Senior Community Center	Cottonwood Golf Center	TownGate Community Center	March Community Center
\$314.50	\$209.60	\$209.60	\$209.60

Fee	Current	Proposed w/CPI	Applied	Fee	Rate
AV Projector	\$54.60	\$55.52	\$55.50	Staff Time/Overtime, Cleaning, Security, Linens, Insurance, False Alarms (Police/Fire)	Cost Recovery
Transfer Fee, each	\$43.30	\$44.03	\$44.00		
Extended Facility Use	150% of regular rate, plus security	150% of regular rate, plus security		Cancellation Fees	Loss of Deposit, plus:
				5-31 days prior to event	50% of Rental Fees
				0-4 days prior to event	100% of Rental Fees

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Mobile Stage Daily Rental Rates*

Group 1	City sponsored/conducted events, governmental agencies with reciprocal agreements.		
Group 2	City of Moreno Valley residents, non-profit resident organizations, Moreno Valley commercial groups for social activities, government agencies (Federal, State, County) and educational institutions. Proof of residency and non-profit status may be required.		
Group 3	Non-Resident and non-resident non-profit agencies. Proof of non-profit status may be required. Commercial use.		
RENTAL LOCATION	Group 3 Applied	Group 2 Applied	Group 1 Applied
	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI
	Current	Current	Current
Within Moreno Valley City Boundaries	\$616.30	\$148.80	n/a
	\$616.38	\$148.86	
	\$606.20	\$146.40	
Outside Moreno Valley City Boundaries	\$660.40	\$633.10	
	\$660.41	\$633.16	
	\$649.50	\$622.70	

*All rentals require mandatory staff fees

Mobile Stage Service/Facility Fees

Fee	Current	Proposed w/CPI	Applied	Fee	Rate
Refundable Security Deposit - Stage Unit	\$103.10	\$104.83	\$104.80	Mandatory Staff Fees: Stage Unit with Platforms - 3 staff Stage Unit - 2 staff	Cost Recovery
Refundable Security Deposit - Sound System	\$103.10	\$104.83	\$104.80		
Sound System, includes: 8 channel amplifier, Two (2) large speakers w/ stands, Two (2) microphones w/ stands, tape deck	\$54.60	\$55.52	\$55.50	Cancellation Fees	Loss of Deposit, plus:
				5-31 days prior to event	50% of Rental Fees
				0-4 days prior to event	100% of Rental Fees

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Equestrian Center Daily Rental Rates

Group 1	City sponsored/conducted events, governmental agencies with reciprocal agreements.
Group 2	City of Moreno Valley residents, non-profit resident organizations, Moreno Valley commercial groups for social activities, government agencies (Federal, State, County) and educational institutions. Proof of residency and non-profit status may be required.
Group 3	Non-Resident and non-resident non-profit agencies. Proof of non-profit status may be required. Commercial use.

Group 3 Applied Proposed w/CPI Current	Group 2 Applied Proposed w/CPI Current	Group 1 Applied Proposed w/CPI Current
\$137.25	\$110.00	
\$137.27	\$110.07	n/a
\$135.00	\$108.25	

Equestrian Center Service/Facility Fees

Fee	Current	Proposed w/CPI	Applied	Fee	Rate
Refundable Security Deposit	\$309.30	\$314.50	\$314.50	Staff Time/Overtime Security	Cost Recovery
Water Key Deposit	\$25.75	\$26.18	\$26.10		
Tractoring Fee, each (if applicable)	\$108.25	\$110.07	\$110.00	Cancellation Fees 5-31 days prior to event 0-4 days prior to event	Loss of Deposit, plus: 50% of Rental Fees 100% of Rental Fees
Standby Tractoring Fee/ per hr	\$54.60	\$55.52	\$55.50		
Lighting/ per hr	\$18.00	\$18.30	\$18.30		
Transfer Fee, each	\$43.30	\$44.03	\$44.00		

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION

Section 6 - Parks and Community Services

Amphitheater Hourly Rates

Group 1	City sponsored/conducted events, governmental agencies, educational institutions with reciprocal agreements and non profit resident organizations.
Group 2	City of Moreno Valley residents, Moreno Valley commercial groups for social activities, government agencies (Federal, State, County), non-resident non-profit agencies and educational institutions.
Group 3	Non-Resident. Commercial use.

	Amphitheater		
	Group 3 Applied	Group 2 Applied	Group 1 Applied
	Proposed w/CPI	Proposed w/CPI	Proposed w/CPI
	Current	Current	Current
M - TH 2 HOUR MINIMUM	\$385.75 \$385.77 \$379.40	\$330.20 \$330.21 \$324.75	\$183.40 \$183.43 \$180.40
FRI - SUN 4 HOUR MINIMUM	\$385.75 \$385.77 \$379.40	\$330.20 \$330.21 \$324.75	\$183.40 \$183.43 \$180.40

Refundable Deposit

Amphitheater	Amphitheater	Amphitheater
Current	Proposed w/CPI	Applied
\$1,031.00	\$1,048.32	\$1,048.30

Amphitheater Service/Facility Fees

Fee	Current	Proposed w/CPI	Applied	Fee	Current	Proposed w/CPI	Applied
Deluxe Sound System	Cost Recovery	Cost Recovery	Cost recovery	Audio/Visual Tech	Cost Recovery	Cost Recovery	Cost Recovery
Theater Lights	Cost Recovery	Cost Recovery	Cost recovery	Staff Time/Overtime			
Special Event Permit	\$223.70	\$227.46	\$227.40	Cleaning			
Transfer Fee, each	\$44.03	\$44.77	\$44.75	Security			
				Linens			
Parking Lot - 1/2 lot - per hr	new fee		\$150.00	Insurance			
Parking Lot - full - per hr	new fee		\$300.00	False Alarms (Police/Fire)			
Extended Facility Use	150% of regular rate, plus security	150% of regular rate, plus security	150% of regular rate, plus security	Cancellation Fees 5-31 days prior	Loss of Deposit, plus 50% of Rental Fees	Loss of Deposit, plus 50% of Rental Fees	Loss of Deposit, plus: 50% of Rental Fees
				0-4 days prior	100% of Rental Fees	100% of Rental Fees	100% of Rental Fees

Attachment: RESOLUTION-FEE SCHEDULE FY 20-21 PROPOSED (4231 : ADOPTION OF RESOLUTION



Report to City Council

TO: Mayor and City Council

FROM: Manuel A. Mancha, Community Development Director

AGENDA DATE: December 15, 2020

TITLE: PROPOSED GENERAL PLAN AMENDMENT AND CHANGE OF ZONE AMENDING FIGURE 2-2 "LAND USE MAP" OF THE MORENO VALLEY GENERAL PLAN AND THE CITY ZONING ATLAS, RESPECTIVELY, AND PROPOSED CONDITIONAL USE PERMITS FOR A DRIVE THROUGH RESTAURANT WITH OUTDOOR SEATING, A FUELING STATION AND CONVENIENCE STORE WITH ALCOHOL SALES AT THE NORTHEAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE

RECOMMENDED ACTION

Recommendations:

1. **ADOPT** Resolution 2020-XX: A Resolution of the City Council of the City of Moreno Valley **CERTIFYING** that the Initial Study/Mitigated Negative Declaration, prepared for General Plan Amendment PEN19-0206, Change of Zone PEN19-0207, and Conditional Use Permits PEN19-0204 and PEN19-0205, on file with the Community Development Department, incorporated herein by this reference, completed in compliance with the California Environmental Quality Act Guidelines, and that the City Council reviewed and considered the information contained in the Initial Study/ Mitigated Negative Declaration, and that the document reflects the City's independent judgment and analysis, and **ADOPTING** the Mitigation Monitoring and Reporting Program prepared for General Plan Amendment PEN19-0206, Change of Zone PEN19-0207, and Conditional Use Permits PEN19-0204 and PEN19-0205;
2. **ADOPT** Resolution 2020-XX: A Resolution of the City Council of the City of Moreno Valley approving General Plan Amendment application PEN19-0206 to amend the General Plan Land Use map to amend the land use designation of 2.04-acres from Residential/Office (R/O) General Plan land use designation to Commercial (C) General Plan land use designation for the areas described in the

Resolution, based on the findings contained in the Resolution, and the revised Land Use Map;

3. **INTRODUCE** and conduct the first reading by title only of Ordinance No. XXX, approving a Change of Zone PEN19-0207 to amend the City Zoning Atlas to rezone 2.04-acres from Office Commercial (OC) District and Office (O) District to Community Commercial (CC) District for the areas described in the Ordinance, based on the findings contained in the Ordinance, and the revised Zoning Atlas;
4. **ADOPT** Resolution No. 2020-XX, A Resolution of the City Council of the City of Moreno Valley approving Conditional Use Permit PEN19-0204 based on the findings contained in this resolution, and subject to the conditions of approval included as Exhibit A;
5. **ADOPT** Resolution No. 2020-XX, A Resolution of the City Council of the City of Moreno Valley approving Conditional Use Permit PEN19-0205 based on the findings contained in this resolution, and subject to the conditions of approval included as Exhibit A; and
6. **SCHEDULE** the second reading and adoption of Ordinance No. XXX for the next regular City Council meeting.

SUMMARY

This report recommends approval of a General Plan Amendment (Case PEN19-0206) and Change of Zone Application (Case No. PEN19-0207) to amend the General Plan land use and Zoning designation boundaries to align to parcel boundaries proposed by the Conditional Use Permit applications (Case No's. PEN19-0204 and PEN19-0205) for a drive-through restaurant with outdoor seating and a fueling station to include a convenience store with off-site alcohol sales. The proposed project is located on the northeast corner of Perris Boulevard and Dracaea Avenue.

The project also requires Certification of a Mitigated Negative Declaration, including approval of a Mitigation Monitoring and Reporting Program (MMRP), pursuant to CEQA.

DISCUSSION

Advisory Board/Commission Recommendation

At its November 12, 2020 meeting, the Planning Commission held a public hearing and voted to recommend that the City Council certify the Mitigated Negative Declaration, adopt a Mitigation Monitoring Reporting Program, and approve the General Plan Amendment, Change of Zone, and Conditional Use Permits.

Project

The applicant, Cadence Acquisition, is requesting approval of the following: a General

Plan Amendment (Case PEN19-0206) and Change of Zone Application (Case No. PEN19-0207) to amend the General Plan land use and Zoning designation boundaries to align to parcel boundaries proposed by the Conditional Use Permit applications (Case No's. PEN19-0204 and PEN19-0205) for a drive-through restaurant with outdoor seating and a fueling station to include a convenience store with off-site alcohol sales.

General Plan Amendment

The City of Moreno Valley General Plan land use map designates the Project site as Residential/Office (R/O) and the proposal would change this to a Commercial (C) land use designation.

The primary purpose of the Residential/Office is to provide areas for the establishment of office-based working environments or residential development of up-to 15 dwelling units per acre. The primary use of the areas designated Commercial (C) is to provide property for business purposes which include the proposed project uses.

The Applicant is proposing a General Plan Amendment to amend the General Plan land use designation boundaries to align with the respective zoning boundaries and correspond to the parcel boundaries of the proposed Project. The proposed General Plan Amendment will result in a total increase of approximately 2.04-acres of Commercial (C) and a corresponding reduction of approximately 2.04-acres of Residential/Office (R/O).

Change of Zone

The site of the proposed Project is currently zoned Office Commercial (OC) District with the portion connecting the site to Atwood Avenue zoned Office (O) District. The primary purpose of Office Commercial (OC) District is to provide for the establishment of businesses, corporate and administrative offices, as well as commercial services which are supportive to major business developments. Retail facilities which support the office developments are permitted, subject to limitations (25% retail). The Office (O) District provides for general office type development creating a compatibility with surrounding residential uses.

The applicant is proposing a Change of Zone of the Project site to Community Commercial (CC) District. The primary purpose of the Community Commercial (CC) District is to provide for the general shopping needs of area residents and workers with a variety of businesses, retail, personal and related or similar services.

Conditional Use Permits

The Municipal Code allows for drive through restaurants, fueling stations and convenience stores with alcohol sales in the Community Commercial (CC) District with the approval of a Conditional Use Permit when located within 300 feet of existing residences or a residential zoning district.

The proposed Conditional Use Permit for the drive through restaurant requires a General Plan Amendment and Change of Zone as the existing underlying General Plan land use designation and the Office Commercial zoning designations do not allow for the use.

The Applicant will develop the 3,000 square foot drive through restaurant with outdoor seating on the northerly portion of the site with shared access to the site from Perris Boulevard. The site provides approximately 400 square feet of outdoor dining area to the east of the building and includes associated parking and a trash enclosure.

As part of the drive-through component of the restaurant proposal the Planning Commission recommended the City Council consider extended hours of operation for hours from 4:30 a.m. to 12:00 a.m., whereas the Municipal Code permits operations of the drive-through component of a restaurant from 6:00 a.m. to 10:00 p.m. due to the parcel's adjacency to a residential zoning district (R15 District) to the east. Staff supports the request with slight modifications and a condition of approval has been recommended that would allow hours of operation of the drive-through from 5:00 a.m. to 12:00 a.m., contingent upon separate approval of a Municipal Code Amendment to allow alternative hours. If such an amendment is not approved, the drive-through will be limited to the hours of 6:00 a.m. to 10:00 p.m.

The fueling station and the approximately 4,088 square foot convenience store will be located on the southwest portion of the site and will include a canopy with six pumps (12 stations) westerly of the store and fronting Perris Boulevard. The fueling station will be placed the furthest away from the existing easterly residential uses. Access to the site is provided by a shared driveway along Perris Boulevard and Dracaea Avenue.

A Conditional Use Permit allows the City to impose special development requirements to ensure that certain uses will not be detrimental to a project's surrounding properties. Conditional uses may be appropriate at one location but not at another because of the potential for impacts on surrounding properties. The following summarizes the Project's design elements that are intended to minimize impacts on nearby residential uses.

- A. A 20 foot wide landscape setback is provided between the Project and all property lines to the north and east with a six foot block wall between the Project site and the nearest residences.
- B. The canopy for the fueling station is more than 100 feet from the east property line and buffered from residential uses by the fueling station building.
- C. Two trash enclosures for the Project are located in the interior of the site approximately 150 away from all residential uses. The Code requirement is to be located a minimum of 45 feet from any residential structure. The trash enclosure would be fully screened and include a covered roof.

Site Surrounding

The approximately 2.04 acre site is located on the northeast corner of Perris Boulevard and Dracaea Avenue and will include a driveway access to Atwood Avenue along the Perris Street frontage.

The surrounding area includes existing single family homes to the east on property zoned Residential 15 (R15) District, existing single family homes to the north on property zoned Office (O) District, undeveloped property directly to the south across Dracaea Avenue is zoned Office Commercial (OC) District, and developed property to the west and southwest across Perris Boulevard is zoned Specific Plan 204 Community Commercial (CC) District.

Access/Parking

The main access to the Project site will be from Perris Boulevard into the center of the Project with right-in, right-out access. A secondary driveway along Dracaea Avenue will allow for full access including a left-in entrance for delivery vehicles. Access to and from the site at Atwood Avenue will run parallel to the Perris Boulevard frontage. The Atwood driveway will be fully improved with landscaping along the street frontage.

Design/Landscaping

The architectural design of the propose Project will consist of contemporary single-story buildings that include a combination of stucco and clad siding treatments with spandrel storefronts and accent metal canopies. Colors are grey with brown tone accents. The perimeter walls will be decorative block, six (6) feet in height.

The Project has been designed to meet required design and landscape standards and objectives set forth in the Municipal Code. The landscape elements of the Project include the landscape setback areas along Perris Boulevard and Dracaea Avenue with enhanced landscaping along the frontage to Atwood Avenue that will include street trees, parking lot landscaping, and landscape treatments along the perimeter of the site and within the bio-retention basin.

Environmental

An Initial Study was prepared by LSA Associates, Inc. in compliance with the California Environmental Quality Act (CEQA) Guidelines. The Initial Study examined the potential of the proposed Project having any significant impacts on the environment. The Initial Study/Mitigation Negative Declaration (IS/MND) provides information in support of the finding that a Mitigated Negative Declaration serves as the appropriate CEQA documentation for the proposed Project in that the proposed Project, with the implementation of the proposed mitigation measures, will not have a significant effect on the environment. Technical studies prepared in support of the IS/MND include the following: Air Quality – GHG, Biology – MSHCP, Western Riverside County MSHCP Consistency Analysis, Cultural Resources Assessment, Noise Analysis, and a Traffic

Study. The electronic files for the IS/MND with appendices are attached to this staff report. Anyone wishing to view the documents can also do so at City Hall.

Mitigation measures are recommended for the proposed Project in the following areas: Biological Resources, Cultural/Tribal Resources, Geology and Soils, Hydrology and Water Quality, Noise and Transportation, and are incorporated into the Mitigation Monitoring and Report Program. The measures for cultural resources have been included to address input from the Tribal governments. The measures are intended to ensure that potential resources that might be discovered are protected. However, these measures are not required to address a known significant impact. Based on the Initial Study, and the proposed mitigation measures, the Project will not cause any significant impacts or environmental damage.

The public comment period for the Notice of Availability for the Initial Study/Mitigated Negative Declaration began on October 23, 2020, and ended on November 12, 2020, which satisfies the required 20-day review period. As of the preparation of this staff report, no comments have been received.

ALTERNATIVES

1. Conduct a public hearing on this project, and take actions to certify the Initial Study/Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program, and approve the General Plan Amendment, Change of Zone and Conditional Use Permit applications, consistent with the Staff recommendations to the Planning Commission. Staff recommends this alternative.
2. Conduct a public hearing on this project, and do not approve the applications for this project. This action would retain the existing General Plan and Zoning Map land use designations for the project site, and would not certify the Initial Study/Mitigated Negative Declaration, or approve the Conditional Use Permit applications. Staff does not recommend this alternative.

FISCAL IMPACT

Not Applicable

NOTIFICATION

The public hearing notice for this project was published in the local newspaper on December 4, 2020. Public notices were sent to all property owners of record within 600 feet of the project site on December 4, 2020. The public hearing notice for this project was posted on the project site on December 4, 2020. As of the preparation of this report, Staff has received no public inquiries in response to the noticing efforts. Should comments regarding this noticing be received prior to the City Council hearing, they will be provided at the Public Hearing.

PREPARATION OF STAFF REPORT

Prepared By:
Julia Descoteaux
Associate Planner

Department Head Approval:
Manuel A. Mancha
Community Development Director

Concurred By:
Patty Nevins
Planning Official

CITY COUNCIL GOALS

Community Image, Neighborhood Pride and Cleanliness. Promote a sense of community pride and foster an excellent image about our City by developing and executing programs which will result in quality development, enhanced neighborhood preservation efforts, including home rehabilitation and neighborhood restoration.

CITY COUNCIL STRATEGIC PRIORITIES

1. **Economic Development**
2. **Public Safety**
3. **Library**
4. **Infrastructure**
5. **Beautification, Community Engagement, and Quality of Life**
6. **Youth Programs**

Objective 1.1: Proactively attract high-quality businesses.

Objective 1.3: Promote local hiring through the expansion of local, quality, high paying jobs, and workforce development efforts.

ATTACHMENTS

1. Resolution No. 2020-__ Initial Study MND
2. Exhibit A to Resolution No. 2020-__ Initial Study MND
3. Exhibit B to Resolution No. 2020-__ Initial Study MND
4. Exhibit C to Resolution No. 2020-__ Initial Study MND
5. Exhibit D to Resolution No. 2020-__ Initial Study MND
6. Resolution No. 2020-__ General Plan Amendment
7. Ordinance No. XXX Change of Zone
8. Exhibit A to Ordinance No. XXX Change of Zone
9. Resolution No. 2020-__ Conditional Use Permit PEN19-0204
10. Exhibit A to Resolution No. 2020-XX Conditions of Approval PEN19-0204
11. Resolution No. 2020-__ Conditional Use Permit PEN19-0205
12. Exhibit A to Resolution No. 2020-XX Conditions of Approval PEN19-0205
13. Project Plans

- 14. Aerial Map
- 15. Mailing Notice
- 16. 600 Foot Radius Map

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/07/20 4:46 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/07/20 6:36 PM

RESOLUTION NUMBER 2020-

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING A MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING PLAN FOR THE PERRIS AND DRACAEA COMMERCIAL PROJECT LOCATED ON THE NORTHEAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE (APNs 479-120-027, -029, -042, AND -043)

WHEREAS, the City of Moreno Valley (“City”) is a general law city and a municipal corporation of the State of California, and the lead agency for the preparation and consideration of environmental documents for projects that are subject to requirements of the California Environmental Quality Act (CEQA¹) and CEQA Guidelines²; and

WHEREAS, Cadence Acquisition LLC., (“Developer”) is seeking approval a General Plan Amendment and a Change of Zone and two Conditional Use Permits for the development of a commercial project to include a drive through restaurant with outdoor seating, a fueling station and convenience store with beer and wine sales (off-site consumption only) (“Project”) for property located at the northeast corner of Perris Boulevard and Dracaea Avenue (APNs 479-120-027, -029, -042, and -043) (“Site”); and

WHEREAS, Planning Division Staff completed an environmental assessment for the proposed Project, and, based on the assessment, decided to prepare an Initial Study (“IS”) and Mitigated Negative Declaration (“MND”) in accordance with Section 6 (ND Procedures) of the City’s Rules and Procedures for the Implementation of the California Environmental Quality Act and the requirements of the CEQA Guidelines Sections 15070 – 15075 and thereafter a Notice of Intent to Adopt a Mitigated Negative Declaration was duly noticed and circulated for public review for a period of 20 days commencing beginning October 23, 2020, ending November 12, 2020; and

WHEREAS, in conformance with CEQA, a Mitigation Monitoring Plan (“MMP”) that includes a program for reporting on and monitoring Project mitigation measures was prepared for the proposed Project and circulated with the Mitigated Negative Declaration; and

WHEREAS, at its November 12, 2020 meeting, the Planning Commission considered the Mitigated Negative Declaration and Mitigation Monitoring Plan for the Perris and Dracaea Commercial Project and recommended approval to the City Council; and

¹ Public Resources Code §§ 21000-21177

² 14 California Code of Regulations §§15000-15387

WHEREAS, on December 15, 2020 a duly noticed public hearing was conducted by the City Council to consider adoption of the Mitigated Negative Declaration and the Mitigation Monitoring Plan and approval of the proposed Project at which time the City Council considered the IS, Mitigated Negative Declaration and the Mitigation Monitoring Plan, together with any comments received during the public review process and the responses prepared; and

WHEREAS, at the conclusion of the public hearing, in the exercise of its own independent judgment, the City Council determined that the Mitigated Negative Declaration and the Mitigation Monitoring Plan were appropriate as all environmental impacts of the Project with mitigation measures are below a level of significance and there is no substantial evidence supporting a fair argument that the Project will have a significant effect on the environment.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

Section 1. Recitals and Exhibits

That the foregoing Recitals and attached exhibits are true and correct and are hereby incorporated by this reference.

Section 2. Evidence

That the City Council has considered all of the evidence submitted into the administrative record for the Mitigated Negative Declaration and Mitigation Monitoring Plan, including, but not limited to, the following:

- (a) Initial Study prepared for the proposed Project, attached hereto as Exhibit A;
- (b) Notice of Intent to Adopt a Mitigated Negative Declaration, attached hereto as Exhibit B;
- (c) Mitigated Negative Declaration, attached hereto as Exhibit C;
- (d) Mitigation Monitoring Plan, attached hereto as Exhibit D;
- (e) Staff Report prepared for the City Council's consideration and all documents, records and references related thereto, and Staff's presentation at the public hearing;
- (f) Testimony and/or comments from all persons that was provided in written format or correspondence, at, or prior to, the public hearing.

Section 3. Findings

That based on the content of the foregoing Recitals and the Evidence contained in the Administrative Record as set forth above, the City Council makes the following findings:

- (a) That the City has independently reviewed, analyzed, and considered the Mitigated Negative Declaration and Mitigation Monitoring Plan, and the whole record before it (including, the Initial Study and comments received) and, based on the foregoing, the City Council hereby finds that all environmental impacts of the proposed Project, with mitigation measures, are below a level of significance and there is no substantial evidence supporting a fair argument that the Project will have a significant effect on the environment.
- (b) That the Mitigated Negative Declaration and Mitigation Monitoring Plan have been completed in compliance with CEQA and are consistent the City's Rules and Procedures for the Implementation of the California Environmental Quality Act.
- (c) That the Mitigated Negative Declaration and Mitigation Monitoring Plan represent the independent judgment and analysis of the City as lead agency for the proposed Project.
- (d) That the Mitigated Negative Declaration and Mitigation Monitoring Plan are adequate to serve as the required CEQA environmental documentation for the proposed Project.

Section 4. Adoption

That based on the foregoing Recitals, Administrative Record and Findings, the City Council hereby recommends that the City Council adopt the Mitigated Negative Declaration attached hereto as Exhibit C and the Mitigation Monitoring Plan attached hereto as Exhibit D.

Section 5. Severability

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Resolution be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this Resolution as hereby adopted shall remain in full force and effect.

Section 6. Effective Date

That this Resolution shall take effect immediately upon the date of adoption.

Section 7. Certification

That the City Clerk for the City Council shall certify to the passage of this Resolution.

PASSED AND ADOPTED THIS _____ day of _____, 2020.

CITY OF MORENO VALLEY

CITY COUNCIL

Dr. Yxstian A. Gutierrez
Mayor of the City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Exhibits:

- Exhibit A: Initial Study
- Exhibit B: Notice of Intent to Adopt a Mitigated Negative Declaration
- Exhibit C: Mitigated Negative Declaration
- Exhibit D: Monitoring Mitigation Plan

Attachment: Resolution No. 2020- Initial Study MND [Revision 4] (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Exhibit A
INITIAL STUDY

Exhibit B

NOTICE OF INTENT TO ADOPT A MITIGATED NEGATIVE DECLARATION

Exhibit C

MITIGATED NEGATIVE DECLARATION

Exhibit D
MITIGATION MONITORING PLAN



CITY OF MORENO VALLEY

DRAFT INITIAL STUDY AND MITIGATED NEGATIVE DECLARATION FOR PERRIS AND DRACAEA COMMERCIAL RETAIL



Perris and Dracaea Commercial Retail (City Case No. PPA18-0018)

October 19, 2020

Lead Agency
CITY OF MORENO VALLEY
 14177 Frederick Street
 Moreno Valley, California 92552

Prepared By
LSA ASSOCIATES, INC.
 1500 Iowa Avenue, Suite 200
 Riverside, California 92507
 (951) 781-9310

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

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MITIGATION MONITORING AND REPORTING PROGRAM (Separate Document)

APPENDICES (Separate Documents)

Appendix A: Air Quality and Greenhouse Gas Analysis for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California, LSA, May 4, 2020.

Appendix B: Western Riverside County Multiple Species Habitat Conservation Plan Consistency Analysis and Biology Report, Commercial Retail Project at Perris

Boulevard/Dracaea Avenue, City of Moreno Valley, Riverside County, California, LSA, September 2020.

Appendix C: Cultural Resources Assessment, Perris Boulevard and Dracaea Avenue Commercial Retail Project, City of Moreno Valley, Riverside County, California, LSA, August 2020.

Appendix D: Noise and Vibration Impact Analysis Memorandum for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California, LSA, September 2020.

Appendix E: Transportation Impact Analysis, Perris/Dracaea Commercial Project, City of Moreno Valley, Riverside County, California, LSA, September 2020.

MITIGATED NEGATIVE DECLARATION

Project Name: Perris and Dracaea Commercial Retail

Project Location: The project is located in Township 3S, Range 3W, Section 8 of the Sunnymead, CA United States Geological Survey 7.5-minute quadrangle map of the San Bernardino Base Meridian. The project site is located on the northeast corner of Perris Boulevard and Dracaea Avenue in the City of Moreno Valley (City), Riverside County. The project site is currently vacant. The site consists of four parcels, Assessor's Parcel Number (APN) 479-120-027, 029, 042, and 043. The parcels are approximately 4,000 feet south of State Route 60 (SR-60) and Interstate 215 (I-215) is approximately 3.5 miles west of the project site (Figure 1 depicts the regional and project location and Figure 2 depicts the existing setting). The March Air Reserve Base is located approximately 2.75 miles southwest of the project site.

Findings: It is hereby determined that, based on the information contained in the attached Initial Study, the project would not have a significant adverse effect on the environment.

Mitigation measures necessary to avoid the potentially significant effects on the environment are included in the attached Initial Study, which is hereby incorporated and fully made part of this Mitigated Negative Declaration. The City of Moreno Valley has hereby agreed to implement each of the identified mitigation measures, which would be adopted as part of the attached Mitigation Monitoring and Reporting Program.



INITIAL STUDY (IS) FOR PERRIS AND DRACAEA COMMERCIAL RETAIL

BACKGROUND INFORMATION AND PROJECT DESCRIPTION:

1. **Project Case Number(s):** PPA18-0018
2. **Project Title:** Perris and Dracaea Commercial Retail
3. **Public Comment Period:**
4. **Lead Agency:** City of Moreno Valley
Julia Descoteaux, Planning Department
14177 Frederick Street
Moreno Valley, California 92552
(951) 413-3209
juliad@moval.org
5. **Documents Posted At:** Moreno Valley City Hall between 7:30 AM and 5:30 PM
Monday through Thursday. Contact Julia Descoteaux,
Planning Department
14177 Frederick Street
Moreno Valley, California 92552
(951) 413-3209
juliad@moval.org
6. **Prepared By:** Dionisios Glentis, Senior Environmental Planner
LSA ASSOCIATES, INC.
1500 Iowa Avenue, Suite 200
Riverside, California 92507
(951) 781-9310
dionisios.glentis@lsa.net
7. **Project Sponsor:**

<p>Applicant/Developer Joey Ly, TAIT & Associates, Inc., for Cadence Capital Investments, LLC 6400 Fiddlers Green Circle, Suite 1820 Green Village, Colorado 80111 714-560-8673 jly@tait.com</p>	<p>Property Owner John David Monjazi and Jonathan J. Monjazi P.O. Box 4541 Oceanside, CA 92052 jmonjazi@gmail.com and johnmonjazi@gmail.com</p>
--	---
8. **Project Location:** The project is located in Township 3S, Range 3W, Section 8 of the Sunnymead, CA United States Geological Survey 7.5-minute quadrangle map of the San Bernardino Base Meridian. The project site is located on the northeast corner of Perris Boulevard and Dracaea Avenue in the City of Moreno Valley (City),

Riverside County. The project site is currently vacant. The site consists of four parcels, Assessor’s Parcel Number (APN) 479-120-027, 029, 042, and 043. The parcels are approximately 4,000 feet south of State Route 60 (SR-60) and Interstate 215 (I-215) is approximately 3.5 miles west of the project site (Figure 1 depicts the regional and project location and Figure 2 depicts the existing setting). The March Air Reserve Base is located approximately 2.75 miles southwest of the project site.

9. **General Plan Designation:** Existing: Residential/Office; Proposed: Commercial

10. **Specific Plan Name and Designation:** Not applicable.

11. **Zoning:** Existing: Office Commercial (OC) District: The primary purpose of the office commercial (OC) district is to provide for the establishment of business, corporate and administrative office, as well as commercial services which are supportive to major business developments. Retail facilities which support the office developments are permitted, subject to limitations specified in this section.

Proposed: Community Commercial (CC) District: The primary purpose of the community commercial (CC) district is to provide for the general shopping needs of area residents and workers with a variety of business, retail, personal and related or similar services. (Ord. 590 § 2, 2001; Ord. 359, 1992).

The project includes an amendment to the General Plan land use designation from Residential/Office to Commercial and a zone change from Office Commercial (OC) District to Community Commercial (CC) District in order to facilitate the proposed development. The project site is located at the northeast corner of a major intersection (Perris Boulevard and Dracaea Avenue) in proximity to existing commercial uses at the northwest and southwest corners of this intersection. Due to the project site’s prominent location at the northeast corner of Perris Boulevard and Dracaea Avenue, the proposed development would incorporate seamlessly with the existing development pattern of the neighborhood, and the proposed service station, convenience store, and drive-through restaurant would serve primarily the residential uses in the immediate vicinity of the site and provide them with the convenience of availing essential amenities close to home.

12. **Surrounding Land Uses and Setting:**

	Land Use	General Plan	Zoning
Project Site	Vacant	Existing: Residential/Office; Proposed: Commercial	Existing: Office Commercial (OC) District; Proposed: Community Commercial (CC) District
North	single-family residential uses	Residential/Office	Office (O) District
South	single-family residential uses and vacant property	Residential/Office	Office (O) District
East	single-family residential uses	Residential: Max.15 du/ac	Multi-family (R15) District
West	single-family residential and commercial uses	Commercial	Community Commercial (CC) District

13. Description of the Site and Project:

Environmental Setting

The project site is located at the northeast corner of Perris Boulevard and Dracaea Avenue and is substantially surrounded by commercial and residential uses. The site itself is vacant and contains ruderal vegetation and four ornamental trees. The property has been historically plowed or disked for weed abatement compliance. A residence constructed prior to 1930 was formerly located within the project boundaries. The residence was demolished prior to 2002, and only the concrete slab foundation, utility pole, and scattered refuse remain. Refer to Figure 2.

Project Description

The project proposes the construction of a drive-through restaurant with outdoor patio space, a convenience store, and service station on an approximate 2-acre site. Proposed on-site structures include a 4,088-square foot convenience store, a 36-foot by 86-foot canopy with 6 multiple product dispensers (MPD) for fueling up to 12 vehicles, two underground storage tanks located west of the canopy, and a 3,000 square foot drive through restaurant with a 400 square foot outdoor patio. The proposed project includes 18 auto parking stalls, 1 of which is designed in accordance with the Americans with Disabilities Act (ADA), for the convenience store and 32 auto parking stalls, 2 of which are ADA, 2 of which are electric vehicle, and 1 which is designated for clean air vehicles, for the drive through restaurant (total of 52 parking stalls throughout the site). The project also includes 2 trash enclosure facilities and bicycle parking located north of the canopy (see Figure 3, Site Plan). The proposed convenience store would be located on the south half of the site with the entrance facing west toward the 12 fueling stations. The proposed drive through restaurant could operate 24 hours per day and is proposed on the north half of the site with the entrance facing south. The project proposes a 6-foot decorative perimeter wall on the northern and eastern boundaries of the site. The site is generally level and is at an elevation of approximately 1,600 feet above mean sea level (amsl).

The City's General Plan Land Use Map designates the project site as Residential/Office and the Zoning as Office Commercial (OC) District, neither of which allow the proposed project's land uses. Therefore, the project would include a General Plan Amendment to change the land use from Residential/Office to Commercial and a Change of Zone from Office Commercial (OC) District to Community Commercial (CC) District.

Ornamental trees are proposed throughout the project site along the perimeter. The site includes other facilities such as two new transformers, a clean air separator (to control gasoline storage tank pressure), and an air/water dispenser (refer to Figure 3, Site Plan). Grading activities include 100 cubic yards (CY) of soils to be cut and 2,300 CY of soils to be fill for a net fill of 2,200 CY.

Ingress/egress to the project site is provided from three driveways; one from Perris Boulevard in the center of the site, one from Dracaea Avenue at the southern portion of the site, and one from Atwood Avenue approximately 320 feet north of the site. The internal drive aisle is designed to allow for semi-trucks delivering fuel to the site to enter from Perris Boulevard and exit the site to Dracaea Avenue. In addition, the

project proposes several Project Design Features (PDFs) that include select transportation facility improvements along the project frontage as detailed below:¹

- Remove the existing two-way-left-turn-lane on Perris Boulevard, from south of Pedro's Taco Shop Driveway to Dracaea Avenue;
- Replace the two-way-left-turn-lane with the raised median along the entire project frontage on Perris Boulevard extending north of the intersection of Perris Boulevard/Atwood Avenue up to south of the Pedro's Taco Shop driveway;
- Create left-turn pockets in the raised median for the northbound and southbound movements at the intersection of Perris Boulevard/Atwood Avenue, with storage lengths of 150 feet and 125 feet, respectively;
- Extend the southbound left-turn pocket at the intersection of Perris Boulevard/Dracaea Avenue by 5 feet; and
- Add a bus bay on the east side of Perris Boulevard, north of the intersection of Perris Boulevard/Auto Center Driveway-Project Driveway 1.

Due to the proposed raised median, Auto Center Driveway will operate as a right-in-right-out driveway, and eastbound and westbound left-turn and through movements will be prohibited at the intersection of Perris Boulevard/Atwood Avenue under with-project conditions.

14. **Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? If so, is there a plan for consultation that includes, for example, the determination of significance of impacts to tribal cultural resources, procedures regarding confidentiality, etc.?**

The City sent the required notifications to interested California Native American tribes pursuant to Assembly Bill (AB) 52 (specifically California Public Resources Code 21080.3.1) and Senate Bill (SB) 18 (specifically California Government Code 65352.4). Eight entities representing seven Native American tribes were notified in accordance with AB 52, while nine entities representing eight Native American tribes were notified in accordance with SB 18.

To date, three tribes, the Pechanga Band of Luiseño Indians (Pechanga), the Soboba Band of Luiseño Indians (Soboba), and the Rincon Band of Luiseño Indians (Rincon) have requested formal consultation with the City, while the Agua Caliente Band of Cahuilla Indians (Aqua Caliente) requested additional information on the project prior to determining if consultation is necessary. Two additional tribes, the Morongo Band of Mission Indians (Morongo) and the San Manuel Band of Mission Indians (San Manuel) informed the City they do not wish to consult on this project. Five entities representing three tribes, the Cahuilla Band of Indians, the Los Coyotes Band of Cahuilla Mission Indians, the Desert Cahuilla Indians/Torres Martinez, the

¹ LSA Associates, Inc. *Transportation Impact Analysis for the Perris/Dracaea Commercial Project*. Page 15 and Figures 4-3 and 4-4. September 2020. (Appendix E).

Santa Rosa Band of Mission Indians, and the Serrano Nation of Mission Indians did not respond to the City.

All of the tribal correspondences pursuant to AB 52 and SB 18 occurred October and November of 2019, and consultation is ongoing between the City and Pechanga, Soboba, and Rincon, while the City continues to engage the Aqua Caliente as appropriate.

NOTE: Conducting consultation early in the CEQA process allows tribal governments, lead agencies, and project proponents to discuss the level of environmental review, identify and address potential adverse impacts to tribal cultural resources, and reduce the potential for delay and conflict in the environmental review process. (See Public Resources Code section 21083.2.) Information may also be available from the California Native American Heritage Commission’s Sacred Lands File per Public Resources Code section 5097.96 and the California Historical Resources Information System administered by the California Office of Historic Preservation. Please also note that Public Resources Code section 21082.3 (c) contains provisions specific to confidentiality.

15. Other public agencies whose approval is required (e.g., permits, financing approval, or participation agreement):

- a. City of Moreno Valley: approval of General Plan Amendment, Change of Zone, and Tentative Parcel Map.
- b. City of Moreno Valley: approval of Grading and Building Permits.
- c. Santa Ana Regional Water Quality Control Board: National Pollutant Discharge Elimination System (NPDES) authorization.

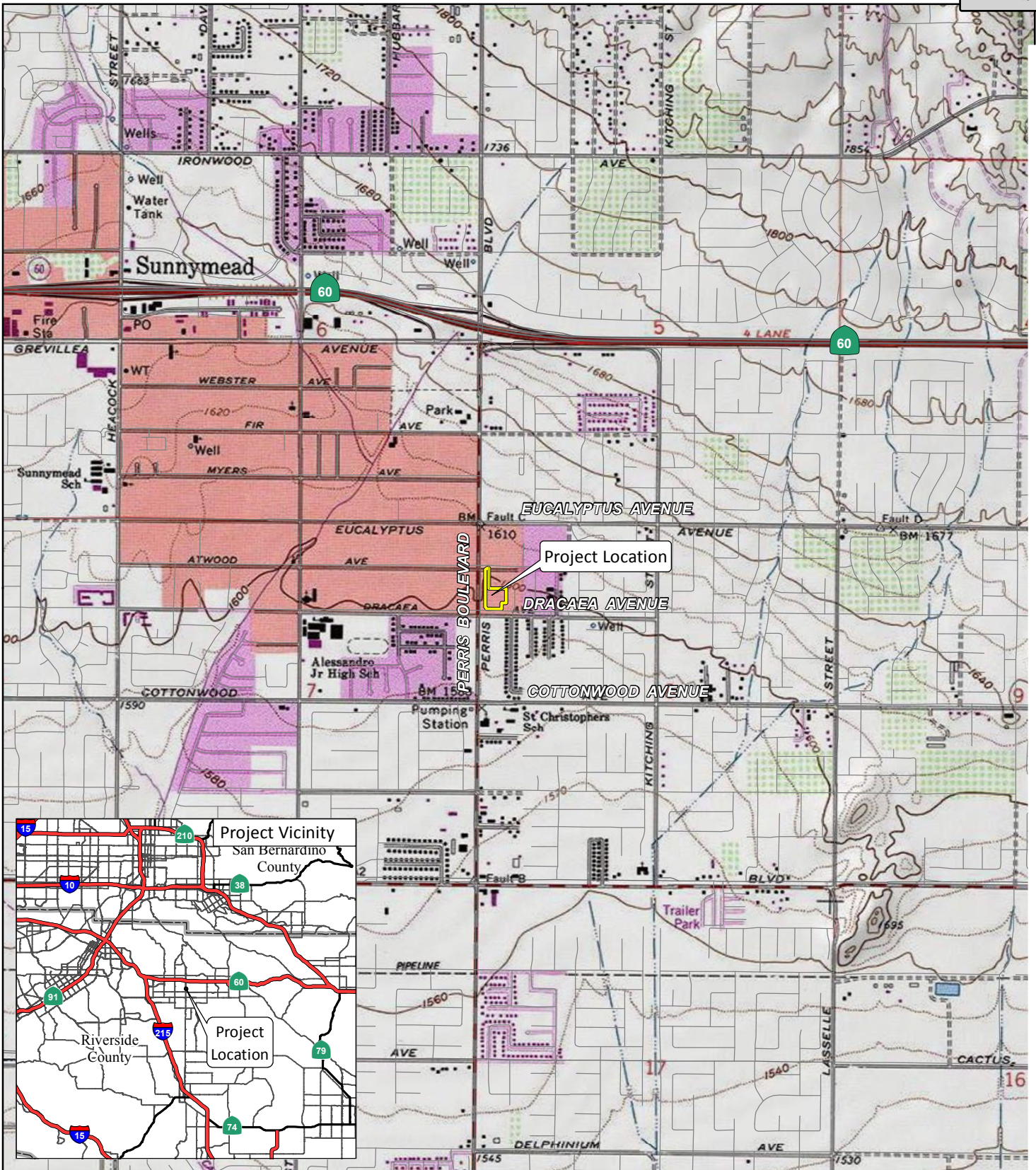
16. Other Technical Studies Referenced in this Initial Study (Provided as Appendices):

None

17. Acronyms:

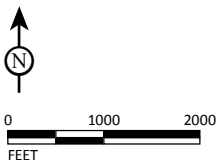
ADA	American with Disabilities Act
ALUC	Airport Land Use Commission
ALUCP	Airport Land Use Compatibility Plan
AQMP	Air Quality Management Plan
CEQA	California Environmental Quality Act
CIWMD	California Integrated Waste Management District
CMP	Congestion Management Plan
DTSC	Department of Toxic Substance Control
DWR	Department of Water Resources
EIR	Environmental Impact Report
EMWD	Eastern Municipal Water District
EOP	Emergency Operations Plan
FEMA	Federal Emergency Management Agency
FMMP	Farmland Mapping and Monitoring Program

GIS	Geographic Information System
GHG	Greenhouse Gas
GP	General Plan
HCM	Highway Capacity Manual
HOA	Home Owners' Association
IS	Initial Study
LHMP	Local Hazard Mitigation Plan
LOS	Level of Service
LST	Localized Significance Threshold
MARB	March Air Reserve Base
MARB/IPA-	March Air Reserve Base/Inland Port Airport
MSHCP	Multiple Species Habitat Conservation Plan
MVFP	Moreno Valley Fire Department
MVPD	Moreno Valley Police Department
MVUSD	Moreno Valley Unified School District
MWD	Metropolitan Water District
NCCP	Natural Communities Conservation Plan
NPDES	National Pollutant Discharge Elimination System
OEM	Office of Emergency Services
OPR	Office of Planning & Research, State
PEIR	Program Environmental Impact Report
PW	Public Works
RCEH	Riverside County Environmental Health
RCFCWCD	Riverside County Flood Control & Water Conservation District
RCP	Regional Comprehensive Plan
RCTC	Riverside County Transportation Commission
RCWMD	Riverside County Waste Management District
RTA	Riverside Transit Agency
RTIP	Regional Transportation Improvement Plan
RTP	Regional Transportation Plan
SAWPA	Santa Ana Watershed Project Authority
SCAG	Southern California Association of Governments
SCAQMD	South Coast Air Quality Management District
SCE	Southern California Edison
SCH	State Clearinghouse
SKRHCP	Stephens' Kangaroo Rat Habitat Conservation Plan
SWPPP	Storm Water Pollution Prevention Plan
SWRCB	State Water Resources Control Board
USFWS	United States Fish and Wildlife
USGS	United States Geologic Survey
VMT	Vehicle Miles Traveled
VVUSD	Valley Verde Unified School District
WQMP	Water Quality Management Plan
WRCOG	Western Riverside Council of Government



LSA

FIGURE :



SOURCE: USGS 7.5' Quad - Sunnymead (1980); ESRI Streetmap, 2013.

I:\CAQ1901\Reports\Cultural\fig1_RegLoc.mxd (4/22/2019)

Perris Boulevard and Dracaea Avenue Commercial Retail Regional and Project Location



LSA

LEGEND

Project Site



0 75 150
FEET

SOURCE: Google (2019)

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FIGURE :

Perris Boulevard and Dracaea Avenue Commercial Retail
Existing Setting

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

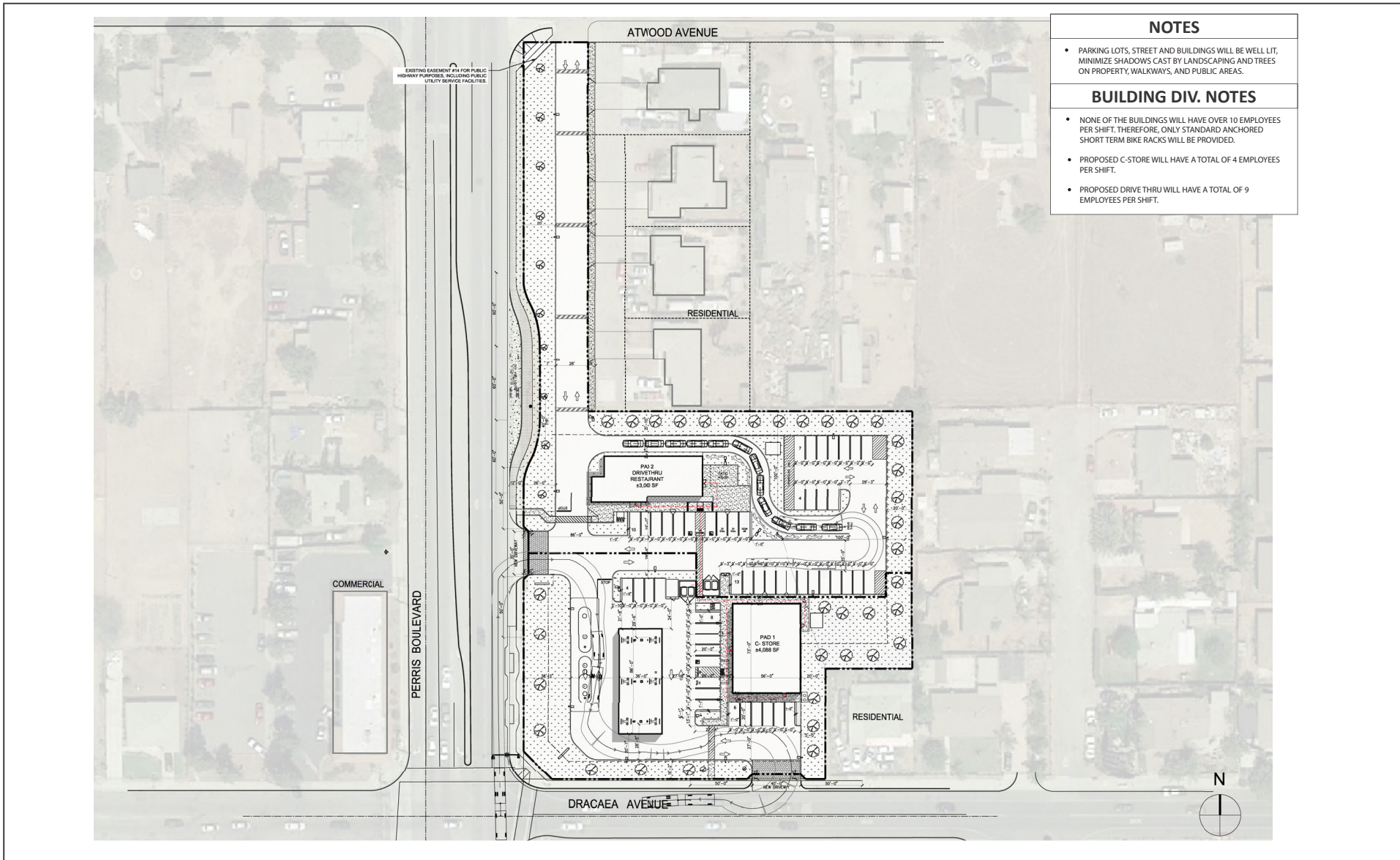
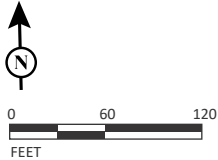


FIGURE 3

LSA



SOURCE: Cadence Capital Investments LLC; June 2020

I:\CAQ1901\G\IS\Site Plan.cdr (9/11/2020)

Perris Boulevard and Dracaea Avenue Commercial Retail
Conceptual Site Plan

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

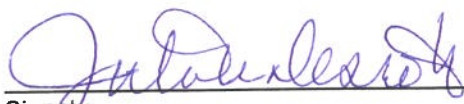
The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | | | | |
|--------------------------|-----------------------------|--------------------------|----------------------------------|--------------------------|------------------------------------|
| <input type="checkbox"/> | Aesthetics | <input type="checkbox"/> | Agriculture & Forestry Resources | <input type="checkbox"/> | Air Quality |
| <input type="checkbox"/> | Biological Resources | <input type="checkbox"/> | Cultural Resources | <input type="checkbox"/> | Energy |
| <input type="checkbox"/> | Geology & Soils | <input type="checkbox"/> | Greenhouse Gas Emissions | <input type="checkbox"/> | Hazards & Hazardous Materials |
| <input type="checkbox"/> | Hydrology & Water Quality | <input type="checkbox"/> | Land Use & Planning | <input type="checkbox"/> | Mineral Resources |
| <input type="checkbox"/> | Noise | <input type="checkbox"/> | Population & Housing | <input type="checkbox"/> | Public Services |
| <input type="checkbox"/> | Recreation | <input type="checkbox"/> | Transportation | <input type="checkbox"/> | Tribal Cultural Resources |
| <input type="checkbox"/> | Utilities & Service Systems | <input type="checkbox"/> | Wildfire | <input type="checkbox"/> | Mandatory Findings of Significance |

DETERMINATION (To be completed by the Lead Agency):

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



 Signature
 Julia Descoteaux

 Printed Name

10/19/2020

 Date
 City of Moreno Valley

 For

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

EVALUATION OF ENVIRONMENTAL IMPACTS:

- 1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a Lead Agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- 2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3) Once the Lead Agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4) "Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The Lead Agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section XVII, "Earlier Analyses," may be cross-referenced).
- 5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or another CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analyses Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where

appropriate, include a reference to the page or pages where the statement is substantiated.

- 7) Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.
- 8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
- 9) The explanation of each issue should identify:
 - a) the significance criteria or threshold, if any, used to evaluate each question; and
 - b) the mitigation measure identified, if any, to reduce the impact to less than significance.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
I. AESTHETICS – Except as provided in Public Resources Code §21099 – Modernization of Transportation Analysis for Transit-Oriented Infill Projects – Would the project:				
a) Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> Scenic vistas are publicly accessible viewpoints that provide views of areas from the project site and only the project site that exemplify a community’s environment. Scenic vistas within the City include Box Springs Mountains and Reche Canyon area to the north, the “Badlands” to the east, and the Mount Russel area to the south.² The only scenic vista visible from the project site is the view of the Box Springs Mountains to the north. Viewer sensitivity to a project varies depending on familiarity with existing views, the sense of ownership of these views, and the activities viewers perform in relationship to the views. Sensitive visual receptors in the project area include viewers from residential areas as residential viewers develop a sense of ownership, belonging, and familiarity with the existing visual setting. Viewers from the commercial uses in the project area are considered to be less sensitive to visual change as they are only present during business hours, are generally preoccupied with business operations, and have limited views from a lack of windows facing scenic vistas.</p> <p>The project site is located on the northeast corner of Perris Boulevard and Dracaea Avenue. Surrounding land uses include single-family residential uses adjacent to the north and east, single-family residential and commercial uses to the west across Perris Boulevard, and single-family residential uses and vacant property to the south across Dracaea Avenue (Figure 2). Commercial uses including a service station and convenience store are located across Perris Boulevard to the west and southwest, and single-family residences and commercial uses are located to the northwest. Wood fencing and block walls exist between the single-family residential homes located to the east and north of the project site, blocking scenic vistas on the first floor of the single-family residential homes as they are at the same elevation as the project site. Nearby structures with two stories would not be affected by the project, as the maximum height of structures on the project site is 30 feet. Views of the Box Spring Mountains to the north of the project site may be intermittently blocked while traveling along Dracaea Avenue due to the placement of ornamental trees in the landscaped areas north of the street and the proposed new structures on the project site. While the development of the proposed project would potentially block some views of Box Spring Mountains to the north of the site, it would not be considered a substantial adverse effect because the views would only be blocked intermittently. Overall, the development of the proposed project would have a less than significant impact on scenic vistas due to its limited size and height, and no mitigation is required.</p>				
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The project site is currently vacant. Two City-designated scenic highways are located within the City which includes State Route 60 (SR-60) and Moreno Beach Drive. State Route 60 is located approximately 0.75 mile north of the project site while Moreno Beach Drive is located approximately 3 miles east of the project site. Although SR-60 is proximate to the project site, it is not an officially designated State scenic highway.³ The project site contains a few scattered trees, none of which are considered to be scenic resources. No rock outcroppings or historic buildings are present on the site. Development of the project would not result in damage to any scenic resources. Therefore, the development of the Project will have a less than significant impact related to scenic resources within a state scenic highway, and no mitigation is required.</p>				
c) In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

² City of Moreno Valley. *City of Moreno Valley General Plan*. Chapter 7 – Conservation. July 11, 2006.

³ California Scenic Highway Mapping System. 2011. “Riverside County.” http://www.dot.ca.gov/hq/LandArch/16_livability/scenic_highways/. (Accessed June 2020).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?</p>				
<p>Response: <i>Less than Significant Impact.</i> The construction phase of the project would introduce the use of construction equipment such as excavators, graders, bulldozers, etc. The presence of the construction equipment, as well as the construction activities would temporarily alter the scenic quality of the project site. Construction staging areas, including earth stockpiling, storage of equipment and supplies, and related activities would contribute to a change in scenery at the project site, which would be a short-term visual impact. Because construction activities would be temporary, scenic quality impacts during construction would be less than significant. No mitigation is required.</p> <p>The proposed project features a variety of architectural elements including façade accents and provides variation in wall planes at the proposed restaurant that serve to avoid a monotonous appearance of the building. This variation would create shadow lines at various parts of the day. It is anticipated that the buildings would utilize a combination of colors and/or materials to establish a mix of textural elements while maintaining visual interest.</p> <p>The proposed landscaping would replace the vacant weedy field with a consistent and integrated vegetation palette. Landscaping on site would be provided in accordance with the City's Municipal Code Chapter 19.17.030 (Landscape and Water Efficiency Requirements), which requires the installation of landscaping on-site. The project would comply with these requirements by using landscaping throughout the site and visible to the public from the street. The project will also incorporate special pavements (accent colors, textures, and patterns) to indicate building entrances and pedestrian pathways.</p> <p>Although the visual characteristic of the project site would change, the proposed project would replace the existing vacant parcels with an attractive, well-designed development using varied architectural elements and massing, landscaping, and color combinations. In addition, the project would be designed and constructed per applicable City Municipal Code and General Plan standards. Therefore, no demonstrable negative aesthetic effect to the existing visual character or quality of the project site or surroundings is anticipated to result. Impacts would be less than significant, and mitigation is not required.</p>				
<p>d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The project site presently does not contain any source of light. Sources of light in the area include street lighting and signal lighting along Perris Boulevard and Dracaea Avenue, as well as from the adjacent residential and commercial uses surrounding the project site. Glare can be both a daytime occurrence resulting from light reflecting off reflective surfaces affecting viewers and a nighttime occurrence from light from headlights on nearby moving vehicles and traffic lights.</p> <p>At night, the project's interior and exterior building lights and landscape lighting would be visible from the adjacent residential uses, and to a lesser extent, from the surrounding public streets. However, these light sources would not have a significant impact on the night sky, as they would not exceed existing background light levels already present within the surrounding area. In addition, new construction shall comply with the City of Moreno Valley's General Plan and Municipal Code requirements for lighting (Chapter 9.08.100 Lighting). Adherence to the City's Municipal Code requirements would ensure that project-related lighting impacts would be less than significant. No mitigation is required.</p> <p>Sources of glare as a result of project implementation include reflective building materials and vehicles parked within and traveling to and from the project site. The amount of glare would depend on the location of the reflective surfaces and the direction of the sun. Any glare produced by the reflective surfaces would be temporary, as the location of the sun would be changing throughout the day. Additionally, the City's Design Review process includes consideration of material composition and colors to reduce potential for substantial glare from the proposed on-site structures. Therefore, impacts from glare would be less than significant. No mitigation is required.</p>				
<p>Sources:</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 2 – Community Development Element – Section 2.3 – Community Design • Chapter 7 – Conservation Element – Section 7.8 – Scenic Resources <ul style="list-style-type: none"> - Figure 7-2 – Major Scenic Resources 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.11 – Aesthetics <ul style="list-style-type: none"> - Figure 5.11-1 – Major Scenic Resources 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code <ul style="list-style-type: none"> • Section 9.10.110 – Light and Glare of the Moreno Valley Municipal Code. • Chapter 9.16 – Design Guidelines • Section 9.17.030 G – Heritage Trees 				
<p>II. AGRICULTURE AND FOREST RESOURCES – In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state’s inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board.</p> <p>Would the project:</p>				
<p>a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> The California Department of Conservation, Farmland Mapping and Monitoring Program (FMMP) compile important farmland maps pursuant to the provisions of Section 65570 of the California Government Code. The maps are updated every two years using a computer mapping system, aerial imagery, public review, and field reconnaissance. According to the FMMP, the project site lies within “Urban and Built Up Land.”⁴ The project site and surrounding area contain no designated Farmland. Therefore, no Prime, Unique, or Statewide Importance Farmland would be converted, and no impacts would occur. No mitigation is required.</p>				
<p>b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> The California Land Conservation Act of 1965 – or commonly known as the Williamson Act, enables local governments to enter into contracts with private landowners for the purpose of restricting specific parcels of land to agricultural or related open space uses. In return, landowners are given a lower property tax assessment. The project site does not contain land that is enrolled in a Williamson Act contract.⁵ According to the City of Moreno Valley’s Zoning Map, the project site is currently zoned for “Office Commercial (OC).” Because the project site not part of a Williamson Act contract, nor is the site zoned for agricultural uses, no impact associated with this issue would occur. No mitigation is required.</p>				
<p>c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

⁴ California Department of Conservation, Farmland Mapping and Monitoring Program. *Riverside County Important Farmland* 2016. <https://www.conservation.ca.gov/dlrp/fmmp/Pages/Riverside.aspx> (Accessed June 4, 2019).
⁵ California Department of Conservation, Division of Land Resource Protection. *Riverside County Williamson Act FY 2015/2016, Sheet 1 of 3*. ftp://ftp.consrv.ca.gov/pub/dlrp/wa/Riverside_w_15_16_WA.pdf. (Accessed June 4, 2019).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Production (as defined by Government Code section 51104(g))?				
<p>Response: <i>No Impact.</i> The project site is currently vacant and undeveloped. The site does not contain any forest land or timberland, nor is it zoned for such uses. Therefore, the project will have no impact on forest land, timberland, or timberland zoned Timberland Production. No mitigation is required.</p>				
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> Please refer to Checklist Response 2.c.</p>				
e) Involve other changes in the existing environment which, due to their location or nature, could result in the conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> As noted above, the project site is currently vacant and is not utilized for agricultural production or timberland production. Neither the project site nor adjacent areas are being used for or zoned for farmland or forest land. Therefore, the development of the proposed project will not result in the conversion of farmland to non-agricultural use or forest land to non-forest uses. No impact would occur. No mitigation is required.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 7 – Conservation Element – Section 7.7 – Agricultural Resources 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.8 – Agricultural Resources <ul style="list-style-type: none"> - Figure 5.8-1 – Important Farmlands 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 				
<p>III. AIR QUALITY – Where available, the significance criteria established by the applicable air quality management district or air pollution control district may be relied upon to make the following determinations. Would the project:</p>				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The project site is located in the South Coast Air Basin (Basin), which is under the jurisdiction of the South Coast Air Quality Management District (SCAQMD). The Basin includes all of Orange County and portions of Los Angeles, Riverside, and San Bernardino Counties. The SCAQMD adopted an Air Quality Management Plan (AQMP), the main purpose of which is to describe air pollution control strategies to be taken by a city, county, or region classified as a nonattainment area in order to bring the area into compliance with federal and state air quality standards. A nonattainment area is considered to have air quality worse than the National Ambient Air Quality Standards (NAAQS) as defined in the Federal Clean Air Act. The Basin is in nonattainment for the federal and state standards for ozone (O₃) and particulate matter less than 2.5 microns in diameter (PM_{2.5}) and in nonattainment for the state standards for particulate matter less than 10 microns in diameter (PM₁₀) and nitrogen dioxide (NO₂). The Basin is in attainment/maintenance/unclassified status for all other federal and state criteria pollutant standards.</p> <p>Consistency with the 2016 AQMP for the Basin means that a project will be consistent with the goals, objectives, and assumptions in the respective plan to achieve the federal and state air quality standards. Pursuant to the methodology provided in Chapter 12 of the 1993 SCAQMD CEQA Air Quality Handbook, consistency with the Basin 2016 AQMP is affirmed when a project (1) does not increase the frequency or severity of an air quality standards violation or cause a new violation; and (2) is consistent with the growth assumptions in the AQMP. For the proposed project to be consistent with the AQMP adopted by the SCAQMD, the pollutants emitted from the project should not exceed the SCAQMD daily threshold or</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>cause a significant impact on air quality, or the project must already have been included in the AQMP projections. Additionally, if feasible mitigation measures are implemented and shown to reduce the impact level from significant to less than significant, a project may be deemed consistent with the AQMP.</p> <p>The proposed project would construct a 3,000-square foot drive through restaurant and a 4,088-square foot convenience store with 12 fuel pumps. The project site is designated as a Residential/Office land use in the City's General Plan Land Use Map as Office Commercial and is zoned as Office Commercial (OC) District. The proposed land uses are not allowed under the existing zoning designation of the site; therefore, the project includes a General Plan Amendment to designate the project site Commercial and a Change of Zone to Community Commercial (CC) District.</p> <p>The project Applicant indicates the project is expected to generate up to 39 employees; up to 27 for the restaurant and up to 12 for the convenience store/service station.⁶ When compared to the planned use of the site for residential/office uses, the proposed changes in the land use designation would not substantially alter the housing and employment forecast in the City already included in the AQMP assumptions. As such, the proposed project is not anticipated to exceed the AQMP assumptions for the project site. In addition, as discussed below in Checklist Response 3.b, construction and operation of the project would not generate criteria air pollutants that would exceed SCAQMD thresholds of significance. Based on the consistency analysis presented above, the proposed project would not conflict with or obstruct implementation of the 2016 AQMP. Impacts would be less than significant, and mitigation is not required.</p>				
<p>b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The following analysis analyzes both short-term impacts caused by construction activities and long-term impacts caused by occupancy and operation of the project as proposed.⁷</p> <p>Short-Term Impacts. During construction, short-term degradation of air quality may occur due to the release of particulate emissions generated by grading, paving, building, and other activities. Emissions from construction equipment are also anticipated and would include CO, NOx, ROG, directly-emitted particulate matter (PM_{2.5} and PM₁₀), and Toxic Air Contaminants (TACs) such as diesel exhaust particulate matter (DPM).</p> <p>Construction-related effects on air quality from the proposed project would be greatest during the site preparation phase due to the disturbance of soils. If not properly controlled, these activities would temporarily generate particulate emissions. Sources of fugitive dust would include disturbed soils at the construction site. Unless properly controlled, vehicles leaving the site would deposit dirt and mud on local streets, which could be an additional source of airborne dust after it dries. PM₁₀ emissions would vary from day to day, depending on the nature and magnitude of construction activity and local weather conditions. PM₁₀ emissions would depend on soil moisture, silt content of soil, wind speed, and the amount of operating equipment. Larger dust particles would settle near the source, while fine particles would be dispersed over greater distances from the construction site.</p> <p>Water or other soil stabilizers can be used to control dust, resulting in emission reductions of 50 percent or more. The SCAQMD has established Rule 403: Fugitive Dust, which would require the applicant to implement measures that would reduce the amount of particulate matter generated during the construction period.</p> <p>In addition to dust-related PM₁₀ emissions, heavy trucks and construction equipment powered by gasoline and diesel engines would generate CO, SO₂, NOx, VOCs and some soot particulate (PM_{2.5} and</p>				

⁶ The restaurant and convenience store/gas station are assumed to operate 24 hours per day, so the 39 employees would be divided in to three 8-hour shifts, with approximately 13 employees on site at a time (9 at the restaurant and 4 at the convenience store).

⁷ LSA Associates, Inc. *Air Quality and Greenhouse Gas Analysis for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California.* May 4, 2020. (Appendix A).

ISSUES & SUPPORTING INFORMATION SOURCES:

Potentially Significant Impact

Less Than Significant with Mitigation Incorporated

Less Than Significant Impact

No Impact

PM₁₀) in exhaust emissions. If construction activities were to increase traffic congestion in the area, CO and other emissions from traffic would increase slightly while those vehicles idle in traffic. These emissions would be temporary in nature and limited to the immediate area surrounding the construction site.

Construction emissions were estimated for the project using the California Emissions Estimator Model (CalEEMod) version 2016.3.2, consistent with SCAQMD recommendations. For purposes of this analysis, the construction schedule was assumed to be approximately six months. The proposed project will be constructed modular portable buildings and the majority of the modular building manufacturing and interior painting will be completed off site. Other precise details of construction activities are unknown at this time; therefore, default assumptions (e.g., construction duration and fleet activities) from CalEEMod were assumed. As shown in Table 3.A, construction emissions would not exceed daily SCAQMD thresholds, so impacts are **less than significant** and mitigation is not required (see Appendix A).

Table 3.A: Estimated Construction Emissions

Construction Phase	Peak Daily Pollutant Emissions (lbs/day)					
	VOC	NOx	CO	SOx	PM ₁₀	PM _{2.5}
Maximum Daily Emissions	9.98	24.74	16.87	0.05	2.37	1.12
SCAQMD Thresholds	75.00	100.00	550.00	150.00	150.00	55.00
Significant Emissions?	No	No	No	No	No	No

Source: Table A, LSA, May 2020 (Appendix A).

CO = carbon monoxide
 lbs/day = pounds per day
 NOx = nitrogen oxides
 PM_{2.5} = particulate matter less than 2.5 microns in size

PM₁₀ = particulate matter less than 10 microns in size
 SCAQMD = South Coast Air Quality Management District
 SOx = sulfur oxides
 VOC = volatile organic compounds

Long-Term Impacts. Long-term air pollutant emission impacts are those associated with area sources and mobile sources related to the proposed project. In addition to the short-term construction emissions, the proposed project would also generate long-term air pollutant emissions, such as those associated with changes in permanent use of the project site. These long-term emissions are primarily mobile source emissions that would result from vehicle trips associated with the proposed project. Area sources, such as natural gas heaters, landscape equipment, and use of consumer products, would also result in pollutant emissions.

PM₁₀ emissions result from running exhaust, tire and brake wear, and the entrainment of dust into the atmosphere from vehicles traveling on paved roadways. Entrainment of PM₁₀ occurs when vehicle tires pulverize small rocks and pavement and the vehicle wakes generate airborne dust. The contribution of tire and brake wear is small compared to the other PM emission processes. Gasoline-powered engines have small rates of particulate matter emissions compared with diesel-powered vehicles.

Energy source emissions result from activities in buildings for which electricity and natural gas are used. The quantity of emissions is the product of usage intensity (i.e., the amount of electricity or natural gas) and the emission factor of the fuel source. Major sources of energy demand include building mechanical systems, such as heating and air conditioning, lighting, and plug-in electronics, such as refrigerators or computers. Greater building or appliance efficiency reduces the amount of energy for a given activity and thus lowers the resultant emissions. The emission factor is determined by the fuel source, with cleaner energy sources, like renewable energy, producing fewer emissions than conventional sources. Area source emissions associated with the project would include emissions from water heating and the use of landscaping equipment.

Emission estimates for operation of the project were calculated using CalEEMod and Table 3.B shows model results. Trip generation rates for the project were based on the project's trip generation estimates, as identified in Table 5-A of the Transportation Impact Analysis report.⁸ Based on the Transportation Impact Analysis report, the proposed project would generate approximately 4,925 average daily trips

⁸ LSA Associates, Inc. *Transportation Impact Analysis, Perris/Dracaea Commercial Project*. Table 5-A. September 2020. (Appendix E).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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(i.e., ADTs without internal capture and pass-by trips), with approximately 416 trips occurring in the a.m. peak hour and approximately 298 trips occurring in the p.m. peak hour.

The primary emissions associated with the project are regional in nature, meaning that air pollutants are rapidly dispersed on release or, in the case of vehicle emissions associated with the project; emissions are released in other areas of the Basin. Table 3.B identifies the daily emissions associated with project operational trip generation, energy, and area sources for ROG, NOx, CO, SOx, PM₁₀, and PM_{2.5}.

The results shown in Table 3.B indicate the project would not exceed the significance criteria for daily ROG, NOx, CO, SOx, PM₁₀, or PM_{2.5} emissions; therefore, the proposed project would have a **less than significant impact** on regional air quality and mitigation would not be required.

Table 3.B: Operational Emissions

Source	Pollutant Emissions (lbs/day)					
	ROG	NOx	CO	SOx	PM ₁₀	PM _{2.5}
Area Sources	0.16	<0.01	<0.01	0	<0.01	<0.01
Energy Sources	0.02	0.22	0.19	<0.01	0.02	0.02
Mobile Sources	6.03	36.81	36.38	0.16	9.67	2.64
Total Project Emissions	6.22	37.03	36.57	0.16	9.69	2.66
SCAQMD Thresholds	55.00	55.00	550.00	150.00	150.00	55.00
Significant Emissions?	No	No	No	No	No	No

Source: Table B, LSA, May 2020 (Appendix A).

- CO = carbon monoxide
- lbs/day = pounds per day
- NOx = nitrogen oxides
- PM_{2.5} = particulate matter less than 2.5 microns in size
- PM₁₀ = particulate matter less than 10 microns in size
- SCAQMD = South Coast Air Quality Management District
- SOx = sulfur oxides
- VOC = volatile organic compounds

The majority of the project-related operational emissions would be due to natural gas for heating and hot water and customer and employee vehicle trips to and from the project. Tables 3.A and 3.B indicate that all emissions of criteria pollutants from the proposed project would be less than the applicable SCAQMD thresholds over both the short and long term, therefore, no significant cumulative air quality impacts would occur and no mitigation is required.

c) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less than Significant Impact. SCAQMD published its *Final Localized Significance Threshold Methodology* in June 2003 and updated it in July 2008,⁹ recommending that all air quality analyses include an assessment of both construction and operational impacts on the air quality of nearby sensitive receptors. Localized significance thresholds (LSTs) represent the maximum emissions from a project site of up to 5 acres that are not expected to result in an exceedance of the National Ambient Air Quality Standards (NAAQS) or California Ambient Air Quality Standards (CAAQS) for CO, NO₂, PM₁₀ and PM_{2.5}. LSTs are based on the ambient concentrations of that pollutant within the project Source Receptor Area (SRA) and the distance to the nearest sensitive receptor. For this project, the appropriate SRA is the Perris Valley Area (SRA 24) according to the project air quality analysis included in Appendix A.

Short-Term LST Impacts. Construction is expected to occur in one phase, and the site is only 2.30 acres; therefore, less than 5 acres would be actively worked on during any given day. The closest sensitive receptors to the site are several existing residences surrounding the project site, the closest of which is are the single-family residences approximately 50 feet from the northern and eastern border of the project site. Table 3.C shows that emissions are well below LST thresholds and thus would be **less than significant**.

Table 3.C: Construction Localized Impacts Analysis

Emissions Sources	NOx	CO	PM ₁₀	PM _{2.5}
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⁹ South Coast Air Quality Management District. *Final Localized Significance Thresholds Methodology*. June 2003, Revised July 2008.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Onsite Emissions (lbs/day)	20.20	16.40	1.50	0.90
Local Significance Thresholds (lbs/day)	170.00	833.00	7.00	4.00
Significant Emissions?	No	No	No	No

Source: Table C, LSA, May 2020 (Appendix A).

Note: Source Receptor Area 24 – Perris Valley, 1 acre, 31 meter distance

CO = carbon monoxide
 lbs/day = pounds per day
 NOx = nitrogen oxides

PM_{2.5} = particulate matter less than 2.5 microns in size
 PM₁₀ = particulate matter less than 10 microns in size

Long-Term LST Analysis. The potential long-term daily air pollutant emissions from the proposed project operational activities were calculated and compared with the appropriate LSTs from the SCAQMD based on CalEEMod 2016.3.2 model data from the project air quality assessment (Appendix A). As shown in Table 3.D, the calculations determined that the operational emission rates would not exceed the LST thresholds for the closest sensitive receptors. Therefore, the proposed operational activity would result in a **less-than-significant impact** related to localized significant air quality. Mitigation is not required.

Table 3.D: Long-Term Operational Localized Impacts Analysis

Emissions Sources	NOx	CO	PM₁₀	PM_{2.5}
On-Site Emissions (lbs/day) ¹	1.80	1.80	0.50	0.10
Local Significance Thresholds (lbs/day)	170.00	833.00	2.00	1.00
Significant Emissions?	No	No	No	No

Source: Table D, LSA, May 2020 (Appendix A).

Note: Source Receptor Area 24 – Perris Valley, 1 acre, 31 meter distance.

¹ CalEEMod clearly delineates the onsite and offsite emissions and mobile source trips within the project area (i.e., driveways and parking lots).

CO = carbon monoxide
 NOx = nitrogen oxides

PM_{2.5} = particulate matter less than 2.5 microns in size
 PM₁₀ = particulate matter less than 10 microns in size

Cumulative Air Quality Impacts. In analyzing cumulative impacts from a proposed project, the analysis must specifically evaluate a project’s contribution to the cumulative increase in pollutants for which the Basin is listed as nonattainment for the State and federal ambient air quality standards. The proposed project would have a cumulatively considerable impact if project-generated emissions would exceed thresholds for NOx, VOC, PM₁₀, and/or PM_{2.5}. If the proposed project does not exceed thresholds and is determined to have less than significant project-specific impacts, it may still have a cumulatively considerable impact on air quality and GHG if the emissions from the project, in combination with emissions from other proposed or reasonably foreseeable future projects, are in excess of established thresholds. However, the proposed project would be considered to have a cumulative impact only if its contribution accounts for a significant portion of the cumulative total emissions.

The geographic extent for the analysis of cumulative impacts related to air quality includes the central area of the South Coast Air Basin. Due to the nonattainment status of the Basin, the primary air pollutants of concern would be NOx and VOCs, which are ozone precursors, and PM₁₀ and PM_{2.5}. Project-related NOx and VOCs are primarily emitted from motor vehicles and construction equipment, while PM₁₀ and PM_{2.5} are emitted primarily as fugitive dust during construction. Because of the nature of ozone as a regional air pollutant, emissions from the entire geographic area for this cumulative impact analysis would tend to be important, although maximum ozone impacts generally occur downwind of the area in which the ozone precursors are released. PM₁₀ and PM_{2.5} impacts, on the other hand, would tend to occur locally; thus, projects occurring in the same general area and in the same time period would tend to create cumulative air quality impacts.

The project would contribute criteria pollutants to the area during project construction. A number of individual projects in the area may be under construction simultaneously with the proposed project. Depending on construction schedules and actual implementation of projects in the area, generation of fugitive dust and pollutant emissions during construction could result in substantial short-term increases in air pollutants. However, each project would be required to comply with the SCAQMD’s standard construction measures. The proposed project’s short-term construction CO, NO₂, PM₁₀, and PM_{2.5} emissions would not exceed the LSTs. Therefore, construction of the proposed project would have a

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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less than significant impact with regard to regional and localized emissions, and impacts would not be cumulatively considerable.

d) Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:

Less Than Significant Impact. Project construction will generate limited odors over the short term, mainly fumes from gasoline- and diesel-powered construction equipment. These odors would be temporary and not likely to be noticeable beyond the project limits. The painting of buildings or the installation of concrete paving may also create temporary odors. SCAQMD Rule 1113 outlines standards for paint applications, while Rule 1108 identifies standards regarding the application of asphalt. Adherence to the standards identified in these SCAQMD Rules would reduce temporary odor impacts to a less than significant level, and no mitigation is required.

Land uses generally associated with long-term objectionable odors include agricultural uses, wastewater treatment plants, food processing plants, chemical plants, composting operations, refineries, landfills, dairies, and fiberglass molding facilities. The site plan currently shows the trash enclosure will be in the center portion of the site, on the west side of the convenience store. The proposed project is a service station and convenience store, and waste odors are not expected to result in significant odor impacts because waste storage is required to adhere to City waste storage requirements (i.e., covered outdoor storage containers that are regularly emptied).

SCAQMD Rule 402 regarding nuisances states: "A person shall not discharge from any source whatsoever such quantities of air contaminants or other material which cause injury, detriment, nuisance, or annoyance to any considerable number of persons or to the public, or which endanger the comfort, repose, health or safety of any such persons or the public, or which cause, or have a natural tendency to cause, injury or damage to business or property." Furthermore, SCAQMD Rule 461 - Gasoline Transfer and Dispensing, requires the installation of enhanced vapor recovery systems that would reduce the amount of vapor that would be emitted into the atmosphere by 95 to 98 percent from levels without such systems. Through the adherence of these existing requirements, the proposed project is not expected to generate long-term objectionable odors. Because the project would not involve any substantial short-term or long-term sources of strong negative odors, impacts are considered **less than significant**. Mitigation is not required.

Sources:

1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 5 – Circulation Element
 - Chapter 6 – Safety Element – Section 6.6 – Air Quality
2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.3 – Air Quality
 - Figure 5.3-1 – South Coast Air Basin
 - Appendix C – Air Quality Analysis, P&D Consultants, July 2003
3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code
 - Section 9.10.050 – Air Quality of the Moreno Valley Municipal Code
 - Section 9.10.150 – Odors of the Moreno Valley Municipal Code
 - Section 9.10.170 – Vibration of the Moreno Valley Municipal Code
4. Moreno Valley Municipal Code Section 12.50.040 – Limitations on Engine Idling

IV. BIOLOGICAL RESOURCES – Would the project:

a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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Response:

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p><i>Less Than Significant With Mitigation Incorporated.</i> A Western Riverside County Multiple Species Habitat Conservation Plan (MSHCP) Consistency Analysis and Biology Report was prepared for the project site in September 2020 (included as Appendix B).¹⁰ A general biological resources survey was conducted on April 12, 2019. The project site is mostly devoid of vegetation since the property has been historically plowed or disked for weed abatement compliance. There are several dead and live ornamental trees along the eastern perimeter. The project site was recently plowed; however, non-native grassland comprised of mouse barley (<i>Hordeum murinum</i>), shortpod mustard (<i>Hirschfeldia incana</i>), stork's bill (<i>Erodium</i> spp.), and red brome (<i>Bromus madritensis</i> ssp. <i>rubens</i>) was observed during the biological survey of the site. Historical aerial imagery of the project site revealed that the project site has remained mostly undeveloped since 1966. No special-status species were found or observed during the field reconnaissance.</p> <p>The project site lies within the MSHCP Plan Area; however, the project site is not located within the MSHCP Criteria Area, Public/Quasi-Public Lands, or conservation areas, nor is the site located within the MSHCP Narrow Endemic Plant Species Survey Area, burrowing owl (<i>Athene cunicularia hypugaea</i>) survey area,¹¹ or any other species survey area. As the project site has been recently plowed or disked for weed abatement, the site does not contain suitable habitat for any threatened or endangered species. Section 6.3.2 of the MSHCP prescribes survey needs for special-status species based on mapped surveys areas for: Criteria Area Plant Species, amphibians, and mammals; however, the project site does not fall within mapped survey areas for any of these special-status species. Species identified in MSHCP Table 9-3 are not considered adequately conserved under the MSHCP. Other species with limited coverage or with no take authorization under the MSHCP include Santa Rosa Plateau fairy shrimp (<i>Linderiella santarosae</i>), bald eagle (<i>Haliaeetus leucocephalus</i>), golden eagle (<i>Aquila chrysaetos</i>), peregrine falcon (<i>Falco peregrinus anatum</i>), and white-tailed kite (<i>Elanus leucurus</i>). No species identified in MSHCP Table 9-3 or the other species listed above are expected to occur on the project site due to the substantial disturbance of the site and a lack of suitable vegetation communities, soils, and hydrology.</p> <p>The project site does not include any suitable habitat for special-status species, and none were observed on the project site during the biological survey. Based on the analysis above, the project is consistent with the MSHCP.</p> <p>Nesting Birds. During the bird breeding season (typically February 1 through August 31), large trees on or adjacent to the project area may be used by hawks, ravens, or other large birds for nesting. Trees, shrubs, and other vegetation may provide nest sites for smaller birds. The trees present on the site can provide habitat for nesting birds that are protected by the Migratory Bird Treaty Act of 1918 (MBTA) (16 USC 703–711) and California Fish and Game Code Section 3503, 3053.5, and 3800. These laws regulate the take, possession, or destruction of the nest or eggs of any migratory bird or bird of prey. However, the USFWS has recently determined that the MBTA should apply only to "... affirmative actions that have as their purpose the taking or killing of migratory birds, their nests, or their eggs" and will not be applied to incidental take of migratory birds pursuant to otherwise lawful activities. To avoid potential effects to fully protected raptors, special-status bird species, and other nesting birds protected by the MBTA and California Fish and Game Code, and for compliance with MSHCP Incidental Take Permit Condition 5, Mitigation Measure BIO-1 is identified below to address potential impacts to nesting birds. With adherence to Mitigation Measure BIO-1, impacts would be reduced to less than significant with mitigation incorporated.</p> <p>Species Associated With Riparian/Riverine Areas And Vernal Pools. The definition of Riparian/Riverine habitats is based on potential for the habitat to support Riparian/Riverine Covered Species. The MSHCP species associated with Riparian/Riverine areas and Vernal Pools, as listed in Section 6.1.2 of the MSHCP, were assessed for the probability of occurring in and adjacent to the project site. No drainage features, ponded areas, or riparian habitat subject to jurisdiction by the CDFW, USACE, and/or the Regional Water Quality Control Board (RWQCB) were found within the project site during site surveys. A review of the <i>Sunnymead, California</i> U.S. Geological Survey (USGS) quadrangle and historic</p>				

¹⁰ LSA Associates, Inc. *Western Riverside County Multiple Species Habitat Conservation Plan Consistency Analysis and Biology Report, Commercial Retail Project at Perris Boulevard/Dracaea Avenue*. City of Moreno Valley. September 2020. (Appendix B).
¹¹ Although the project site is not within a burrowing owl survey area, a burrowing owl habitat assessment was conducted in accordance with guidelines (CDFW's 2012 *Staff Report on Burrowing Owl Mitigation and Burrowing Owl Survey Instructions for the Western Riverside Multiple Species Habitat Conservation Plan Area*, Riverside County Environmental Programs Department, March 29, 2006). The assessment concluded there is no suitable habitat for the burrowing owl.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>aerial photographs (NETROnline) did not reveal any previously mapped drainage features. There are no vernal pools or other ponded areas suitable for sensitive fairy shrimp species on the project site. Soils in the project area are well drained from repeated plowing and have been previously mapped by the Natural Resources Conservation Service (NRCS) as Ramona sandy loam, with 0 to 2 percent slopes. No hydrophytic vegetation occurs in the project area. The project site does not contain any suitable riparian vegetation or habitat for special-status riparian birds. No additional surveys or mitigation are required.</p> <p>Stephens' Kangaroo Rat. The project site is located within the Stephens' Kangaroo Rat Habitat Conservation Plan area and a fee payment will be required prior to issuance of a grading permit. The project will comply with the Habitat Conservation Plan for the Stephens' Kangaroo Rat in Western Riverside County (Riverside County Habitat Conservation Agency, February 1995) and fulfill its compensatory mitigation requirements under this plan through the payment of a fee.</p> <p>Mitigation Measure BIO-1: A nesting bird pre-construction survey will be conducted by a qualified biologist three days prior to ground-disturbing activities. Should nesting birds be found, an exclusionary buffer will be established by the qualified biologist. The buffer may be up to 500 feet in diameter depending on the species of nesting bird found. This buffer will be clearly marked in the field by construction personnel under guidance of the qualified biologist and construction or clearing will not be conducted within this zone until the qualified biologist determines that the young have fledged or the nest is no longer active. Nesting bird habitat within the project area will be resurveyed during bird breeding season if there is a lapse in construction activities longer than seven days.</p>				
<p>b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant Impact.</i> Fieldwork for a jurisdictional delineation was conducted on April 12, 2019. The site was surveyed by foot and was evaluated for areas of potential jurisdiction according to USACE, CDFW, and MSHCP criteria. Please refer to Checklist Response 4.a). No riparian or riverine habitats were identified within or adjacent to the project site. A less than significant impact related to this issue would occur, and no mitigation is required.</p>				
<p>c) Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> The USACE regulates discharges of dredged or fill material into waters of the United States (U.S.). These waters include wetlands and nonwetland bodies of water that meet specific criteria. This connection may be direct through a tributary system linking a stream channel with traditional navigable waters (TNW) used in interstate or foreign commerce or may be indirect through a nexus identified in the USACE regulations. In the past, an indirect nexus could potentially be established if isolated waters provided habitat for migratory birds, even in the absence of a surface connection to navigable water of the U.S. In order to be considered a jurisdictional wetland under Section 404, an area must possess three wetland characteristics: hydrophytic vegetation, hydric soils, and wetland hydrology. Each characteristic has a specific set of mandatory wetland criteria that must be satisfied in order for that particular wetland characteristic to be met.</p> <p>The CDFW, under Section 1602 of the California Fish and Game Code, regulates alterations to lakes, rivers, and streams (defined by the presence of a channel bed and banks, and at least an intermittent flow of water) where fish or wildlife resources may be adversely affected. The RWQCB is responsible for the administration of Section 401 of the Clean Water Act. Typically, the areas subject to jurisdiction of the RWQCB coincide with those of the USACE (i.e., waters of the U.S. including any wetlands). The</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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RWQCB can also assert authority over “waters of the State” under waste discharge requirements pursuant to the Porter-Cologne Water Quality Control Act. Please refer to Checklist Response 4.a. The project site is currently vacant and is currently surrounded by existing roadways, residential, and commercial uses. No federally protected wetlands, marshes, vernal pools, or coastal areas were identified within the project area. Therefore, **no impact** related to this issue would occur. No mitigation is required.

d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with an established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. Wildlife movement includes seasonal migration along corridors, as well as daily movements for foraging and reaching water sources. Migration corridors may include areas of unobstructed movement for deer, riparian corridors providing cover for migrating birds, routes between breeding waters and upland habitat for amphibians, and between roosting and feeding areas for birds. The project site is not adjacent to any existing or proposed linkage or core areas as identified in the MSHCP. The project will not affect wildlife movement since the property is surrounded by urban development and the project site does not serve as a wildlife movement corridor. A **less than significant** impact related to this issue would occur, and no mitigation is required.

e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. According to the City of Moreno Valley Municipal Code (as amended), Chapter 9.17, Landscape and Water Efficiency Requirements; Section 9.17.030, Landscape and Irrigation Standards; Part E, Trees, projects necessitating the removal of existing trees with four-inch or greater trunk diameters (calipers), shall be replaced at a three to one ratio, with minimum twenty-four (24) inch box size trees of the same species, or a minimum thirty-six (36) inch box for a one to one replacement, where approved. Additionally, Part G defines Heritage Trees as any tree that defines the historical and cultural character of the City, including older palm and olive trees, and/or any tree designated as such by official action; trees with a 15-inch diameter measured 24 inches above ground level; or trees that have reached a height of 15 feet or greater.

The biological assessment of the project included an inventory of all trees on site (Appendix B). The site contains four (4) trees, as detailed in Table 4.A, and several occurrences of tree of heaven (*Ailanthus altissima*).¹² Tree of heaven are not considered ornamental trees in California but are an invasive species classified by the state as noxious weeds that should be eradicated from the site.¹³

Table 4.A: Tree Inventory

Tree No.	Species ¹	Height (feet)	Caliper (inches) ²	Rating/ Transplantable	Notes
001	black locust (<i>Robinia pseudoacacia</i>)	24	8	Poor/No	Poor structure (codominant leaders, pleaching)
002	black locust (<i>Robinia pseudoacacia</i>)	18	9.5	Poor/No	Poor structure (codominant leaders, pleaching); unhealed old branch cuts
003	black locust (<i>Robinia pseudoacacia</i>)	15	6	Poor/No	Poor structure (codominant leaders, pleaching)

¹² LSA Associates, Inc. *Western Riverside County Multiple Species Habitat Conservation Plan Consistency Analysis and Biology Report, Commercial Retail Project at Perris Boulevard/Draacea Avenue.* Page 4. City of Moreno Valley. September 2020. (Appendix B).
¹³ California Department of Food and Agriculture. Pest Rating Proposals and Final Ratings. *Tree of Heaven / Ailanthus altissima (Miller).* <http://blogs.cdфа.ca.gov/Section3162/?p=4399>. (accessed August 31, 2020).

ISSUES & SUPPORTING INFORMATION SOURCES:				Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
004	eucalyptus (<i>Eucalyptus</i> sp.)	35	14	Poor/No	Poor structure (codominant leaders, pleaching); unhealed old branch cuts		
<p>Source: LSA Associates, Inc. <i>Western Riverside County Multiple Species Habitat Conservation Plan Consistency Analysis and Biology Report, Commercial Retail Project at Perris Boulevard/Dracaea Avenue</i>. City of Moreno Valley. September 2020. (Appendix B).</p> <p>1 Trees with minimum four-inch caliper measured two feet from ground surface or which are at least 15 feet tall. 2 Measured two feet from ground surface.</p> <p>Although four trees are 15 feet or taller, none qualify as heritage trees because none of the trees retain the integrity necessary to embody the historical and cultural character of the City. The project site is infill and surrounded substantially by residential uses with contemporary landscaping. The on-site trees are located on a vacant site and therefore are outliers without the required context to convey the historical and cultural character of the City because the historic-era homestead with which they may have been associated in the past is demolished. Therefore, all trees on the project site will be managed in accordance with Part E of Section 9.17.030 of the City Municipal Code, which requires all trees proposed for removal to be replaced at a three to one ratio, with minimum twenty-four (24) inch box size trees of the same species, or a minimum thirty-six (36) inch box for a one to one replacement, where approved. Compliance with these standards is required of all projects in the City as a matter of regulatory policy (i.e., City Municipal Code) and therefore does not constitute mitigation. Impacts are less than significant, and mitigation is not required.</p>							
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or another approved local, regional, or state habitat conservation plan?				<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant with Mitigation Incorporated.</i> The MSHCP is a comprehensive, multi-jurisdictional Habitat Conservation Plan focusing on conservation of species and their associated habitats in Western Riverside County. MSHCP participants include the County and fourteen cities, including the City of Moreno Valley who is a signatory to the plan. The MSHCP will allow Riverside County and its cities to better control local land-use decisions and maintain a strong economic climate in the region while addressing the requirements of the state and federal Endangered Species Acts. The plan covers 1.26 million acres and protects 146 native species of plants and animals.</p> <p>As described in Checklist Response 4.a above, the project site does not contain the habitat types described for conservation in the MSHCP, and no special-status species were observed on the project site during the biological survey. Based on the analysis above, the project is consistent with the MSHCP. The project site is mostly devoid of vegetation since the property has been historically plowed or disked for weed abatement. A habitat assessment for burrowing owl (<i>Athene cunicularia</i>), a California species of special concern, was completed (Appendix B), and the project site does not contain suitable habitat for burrowing owl. However, the site may be utilized by nesting birds that are protected by the MBTA and Fish and Game Code. Implementation of Mitigation Measures BIO-1, provided above, would reduce impacts to nesting birds to less than significant levels.</p> <p>The project site is not within the MSHCP Criteria Area, Public/Quasi-Public Lands, or conservation area, nor is the site located within any species survey areas, including for plants.</p> <p>As described above in Checklist Response 4.b, the project would not impact Riparian/Riverine areas. The project site is located within the Stephens' Kangaroo Rat Habitat Conservation Plan area and a fee payment will be required prior to issuance of a grading permit. The project will comply with the Habitat Conservation Plan for the Stephens' Kangaroo Rat in Western Riverside County (Riverside County Habitat Conservation Agency, February 1995) and fulfill its compensatory mitigation requirements under this plan.</p> <p>With implementation of Mitigation Measure BIO-1, the project as planned is consistent with the applicable MSHCP requirements, and impacts would be less than significant with mitigation incorporated.</p>							
<p>Sources:</p> <p>1. Moreno Valley General Plan, adopted July 11, 2006</p>							

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<ul style="list-style-type: none"> • Chapter 7 – Conservation Element – Section 7.1 – Biological Resources 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.9 – Biological Resources <ul style="list-style-type: none"> - Figure 5.9-1 – Planning Area Biological Geographic Sections - Figure 5.9-2 – Planning Area Vegetation Community - Figure 5.9-3 – Project Site Location within the MSHCP Area - Figure 5.9-4 – Reche Canyon/Badlands Area Plan • Appendix E – Biological Resources Study, Appendix E 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code <ul style="list-style-type: none"> • Section 9.17.030 G – Heritage Trees 4. Moreno Valley Municipal Code Chapter 8.60 – Threatened and Endangered Species 5. Western Riverside County Multiple Species Habitat Conservation Plan (MSHCP), http://www.wrc-rca.org/about-rca/multiple-species-habitat-conservation-plan/ 6. Stephens’ Kangaroo Rat Habitat Conservation Plan (SKRHCP), Governing Documents RCHCA, CA 				

V. CULTURAL RESOURCES – Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource pursuant to §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Response:
Less than Significant with Mitigation Incorporated. A Cultural Resources Assessment¹⁴ was prepared to identify and determine whether any historical or archaeological resources may be present within the project limits (included as Appendix C). A records search was conducted on March 28, 2019, at the Eastern Information Center (EIC) at the University of California, Riverside. The search included a review of all recorded historic and prehistoric archaeological sites within one mile of the project site, as well as a review of known cultural resource survey and excavation reports. In addition, the California State Historic Property Data File (HPD), which includes the National Register of Historic Places (National Register), California Historical Landmarks (CHL), and California Points of Historical Interest (CPHI), various local historic registers, and historic maps were searched. Data from the EIC found 15 cultural resource studies were conducted within one mile of the project, none of which are located within the project limits. Although no cultural resources have been documented within the project area, five historic period residences, all eligible for listing in the National Register of Historic Places (National Register). The nearest resource (33-007280) is approximately 0.5 mile to the southwest.

In March 2019, a review of historic period maps, aerial photographs, and additional research was conducted. Review of historic period maps and online research indicated there was formerly a house on the one of the currently undeveloped parcel (APN 479-120-027) constructed prior to 1930 and removed by 2002. No prehistoric resources are recorded within one mile.

An intensive pedestrian field survey of the project area was conducted on April 25, 2019, and included walking in 10-meter wide transects throughout the project limits. The field survey revealed that the project area is almost completely obscured by Spring growth vegetation, and visibility was exceptionally poor throughout the project site at less than 5 percent. Modern refuse was noted on the surface.

A residence constructed prior to 1930 was formerly located within the project boundaries, and associated features (a utility pole and slab) and a glass bottle fragment dating to the historic period were identified during the survey. The residence was demolished prior to 2002, and only the concrete slab foundation, utility pole, and glass bottle fragment remain. Demolition of the historic-era residence has eliminated the context required to convey any historical significance of the property. The remnant features (utility pole and slab) and glass bottle fragment are typical examples of regionally and locally ubiquitous features and

¹⁴ LSA Associates, Inc. *Cultural Resources Assessment, Perris Boulevard and Dracaea Avenue Commercial Retail Project, City of Moreno Valley.* August 2020. (Appendix C).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>artifacts with no unique character-defining features that could contribute to the significance of the property or to the features and artifacts themselves. Therefore, their removal during construction would not constitute a significant impact. Due to the former presence of a pre-Depression Era residence, the project area retains some potential for associated subsurface resources. Therefore, archaeological monitoring is prescribed during ground-disturbing activities. Mitigation Measures TCR-1 through TCR-6, although prescribed to address Tribal Cultural Resources, will serve to ensure archaeological and historic-era cultural resources inadvertently encountered during ground disturbing activities are managed pursuant to CEQA Guidelines Section 15064.5 (refer to Section 18 below). Impacts related to historical and archaeological resources therefore would be less than significant with mitigation incorporated.</p>				
<p>c) Disturb any human remains, including those interred outside of formally dedicated cemeteries?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant with Mitigation Incorporated.</i> In the event that human remains (or remains that may be human) are discovered at the project site, no further disturbance shall occur within 100 feet of the find until the project Applicant has notified the Riverside County Coroner and the City of Moreno Valley Planning Official or designee, and the County Coroner has made a determination of origin and disposition.¹⁵ Section 7050.5 of the California Health and Safety Code requires that excavation be stopped in the vicinity of the discovered human remains until the coroner can determine whether the remains are those of a Native American. If human remains are determined to be of Native American origin, the project Applicant shall comply with State regulations relating to the disposition of Native American burials that occur within the jurisdiction of the Native American Heritage Commission (NAHC) (PRC Section 5097). The County Coroner shall contact the NAHC to determine the most likely descendant(s) (MLDs). The MLD shall complete his or her inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site. The MLD shall oversee disposition of the remains to determine the most appropriate means of treating the human remains and any associated grave artifacts.</p> <p>The specific locations of Native American burials and reburials shall be proprietary and not disclosed to the general public. The County Coroner shall notify the NAHC in accordance with California Public Resources Code 5097.98. Additionally, Section 7052 of the California Health and Safety Code states that disturbance of Native American cemeteries is a felony. Adherence to State regulations as described above is required for all development and is codified in Mitigation Measure TCR-6 through the City's consultation with Native American tribes pursuant to SB 18 and AB 52. Therefore, impacts associated with the inadvertent discovery of human remains would be less than significant with mitigation incorporated.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 7 – Conservation Element – Section 7.2 – Cultural and Historical Resources 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.10 – Cultural Resources <ul style="list-style-type: none"> - Figure 5.10-1 – Locations of Listed Historic Resource Inventory Structures - Figure 5.10-2 – Location of Prehistoric Sites - Figure 5.10-3 – Paleontological Resource Sensitive Areas • Appendix F – Cultural Resources Analysis, Study of Historical and Archaeological Resources for the Revised General Plan, City of Moreno Valley, Archaeological Associates, August 2003. 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 4. Moreno Valley Municipal Code Title 7 – Cultural Preservation 5. Cultural Resources Inventory for the City of Moreno Valley, Riverside County, California, prepared by Daniel F. McCarthy, Archaeological Research Unit, University of California, Riverside, October 1987 (<i>This document cannot be provided to the public due to the inclusion of confidential information pursuant to Government Code Section 6254.10.</i>) 				

¹⁵ California Health and Safety Code. *Division 7, Dead Bodies*; Chapter 2, *General Provisions*, § 7050.5.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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VI. ENERGY – Would the project:

a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:

Less than Significant Impact. The proposed project would increase the demand for electricity, natural gas, and gasoline. The discussion and analysis provided below is based on data included in the CalEEMod output, which is included in Appendix A.

Construction-Period Energy Use. The anticipated construction schedule assumes that the proposed project would be built over a six month period. The proposed project would require grading, site preparation, and building activities during construction.

Construction of the proposed project would require energy for the manufacturing and transportation of construction materials, preparation of the site for grading activities and construction of the fuel facility. Petroleum fuels (e.g., diesel and gasoline) would be the primary sources of energy for these activities. Construction activities are not anticipated to result in an inefficient use of energy as gasoline and diesel fuel would be supplied by construction contractors who would conserve the use of their supplies to minimize their costs on the project. Energy usage on the project site during construction would be temporary in nature and would be relatively small in comparison to the State’s available energy sources. In addition, to increase energy efficiency on the site during project construction, the project would restrict equipment idling times to 5 minutes or less and/or would require construction workers to shut off idle equipment. Energy usage on the project site during construction would be temporary in nature and would be relatively small in comparison to the State’s available energy sources. Therefore, construction energy impacts would be **less than significant**.

Operational Energy Use. Energy consumed by the proposed project would be associated with natural gas use, electricity consumption, and fuel used for vehicle trips associated with the project. Energy and natural gas was estimated for the project using default energy intensities by building type in CalEEMod. In addition, the proposed buildings would be constructed to CALGreen standards as required by the State, which were included in CalEEMod inputs. Electricity and natural gas usage estimates associated with the proposed project are shown in Table 6A.

Table 6.A: Estimated Annual Energy Use of the Proposed Project

Land Use	Electricity Use (kWh per year)	Natural Gas Use (therms per year)	Gasoline (gallons per year)
Convenience Market with Gas Pumps	51,631	91	84,254
Fast Food Restaurant with Drive Through	142,440	8,203	110,022
Parking Lot	7,280	0	0
Total	201,351	8,294	194,276

Source: CalEEMod Modeling, LSA, May 2020 (Appendix A).

As shown in Table 6.A, the estimated potential increased electricity demand associated with the proposed project is 201,351 kilowatt-hours (kWh) per year. In 2018, California consumed approximately 284,436 gigawatt-hours (GWh) or 284,436,261,600 kWh. Of this total, Riverside County consumed 16,256 GWh or 16,256,705,441 kWh.¹⁶ Therefore, electricity demand associated with the proposed project would be less than 0.02 percent of Riverside County’s total electricity demand.

As shown in Table 6.A, the estimated potential increased natural gas demand associated with the proposed project is 8,294 therms per year. In 2018, California consumed approximately 12,666 million therms or 12,666,398,560 therms, while Riverside County consumed approximately 398 million therms

¹⁶ California Energy Commission. *Energy Consumption Data Management Service. Electricity Consumption by County.* 2017. www.ecdms.energy.ca.gov/electbycounty.aspx. (Accessed June 2020).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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or approximately 398,538,428 therms.¹⁷ Therefore, natural gas demand associated with the proposed project would be less than 0.03 percent of Riverside County's total natural gas demand.

In addition, the proposed project would result in energy usage associated with gasoline to fuel project-related trips. As shown above in Table VI.A, vehicle trips associated with the proposed project would consume approximately 194,276 gallons of gasoline per year. In 2015, vehicles in California consumed approximately 15.1 billion gallons of gasoline.¹⁸ Therefore, gasoline demand generated by vehicle trips associated with the proposed project would be a minimal fraction of gasoline and diesel fuel consumption in California.

The proposed project would also be required to implement energy reduction design features and comply with the most recent energy building standards consistent with applicable plans and policies. Therefore, the proposed project would not result in the wasteful, inefficient, or unnecessary consumption of fuel or energy and would incorporate renewable energy or energy efficiency measures into building design, equipment use, and transportation. Therefore, construction and operation period impacts related to consumption of energy resources would be **less than significant**. Mitigation is not required.

b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:

Less than Significant Impact. In 2002, the Legislature passed Senate Bill 1389, which required the California Energy Commission (CEC) to develop an integrated energy plan every two years for electricity, natural gas, and transportation fuels, for the California Energy Policy Report. The plan calls for the State to assist in the transformation of the transportation system to improve air quality, reduce congestion, and increase the efficient use of fuel supplies with the least environmental and energy costs. To further this policy, the plan identifies a number of strategies, including assistance to public agencies and fleet operators in implementing incentive programs for zero emission (ZE) vehicles and their infrastructure needs, and encouragement of urban designs that reduce VMT and accommodate pedestrian and bicycle access.

The CEC adopted the 2017 Integrated Energy Policy Report.¹⁹ The 2017 Integrated Energy Policy Report provides the results of the CEC's assessments of a variety of energy issues facing California. Many of these issues will require action if the State is to meet its climate, energy, air quality, and other environmental goals while maintaining energy reliability and controlling costs. The 2017 Integrated Energy Policy Report covers a broad range of topics, including implementation of Senate Bill 350, integrated resource planning, distributed energy resources, transportation electrification, solutions to increase resiliency in the electricity sector, energy efficiency, transportation electrification, barriers faced by disadvantaged communities, demand response, transmission and landscape-scale planning, the California Energy Demand Preliminary Forecast, the preliminary transportation energy demand forecast, renewable gas (in response to Senate Bill 1383), updates on Southern California electricity reliability, natural gas outlook, and climate adaptation and resiliency.

As indicated above, energy usage on the project site during construction and grading would be temporary in nature. In addition, energy usage associated with operation of the proposed project would be relatively small in comparison to the State's available energy sources and energy impacts would be negligible at the regional level. Because California's energy conservation planning actions are conducted at a regional level, and because the project's total impact to regional energy supplies would be minor, the proposed project would not conflict with California's energy conservation plans as described in the CEC's 2017 Integrated Energy Policy Report. Thus, as shown above, the project would avoid or reduce the inefficient, wasteful, and unnecessary consumption of energy and not result in any irreversible or irretrievable commitments of energy. Therefore, the proposed project would not result in the wasteful, inefficient, or unnecessary consumption of energy resources during project construction or operation and this impact would be **less than significant**. Mitigation is not required.

Sources:

¹⁷ California Energy Commission. *Energy Consumption Data Management Service. Gas Consumption by County.* 2017. www.ecdms.energy.ca.gov/gasbycounty.aspx. (Accessed June 2020).
¹⁸ California Energy Commission. *California Gasoline Data, Facts, and Statistics.* 2020. <https://www.energy.ca.gov/data-reports/energy-almanac/transportation-energy/california-gasoline-data-facts-and-statistics>. (Accessed June 2020).
¹⁹ California Energy Commission. *2017 Integrated Energy Policy Report. Publication Number: CEC-100-2017-001-CMF.* 2017.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 7 – Conservation Element – Section 7.6 – Energy Resources 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 				
VII. GEOLOGY AND SOILS – Would the project:				
a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to https://www.conservation.ca.gov/cgs/Documents/SP_042.pdf	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant Impact.</i> The project site is located within the Peninsular Range Geomorphic Province, an area characterized by active northeast trending strike slip faults, including the San Jacinto Fault and the Elsinore Fault. Based on the City's General Plan Safety Element, the project site is not located within the boundaries of an Earthquake Fault Zone as defined by the Alquist-Priolo Earthquake Fault Zoning Act of 1972 (California Geological Survey 2005).²⁰ There are no known active or potentially active faults that traverse the project site and the risk of ground rupture due to a fault displacement beneath the site is low. The closest known fault is the San Jacinto Fault zone approximately 6 miles east of the project site. As the project site is not within an identified fault zone, implementation of the project would not exacerbate the risks associated with fault rupture. Therefore impacts related to earthquake faults would be less than significant. No mitigation is required.</p>				
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> Like all of southern California, the project site is located in a seismically active area and, therefore, will continue to be subject to ground shaking resulting from activity on local and regional faults. In addition, the site lies in relative close proximity to the San Jacinto fault, an active fault.²¹ Therefore, during the life of the project, there is a high likelihood that there will be similar levels of ground shaking from this fault zone. The project is required to be designed and constructed in accordance with the current California Building Code (CBC) requirements. Adherence to the CBC is anticipated to address the issues related to potential ground shaking. Implementation of the project would not exacerbate the risks associated with seismic ground shaking. With the implementation of CBC requirements, seismic-related impacts would be less than significant. No mitigation is required.</p>				
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> Liquefaction involves a sudden loss in strength of a saturated, cohesionless soil (e.g., predominantly sand, low plasticity silts, or sand silt mixtures) caused by cyclic loading such as an earthquake. This results in temporary transformation of the soil to a fluid mass. There are three factors that must exist concurrently in order for liquefaction to occur. These factors include:</p> <ul style="list-style-type: none"> • A source of ground shaking, such as an earthquake, capable of generating soil mass distortions; • A relatively loose silty and/or sandy soil; and • A relatively shallow groundwater table (within approximately 50 feet below ground surface) or completely saturated soil conditions that will allow positive pore pressure generation. 				

²⁰ City of Moreno Valley. City of Moreno Valley General Plan. Figure 6-3: Geologic Faults and Liquefaction. July 11, 2006.
²¹ *Ibid.* Chapter 6 Safety.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>The project site is not located within a liquefaction zone.²² Because the project site is not located in an area that is susceptible to ground-related failure such as liquefaction, implementation of the project would not exacerbate the risks associated with ground related failure. Therefore, a less than significant impact related to this issue would occur. No mitigation is required.</p>				
<p>iv) Landslides?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The geologic and topographic characteristics of an area, often determine its potential for landslides. Steep slopes, the extent of erosion, and the rock composition of a hillside all contribute to the potential for slope failure and landslide events. Common triggering mechanisms of slope failure include undercutting of slopes by erosion or grading, saturation of marginally stable slopes by rainfall or irrigation, and seismic shaking of marginally stable slopes during earthquakes. As described in the City’s Safety Element of the General Plan, there is the potential for landslides in the Badlands area within the easternmost portion of the City’s Sphere of Influence because there are steep slopes, and the underlying geological material is poorly consolidated. The project site is generally flat with no evidence of landslides occurring on-site and the project site, and it is not located near the Badlands area. There are no natural or artificial slopes on the project site that have the potential for landslides. As the proposed project is not expected to be exposed to a landslide hazard and would not exacerbate landslide risks, a less than significant impact related to this issue would occur. No mitigation is required.</p>				
<p>b) Result in substantial soil erosion or the loss of topsoil?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> Soils are classified by the United States Department of Agriculture (USDA) Natural Resource Conservation Service into four hydrologic soils groups based on the soil’s runoff potential. “Hydrologic soil group” is a term that represents a group of soils having similar runoff potential under similar storm and cover conditions. Soil properties that influence runoff potential are those that influence the minimum rate of infiltration for bare soil after prolonged wetting. The two soil units mapped within the project site are: Ramona sandy loam, 0 to 2 percent slopes, Major Land Resource Area (MLRA) 19 (RaA) and Ramona sandy loam, 2 to 5 percent slopes, eroded (RaB2).²³ RaA soils are in hydrologic soil group B which have moderate infiltration rates when thoroughly wetted and consist mainly of moderately deep to deep, moderately well to well drained soils with moderately fine to moderately coarse textures. RaB2 soils are in hydrologic soil group C which have low infiltration rates when thoroughly wetted and consist mainly of soils with a layer that impedes downward movement of water and soils with moderately fine to fine texture. Soil types of Group C would be particularly prone to erosion during construction of the proposed project, especially during heavy rains. In addition, the proposed project would require the excavation and movement of on-site soils, which could provide for further runoff or erosion issues. Therefore, construction of the proposed project could result in potentially significant impacts related to erosion. Prior to the issuance of grading permits, the project Applicant would be required to prepare and submit detailed grading plans. These plans must be prepared in conformance with applicable standards of the City’s Grading Ordinance. Development of the site would involve more than one acre of ground disturbance; therefore, the proposed project is required to obtain a National Pollutant Discharge Elimination System (NPDES) permit. A Storm Water Pollution Prevention Plan (SWPPP) would also be required to address erosion and discharge impacts associated with the proposed onsite grading by implementing appropriate best management practices (BMPs). The project applicant would be required to adhere to the SWPPP which would require the implementation of BMPs, including erosion control measures, to minimize construction impacts. Additionally, upon completion of all construction activities, all areas of temporary disturbance would be restored and revegetated. The proposed project would have a less than significant impact associated with soil erosion. No mitigation is required.</p>				
<p>c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on-</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

²² *Ibid.* Figure 6-3: Geologic Faults and Liquefaction.

²³ United States Department of Agriculture, Natural Resources Conservation Service. *Web Soil Survey.* <https://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>. (Accessed June 17, 2019).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?				
<p>Response: <i>Less than Significant Impact.</i> As discussed under Checklist Response 7.a.iii and 7.a.iv, liquefaction and landslides are not significant hazards at the project site. Lateral spreading refers to ground or slope deformation due to the presence of weak or liquefiable soils in the subsurface combined with strong seismic shaking. Due to a lack of liquefaction and soft soils, the potential for lateral spreading at the site is not significant. Subsidence is the sudden sinking or gradual downward settling of the earth's surface with little or no horizontal motion. Subsidence is caused by a variety of activities, which includes (but is not limited to) withdrawal of groundwater, pumping of oil and gas from underground, the collapse of underground mines, liquefaction, and hydro-compaction. The proposed project does not include the withdrawal of groundwater or other resources from underground sources. Settlement ("seismic compaction") of loose to medium-dense clean dry sands can occur during seismic shaking. The project Applicant would be required to prepare a final geotechnical report, which shall include project-specific recommendations and construction specifications that meet or exceed seismic design requirements. The specific design recommendations described in the final geotechnical report would be incorporated into all project-related construction documents. Adherence to the specific design recommendations described in the final geotechnical report would ensure that impacts related to on-site or off-site landslides, lateral spreading, subsidence, liquefaction, or collapse remain less than significant. No mitigation is required.</p>				
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant Impact.</i> Expansive soils generally have a significant amount of clay particles, which can give up water (shrink) or take on water (swell). The change in volume exerts stress on buildings and other loads placed on these soils. The extent of shrink/swell is influenced by the amount and kind of clay in the soil. The occurrence of these soils is often associated with geologic units having marginal stability. The distribution of expansive soils can be widely dispersed, and they can occur in hillside areas as well as low-lying alluvial basins. No significant deposits of fine grained soils (silt and clay) were observed, but could be locally present. On-site soils are anticipated to be non-expansive or have a very low expansion potential. However, there may be localized, discontinuous layers of clayey soils with higher expansion potential. As described above, a final geotechnical report would be prepared and would include project-specific recommendations and construction specifications that meet or exceed seismic design requirements. The specific design recommendations described in the final geotechnical report would be incorporated into all project-related construction documents. Adherence to the specific design recommendations described in the final geotechnical report would ensure that impacts related to expansive soils remain less than significant. Mitigation is not required.</p>				
e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> The proposed project is expected to connect to existing sewer infrastructure. The project would not use septic tanks or other alternative wastewater disposal system. Therefore, the development of the proposed project would have no impact related to this issue. Mitigation is not required.</p>				
f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant with Mitigation Incorporated.</i> California Administrative Code, Title 14, Section 4307 states that no person shall remove, injure, deface or destroy any object of paleontological, archaeological, or historical interest or value. Compliance with Section 4307 is required for all development and would apply to the project as a matter of regulatory policy. As described in the City's</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>General Plan Final Program EIR,²⁴ the City's Planning Area contains sedimentary rock-units (Mt. Eden Formation and San Timoteo Formation) that have the potential to contain significant nonrenewable paleontological resources. Figure 5.10-3 of the City's Final Program EIR indicates that the project site is not underlain by these two sedimentary rock-units and that there is a low potential for sensitive paleontological resources to occur on the project site. Based on the site characteristics as described in the City's Final Program EIR, construction of the project is not expected to impact, either directly or indirectly, any known unique paleontological resource or site of unique geologic features. Given the site's history of disturbance, the potential for undiscovered paleontological resources is considered low. However, ground-disturbing activities at the project site still have the potential to disturb previously unknown subsurface resources. Mitigation Measure GEO-1 requires the construction manager and any contractors to attend a Paleontological Resources Worker Sensitivity Training prior to ground disturbance to ensure they are aware of the potential for paleontological resources to occur on-site and the discovery protocol to follow in case such resources are encountered. Mitigation Measure GEO-2 would ensure the project paleontologist is engaged to evaluate the significance of any paleontological resources identified during ground disturbing activities. With implementation of Mitigation Measures GEO-1 and GEO-2, paleontological resources impacts would be less than significant with mitigation incorporated.</p>				
<p>Mitigation Measure GEO-1: The Project Applicant shall retain a qualified paleontologist to attend the pre-grading meeting with the City, the construction manager and any contractors. The paleontologist will conduct a mandatory Paleontological Resources Worker Sensitivity Training to those in attendance. The Training will include a brief review of the paleontological sensitivity of the Project and the surrounding area; what resources could potentially be identified during earthmoving activities; the protocols that apply in the event inadvertent discoveries of paleontological resources are identified, including who to contact and appropriate avoidance measures until the find(s) can be properly evaluated; and any other appropriate protocols. All new construction personnel that will conduct earthwork or grading activities that begin work on the Project following the initial Training must take the Paleontological Sensitivity Training prior to beginning work, and the Project paleontologist shall make themselves available to provide the training on an as-needed basis.</p>				
<p>Mitigation Measure GEO-2: If paleontological resources (fossils) are discovered during project grading, work shall be halted within 100 feet of the find until a qualified paleontologist assesses the significance of the find. The project paleontologist shall monitor remaining earthmoving activities at the project site and shall be equipped to record and salvage fossil resources that may be unearthed during grading activities. The paleontologist shall be empowered to temporarily halt or divert grading equipment to allow recording and removal of the unearthed resources. Any fossils found shall be evaluated in accordance with the CEQA Guidelines and offered for curation at an accredited facility approved by the City of Moreno Valley. Once grading activities have ceased or the paleontologist determines that monitoring is no longer necessary, monitoring activities shall be discontinued. This measure shall be implemented to the satisfaction of the City Planning Division.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 6 – Safety Element – Section 6.5 – Geologic Hazards <ul style="list-style-type: none"> - Figure 6-3 – Geologic Faults & Liquefaction • Chapter 7 – Conservation Element – Section 7.4 -- Soils 				

²⁴ City of Moreno Valley. *City of Moreno Valley General Plan Final Program EIR*. Chapter 5.10 – Cultural Resources. July 2006.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<ol style="list-style-type: none"> 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.6 – Geology and Soils <ul style="list-style-type: none"> - Figure 5.6-1 – Geology - Figure 5.6-2 – Seismic Hazards 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 4. Moreno Valley Municipal Code Chapter 8.21 – Grading Regulations 5. Local Hazard Mitigation Plan, City of Moreno Valley Fire Department, adopted October 4, 2011, amended 2017, http://www.moval.org/city_hall/departments/fire/pdfs/haz-mit-plan.pdf <ul style="list-style-type: none"> • Chapter 4 – Earthquake <ul style="list-style-type: none"> - Figure 4-1 – Right-Lateral Strike -Slip Fault - Figure 4-1.1 – Moreno Valley Geologic Faults and Liquefaction 2016 - Figure 4-1.2 – Moreno Valley Area Ground Shaking Map • Chapter 8 – Landslide <ul style="list-style-type: none"> - Figure 8-1 – Moreno Valley Slope Analysis 2016 6. Emergency Operations Plan, City of Moreno Valley, March 2009, http://www.moval.org/city_hall/departments/fire/pdfs/mv-eop-0309.pdf <ul style="list-style-type: none"> • Threat Assessment 1 – Major Earthquakes <ul style="list-style-type: none"> - Figure 9 – Types of Faults - Figure 10 – Earthquake Faults - Figure 11 – Comparison of Richter Magnitude and Modified Mercalli Intensity - Figure 12 – Magnitude 4.5 or Greater Earthquake Map - Figure 13 – Geologic Faults and Liquefaction 				

VIII. GREENHOUSE GAS EMISSIONS – Would the project:

a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:

Less than Significant Impact. This section provides an analysis of greenhouse gas (GHG) emissions associated with the proposed project. This analysis examines the short-term construction and long-term operational impacts of the proposed project as it relates to greenhouse gases. A detailed assessment of project-related GHG emissions is included in Appendix A.

Construction GHG Emissions. Construction activities associated with the proposed project would produce combustion emissions from various sources. During construction, GHGs would be emitted through the operation of construction equipment and from worker and builder supply vendor vehicles, each of which typically uses fossil-based fuels to operate. The combustion of fossil-based fuels creates GHGs such as CO₂, CH₄, and N₂O. Furthermore, CH₄ is emitted during the fueling of heavy equipment. Exhaust emissions from on-site construction activities would vary daily as construction activity levels change.

The SCAQMD does not have an adopted threshold of significance for construction-related greenhouse gas emissions. However, lead agencies are required to quantify and disclose greenhouse gas emissions that would occur during construction. The SCAQMD then requires the construction GHG emissions to be amortized over the life of the project, defined as 30 years, added to the operational emissions, and compared to the applicable interim GHG significance threshold tier.

Using CalEEMod, it is estimated that the project would generate approximately 127.01 metric tons of CO₂e during construction of the project. When annualized over the 30-year life of the project, annual emissions would be 4.23 metric tons CO₂e.

Operational GHG Emissions. Long-term operation of the proposed project would generate GHG emissions from area and mobile sources as well as indirect emissions from sources associated with energy consumption. Mobile-source GHG emissions would include project-generated vehicle trips associated with trips to the proposed project. Area-source emissions would be associated with activities such as landscaping and maintenance on the project site, and other sources.

Following guidance from the SCAQMD, GHG emissions were estimated using CalEEMod. Motor vehicle emissions are the largest source of GHG emissions for the project at approximately 95 percent of the

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>total. Energy use is the next largest category at 4 percent. Water and waste are about 1 percent of the total emissions. Calculations are included in Attachment C to Appendix A.</p> <p>The project would have less than significant GHG emissions if it would result in operational-related GHG emissions of less than 3,000 metric tons of CO₂e per year.²⁵ Based on the analysis results, the proposed project would result in approximately 2,584 metric tons of CO₂e per year, which would be well below the SCAQMD’s numeric threshold of 3,000 metric tons of CO₂e per year.²⁶ Therefore, operation of the proposed project would not generate significant GHG emissions, and this impact would be less than significant. No mitigation is required.</p>				
<p>b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response:</p> <p><i>Less than Significant Impact.</i> The Energy Efficiency and Climate Action Strategy were adopted by the City on October 5, 2012. In 2014, Moreno Valley was one of 12 cities that collaborated with the Western Riverside Council of Governments (WRCOG) on a Subregional Climate Action Plan (Subregional CAP) that includes 36 measures to guide Moreno Valley’s GHG reduction efforts through 2020. The Energy Efficiency and Climate Action Strategy is a policy document that identifies ways that the City of Moreno Valley can reduce energy and water consumption and greenhouse gas emissions. By using energy more efficiently, harnessing renewable energy to power buildings and vehicles, improving access to sustainable transportation modes, recycling more waste, conserving water, and building local food systems, the City can support the local economy, create new green jobs, and improve public health and community quality of life. The Energy Efficiency and Climate Action Strategy contains GHG reduction measures organized into four primary sectors, as defined by the following policy goals:</p> <ul style="list-style-type: none"> • Energy: <ul style="list-style-type: none"> • Energy measures designed to increase communitywide building and equipment efficiency and renewable energy use, and promote energy efficiency and renewable energy generation for use supporting municipal operations that support the community. • Transportation and Land Use <ul style="list-style-type: none"> • Transportation and land use measures that would reduce single-occupancy vehicle travel, increase non-motorized travel, improve public transit access, increase motor vehicle efficiency, encourage alternative fuel vehicles and promote sustainable growth patterns. • Water <ul style="list-style-type: none"> • Water measures that would conserve potable water and reduce water demand by the community and municipal operations. • Solid Waste <ul style="list-style-type: none"> • Solid waste measures that would reduce solid waste sent to landfills that is generated by the community and municipal operations. <p>As discussed above, the Energy Efficiency and Climate Action Strategy outlines a programmatic approach to review the potential GHG-related impacts associated with new development. As detailed in Appendix A, the project would be consistent with all applicable GHG reduction strategies of the Energy Efficiency and Climate Action Strategy. Furthermore, the project would not conflict with any State and City regulations intended to reduce GHG emissions statewide, and would be consistent with applicable plans and programs designed to reduce GHG emissions. Therefore, the project would have a less than significant impact. No mitigation is required.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 				

²⁵ LSA Associates, Inc. *Air Quality and Greenhouse Gas Analysis for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California*. Page 9. May 2020. (Appendix A)

²⁶ *Ibid.* Page 15.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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4. California’s 2017 Climate Change Scoping Plan, prepared by the California Air Resources Board, November 2017, https://www.arb.ca.gov/cc/scopingplan/scoping_plan_2017.pdf, accessed April 24, 2019

IX. HAZARDS AND HAZARDOUS MATERIALS – Would the project:

a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:

Less than Significant Impact. The transport, use, and storage of hazardous materials during the construction and operation of the site would be conducted pursuant to all applicable local, state and federal laws, and in cooperation with the Riverside County Fire Department, Riverside County Department of Environmental Health, Hazardous Materials Division (DEH), Environmental Protection and Oversight Division, and California Occupational Safety and Health Administration. Additionally, the United States Department of Transportation Office of Hazardous Materials Safety prescribes strict regulations for the safe transportation of hazardous materials by truck and rail on State highways and rail lines, as described in Title 49 of the *Code of Federal Regulations*, and implemented by Title 13 of the California Code of Regulations.

The proposed service station would utilize hazardous materials on a daily basis including gasoline, oil, solvents, and cleaning products. Two underground storage tanks (USTs) (one is 20,000 gallons and the other is 12,000 gallons/8,000 gallons combination tank) are proposed on the west side of the proposed canopy along with 6 MPDs (12 total fueling stations). Accordingly, the project would develop a Hazardous Materials Business Emergency Plan administered by the Riverside County Fire Department, as applicable, in accordance with California Health and Safety Code Section 25507 and other local, state, and federal standards, ordinances, and regulations. As required by Health and Safety Code Section 25507, a business shall establish and implement a Hazardous Materials Business Emergency Plan for emergency response to a release or threatened release of a hazardous material in accordance with the standards prescribed in the regulations adopted pursuant to Section 25503 if the business handles a hazardous material or a mixture containing a hazardous material that has a quantity at any one time above the thresholds described in Section 25507(a) (1) through (8).

Depending on the specific tenants of the project site, the project would also be required to implement health and safety policies and procedures regarding hazardous materials used where employees would be expected to handle or work around hazardous materials. Pursuant to the Federal Hazard Communication Standard (29 CFR 1910.1200) and the Laboratory Standard (29 CFR 1910.1450), Safety Data Sheets (SDS) outlining procedures to address spills and leaks for individual chemicals would be used to conduct chemical safety training for all employees who work with chemicals in order to minimize the occurrence of accidental chemical releases and ensure that, when one does occur, it is handled in a safe manner.

These regulations inherently safeguard life and property from the hazards of fire/explosion arising from the storage, handling, and use of hazardous substances, materials, and devices, as well as hazardous conditions due to the use or occupancy of buildings. Through compliance with all applicable federal, state, and local laws, impacts to the public or environment from the routine transportation, use and disposal of hazardous materials would be **less than significant**. No mitigation is required.

b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:

Less Than Significant Impact. Refer to Checklist Response 9.a. There is no indication the project site was used for agricultural purposes. Review of historic period maps and online research indicated there was formerly a house on one of the currently undeveloped parcel (APN 479-120-027) constructed prior to 1930 and removed by 2002 (Appendix C). As detailed above, the service station use would be required to comply would all applicable federal, state, and local laws and regulations regarding hazardous materials. The underground fuel storage tanks would also require permitting and monitoring by the City Fire Department and the County Department of Environmental Health as the Certified Unified Program

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Agency (CUPA) for Riverside County. With compliance with regulations, the project would have a less than significant impact related to the release of hazardous materials, and no mitigation is required.				
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Response: <i>No Impact.</i> The nearest existing or proposed school is the Riverside County Education Academy located at 13730 Perris Boulevard approximately 0.45 mile south of the project site. Since no existing schools or proposed schools are within 0.25 mile of the project site, no impact associated with this issue would occur. No mitigation is required.				
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Response: <i>No Impact.</i> According to the EnviroStor website, ²⁷ no hazardous material sites on or adjacent to the proposed project site were identified. The closest site is the Moreno Valley Regional Learning Center located approximately 2,400 feet south of the project site. The site type was a school investigation with a “no further action determination” as of May 21, 2007. Therefore, no impact related to this issue would occur, and no mitigation is required.				
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Response: <i>Less Than Significant Impact.</i> The closest airport is the March Air Reserve Base/Inland Port Airport located approximately 2.75 miles southwest of the project site. The project site is not located within the airport influence zone of March Air Reserve Base. ²⁸ Due to the distance of this airport from the project site, implementation of the proposed project would not result in a safety hazard or excessive noise levels for people residing or working in the area. Therefore, no impact related to this issue would occur, and no mitigation is required.				
f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Response: <i>Less Than Significant Impact.</i> The project site is currently vacant and unoccupied. Construction activities that could temporarily restrict vehicular traffic would incorporate appropriate measures to facilitate the passage of persons and vehicles through/around any temporary road closures in accordance with the California Fire Code. During construction, standard traffic control devices such as warning signs, warning lights, and flaggers will be utilized as applicable to minimize obstructions and ensure the safe passage of emergency vehicles as necessary for the purposes of coordinating efforts during local, State, and/or federal emergency events, including response to hazardous materials incidents. Implementation of these traffic control measures will include guidance and navigational tools throughout the project area in order to maintain traffic flow and safety during construction. The project is proposed with three driveways (one each along Perris Boulevard, Dracaea Avenue, and Atwood Avenue) that would provide entry and exit points for emergency access. The project site does				

²⁷ California Department of Toxic Substances Control. *EnviroStor, Site/Facility Search*. https://www.envirostor.dtsc.ca.gov/public/map/?global_id=60000502. (Accessed June 20, 2019).
²⁸ Riverside County Airport Land Use Commission. *March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan*. Map MA-1: Compatibility Map. November 13, 2017.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>not include gates, thereby facilitating immediate fire department access to the site in the event of an emergency. Fire department emergency vehicle apparatus access road locations and design shall be in accordance with the California Fire Code, Riverside County Ordinance No. 787, and Moreno Valley/Riverside County Fire Department Standards to ensure proper roadway turning radii, fire lane widths, etc. Additionally, the project site layout includes provisions for emergency vehicle access, which also would be reviewed for adequacy by the Moreno Valley/Riverside County Fire Department. Therefore, impacts would be less than significant, and mitigation is not required.</p>				
<p>g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> No portion of the project site and its surroundings are identified as being within a “very high” wildfire zone.²⁹ Additionally, fire hazard severity zones published by the California Department of Forestry and Fire Protection do not identify the site as being within either a moderate, high, or very high fire hazard severity zone.³⁰ Therefore, the proposed project would not expose people or property to new increased wildland fire risks. Therefore, there would be no impact relating to exposure of people or structures to wildland fires from project implementation. No mitigation is required.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 6 – Safety Element – Section 6.2.8 – Wildland Urban Interface • Chapter 6 – Safety Element – Section 6.9 – Hazardous Materials • Chapter 6 – Safety Element – Section 6.10 – Air Crash Hazards <ul style="list-style-type: none"> - Figure 6-5 – Air Crash Hazards 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.5 – Hazards and Hazardous Materials <ul style="list-style-type: none"> - Figure 5.5-1 – Hazardous Materials Sites - Figure 5.5-2 – Floodplains and High Fire Hazard Areas - Figure 5.5-3 – City Areas Affected by Aircraft Hazard Zones 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 4. March Air Reserve Base (MARB)/March Inland Port (MIP) Airport Land Use Compatibility Plan (ALUCP) on November 13, 2014, (http://www.rcaluc.org/Portals/13/17%20-%20Vol.%201%20March%20Air%20Reserve%20Base%20Final.pdf?ver=2016-08-15-145812-700) 5. Local Hazard Mitigation Plan, City of Moreno Valley Fire Department, adopted October 4, 2011, amended 2017, http://www.moval.org/city_hall/departments/fire/pdfs/haz-mit-plan.pdf <ul style="list-style-type: none"> • Chapter 5 – Wildland and Urban Fires <ul style="list-style-type: none"> - Figure 5-2 – Moreno Valley High Fire Area Map 2016 • Chapter 12 – Dam Failure/Inundation <ul style="list-style-type: none"> - Figure 12-2 Moreno Valley Evacuation Routes Map 2015 • Chapter 13 – Pipeline <ul style="list-style-type: none"> - Figure 13-1 – Moreno Valley Pipeline Map 2016 • Chapter 14 – Transportation <ul style="list-style-type: none"> - Figure 14-1.1 – Moreno Valley Air Crash Hazard Area Map 2016 • Chapter 16 – Hazardous Materials Accident <ul style="list-style-type: none"> - Moreno Valley Hazardous Materials Site Locations Map 2016 6. Emergency Operations Plan, City of Moreno Valley, March 2009, http://www.moval.org/city_hall/departments/fire/pdfs/mv-eop-0309.pdf <ul style="list-style-type: none"> • Hazard Mitigation and Hazard Analysis • Threat Assessment 2 – Hazardous Materials 				

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

²⁹ City of Moreno Valley. *Moreno Valley General Plan Final Program EIR*. Floodplains and High Fire Hazard Areas, Figure 5.5-2. July 2006.
³⁰ California Department of Forestry and Fire Protection. *Fire Hazard Severity Zones in SRA and Very High Fire Hazard Severity Zones in LRA*. 2007. http://www.fire.ca.gov/fire_prevention/ffsz_maps_riversidewest. (Accessed June 20, 2020).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<ul style="list-style-type: none"> • Threat Assessment 3 – Wildfire • Threat Assessment 6 – Transportation Emergencies <ul style="list-style-type: none"> - Figure 17 – Air Crash Hazards 				
X. HYDROLOGY AND WATER QUALITY – Would the project:				
a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant with Mitigation Incorporated.</i> The Santa Ana Regional Water Quality Control Board (RWQCB) adopted the Water Quality Control Plan for the Santa Ana Region (Basin Plan, updated February 2016), which sets water quality standards for all ground and surface waters within the project’s region.</p> <p>During construction, there is the potential for soil erosion and discharge of pollutants into drainages or storm drains. Therefore, as described in Mitigation Measure HYD-1, the project shall comply with the provisions of the Construction General Permit Order No. 2009-0009-DWQ as required by the RWQCB. Additionally, as described in Mitigation Measure HYD-2, the project shall prepare a Storm Water Pollution Prevention Plan (SWPPP) pursuant to the provisions of the Riverside County Flood Control and Water Conservation District National Pollutant Discharge Elimination System (NPDES) Permit Order No. R8-2013-0024, NPDES No. CAS 618033. Finally, as described in Mitigation Measure HYD-3, the project shall submit a Final Water Quality Management Plan (Final WQMP) to the City for review and approval to ensure operation of the project complies with the NPDES Permit.</p> <p>The NPDES permit regulates storm water discharges from construction sites which result in disturbance of at least one acre of soil, and/or are smaller sites that are part of a larger common plan of development. Additionally, as part of the SWPPP, the project shall identify best management practices (BMPs) to address water quality impacts associated with construction operations. Construction BMPs would include, but not be limited to, erosion control and sediment control BMPs designed to minimize erosion and retain sediment on site and good housekeeping BMPs to prevent spills, leaks, and discharge of construction debris and waste into receiving waters. The SWPPP would be developed, and construction BMPs selected and implemented, to target pollutants of concern during construction. The construction BMPs would be designed to retain sediment and other pollutants on site so they would not reach receiving waters or degrade beneficial uses. In addition, the project applicant has submitted project-specific Water Quality Management Plans (WQMPs) for the proposed project, which would be required to be implemented throughout project operation.^{31,32}</p> <p>Implementation of Mitigation Measures HYD-1 through HYD-3 would ensure potential water quality standards impacts would be less than significant with mitigation incorporated.</p> <p>Mitigation Measure HYD-1: Prior to the issuance of a grading permit, the project applicant shall file and obtain a Notice of Intent (NOI) with the Regional Water Quality Control Board (RWQCB) in order to be in compliance with the State NPDES General Construction Storm Water Permit for discharge of surface runoff associated with construction activities. Evidence that this has been obtained (i.e., a copy of the Waste Discharger’s Identification Number) shall be submitted to the City for coverage under the NPDES General Construction Permit. The NOI shall address the potential for an extended and discontinuous construction period based on funding availability.</p> <p>Mitigation Measure HYD-2: Prior to the issuance of a grading permit, the project applicant shall prepare a Storm Water Pollution Prevention Plan (SWPPP). The SWPPP shall include a surface water control plan and erosion control</p>				

³¹ Kimley Horn and Associates, Inc. *Project Specific Water Quality Management Plan, 7-Eleven Moreno Valley.* May 28, 2019.

³² Kimley Horn and Associates, Inc. *Project Specific Water Quality Management Plan, Drive-Thru Restaurant Moreno Valley.* May 28, 2019.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>Mitigation Measure HYD-3:</p>				

plan citing specific measures to control on-site and off-site erosion during the entire grading and construction period. In addition, the SWPPP shall emphasize structural and nonstructural Best Management Practices (BMPs) to control sediment and non-visible discharges from the site. The SWPPP would include inspection forms for routine monitoring of the site during construction phase to ensure NPDES compliance and additional BMPs and erosion control measures would be documented in the SWPPP and utilized if necessary. The SWPPP shall address the potential for an extended and discontinuous construction period based on funding availability. The SWPPP would be kept on site for the entire duration of project construction and would be available to the local RWQCB for inspection at any time. BMPs included in the SWPPP may include the following:

- Sediment discharges from the site may be controlled by the following: sandbags, silt fences, straw wattles and temporary basins (if deemed necessary), and other discharge control devices. The construction and condition of the BMPs would be periodically inspected during construction and repairs would be made when necessary as required by the SWPPP.
- Materials that have the potential to contribute to non-visible pollutants to storm water must not be placed in drainage ways and must be contained, elevated, and placed in temporary storage containment areas.
- All loose piles of soil, silt, clay, sand, debris, and other earthen material shall be protected in a reasonable manner to eliminate any discharge from the site. Stockpiles would be surrounded by silt fences and covered with plastic tarps.
- In addition, the construction contractor shall be responsible for performing and documenting the application of BMPs identified in the SWPPP. Weekly inspections shall be performed on sandbag barriers and other sediment control measures called for in the SWPPP. Monthly reports and inspection logs shall be maintained by the Contractor and reviewed by the City and the representatives of the State Water Resources Control Board. In the event that it is not feasible to implement specific BMPs, the City can make a determination that other BMPs would provide equivalent or superior treatment either on or off site.

Prior to the issuance of a grading permit, the Project Applicant shall submit a Final Water Quality Management Plan (Final WQMP) to the City of Moreno Valley (City) for review and approval. The project shall implement project design features identified in the Final WQMP. The Final WQMP shall demonstrate that any proposed on-site development plan includes best management practices (BMPs) for source control, pollution prevention, site design, low impact development (LID) implementation, and structural treatment control. BMPs shall be designed and implemented to address 303(d) listed pollutants and retain the project site's minimum design capture volume and hydromodification volume to ensure post-development storm water runoff volume or time of concentration does not exceed pre-development storm water runoff by more than 10 percent of the two-year peak flow in accordance with the Santa Ana Regional Water Quality Control Board Order Number R8-2010-0033, National Pollutant Discharge Elimination System (NPDES) Permit No. CAS618033, as amended by Order No. R8-2013-0024, also known as the Municipal Separate Storm Sewer System (MS4) permit. The proposed LID BMPs

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>specified in the Final WQMP shall be incorporated into the grading and development plans submitted to the City for review and approval. Periodic maintenance of any required BMPs and landscaped areas during project occupancy and operation shall be in accordance with the schedule outlined in the Final WQMP. This measure shall be implemented to the satisfaction of the City Public Works Department.</p>				
<p>b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant with Mitigation Incorporated.</i> Groundwater in the project area is managed and distributed by the Eastern Municipal Water District (EMWD). The EMWD is required by state law (Urban Water Planning Management Act or AB 797 in 1983) to prepare an Urban Water Management Plan (UWMP) to identify its sources/supplies of potable water, its historical and projected consumption by its customers, and evaluate various mandated scenarios for water shortages (e.g., single dry year, and multiple dry years) to assure its customers and the state that it would have adequate water supplies now and in the future, even under expected drought conditions.</p> <p>The EMWD considers current groundwater production to be utilized completely by existing customers, as the majority of EMWD's current and projected water supplies are imported through the Metropolitan Water District (MWD).³³ New developments, including the proposed project, would be supplied with imported water from one of the following sources: (1) treated imported water from MWD; (2) untreated imported water from MWD, which is subsequently treated by EMWD; or (3) untreated imported water treated by EMWD and recharged into groundwater basins for later withdrawal.</p> <p>MWD's 2015 Urban Water Management Plan (UWMP) provides information about MWD's regional supply reliability and projected demands. Based on information provided by EMWD and other member agencies, MWD concludes that it is able to meet projected demands for all member agencies through 2040, even during dry periods.³⁴ Under extreme conditions, water supplies could be allocated using the MWD Water Supply Allocation Plan (WSAP) to preserve supplies in storage by requiring a reduction in demand by member agencies, including the EMWD, pursuant to SB 1168 and 1319, and AB 1739. Since the proposed project would not be served via groundwater, and the project through implementation of Mitigation Measure HYD-3 would not preclude or obstruct on-site infiltration of storm water into the local groundwater aquifer, the project would not deplete groundwater supplies or interfere with groundwater recharge. Impacts would be less than significant with mitigation incorporated.</p>				
<p>c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:</p>				
<p>i) Result in substantial erosion or siltation on- or off-site?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant with Mitigation Incorporated.</i> Development of the proposed project (buildings and pavement) would alter the amount of existing impervious surface area and the amount of generated runoff. Currently, runoff generally drains from the southeast to the northwest. No streams, rivers, or other drainage features are located on site. The proposed project would construct structures and impervious surfaces that could potentially alter the current drainage pattern. Pursuant to the requirements of the NPDES permit, as discussed previously, excess flows and sediment would be captured by BMPs identified in the SWPPP and WQMP (Mitigation Measures HYD-2 and HYD-3). Therefore, the project is not expected to result in substantial erosion or siltation. Additionally, a drainage study determined that implementation of the BMPs outlined in the WQMP would decrease the ultimate amount of runoff to the</p>				

³³ Eastern Municipal Water District. 2015 Urban Water Management Plan. Page 7-1. June 2016.
³⁴ The Metropolitan Water District of Southern California. 2015 Urban Water Management Plan. Tables 2-4, 2-5, and 2-6. June 2016.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
existing storm drain system. ³⁵ Therefore, impacts would be less than significant with mitigation incorporated .				
ii) Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant with Mitigation Incorporated.</i> Refer to Section 10.c.i. No streams, rivers, or other drainage features are located on site. Pursuant to the requirements of the NPDES permit, as discussed previously, the project would result in a decrease in stormwater runoff, and excess flows and sediment would be captured by BMPs identified in the SWPPP and WQMP (Mitigation Measures HYD-2 and HYD-3). Therefore, impacts would be less than significant with mitigation incorporated.</p>				
iii) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
iv) Impede or redirect flood flows?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant with Mitigation Incorporated.</i> Refer to Section 10.c.i. The site slopes down from north to south toward Dracaea Avenue. Through implementation of Mitigation Measure HYD-2, a SWPPP would be implemented to ensure the project does not generate substantial polluted runoff during construction. Additionally, Mitigation Measure HYD-3 would require the project Applicant to prepare a Final WQMP that shall demonstrate the BMPs for source control, pollution prevention, site design, LID implementation, and structural treatment control are designed and implemented to address 303(d) listed pollutants. The BMPs shall retain the project site's minimum design capture volume and hydromodification volume to ensure post-development storm water runoff volume or time of concentration does not exceed pre-development storm water runoff by more than 10 percent of the two-year peak flow in accordance with the City's MS4 permit. Therefore, impacts would be less than significant with mitigation incorporated.</p>				
d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> The project site is not located within a 100-year flood hazard area.³⁶ Two locations of concern exist within the City of Moreno Valley: Poorman's Reservoir (Pigeon Pass Reservoir) and Lake Perris. Dam failure at Poorman's Reservoir could result in extensive flooding along the downstream watercourse. Dam failure at Lake Perris would only affect a very small area south of Nandina Avenue along the Perris Valley Storm Drain and the Mystic Lake area in the southeast corner of the City.³⁷ According to the City's General Plan, the project site would not expose people or structures to a risk of loss, injury or death involving the failure of a levee or dam.³⁸</p> <p>The Project site is located approximately 4.75 miles north of Lake Perris. At this distance, the Project site is not considered susceptible to seiche-related hazards originating at Lake Perris. Since the project is not in a flood hazard, tsunami, or seiche zone, risk from release of pollutants from project inundation would be less than significant. Mitigation is not required.</p>				
e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> Refer to Checklist Responses 10.a, 10.b, and 10.c.i. With implementation of Mitigation Measures HYD-1 through HYD-3 impacts from conflict with a water quality control plan or</p>				

³⁵ Kimley Horn and Associates, Inc. *City of Moreno Valley Preliminary Drainage Study for Cadence Capitol Investments, Perris Blvd and Dracaea Avenue, APN: 479-120-027, 029, 042, 043.* Page 5. April 2020.
³⁶ City of Moreno Valley. *City of Moreno Valley General Plan.* Figure 6-4 "Flood Hazards," Chapter 6-Safety. July 11, 2006.
³⁷ City of Moreno Valley. *City of Moreno Valley General Plan.* Chapter 6-Safety. July 11, 2006.
³⁸ *Ibid.*

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a sustainable groundwater management plan would remain less than significant . Mitigation is not required.				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 6 – Safety Element – Section 6.7 – Water Quality <ul style="list-style-type: none"> - Figure 6-4 – Flood Hazards • Chapter 7 – Conservation Element – Section 7.5 – Water Resources <ul style="list-style-type: none"> - Figure 7-1 Water Purveyor Service Area Map 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.5 – Hazards and Hazardous Materials <ul style="list-style-type: none"> - Figure 5.5-2 – Floodplains and High Fire Hazard Areas • Section 5.7 – Hydrology and Water Quality <ul style="list-style-type: none"> - Figure 5.7-1 – Storm Water Flows and Major Drainage Facilities - Figure 5.7-2 – Groundwater Basins 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code <ul style="list-style-type: none"> • Section 9.10.080 – Liquid and Solid Waste 4. Moreno Valley Municipal Code Chapter 8.12 – Flood Damage Prevention 5. Moreno Valley Municipal Code Chapter 8.21 – Grading Regulations 6. Eastern Municipal Water District (EMWD) Groundwater Reliability Plus, http://gwrplus.org/ 7. Eastern Municipal Water District (EMWD) 2015 Urban Water Management Plan 				

XI. LAND USE AND PLANNING – Would the project:

a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Response:
No Impact. The project proposes the development of a service station and drive through restaurant on land with a General Plan designation of Residential /Office and a zoning designation of Office Commercial (OC) District. The proposed uses are not allowed under the existing zoning designation of the site; therefore, the project includes a General Plan Amendment to designate the project site Commercial and a Change of Zone to Community Commercial (CC) District. Surrounding land uses include single-family residential uses adjacent to the north and east, single-family residential and commercial uses to the west across Perris Boulevard, and single-family residential uses and vacant property to the south across Dracaea Avenue. Commercial uses including a service station and convenience store are located across Perris Boulevard to the west and southwest, and single-family residences and commercial uses are located to the northwest (refer to Figures 1 and 2). The project is proposed on vacant property at the northeast corner of Perris Boulevard and Dracaea Avenue and would not introduce features such as highways or transit lines that would divide an established community. **No impact** regarding dividing an established neighborhood would occur. No mitigation is required.

b) Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less than Significant Impact. As previously discussed, the proposed project includes processing a General Plan Amendment to the land use designation of the site from Residential/Office to Commercial and a Change of Zone from Office Commercial (OC) District to Community Commercial (CC) District, so the proposed uses would be compatible with City zoning regulations. Pursuant to CEQA, policy conflicts do not in and of themselves constitute a significant environmental impact. Policy conflicts are considered to be environmental impacts only when they would result in direct physical impacts or where those conflicts relate to avoiding or mitigating environmental impacts. As such, this Initial Study analyzes associated physical environmental impacts that could result from development of the project site as proposed (Commercial) under each topical section. The Initial Study evaluates those impacts against the baseline condition (vacant undeveloped site) where applicable (e.g., noise, air quality) and against the previously anticipated Residential/Office use per the General Plan (e.g., population and housing, growth

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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inducement). As indicated throughout this Initial Study, the proposed project would not result in any direct physical impacts that cannot be mitigated to a less-than-significant level, as described throughout this Initial Study. Therefore, this impact would be **less than significant**. No additional mitigation is required.

- Sources:**
1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 2 – Community Development Element – Section 2.1 – Land Use
 - Figure 2-1 – Neighboring Lands Uses
 - Figure 2-2 – Land Use Map
 - Chapter 8 – 2014 – 2021 Housing Element
 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.12 – Population and Housing
 - Attachments #1 - #10 – Housing Sites Inventory
 - Exhibits A1 – A11, C, D, and E – Maps of Housing Sites
 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code

XII. MINERAL RESOURCES – Would the project:

a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Response:
No Impact. The Project site is vacant and undeveloped. As described in the City’s General Plan Final Program EIR, no regionally or statewide significant mineral resources are located within the City’s planning area.³⁹ The mineral resources known to occur within the City of Moreno Valley’s Sphere of Influence include sand, gravel and rock which is used to make concrete and as road base.⁴⁰ Due to the absence of significant mineral resources within or near the project site, **no impact** would occur. Mitigation is not required.

- Sources:**
1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 7 – Conservation Element – Section 7.9 – Mineral Resources
 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.14 – Mineral Resources
 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code
 - Section 9.02.120 – Surface Mining Permits
 4. Moreno Valley Municipal Code Section 8.21.020 A 7 – Permits Required
 5. The Surface Mining and Reclamation Act of 1975 (SMARA, Public Resources Code, Sections 2710-2796), <https://www.conservation.ca.gov/dmr/lawsandregulations>
 - 6.

XIII. NOISE – Would the project result in:

a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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Response:

³⁹ City of Moreno Valley. *Moreno Valley General Plan Final Program EIR*. Chapter 5.14 – Mineral Resources. July 11 2006.
⁴⁰ City of Moreno Valley. *City of Moreno Valley General Plan*. Chapter 7 – Conservation, 7.8 Mineral Resources. July 11, 2006.

ISSUES & SUPPORTING INFORMATION SOURCES:

Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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A detailed assessment of noise impacts for the proposed project based on the project development characteristics is included in Appendix D.⁴¹

Less than Significant with Mitigation Incorporated

Short-Term Noise. Two types of short-term noise impacts would occur during construction on the project site. First, construction crew commutes and the transport of construction equipment to the project site would incrementally increase noise levels on access roads leading to the site. There would be a relatively high single-event noise exposure potential causing intermittent noise from large trucks passing at 50 feet that would generate up to a maximum of 84 dBA. The grading phase would generate the highest daily construction vehicle trips based on the CalEEMod output, shown in Attachment C of Appendix A. Project construction vehicle trips associated with construction crew commute trips and transport of construction equipment are estimated to reach up to 26 vehicles per hour or 201 vehicles per day (roundtrip) during the grading construction phase. Perris Boulevard and Dracaea Avenue would be used to access the project site, which has estimated existing hourly/daily traffic volumes of 2,958/29,577 and 491/4,912, respectively. Construction-related traffic would increase hourly traffic noise levels by up to 0.2 dBA and would not increase daily traffic noise levels along Perris Boulevard and Dracaea Avenue. A noise level increase of less than 3 dBA would not be perceptible to the human ear in an outdoor environment. Therefore, no noise impacts from short-term construction-related traffic associated with worker commutes and equipment transport to the project site would occur.

The second type of short-term noise impact is related to noise generated during construction activities on the project site. Construction is completed in discrete steps, each of which has its own mix of equipment, and consequently, its own noise characteristics. These various sequential phases would change the character of noise generated on the project site. Therefore, the noise levels vary as construction progresses. Despite the variety in the type and size of construction equipment, similarities in the dominant noise sources and patterns of operation allow construction-related noise ranges to be categorized by work phase. Table 13.A lists the typical construction equipment noise levels (L_{max}) recommended for noise impact assessments, based on a distance of 50 feet between the equipment and a noise receptor.

Typical noise levels range up to 88 dBA L_{max} at 50 feet during the noisiest construction phases. The site preparation phase, which includes excavation and grading of the site, tends to generate the highest noise levels because the noisiest construction equipment is earthmoving equipment. Earthmoving equipment includes excavating machinery such as backfillers, bulldozers, draglines, and front loaders. Earthmoving and compacting equipment includes compactors, scrapers, and graders.

Table 13.A: Typical Construction Equipment Noise Levels

Equipment Description	Acoustical Usage Factor (%)	Maximum Noise Level (L _{max} at 50 feet)
Concrete/Industrial Saw	20	90
Dozers	40	85
Dump Trucks	40	84
Excavators	40	85
Flat Bed Trucks	40	84
Graders	40	85
Front-End Loaders	40	80
Man Lifts	20	85
Pickup Trucks	40	55
Scrapers	40	85

Source: Table I, LSA, September 2020 (Appendix D).

Site preparation is expected to require the use of bulldozers, graders, and water trucks/pickup trucks. Noise associated with the use of construction equipment is estimated to be between 55 and 85 dBA L_{max} at a distance of 50 feet from the active construction area for the site preparation phase. As shown in

⁴¹ LSA Associates, Inc. *Noise and Vibration Impact Analysis Memorandum for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California.* September 2020. (Appendix D).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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Table 13.A, the maximum noise level generated by each grader and bulldozer is assumed to be approximately 85 dBA L_{max} at 50 feet. The maximum noise level generated by water trucks/pickup trucks is approximately 55 dBA L_{max} at 50 feet from these vehicles. Each doubling of the sound sources with equal strength increases the noise level by 3 dBA. Assuming that each piece of construction equipment operates at some distance from the other equipment, the worst-case combined noise level during this phase of construction would be 88 dBA L_{max} at a distance of 50 feet from the active construction area. Based on a usage factor of 40 percent, the worst-case combined noise level during this phase of construction would be 84 dBA L_{eq} at a distance of 50 feet from the active construction area.

The closest residential property lines are located within 50 feet immediately adjacent to the northern and eastern project construction boundary and would be subject to short-term construction noise levels of 88 dBA L_{max} (84 dBA L_{eq}) or higher when construction occurs at the project construction boundary. Noise generated by project construction activities would result in a temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project. Compliance with the construction hours specified in the City’s Municipal Code Section 8.21.050(O) and Section 11.80.030(D)(7) and implementation of **Mitigation Measure NOI-1** would reduce short-term construction noise impacts to **less than significant with mitigation incorporated**.

Mitigation Measure NOI-1: Prior to the issuance of a grading permit and during all phases of construction, the project applicant shall ensure the following measures are incorporated:

- The construction contractor shall limit all grading-related activities, including operation of grading equipment, to between the hours of 7:00 a.m. and 7:00 p.m. Monday through Friday and between the hours of 8:00 a.m. and 4:00 p.m. on Saturday in accordance with Moreno Valley Municipal Code Section 8.21.050(O).
- The construction contractor shall limit the operation of any tools or equipment used in construction, drilling, repair, alteration or demolition work, to between the hours of 7:00 a.m. and 8:00 p.m. every day in accordance with Moreno Valley Municipal Code Section 11.80.030(D)(7).
- During all project site excavation and grading, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers consistent with manufacturers’ standards.
- The project contractor shall place all stationary construction equipment so that emitted noise is directed away from sensitive receptors nearest the project site.
- The construction contractor shall locate equipment staging in areas that would create the greatest distance between construction-related noise sources and noise-sensitive receptors nearest the project site during all project construction.

Long-Term Noise. Traffic Noise: The FHWA Highway Traffic Noise Prediction Model (FHWA-RD-77 108) was used to evaluate traffic-related noise conditions along roadway segments in the project vicinity. This model requires various parameters, including traffic volumes, vehicle mix, vehicle speed, and roadway geometry, to compute typical equivalent noise levels during daytime, evening, and nighttime hours. The resultant noise levels are weighted and summed over 24-hour periods to determine the CNEL values. The existing, 2022 opening year, and 2040 (General Plan Buildout) without and with project ADT volumes were obtain from the Transportation Impact Analysis (included in Appendix E). The standard vehicle mix for Southern California roadways was used for traffic on these roadway segments. Tables L, M, and N in Appendix D provide the traffic noise levels for the existing, 2022 opening year, and 2040 (General Plan Buildout) without and with project, respectively. These noise levels represent the worst-case scenario, which assumes that no shielding is provided between traffic and the location where the noise contours are drawn. Attachment C in Appendix D provides the specific assumptions used in developing these noise levels and model printouts.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

ISSUES & SUPPORTING INFORMATION SOURCES:

Potentially Significant Impact

Less Than Significant with Mitigation Incorporated

Less Than Significant Impact

No Impact

Tables L, M, and N in Appendix D show that the project-related traffic noise increase would be up to 0.2 dBA along Perris Boulevard, 1.6 dBA along Dracaea Avenue, and 13.2 dBA along Sunset Lane. It should be noted that traffic noise on Sunset Lane would increase due to access to the project from Sunset Lane. However, the overall traffic noise increase for the residences along Sunset Lane would be 0.2 dBA when traffic noise on Perris Boulevard is factored in.⁴² Noise level increases less than 3 dBA would not be perceptible to the human ear in an outdoor environment. Therefore, the proposed project would have a **less than significant impact** related to traffic noise from project-related traffic on off-site sensitive receptors. No noise reduction measures are required.

Truck Deliveries and Truck Loading and Unloading Activities: Section 11.80.030(C) of the City Municipal Code indicates no person shall maintain, create, operate or cause to be operated on private property any source of sound in such a manner as to create any non-impulsive sound which exceeds [the City’s exterior daytime and nighttime noise standard of 65 dBA Leq and 60 dBA Leq] when measured at a distance of 200 feet or more from the real property line of the source of the sound. Because sensitive receptors may occur closer than 200 feet from the real property line of the noise source, this analysis includes assessment of noise impacts also based on equivalent exterior daytime and nighttime noise standards to account for actual distances between the noise source and the sensitive receptor. For example, while the City’s exterior daytime and nighttime noise standard is respectively 65 dBA Leq and 60 dBA Leq when measured at a distance of 200 feet or more from the real property line of the sound source, the equivalent exterior daytime and nighttime noise standard at 100 feet from the real property line of the sound source would be respectively 71 dBA Leq and 66 dBA Leq because sound levels increase approximately 6 dB for each halving of distance from the source.⁴³

Noise levels generated by delivery trucks would be similar to noise readings from truck loading and unloading activities, which generate a noise level of 65 dBA Leq at 50 feet.⁴⁴ Although a typical truck unloading process takes an average of 15–20 minutes, this maximum noise level occurs in a much shorter period of time (less than 5 minutes). The closest residential properties to the anticipated truck delivery and loading/unloading areas are located as close as 100 feet to the east and north. As Table 13.B shows, noise levels generated by truck delivery and unloading/unloading activities would be 53 dBA Leq at 200 feet from the source and 59 dBA Leq at 100 feet from the source. Therefore, truck delivery and loading/unloading activities would not exceed the City’s exterior daytime and nighttime noise standard of 65 dBA Leq and 60 dBA Leq when measured at a distance of 200 feet or equivalent standard of 71 dBA Leq and 66 dBA Leq when measured at a distance of 100 feet. Therefore, the project would have a **less-than-significant impact** related to truck deliveries and truck loading/unloading activities on off-site sensitive receptors. No noise reduction measures are required.

Table 13.B: Truck Delivery and Unloading Activities

Activity	Reference Noise Level (dBA Leq)	Reference Distance (feet)	Noise Attenuation at 100 feet / 200 feet (dBA) ¹	Noise Level at 100 feet / 200 feet (dBA Leq)
Truck Delivery and Unloading Activities	65	50	6 / 12	59 / 53

Source: Table O, LSA, September 2020 (Appendix D).

¹ Sound levels decrease approximately 6 dB for each doubling of distance from the source. California Department of Transportation. *Technical Noise Supplement to the Traffic Noise Analysis Protocol*. Pages 2-27 and 2-28. September 2013.

HVAC Equipment: The project would construct a new convenience store and a restaurant with a drive-through lane that would have up to four rooftop HVAC units on each building. The HVAC equipment could potentially operate 24 hours per day. Rooftop HVAC equipment would generate noise levels of 66.5 dBA Leq at 5 feet. Four rooftop HVAC units operating together would generate a noise level of 73 dBA Leq at 5 feet.⁴⁵ The closest residential properties to the HVAC equipment are located as close as 45 feet to the east and north. As Table 13.C shows, noise levels generated by four HVAC units operating

⁴² Perris Boulevard is parallel to Sunset Lane approximately 70 feet to the west. The detailed noise calculations are provided in Attachment D of Appendix D.
⁴³ California Department of Transportation. *Technical Noise Supplement to the Traffic Noise Analysis Protocol*. Pages 2-27 and 2-28. September 2013.
⁴⁴ LSA Associates, Inc. *Noise and Vibration Impact Analysis Memorandum for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California*. Page 19 and Table O. September 2020.
⁴⁵ *Ibid.* Page 19 and Table P.

ISSUES & SUPPORTING INFORMATION SOURCES:

Potentially Significant Impact

Less Than Significant with Mitigation Incorporated

Less Than Significant Impact

No Impact

simultaneously would be 41 dBA L_{eq} at 200 feet from the source and 54 dBA L_{eq} at 45 feet from the source. Therefore, operation of HVAC equipment would not exceed the City's exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} when measured at a distance of 200 feet or equivalent standard of 78 dBA L_{eq} and 73 dBA L_{eq} when measured at a distance of 45 feet. Therefore, the project would have a **less-than-significant impact** related to HVAC equipment on off-site sensitive receptors. No noise reduction measures are required.

Table 13.C: HVAC Equipment

Activity	Number of HVAC units	Reference Noise Level (dBA L_{eq})	Reference Distance (feet)	Noise Attenuation at 100 feet / 200 feet (dBA) ¹	Noise Level at 100 feet / 200 feet (dBA L_{eq})
HVAC Equipment	4	73	5	19 / 32	54 / 41

Source: Table Q, LSA, September 2020 (Appendix D).

¹ Sound levels decrease approximately 6 dB for each doubling of distance from the source. California Department of Transportation. *Technical Noise Supplement to the Traffic Noise Analysis Protocol*. Pages 2-27 and 2-28. September 2013.

Drive Through Menu Board Speaker Noise: The project would construct a restaurant with a drive-through speakerphone that is part of the menu board. Although Section 9.09.080(C)(1) of the City's Municipal Code limits the hours of drive through operation to between the hours of 6:00 a.m. and 10:00 p.m. when located on a site adjacent to any residentially zoned property, Section 9.09.020 allows the decision-making body discretion to allow an alternative that "adequately protects the public health, safety and welfare." There is potential for the proposed drive through restaurant to operate 24 hours per day, so this analysis considers menu board speaker noise during both daytime and nighttime hours.

Section 9.09.080(C)(6) of the Code indicates the drive through menu board speaker system shall be designed to compensate for ambient noise levels in the immediate area and shall not be located within 100 feet of any property used for residential uses. Noise generated from drive through menu board speakers is approximately 81 dBA at 1 foot.⁴⁶ The closest residential properties to the menu board speaker are located 100 feet to the east and north. As Table 13.D shows, noise levels generated by the menu board speaker would be 35 dBA L_{eq} at 200 feet from the source and 41 dBA L_{eq} at 100 feet from the source. Therefore the drive through menu board speaker system would not exceed the City's exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} when measured at a distance of 200 feet or equivalent standard of 71 dBA L_{eq} and 66 dBA L_{eq} when measured at a distance of 100 feet. Therefore, operation of the drive through menu board speaker 24 hours per day would have a **less than significant impact** related to noise on off-site sensitive receptors. Mitigation is not required.

Table 13.D: Drive Through Menu Board Speaker Noise

Activity	Reference Noise Level (dBA L_{eq})	Reference Distance (feet)	Noise Attenuation at 100 feet / 200 feet (dBA) ¹	Noise Level at 100 feet / 200 feet (dBA L_{eq})
Drive Through Menu Board Speaker Operation	81	1	40 / 46	41 / 35

Source: Table Q, LSA, September 2020 (Appendix D).

¹ Sound levels decrease approximately 6 dB for each doubling of distance from the source. California Department of Transportation. *Technical Noise Supplement to the Traffic Noise Analysis Protocol*. Pages 2-27 and 2-28. September 2013.

Parking Lot Activities: The project would include parking areas on the project site. Noise generated from parking activities includes vehicles traveling at slow speeds, engine start-up noise, car door slams, car horns, car alarms, and tire squeals. Representative parking activities would generate approximately 60 to 70 dBA L_{max} at 50 feet.⁴⁷ The closest residential properties to the project parking lot are located 25 feet to the east and north. As Table 13.E shows, noise levels generated by parking lot activities would be 58 dBA L_{max} at 200 feet from the source and 76 dBA L_{max} at 25 feet from the source. Although parking activities generate relatively high maximum instantaneous noise levels (L_{max}), the equivalent continuous sound level (L_{eq}) would be much lower over an hour period. Therefore, noise generated from parking lot

⁴⁶ *Ibid.* Page 20 and Table Q.

⁴⁷ *Ibid.* Page 21 and Table R.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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activities would not exceed the City’s exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} when measured at a distance of 200 feet or equivalent standard of 83 dBA L_{eq} and 78 dBA L_{eq} when measured at a distance of 100 feet. Impacts from parking lot activities would be **less than significant**. Mitigation is not required.

Table 13.E: Parking Lot Activities

Activity	Reference Noise Level (dBA L_{max})	Reference Distance (feet)	Noise Attenuation at 25 feet / 200 feet (dBA) ¹	Noise Level at 25 feet / 200 feet (dBA L_{eq})
Parking Lot Activities	70	50	-6 / 46	76 / 58

Source: Table S, LSA, September 2020 (Appendix D).
¹ Sound levels decrease approximately 6 dB for each doubling of distance from the source. California Department of Transportation. *Technical Noise Supplement to the Traffic Noise Analysis Protocol*. Pages 2-27 and 2-28. September 2013.

With implementation of **Mitigation Measure NOI-1**, short-term construction noise impacts would be reduced to **less than significant with mitigation incorporated**. The project would not increase long-term ambient noise in excess of the City’s exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} when measured at a distance of 200 feet or more from the real property line of the source of the sound or the equivalent noise standard when measured at the nearest sensitive receptors. Therefore, long-term noise would be **less than significant**, and mitigation is not required.

b) Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant with Mitigation Incorporated. This construction vibration impact analysis discusses the level of human annoyance using vibration levels in VdB and would assess the potential for building damage using peak particle velocity (PPV), which is measured in inches per second (in/sec). Vibration thresholds for significance are established by the City under Section 9.10.170 of the Municipal Code and by the Federal Transit Administration (FTA) *Transit Noise and Vibration Impact Assessment Manual*.⁴⁸

Section 9.10.170 of the Municipal Code prohibits vibration that can be felt at or beyond the property line. However, construction activity is exempt from Section 9.10.170 pursuant to Section 9.10.030, which states temporary construction, maintenance, or demolition activities between the hours of 7:00 a.m. and 7:00 p.m. are exempt from the provisions of Chapter 9.10 (Performance Standards) of the City Municipal Code. Implementation of **Mitigation Measure NOI-1** would ensure construction activities are restricted to within the permissible hours of operation in accordance with Municipal Code Section 8.21.050(O) and Section 9.10.030.

As shown in Table 13.F, the FTA guidelines indicate that a vibration level up to 102 VdB (equivalent to 0.5 PPV [in/sec]) is considered safe for buildings consisting of reinforced concrete, steel, or timber (no plaster), and would not result in any construction vibration damage. For a non-engineered timber and masonry building, the construction vibration damage criterion is 94 VdB (0.2 PPV [in/sec]). For a fragile building, the construction vibration damage criterion is 90 VdB (0.12 PPV [in/sec]).

Table 13.F shows the PPV and VdB values at a distance of 25 feet from the construction vibration source. As shown in Table 13.F, large bulldozers and other heavy-tracked construction equipment (except for pile drivers and vibratory rollers) generate approximately 87 VdB of ground-borne vibration when measured at a distance of 25 feet, based on the Transit Noise and Vibration Impact Assessment Manual. Project construction is expected to use a small bulldozer, large bulldozer, and a loaded truck, which would generate 58 VdB (0.003 PPV [in/sec]), 87 VdB (0.089 PPV [in/sec]), and 86 VdB (0.076 PPV [in/sec]) at 25 feet, respectively. The greatest levels of vibration are anticipated to occur during the site preparation and grading phase. All other phases are expected to result in lower vibration levels. The distance to the nearest buildings for vibration impact analysis is measured between the nearest off-site buildings and the project boundary (assuming the construction equipment would be used at or near the project boundary) because vibration impacts occur normally within buildings. An exception to this would

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

⁴⁸ Federal Transit Administration. *Transit Noise and Vibration Impact Assessment Manual*. September 2018.

ISSUES & SUPPORTING INFORMATION SOURCES:

Potentially Significant Impact

Less Than Significant with Mitigation Incorporated

Less Than Significant Impact

No Impact

be the location of loaded trucks because they would be limited to a certain areas on the project site and would not operate at the project construction boundary.

Table 13.F: Vibration Source Amplitudes for Construction Equipment

Equipment	Reference PPV/L _v at 25 feet	
	PPV (in/sec)	L _v (VdB)
Hoe Ram	0.089	87
Large Bulldozer	0.089	87
Caisson Drilling	0.089	87
Loaded Trucks	0.076	86
Jackhammer	0.035	79
Small Bulldozer	0.003	58

Sources: Table J, LSA, September 2020 (Appendix D).

ft = feet

L_v = velocity in decibels

VdB = vibration velocity decibels

in/sec = inches per second

PPV = peak particle velocity

The formula for vibration transmission is provided below.

$$L_{vD} (D) = L_{vD} (25 \text{ feet}) - 30 \text{ Log} (D/25)$$

$$PPV_{\text{equip}} = PPV_{\text{ref}} \times (25/D)^{1.5}$$

Table 13.G lists the projected vibration levels from various construction equipment expected to be used on the project site to the nearest buildings in the project vicinity. As shown in Table 13.G, residential structures located east of the project would experience the highest construction vibration levels of up to 94 VdB (0.191 PPV [in/sec]). This vibration level would have the potential to result in community annoyance and because vibration levels would exceed the FTA's community annoyance threshold of 78 VdB for residential uses. However, this vibration level would not have the potential to damage residential structures because vibration levels would not exceed the FTA's damage threshold of 94 VdB (0.2 PPV [in/sec]) for residential structures constructed of non-engineered timber and masonry. Other residential and commercial structures in the project area shown in Table 13.G would experience lower vibration levels due to either the use of a small bulldozer or greater distance from the project construction boundary.

Table 13.G: Summary of Construction Equipment and Activity Vibration

Direction	Equipment/Activity	Reference Vibration Level (VdB) at 25 feet	Reference Vibration Level (PPV) at 25 feet	Distance (feet)	Maximum Vibration Level (VdB)	Maximum Vibration Level (PPV)
North	Small Bulldozer	58	0.003	8	73	0.017
	Loaded Trucks	86	0.076	15	93	0.164
East	Large Bulldozer	87	0.089	15	94	0.191
	Loaded Trucks	86	0.076	15	93	0.164
Southwest	Small Bulldozer	58	0.003	8	73	0.017
	Loaded Trucks	86	0.076	15	93	0.164
West	Large Bulldozer	87	0.089	130	66	0.008
	Loaded Trucks	86	0.076	130	65	0.006

Source: Table K, LSA, September 2020 (Appendix D).

Note: The FTA-recommended building damage threshold is 0.2 PPV (in/sec) or approximately 94 VdB at the receiving residential/church structure and 0.3 PPV (in/sec) or approximately 98 VdB at the receiving commercial structure.

ft = foot/feet

in/sec = inch/inches per second

FTA = Federal Transit Administration

PPV = peak particle velocity

VdB = vibration velocity decibels

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>Implementation of Mitigation Measure NOI-1 would exempt any vibration that could be felt off-site during construction. However, there is potential for damage to buildings because the use of both small and large bulldozers on the project site would have the potential to exceed the FTA’s damage thresholds at the north and southeast borders of the site. Implementation of Mitigation Measure NOI-2 would ensure that this impact would be reduced to less than significant with mitigation incorporated.</p> <p>Mitigation Measure NOI-2: The project applicant shall ensure that only small bulldozers are used within 15 feet of the residential structures immediately north and southeast of the project site to ensure vibration levels would not exceed the Federal Transit Administration’s damage threshold of 94 velocity decibels (VdB) (0.2 peak particle velocity (PPV) [in/sec]) for buildings constructed of non-engineered timber and masonry. The project Applicant shall provide evidence to the City that this measure is incorporated into the project grading plans, and directional signage is placed on the construction site to direct equipment operators. This measure shall be implemented to the satisfaction of the Director of Building and Safety or designee.</p> <p>The proposed project would not generate vibration during operation. According to the FTA <i>Transit Noise and Vibration Impact Assessment Manual</i>, vibration levels generated from project-related traffic on the adjacent roadways (Perris Boulevard, Dracaea Avenue, and local roadways leading to the project site) are unusual for on-road vehicles because the rubber tires and suspension systems of on-road vehicles provide vibration isolation.⁴⁹ Since the project consists of typical neighborhood-serving commercial uses (i.e., convenience store, service station, and restaurant) and includes new on-site drive aisles and roadway improvements along the project site frontage, vibration generated from operation of the project, including project-related traffic on the adjacent paved roadways, would be less than significant. The Project would not conflict with Section 9.10.170 of the Municipal Code, which prohibits vibration that can be felt at or beyond the property line, and no vibration reduction measures are required during operation.</p>				
<p>c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?</p>	□	□	□	☒
<p>Response: <i>No Impact.</i> The proposed project is approximately 2.75 miles from the nearest airport (March Air Reserve Base). Based on the Riverside County Airport Land Use Compatibility Plan, the project is located outside of the 60-dBA CNEL noise contour of the airport. In addition, there are no private airstrips or heliports within 2 miles of the project site. The project would not expose people residing or working in the project area to excessive noise levels from aircraft noise. Therefore, no impacts would occur related to aircraft noise. Mitigation is not required.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 6 – Safety Element – Section 6.4 – Noise <ul style="list-style-type: none"> - Figure 6-2 – Buildout Noise Contours 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.4 – Noise <ul style="list-style-type: none"> - Figure 5.4-1 – March Air Reserve Base Noise Impact Area - Figure 5.4-2 – Buildout Noise Contours – Alternative 1 - Figure 5.4-3 -- Buildout Noise Contours – Alternative 2 - Figure 5.4-4 -- Buildout Noise Contours – Alternative 3 • Appendix D – Noise Analysis, Wieland Associates, Inc., June 2003. 				

⁴⁹ Ibid. Page 112.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code <ul style="list-style-type: none"> • Section 9.10.140 Noise and Sound 4. Moreno Valley Municipal Code Chapter 11.80 Noise Regulations 5. March Air Reserve Base (MARB)/March Inland Port (MIP) Airport Land Use Compatibility Plan (ALUCP) on November 13, 2014, (http://www.rcaluc.org/Portals/13/17%20-%20Vol.%201%20March%20Air%20Reserve%20Base%20Final.pdf?ver=2016-08-15-145812-700)				

XIV. POPULATION AND HOUSING – Would the project:

a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less than Significant Impact. The project site is located within a corridor along Perris Boulevard that is planned for a mix of residential, office, and commercial development. Therefore, development of the site is planned by the City. The extent to which new jobs created by a project are filled by existing residents is a factor that tends to reduce the growth-inducing effect of a project. Construction of the project would create short-term construction jobs; however, these short-term positions are anticipated to be filled by workers who, for the most part, already reside in or near the City. Therefore, construction of the project would not generate a permanent increase in population within the project area.

The Applicant indicates the project is expected to generate up to 39 employees, up to 27 for the restaurant and up to 12 for the convenience store/service station.⁵⁰ When compared to the existing planned use of the site for residential/office uses, the proposed changes in the land use designation to commercial would not substantially alter the housing and employment forecast in the City. Additionally, the project does not include a residential component, which would otherwise generate a permanent increase in population. Finally, streets, utilities, and municipal services such as police and fire services currently exist in the project area to serve the site. Therefore, the project would not induce a substantial unplanned population growth in the area. Impacts would be **less than significant**. Mitigation is not required.

b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Response:
No Impact. The project site is vacant and does not contain any residences that would be removed due to development of the proposed project. Therefore, the project would not displace a substantial number of existing people or housing, requiring the construction of replacement of housing elsewhere. **No impact** would occur, and no mitigation is required.

Sources:

1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 2 – Community Development Element – Section 2.1 – Land Use
 - Figure 2-1 – Neighboring Lands Uses
 - Figure 2-2 – Land Use Map
 - Chapter 8 – 2014 – 2021 Housing Element
2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.12 – Population and Housing
 - Attachments #1 - #10 – Housing Sites Inventory
 - Exhibits A1 – A11, C, D, and E – Maps of Housing Sites
3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code

⁵⁰ The restaurant and convenience store/gas station are assumed to operate 24 hours per day, so the 39 employees would be divided in to three 8-hour shifts, with approximately 13 employees on site at a time (9 at the restaurant and 4 at the convenience store).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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XV. PUBLIC SERVICES – Would the project:

a) Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i) Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Response:
Less Than Significant Impact. The City of Moreno Valley contracts with the Riverside County Fire Department to provide fire protection, fire prevention, and emergency services within the City. The Moreno Valley Fire Department is administered and operated by the California Department of Forestry and Fire Protection/Riverside County Fire Department’s regional, integrated, cooperative fire protection organization. The Sunnymead Fire Station 2, located at 24935 Hemlock Road, is the closest station to the site approximately 1 mile to the north. According to the City’s General Plan Final Program EIR, Fire Station 2 houses one fire engine, one 100-foot ladder truck, and one rescue squad.⁵¹

As previously stated, the project site is not located within an identified Fire Hazard Severity Zone. Although the proposed project would introduce new structures to a vacant site, the project site is located in an area that is already served by existing fire protection services. Furthermore, the project would be required to adhere to City standards requiring the Applicant to provide evidence of an on-site fire hydrant system to the Fire Chief to ensure adequate fire flow for use by the Riverside County Fire Department in the event of an on-site fire.

The project site is located within a corridor along Perris Boulevard that is planned for a mix of residential, office, and commercial development. The proposed project would comply with 2019 California Building Code and Fire Code standards, including the provision for fire sprinklers, ignition-resistant design and materials, and emergency access. The site is located within the existing service area of Fire Station 2, and current levels of fire service provided to the project site would be maintained following project implementation. Therefore, the proposed project would result in a **less than significant impact** related to fire protection services and would not necessitate new fire protection facilities. No mitigation is required.

ii) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. The City of Moreno Valley contracts with the Riverside County Sheriff’s Department to provide police protection services within the City. The Moreno Valley Police Department headquarters is located in the Public Safety Building at 22850 Calle San Juan de Los Lagos in the City’s Civic Center, but there are also satellite offices in strategic locations throughout the City that provide a place for officers to write reports, make phone calls, and tend to other responsibilities without leaving the field. The project would develop a drive-through restaurant and convenience store with service station that could generate 39 new employees in the City, up to 27 for the restaurant and up to 12 for the convenience store/service station. The new commercial uses would incrementally increase the need for police services in the area.

The Project would incorporate Crime Prevention Through Environmental Design (CPTED) features to keep service demand increases to a minimum. For example, the Project would incorporate public zones and private zones via physical and symbolic barriers to define acceptable uses of the commercial facilities and determine who has a right to occupy such zones. Additionally, the Project site would be equipped with formal surveillance through the use of closed-circuit television, electronic monitoring, and potentially security patrols, as well as informal surveillance such as architecture, landscaping, and lighting designed to minimize visual obstacles and eliminate places of concealment for potential assailants.

The City monitors staffing levels to ensure that adequate police protection and response times continue to be provided as individual development projects are proposed and on an annual basis as part of the

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

⁵¹ City of Moreno Valley. *Moreno Valley General Plan Final Program EIR*. Chapter 5.13 – Public Services and Utilities. July 2006.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>City Council’s budgeting process. The continual monitoring of police staffing levels by the City would ensure the proposed Project would not result in a significant reduction in police response times.</p> <p>Funding for new police facilities commensurate with the increased demand for services in the City would be provided from capital improvement fees levied on new development. These development impact fees (DIFs) are one-time charges applied to new development and are imposed to raise revenue for the construction or expansion of capital facilities such as police stations located outside of the boundaries of a new development that benefit the area. DIFs enable the City to collect fair-share fees from new development projects to fund new infrastructure and services, including police services. DIFs are collected for specific infrastructure needs and are deposited into different accounts representing these requirements.</p> <p>The Project would be designed and operated per applicable standards required by the City for new development with regard to public safety. The Project would be required to pay DIFs used to fund capital costs associated with constructing new public safety structures and purchasing equipment for new public safety structures. In addition, the City maintains mutual aid agreements with police agencies in the surrounding cities (e.g., Riverside and Perris) and with the Riverside County Sheriff’s Department, which allow for the services of nearby police departments to assist the Moreno Valley Police Department during major emergencies. Payment of DIFs commensurate with the increased demand for services in the City would offset any increase in demand for police services.</p> <p>The project site is located in an area that is already served by police protection services. Because the project site is located within the existing service area of the Moreno Valley Police Department, the proposed project constructed in accordance with applicable policies designed to minimize crime (e.g., CPTED) would not require new or physically altered police protection facilities, the construction of which could cause significant environmental effects. Therefore, impacts would be less than significant and mitigation is not required.</p>				
<p>iii) Schools?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Response: <i>No Impact.</i> The Project does not include housing; therefore, no increase in the number of school-age students is expected. California Government Code (Section 65995[b]) establishes the base amount of allowable developer fees imposed by school districts. These base amounts are commonly referred to as “Level 1 fees” and are subject to inflation adjustment every two years. School districts are placed into a specific “level” based on school impact fee amounts that are imposed on the development. With the adoption of Senate Bill 50 and Proposition 1A in 1998, schools meeting certain criteria can now adopt Level 2 and 3 developer fees. The amount of fees that can be charged over the Level 1 amount is determined by the district’s total facilities needs and the availability of State matching funds. If there is State facility funding available, districts are able to charge fees equal to 50 percent of their total facility costs, termed “Level 2” fees. If, however, there are no State funds available, “Level 3” fees may be imposed for the full cost of their facility needs.⁵²</p> <p>Per California Government Code, “The payment or satisfaction of a fee, charge, or other requirement levied or imposed ... are hereby deemed to be full and complete mitigation of the impacts ... on the provision of adequate school facilities.” The project Applicant would be required to pay these development fees in accordance with Government Code 65995 and Education Code 17620. Through payment of development fees, no impacts related to school services would occur. Mitigation is not required.</p>				
<p>iv) Parks?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The project does not include a residential component and would not contribute to a direct increase in population. The 39 new employees anticipated by the Project are expected to reside generally in the vicinity of the site. As there is no direct increase in population anticipated from the proposed project, demand on existing park facilities is expected to be minimal.</p>				

⁵² California State Legislature, Legislative Analyst’s Office. *An Evaluation of the School Facility Fee Affordable Housing Assistance Programs*, January 2001. http://www.lao.ca.gov/2001/011701_school_facility_fee.html (accessed May 26, 2020).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Therefore, no new park facilities would be required to serve the project. Impacts would be less than significant , and mitigation is not required.				
v) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The project would not cause an increase in population resulting in a significant impact on other public facilities such as libraries and hospital services. The proposed project does not include a residential component, and the 39 new employees anticipated by the Project are expected to reside generally in the vicinity of the site. Therefore, the project would not contribute to a direct increase in population, and no significant increase in demand on library or medical facilities would occur. Therefore, no new public facilities would be required to serve the project. Impacts would be less than significant, and mitigation is not required.</p>				
<p>Sources:</p> <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 2 – Community Development Element – Section 2.5 – Schools <ul style="list-style-type: none"> - Figure 2-3 – School District Boundaries • Chapter 2 – Community Development Element – Section 2.6 – Library Services • Chapter 2 – Community Development Element – Section 2.7 – Special Districts • Chapter 2 – Community Development Element – Section 2.5 – Other City Facilities • Chapter 4 – Parks, Recreation and Open Space Element – Section 4.3 – Parks and Recreation <ul style="list-style-type: none"> - Figure 4-2 – Future Parklands Acquisition Areas - Figure 4-3 – Master Plan of Trails • Chapter 6 – Safety Element – Section 6.1 – Police Protection and Crime Preventions • Chapter 6 – Safety Element – Section 6.2 – Fire and Emergency Services <ul style="list-style-type: none"> - Figure 6-1 – Fire Stations 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.13 – Public Services <ul style="list-style-type: none"> - Figure 5.13-1 – Location of Public Facilities 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 				
XVI. RECREATION – Would the project:				
a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The project does not include a residential component and would not contribute to a direct increase in population. The 39 new employees anticipated by the Project are expected to reside generally in the vicinity of the site. Therefore, the project would not substantially increase demand on existing neighborhood or regional parks or on other recreational facilities so as to increase or accelerate their physical deterioration. Impacts would be less than significant, and mitigation is not required.</p>				
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> The proposed project does not include any recreational facilities or parkland. Furthermore, the project does not include any residential development and would not directly increase population in the project area. Therefore, the project would not require the construction or</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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expansion of recreational facilities. Impacts would be **less than significant**, and mitigation is not required.

Sources:

1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 4 – Parks, Recreation and Open Space Element – Section 4.3 – Parks and Recreation
 - Figure 4-1 Open Space
 - Figure 4-2 – Future Parklands Acquisition Areas
 - Figure 4-3 – Master Plan of Trails
2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.13 – Public Services
 - Figure 5.13-1 – Location of Public Facilities
3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code

XVII. TRANSPORTATION – Would the project:

a) Conflict with program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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Response:

Less than Significant with Mitigation Incorporated. The following is based on the Transportation Impact Analysis (TIA) prepared for the proposed project (Appendix E).⁵³

Transit, Pedestrian, and Bicycle Facilities. The project includes a drive-through restaurant and convenience store with service station. Due to the automobile-centered nature of the proposed project, it is not expected to generate significant transit, pedestrian, and bicycle trips internal or external to the site. The project would be required to include parking facilities for bicycles and accommodate bicycle routes along its roadway frontage. Additionally, Riverside Transit Agency maintains a bus stop for bus route 19 adjacent to the site along Perris Boulevard. The project includes relocation of the bus stop approximately 250 feet to the north of its existing location, farther away from the intersection of Perris Boulevard and Dracaea Avenue. The relocated bus stop would include a dedicated turnout for busses to pull off the roadway for safer service to the public (refer to Figure 3). Development of the site therefore would facilitate additional patronage of the bus stop by introducing employees and customers to the site and improving the safe operation of the bus stop adjacent to the site. The project would not preclude or otherwise adversely affect existing or proposed transit, pedestrian, or bicycle projects or policies identified by the City. Therefore, impacts from conflict with a program, plan, ordinance, or policy addressing transit, pedestrian, or bicycle facilities, would be **less than significant**.

Roadway Facilities. The California Court of Appeals, in its *Citizens for Positive Growth & Preservation v. City of Sacramento* decision, determined that automobile delay, as described solely by level of service (LOS), shall not be considered a significant impact on the environment. Therefore, the following discussion of potential LOS impacts is provided to demonstrate project compliance with the City General Plan as it pertains to the circulation system.

Access to the service station on the project site would be provided via three driveways; one from Perris Boulevard in the center of the site, one from Dracaea Avenue at the southern portion of the site, and one from Atwood Avenue approximately 320 feet north of the site. The internal drive aisle is designed to allow for semi-trucks delivering fuel to the site to enter from Perris Boulevard and exit the site to Dracaea Avenue. As detailed in Table 17.A, the project is expected to generate 3,471 daily trips with 324 trips occurring during the a.m. peak hour and 204 trips occurring during the p.m. peak hour (see Appendix E).

⁵³ LSA Associates, Inc. *Transportation Impact Analysis for the Perris/Dracaea Commercial Project*. September 2020. (Appendix E).

Table 17.A: Project Trip Generation

Land Use	Units	A.M. Peak Hour			P.M. Peak Hour			Daily Trips
		In	Out	Total	In	Out	Total	
Service Station with Convenience Store								
Trips/Unit	12 Vehicle Fueling Positions	6.36	6.11	12.47	7.13	6.86	13.99	205.36
Trip Generation		76	73	149	86	82	168	2,464
Pass-by Trips		(47)	(45)	(92)	(48)	(46)	(94)	(1,454)
Total Net Trips		29	28	57	38	36	74	1,010
Drive-Through Restaurant								
Trips/Unit	3,000 square feet	45.38	43.61	88.99	21.69	21.69	43.38	820.38
Trip Generation		136	131	267	65	65	130	2,461
Total Trip Generation		212	204	416	151	147	298	4,925
Pass-By Trips ¹		(47)	(45)	(92)	(48)	(46)	(94)	(1,454)
Total Net Trip Generation		165	159	324	103	101	204	3,471

Source: Table 5-A – Transportation Impact Analysis, LSA, September 2020 (Appendix E).

¹ “Pass-By” trips are intermediate stops at the project site en route to a destination without diverting from the main route and therefore are not actually “new” trips added to the surrounding circulation system.

The study area intersections and roadways were identified via the Scoping Agreement process. The study intersections are listed as follows:⁵⁴

- Perris Boulevard/Eucalyptus Avenue;
- Perris Boulevard/Atwood Avenue;
- Perris Boulevard/Auto Center Driveway – Project Driveway 1;
- Perris Boulevard/Dracaea Avenue;
- Perris Boulevard/Cottonwood Avenue;
- Project Driveway 2/Dracaea Avenue; and
- Sunset Lane/Atwood Avenue.

The roadway segments are listed as follows:

- Perris Boulevard, between Eucalyptus Avenue and Atwood Avenue;
- Perris Boulevard, between Atwood Avenue and Dracaea Avenue;
- Perris Boulevard, between Dracaea Avenue and Cottonwood Avenue; and
- Dracaea Avenue, between Perris Boulevard and Birchwood Drive.

Study intersections analyzed are under the jurisdiction of the City of Moreno Valley. The City uses both LOS C and LOS D as its minimum level of service criteria for intersections and roadway segments. As stated in the City’s General Plan and TIA Guidelines, LOS D is applicable to intersections and roadway segments adjacent to employment-generated land uses, while LOS C is applicable to all other areas. The City’s *Transportation Impact Analysis Guidelines* identify operational deficiencies and require improvements under the following conditions:

- Projects that result in intersections and roadway segments adjacent to employment-generated land uses to operate at LOS E or F, and in all other areas to operate at LOS D, E, or F;

or

- Projects that add 5 or more seconds of delay to an intersection that is already projected to operate without project traffic at unsatisfactory LOS;

and

- The intersection meets the peak hour traffic signal warrant after the addition of project traffic.

If the conditions above are satisfied, improvements should be identified that achieve LOS D or better for case a) above or to pre-project LOS and delay for case b) and c) above.

Furthermore, the City’s *Transportation Impact Analysis Guidelines* prescribe a roadway segment threshold in term of volume-to-capacity (V/C) whereby any roadway segment that operates at unacceptable LOS in the no project scenario where the project adds traffic in excess of 5 percent of the

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>roadway capacity (i.e., a V/C ratio increase of 0.05) must identify improvements to add capacity to the segment.</p> <p>The project proposes several Project Design Features (PDFs) that include select transportation facility improvements along the project frontage as detailed below:⁵⁵</p> <ul style="list-style-type: none"> Remove the existing two-way-left-turn-lane on Perris Boulevard, from south of Pedro’s Taco Shop Driveway to Dracaea Avenue; Replace the two-way-left-turn-lane with the raised median along the entire project frontage on Perris Boulevard extending north of the intersection of Perris Boulevard/Atwood Avenue up to south of the Pedro’s Taco Shop driveway; Create left-turn pockets in the raised median for the northbound and southbound movements at the intersection of Perris Boulevard/Atwood Avenue, with storage lengths of 150 feet and 125 feet, respectively; Extend the southbound left-turn pocket at the intersection of Perris Boulevard/Dracaea Avenue by 5 feet; and Add a bus bay on the east side of Perris Boulevard, north of the intersection of Perris Boulevard/Auto Center Driveway-Project Driveway 1. <p>Due to the proposed raised median, Auto Center Driveway will operate as a right-in-right-out driveway, and eastbound and westbound left-turn and through movements will be prohibited at the intersection of Perris Boulevard/Atwood Avenue under with project conditions.⁵⁶</p> <p>Existing Conditions Summary. As shown in Table 4-D in Appendix E, the intersection of Perris Boulevard/Atwood Avenue operates at an unsatisfactory LOS under existing conditions.⁵⁷ All other study intersections and roadway segments currently operate at a satisfactory LOS under existing without project conditions.</p> <p>Project Completion (2022) Conditions Summary. Traffic volumes for project completion without project conditions were developed by applying a 2 percent annual growth rate to the existing traffic volumes for all study intersections and adding trips from cumulative projects⁵⁸ in the area. As stated above, the project will include several PDFs, including a raised median along Perris Boulevard extending north of the intersection of Perris Boulevard/Atwood Avenue. The median will prevent eastbound and westbound left-turn and through movements at this intersection, and Auto Center Driveway will operate as a right-in-right-out driveway under with project conditions.</p> <p>As shown in Table 6-C and 6-D in Appendix E, the intersection of Perris Boulevard/Atwood Avenue and three roadway segments on Perris Boulevard would operate at unsatisfactory LOS under year 2022 without project conditions. However, the PDFs specified above would improve the LOS at the intersection of Perris Boulevard/Atwood Avenue to C or better in accordance with the City’s General Plan standard.⁵⁹ No further intersection improvement are required to ensure this intersection operates at satisfactory LOS; however, additional improvements are required to ensure adequate turn-pocket storage lengths (i.e., queuing), as specified below.</p> <p>Although the three roadway segments along Perris Boulevard would continue to operate at unsatisfactory LOS under project completion (2022) with project conditions, implementation of the project and associated PDFs would increase the V/C ratio by 0.03 or less at these roadway segments, so further improvements are not required.⁶⁰ All other study intersections and roadway segments are forecast to operate at a satisfactory LOS under project completion (2022) without and with project conditions.</p> <p>General Plan Build-Out (2040) Conditions Summary. As shown in Table 7-B and 7-C in Appendix E, the intersections of Perris Boulevard/Atwood Avenue (both a.m. and p.m. peak hours) and Perris Boulevard/Dracaea Avenue (p.m. peak hour only) and three roadway segments on Perris Boulevard</p>				

⁵⁴ *Ibid.*

⁵⁵ *Ibid.* Page 15 and Figures 4-3 and 4-4.

⁵⁶ *Ibid.*

⁵⁷ *Ibid.* Table 4-D - Existing Intersection Levels of Service.

⁵⁸ According to Appendix E, the cumulative projects are expected to generate 1,184 a.m. peak hour trips, 1,604 p.m. peak hour trips, and 20,032 daily trips.

⁵⁹ *Ibid.* Table 6-C - Project Completion (2022) Intersection Levels of Service.

⁶⁰ *Ibid.* Page 37 and Table 6-D - Project Completion (2022) Roadway Segment Levels of Service.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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would operate at unsatisfactory LOS under General Plan build-out conditions. Even with the proposed PDFs specified above, the Perris Boulevard/Dracaea Avenue (p.m. peak hour only) intersection is still forecast to operate at unsatisfactory LOS,⁶¹ and improvements must be implemented at this intersection under General Plan build-out with project conditions (refer to Table 17.B below) to ensure satisfactory LOS. Additional improvements at this intersection are required to ensure adequate turn-pocket storage lengths (i.e., queuing), as specified below.

Although the three roadway segments along Perris Boulevard would continue to operate at unsatisfactory LOS under General Plan build-out with project conditions, implementation of the project and associated PDFs would increase the V/C ratio by 0.03 or less at these roadway segments, so further improvements are not required.⁶²

Site Circulation Analysis. An analysis of fuel tanker-truck circulation on-site indicates fuel tanker-trucks would not interfere with the drive-through operations for any of the facilities or the overall site circulation since discharging fuel for the gas station would occur during the off-peak hours for the gas station. Truck turning templates were obtained from the Caltrans Design Manual Turn Templates. Large semitrailer templates were used to evaluate availability of adequate truck turning radii as a conservative estimate. The trucks would have adequate turning radii to ingress using the project driveway on Perris Boulevard and egress using the project driveway on Atwood Avenue. Additionally, adequate space is available on-site for fuel tanker-trucks to park during fuel discharge operations.

Queuing Analysis. An analysis of turn-pocket storage lengths is summarized in Tables 9-A, 9-B, and 9-C in Appendix E. The analysis summarizes the 95th percentile back-of-queue lengths at the study intersections under existing, project completion (2022), and General Plan build-out (2040) conditions. Pursuant to discussions with City staff, queues have only been reported for those turn movements where the project adds to the traffic volumes. Queues for select movements affected by project traffic at the Perris Boulevard/Eucalyptus Avenue intersection and Perris Boulevard/Cottonwood Avenue intersection under project completion (2022) conditions and at the Perris Boulevard/Dracaea Avenue intersection under General Plan build-out (2040) conditions are projected to exceed the existing available turn-pocket storage lengths. Improvements to these intersections as specified in Table 17.B are required to ensure adequate turn-pocket storage lengths.

Improvements for Intersections and Roadway Segments and Funding Mechanism. Funding mechanisms for the proposed improvements at study intersections and roadway segments are listed in Table 17.B. Where there is a funding mechanism (Transportation Uniform Mitigation Fee (TUMF) program) for the recommended improvements, payment into the TUMF program is considered sufficient project obligation to “mitigate the cumulative regional transportation impacts of new development on regional arterial highways.” With implementation of the PDFs described above, the addition of project traffic (project completion (2022) with project conditions) does not create an LOS deficiency, and additional improvements are not required. At locations where the project adds to or creates a forecast LOS deficiency even with implementation of PDFs and there is no funding mechanism in place (Perris Boulevard/Dracaea Avenue under General Plan build-out conditions) or where the addition of project traffic is expected to exceed the existing available turn-pocket storage lengths (Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Cottonwood Avenue under project completion (2022) conditions and at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Dracaea Avenue under General Plan build-out (2040) conditions), the project is responsible for its fair-share payment.

Table 17.B: Recommended Improvements for Intersections and Funding Mechanism

Inter-section	Project Completion (2022) with Project Improvements	General Plan Build-Out (2040) with Project Improvements	Fund-ing Mechanism	Improve-ments Covered by TUMF Program	Improvements Covered by Fair Share Funding Mechanism	Fair Share %
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⁶¹ *Ibid.* Page 49 and Table 7-B - General Plan Buildout (2040) Levels of Service.

⁶² *Ibid.* Page 49 and Table 7-C - General Plan Buildout (2040) Roadway Segment Levels of Service.

ISSUES & SUPPORTING INFORMATION SOURCES:				Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
1. Perris Boulevard / Eucalyptus Avenue	Extend storage length for NBL turn lane from 95 feet to 280 feet, Extend storage length for WBL turn lane from 100 feet to 135 feet.	Project Completion Improvements + Extend EBR turn lane from 50 feet to 155 feet.	Fair Share	—	Extend storage length for NBL turn lane from 95 feet to 280 feet, Extend EBR turn lane from 50 feet to 155 feet, Extend storage length for WBL turn lane from 100 feet to 135 feet.	5.89%	
4. Perris Boulevard / Dracaea Avenue	—	Add a EBL turn lane with storage length of 100 feet, Add a WBL turn lane with storage length of 135 feet.	Fair Share	—	Add a EBL turn lane with storage length of 100 feet, Add a WBL turn lane with storage length of 135 feet.	14.86 %	
5. Perris Boulevard / Cottonwood Avenue	Extend storage length for SBL turn lane from 95 feet to 190 feet, Extend storage length for EBL turn lane from 115 feet to 185 feet.	Project Completion Improvements	Fair Share	—	Extend storage length for SBL turn lane from 95 feet to 190 feet, Extend storage length for EBL turn lane from 115 feet to 185 feet.	8.41%	

Source: Table 11-B. Transportation Impact Analysis, LSA, September 2020 (Appendix E).

EB = Eastbound, WB = Westbound, L = Left, R = Right, TUMF = Transportation Uniform Mitigation Fee program

Project Fair Share Percentage is the highest fair share value of the AM and PM peak hour when both peak hours are impacted by the project, or only in the peak hour where the project has an impact, and is calculated based on project traffic as a percentage of total growth from existing to General Plan build-out conditions.

¹ Improvements recommended based on the City's General Plan roadway classification.

Although the City's General Plan and *Transportation Impact Analysis Guidelines* identify intersection thresholds of significance in accordance with LOS, *CEQA Guidelines* Section 15064.3, subdivision (b) establishes "vehicle miles traveled" criteria in lieu of LOS for analyzing transportation impacts under CEQA. Nevertheless, **Mitigation Measure TRA-1** is prescribed to ensure implementation of the improvements to intersections and payment of fair share fees as specified in Table 17.B to avoid conflict with the City's General Plan and *Transportation Impact Analysis Guidelines* LOS criteria for intersections and to ensure adequate turn-pocket storage lengths.

Mitigation Measure TRA-1: Prior to issuance of an occupancy permit, the applicant shall provide evidence to the City that payment of fair share is made, as identified in the project-specific Transportation Impact Analysis (Appendix E) and Table 17.B of the Initial Study, for improvements at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Cottonwood Avenue under project completion (2022) conditions and at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Dracaea Avenue under General Plan build-out (2040) conditions. This measure shall be implemented to the satisfaction of the City of Moreno Valley Director of Engineering, or designee.

Through payment of fair share for improvements at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Cottonwood Avenue under project completion (2022) conditions and at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Dracaea Avenue under General Plan build-out (2040) conditions, as prescribed in **Mitigation Measure TRA-1**, the project would maintain the LOS standards outlined in the City's General Plan and *Transportation Impact Analysis Guidelines* and ensure adequate turn-pocket storage lengths. Therefore, the project would not conflict with a program, plan, ordinance or policy addressing the circulation system. Impacts would be **less than significant with mitigation incorporated**.

b) Conflict or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b) ?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>Response: <i>Less than Significant Impact.</i> CEQA Guidelines Section 15064.3, subdivision (b) establishes “vehicle miles traveled” criteria in lieu of LOS for analyzing transportation impacts and was signed into law as Senate Bill (SB) 743 in 2013. With the adopted guidelines, transportation impacts are to be evaluated based on a project’s effect on vehicle miles traveled (VMT). The City is a member of Western Riverside Council of Governments (WRCOG). Pursuant to the City’s Vehicle Miles Traveled (VMT) analysis guidelines, retail land uses less than 50,000 square feet, which tend to serve the local community and therefore have the potential to reduce VMT, are screened out from further VMT assessment.</p> <p>It is anticipated that the proposed commercial retail project, as a small-scale service station with convenience store and drive-through restaurant, would serve primarily the residential uses in the immediate vicinity of the site and provide them with the convenience of availing essential amenities close to home. Therefore, trip lengths would be shortened, and vehicle travel would be reduced. This project has a much lower square footage (7,088 square feet for all uses combined) compared to the 50,000 square-foot threshold. Therefore, the project is considered a local serving retail land use that would have a less than significant impact on VMT. Mitigation is not required.</p>				
<p>c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant Impact.</i> The project is on a small infill site (under 5 acres) located on one corner of a major intersection with no long roadway segments within the property. The design of roadways must provide adequate sight distance and traffic control measures. Roadway improvements in and around the project site would be designed and constructed to satisfy all City requirements for street widths, corner radii, intersection control as well as incorporate design standards tailored specifically to site access requirements. In addition, entrances and exits to and from parking and loading facilities would be marked with appropriate directional signage.</p> <p>The project includes relocation of the bus stop approximately 250 feet to the north of its existing location, farther away from the intersection of Perris Boulevard and Dracaea Avenue. The relocated bus stop would include a dedicated turnout for busses to pull off the roadway for safer service to the public (refer to Figure 3). Development of the site therefore would facilitate additional patronage of the bus stop by introducing employees and customers to the site and improving the safe operation of the bus stop adjacent to the site.</p> <p>As stated above, tanker-truck fuel delivery to the service station would occur during the off-peak hours, so there would be no circulation issues for fuel tanker-trucks. The trucks would have adequate turning radii to ingress using the project driveway on Perris Boulevard and egress using the project driveway on Dracaea Avenue. Additionally, adequate space is available on-site for fuel tanker-trucks to park during fuel discharge operations.</p> <p>The City, at final plan check, would ensure that all improvements associated with the project are consistent with City standards and requirements. Adherence to applicable City requirements would ensure the proposed development would not include any sharp curves or dangerous intersections. Therefore, no substantial increase in hazards due to a design feature would occur. Impacts would be less than significant. Mitigation is not required.</p>				
<p>d) Result in inadequate emergency access?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant Impact.</i> Construction activities that may temporarily restrict vehicular traffic would be required to implement appropriate measures to facilitate the passage of persons and vehicles through/ around any required road closures. Typical City requirements include prior notification of any lane or road closures with sufficient signage before and during any closures, flag crews with radio communication when necessary to coordinate traffic flow, etc. The Applicant would be required to comply with these requirements, which would maintain emergency access and allow for evacuation if needed during construction activities. Compliance with these requirements would ensure that short-term impacts related to this issue are less than significant. Mitigation is not required.</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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In accordance with the California Fire Code, the project applicant is required to design, construct, and maintain structures, roadways, and facilities to maintain appropriate emergency/evacuation access to and from the Project site. The proposed project is in an urban setting, and direct access to the site would be available via Perris Boulevard, Dracaea Avenue, and Atwood Avenue, all of which are designed and would be constructed to accommodate emergency vehicles and services. These improvements would be subject to compliance with the Fire Code and would be reviewed by the Moreno Valley Fire Department and Police Department through the City’s general development review process. Proper site design and compliance with standard and emergency City access requirements would allow for evacuation if necessary during ongoing project operations. This would ensure that long-term impacts related to this issue are **less than significant**. Mitigation is not required.

Sources:

1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 5 Circulation Element
 - Figure 9-1 – Circulation Plan
 - Figure 9-2 – LOS Standards
 - Figure 9-3 – Roadway Cross-Sections
 - Figure 9-4 – Bikeway Plan
2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.2 – Traffic/Circulation
 - Figure 5.2-1 – Circulation Plan
 - Figure 5.2-2 – General Plan Roadway Cross-Sections
 - Figure 5.2-3 – Year 2000 Number of Through Lanes
 - Figure 5.2-4 – Year 2000 Daily Volume/Capacity (V/C) Ratios
 - Figure 5.2-5 – Year 2000 Average Daily Traffic Volumes
 - Figure 5.2-6 – Proposed Circulation Plan
 - Figure 5.2-7 – LOS Standards
 - Appendix B – Traffic Analysis, City of Moreno Valley General Plan Traffic Study, Urban Crossroads, June 2004.
3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code
4. Moreno Valley Municipal Code Chapter 3.18 Special Gas Tax Street Improvement Fund
5. Moreno Valley Master Bike Plan, adopted January 2015
6. Riverside County Transportation Commission, Congestion Management Program, December 14, 2011

XVIII. TRIBAL CULTURAL RESOURCES – Would the project:

a) Cause a substantial adverse change in the significance of a tribal cultural resource, defined in [Public Resources Code Section 21074](#) as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k) , or	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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Response:

Less Than Significant with Mitigation Incorporated. The term “California Native American tribe” is defined as “a federally recognized California Native American tribe or a non-federally recognized California Native American tribe that is on the contact list maintained by the Native American Heritage Commission (NAHC).”

Chapter 905, Statutes of 2004 (i.e., Senate Bill 18) of the California Government Code requires a City to consult with California Native American tribes for the purpose of preserving specified places, features, and objects described in Sections 5097.9 and 5097.995 of the Public Resources Code that are located within the city or county’s jurisdiction prior to the adoption or amendment of a General Plan. Senate Bill

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>(SB) 18 requires the Lead Agency (i.e., City of Moreno Valley) to refer to the California Native American tribes specified by the NAHC and to provide them with opportunities for consultation.</p> <p>Chapter 532, Statutes of 2014 (i.e., Assembly Bill 52), requires Lead Agencies evaluate a project’s potential to impact “tribal cultural resources.” Such resources include “sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American Tribe that are eligible for inclusion in the California Register of Historical Resources or included in a local register of historical resources.” Assembly Bill (AB) 52 also gives Lead Agencies the discretion to determine, supported by substantial evidence, whether a resource qualifies as a “tribal cultural resource.”</p> <p>CEQA defines a “historical resource” as a resource that meets one or more of the following criteria: (1) is listed in, or determined eligible for listing in, the California Register of Historical Resources (California Register); (2) is listed in a local register of historical resources as defined in PRC §5020.1(k); (3) is identified as significant in a historical resource survey meeting the requirements of PRC §5024.1(g); or (4) is determined to be a historical resource by a project’s Lead Agency (PRC §21084.1 and <i>State CEQA Guidelines</i> §15064.5[a]).</p> <p>“Local register of historical resources” means a list of properties officially designated or recognized as historically significant by a local government pursuant to a local ordinance or resolution.</p> <p>Per SB 18 (specifically California Government Code 65352.4), “consultation” means the meaningful and timely process of seeking, discussing, and considering carefully the views of others, in a manner that is cognizant of all parties’ cultural values and, where feasible, seeking agreement. Consultation between government agencies and Native American tribes shall be conducted in a way that is mutually respectful of each party’s sovereignty. Consultation shall also recognize the tribes’ potential needs for confidentiality with respect to places that have traditional tribal cultural significance. The City engaged the NAHC for a contact list of tribes pursuant to California Government Code 65352.3 and contacted nine entities representing eight tribes in accordance with SB 18.</p> <p>Per AB 52 (specifically California Public Resources Code 21080.3.1), Native American consultation is required upon request by interested California Native American tribes that have previously requested that the City provide them with notice of such projects. The City mailed notices of the proposed project to eight entities representing seven Native American tribes in accordance with AB 52.</p> <p>To date, three tribes, the Pechanga Band of Luiseño Indians (Pechanga), the Soboba Band of Luiseño Indians (Soboba), and the Rincon Band of Luiseño Indians (Rincon) have requested formal consultation with the City, while the Agua Caliente Band of Cahuilla Indians (Aqua Caliente) requested additional information on the project prior to determining if consultation is necessary. Two additional tribes, the Morongo Band of Mission Indians (Morongo) and the San Manuel Band of Mission Indians (San Manuel) informed the City they do not wish to consult on this project. Five entities representing three tribes, the Cahuilla Band of Indians, the Los Coyotes Band of Cahuilla Mission Indians, the Desert Cahuilla Indians/Torres Martinez, the Santa Rosa Band of Mission Indians, and the Serrano Nation of Mission Indians did not respond to the City.</p> <p>All of the tribal correspondences pursuant to SB 18 and AB 52 occurred in October and November of 2019, and consultation is ongoing between the City and Pechanga, Soboba, and Rincon, while the City continues to engage the Aqua Caliente as appropriate.</p> <p>Pursuant to SB 18 and AB 52, the City has prescribed the following Mitigation Measures for the protection of Tribal Cultural Resources:</p> <p>Mitigation Measure TCR-1: Prior to the issuance of a grading permit, the Developer shall retain a professional archaeologist to conduct monitoring of all mass grading and trenching activities. The Project Archaeologist shall have the authority to temporarily redirect earthmoving activities in the event that suspected archaeological resources are unearthed during Project construction. The Project Archaeologist, in consultation with the Consulting Tribe(s), the contractor, and the City, shall develop a Cultural Resources Management Plan (CRMP) in consultation pursuant to the definition in AB52 to address the details, timing and responsibility of all archaeological and cultural activities that will occur on the project site. A consulting tribe is defined as a tribe that initiated the AB 52 tribal</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact

consultation process for the Project, has not opted out of the AB52 consultation process, and has completed AB 52 consultation with the City as provided for in Cal Pub Res Code Section 21080.3.2(b)(1) of AB52. Details in the Plan shall include:

- a. Project grading and development scheduling;
- b. The Project archeologist and the Consulting Tribes(s) as defined in **TCR-1** shall attend the pre-grading meeting with the City, the construction manager and any contractors and will conduct a mandatory Cultural Resources Worker Sensitivity Training to those in attendance. The Training will include a brief review of the cultural sensitivity of the Project and the surrounding area; what resources could potentially be identified during earthmoving activities; the requirements of the monitoring program; the protocols that apply in the event inadvertent discoveries of cultural resources are identified, including who to contact and appropriate avoidance measures until the find(s) can be properly evaluated; and any other appropriate protocols. All new construction personnel that will conduct earthwork or grading activities that begin work on the Project following the initial Training must take the Cultural Sensitivity Training prior to beginning work and the Project archaeologist and Consulting Tribe(s) shall make themselves available to provide the training on an as-needed basis;
- c. The protocols and stipulations that the contractor, City, Consulting Tribe(s) and Project archaeologist will follow in the event of inadvertent cultural resources discoveries, including any newly discovered cultural resource deposits that shall be subject to a cultural resources evaluation.

Mitigation Measure TCR-2:

Prior to the issuance of a grading permit, the Developer shall secure agreements with the Soboba Band of Luiseño Indians (Soboba) and Pechanga Band of Luiseño Indians (Pechanga) for tribal monitoring. The Developer is also required to provide a minimum of 30 days advance notice to the tribes of all mass grading and trenching activities. The Native American Tribal Representatives shall have the authority to temporarily halt and redirect earth moving activities in the affected area in the event that suspected archaeological resources are unearthed. If the Native American Tribal Representatives suspect that an archaeological resource may have been unearthed, the Project Archaeologist or the Tribal Representatives shall immediately redirect grading operations in a 100-foot radius around the find to allow identification and evaluation of the suspected resource. In consultation with the Native American Tribal Representatives, the Project Archaeologist shall evaluate the suspected resource and make a determination of significance pursuant to California Public Resources Code Section 21083.2.

Mitigation Measure TCR-3:

In the event that Native American cultural resources are discovered during the course of grading (inadvertent discoveries), the following procedures shall be carried out for final disposition of the discoveries:

- a) One or more of the following treatments, in order of preference, shall be employed with the tribes. Evidence of such shall be provided to the City of Moreno Valley Planning Department:
 - i. Preservation-In-Place of the cultural resources, if feasible. Preservation in place means avoiding the resources, leaving them in the place they were found with no development affecting the integrity of the resources.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>ii. Onsite reburial of the discovered items as detailed in the treatment plan required pursuant to Mitigation Measure TCR-1. This shall include measures and provisions to protect the future reburial area from any future impacts in perpetuity. Reburial shall not occur until all legally required cataloging and basic recordation have been completed. No recordation of sacred items is permitted without the written consent of all Consulting Native American Tribal Governments as defined in TCR-1.</p> <p>Mitigation Measure TCR-4: The City shall verify that the following note is included on the Grading Plan:</p> <p>“If any suspected archaeological resources are discovered during ground-disturbing activities and the Project Archaeologist or Native American Tribal Representatives are not present, the construction supervisor is obligated to halt work in a 100-foot radius around the find and call the Project Archaeologist and the Tribal Representatives to the site to assess the significance of the find.”</p> <p>Mitigation Measure TCR-5: If potential historic or cultural resources are uncovered during excavation or construction activities at the project site, work in the affected area must cease immediately and a qualified person meeting the Secretary of the Interior’s standards (36 CFR 61), Tribal Representatives, and all site monitors per the Mitigation Measures, shall be consulted by the City to evaluate the find, and as appropriate recommend alternative measures to avoid, minimize or mitigate negative effects on the historic, or prehistoric resource. Determinations and recommendations by the consultant shall be immediately submitted to the Planning Division for consideration, and implemented as deemed appropriate by the Community Development Director, in consultation with the State Historic Preservation Officer (SHPO) and any and all Consulting Native American Tribes as defined in TCR-1 before any further work commences in the affected area.</p> <p>Mitigation Measure TCR-6: If human remains are discovered, no further disturbance shall occur in the affected area until the County Coroner has made necessary findings as to origin. If the County Coroner determines that the remains are potentially Native American, the California Native American Heritage Commission shall be notified within 24 hours of the published finding to be given a reasonable opportunity to identify the “most likely descendant”. The “most likely descendant” shall then make recommendations, and engage in consultations concerning the treatment of the remains (California Public Resources Code 5097.98). (GP Objective 23.3, CEQA).</p> <p>Adherence to Mitigation Measures TCR-1 through TCR-6 would ensure impacts to tribal cultural resources are reduced to less than significant with mitigation incorporated.</p>				
<p>ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response:</p> <p><i>Less Than Significant with Mitigation Incorporated.</i> CEQA defines a “historical resource” as a resource that meets one or more of the following criteria: (1) is listed in, or determined eligible for listing in, the</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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California Register of Historical Resources (California Register); (2) is listed in a local register of historical resources as defined in PRC §5020.1(k); (3) is identified as significant in a historical resource survey meeting the requirements of PRC §5024.1(g); or (4) is determined to be a historical resource by a project’s Lead Agency (PRC §21084.1 and *State CEQA Guidelines* §15064.5[a]).

A resource may be listed as a historical resource in the California Register if it meets any of the following National Register of Historic Places criteria as defined in PRC §5024.1(C):

- A. Is associated with events that have made a significant contribution to the broad patterns of California’s history and cultural heritage.
- B. Is associated with the lives of persons important in our past.
- C. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values.
- D. Has yielded, or may be likely to yield, information important in prehistory or history.

A “substantial adverse change” to a historical resource, according to PRC §5020.1(q), “means demolition, destruction, relocation, or alteration such that the significance of a historical resource would be impaired.”

CEQA Guidelines do not preclude identification of historical resources as defined in Public Resources Code Sections 5020.1(j) or 5024.1. Pursuant to *State CEQA Guidelines* Section 15064.5[c][4], if an archaeological resource is neither a unique archaeological nor a historical resource, the effects of the project on those resources shall not be considered a significant effect on the environment. It shall be sufficient that both the resource and the effect on it are noted in the Initial Study, but they need not be considered further in the CEQA process.

The City has engaged the NAHC for a contact list of tribes pursuant to California Government Code 65352.3 (SB 18) and California Public Resources Code 21080.3.1 (AB 52). To date, three tribes, the Pechanga Band of Luiseño Indians (Pechanga), the Soboba Band of Luiseño Indians (Soboba), and the Rincon Band of Luiseño Indians (Rincon) have requested formal consultation with the City, while the Agua Caliente Band of Cahuilla Indians (Aqua Caliente) requested additional information on the project prior to determining if consultation is necessary. Two additional tribes, the Morongo Band of Mission Indians (Morongo) and the San Manuel Band of Mission Indians (San Manuel) informed the City they do not wish to consult on this project. Five entities representing three tribes, the Cahuilla Band of Indians, the Los Coyotes Band of Cahuilla Mission Indians, the Desert Cahuilla Indians/Torres Martinez, the Santa Rosa Band of Mission Indians, and the Serrano Nation of Mission Indians did not respond to the City.

All of the tribal correspondences pursuant to SB 18 and AB 52 occurred in October and November of 2019, and consultation is ongoing between the City and Pechanga, Soboba, and Rincon, while the City continues to engage the Aqua Caliente as appropriate. With implementation of **Mitigation Measures TCR-1** through **TCR-6**, the project archaeologist, Applicant, the City Community Development Director or designee, and interested Tribal representatives shall confer regarding the appropriate disposition of any unanticipated Tribal Cultural Resources encountered during ground disturbing activities. Therefore, the proposed project would have a **less than significant impact with mitigation incorporated** on Tribal Cultural Resources.

Sources:

1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 7 – Conservation Element – Section 7.2 – Cultural and Historical Resources
2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.10 – Cultural Resources
 - Figure 5.10-1 – Locations of Listed Historic Resource Inventory Structures
 - Figure 5.10-2 – Location of Prehistoric Sites
 - Figure 5.10-3 – Paleontological Resource Sensitive Areas
 - Appendix F – Cultural Resources Analysis, Study of Historical and Archaeological Resources for the Revised General Plan, City of Moreno Valley, Archaeological Associates, August 2003.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code
4. Moreno Valley Municipal Code Title 7 – Cultural Preservation
5. Cultural Resources Inventory for the City of Moreno Valley, Riverside County, California, prepared by Daniel F. McCarthy, Archaeological Research Unit, University of California, Riverside, October 1987 (*This document cannot be provided to the public due to the inclusion of confidential information pursuant to Government Code Section 6254.10.*)

XIX. UTILITIES AND SERVICE SYSTEMS – Would the project:

a) Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. The proposed commercial retail facilities will connect to existing utilities, including water, drainage, electric power, natural gas, and telecommunications located beneath Perris Boulevard and Dracaea Avenue. The approval of drainage improvements occurs through the building plan check process. As part of this process, all project-related drainage features would be required to meet City and RWQCB standards. On-site project-related drainage features would be designed, installed, and maintained per City standards and the requirements identified in the Final WQMP (per **Mitigation Measure HYD-3**).

General Plan Policy 2.12.1 requires that adequate septic or sewer service capacity will be available in a timely manner prior to approval of any development application. Policy 2.13.3 requires each project to provide the infrastructure needed to support the project at the time it is needed.⁶³ All proposed improvements and interconnection to drainage, electric power, natural gas, telecommunications, water, and wastewater facilities would be installed simultaneously with finished grading activities and required roadway frontage improvements for the project site. As a result, interconnection to the existing utilities surrounding the site would occur within disturbed footprints and therefore not result in substantial disturbance of native habitat or soils, or existing roadways or utilities. There would be no significant environmental effects specifically related to the installation of utility interconnections that are not encompassed within the project’s construction and operational footprint, and therefore already identified, disclosed, and subject to all applicable mitigation measures, as well as local, State, and federal regulations, as part of this Initial Study. Therefore, impacts related to relocation of utilities would be **less than significant**. No additional mitigation is required.

b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. The City of Moreno Valley is served by two water purveyors: Eastern Municipal Water District (EMWD) and the Box Springs Mutual Water Company.⁶⁴ However, the EMWD is the primary water purveyor from the City and would provide water to the project site. The EMWD considers current groundwater production to be utilized completely by existing customers, as the majority of EMWD’s current and projected water supplies are imported through the Metropolitan Water District of Southern California (MWD).⁶⁵ New developments, including the proposed project, will be supplied with imported water from one of the following sources: (1) treated imported water from MWD; (2) untreated imported water from MWD, which is subsequently treated by EMWD; or (3) untreated imported water treated by EMWD and recharged into groundwater basins for later withdrawal.

The proposed project is expected to employ approximately 39 staff and generate approximately 4,925 average daily trips (i.e., ADTs without internal capture and pass-by trips) (refer to Table XVII.A). Dividing

⁶³ City of Moreno Valley. *Moreno Valley General Plan Final Program EIR*. Section 5.13 Public Services and Utilities. July 2006.
⁶⁴ *Ibid.*
⁶⁵ Eastern Municipal Water District. *2015 Urban Water Management Plan*. Page 7-1. June 2016.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>the daily trips in half would account for one trip to the project site and the second trip from the site, so 2,463 vehicles would visit the site per day. Assuming two persons per vehicle, the proposed commercial retail uses could experience 4,925 patrons per day. EMWD's 2015 average daily per capita water demand for commercial uses is 17.6 gallons per day.⁶⁶ Therefore, the 39 employees and 4,925 patrons per day would demand approximately 87,366 gallons (0.268 acre-foot) of water per day.⁶⁷ This is a worst-case scenario since approximately half of the trips (2,464) estimated for the project would be related to the service station with convenience store where many of these patrons are expected to purchase fuel without utilizing other services or amenities that would require water consumption.</p> <p>MWD's 2015 UWMP provides information about MWD's regional supply reliability and projected demands based on official regional demographic and economic projects from SCAG's 2012 Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS) and the San Diego Association of Governments (SANDAG) Series 13: 2050 Regional Growth Forecast from October 2013.⁶⁸ Based on information provided by EMWD and other member agencies, MWD concludes that it is able to meet projected demands for all member agencies through 2040, even during dry periods.⁶⁹ Under extreme conditions, water supplies could be allocated using MWD's WSAP to preserve supplies in storage by requiring a reduction in demand by member agencies, including the EMWD, pursuant to SB 1168 and 1319, and AB 1739. Commercial customers face event-driven penalties and could face fines if found violating water use restrictions. EMWD's 2015 UWMP also discloses that in the event of a water supply shortage or water emergency, the City has in place water shortage contingency plans, which ensure provision of priority water services to all its existing and anticipated customers. Since the EMWD and MWD have the ability to meet all of their existing entitlements and projected supplemental demand through 2040, even under a repeat of historic multiple-year drought scenarios, sufficient water supplies are available to serve the proposed project. Impacts would be less than significant, and mitigation is not required.</p>				
<p>c) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less Than Significant Impact.</i> Wastewater services in the City of Moreno Valley are provided by the EMWD, serving most of the City and surrounding areas, and the Edgemont Community Services District.⁷⁰ The EMWD provides wastewater services to approximately 239,000 customers within its service area and currently treats approximately 43 million gallons per day of wastewater at its five active regional water reclamation facilities through 1,813 miles of sewer pipelines.⁷¹ These reclamation plants include the San Jacinto Regional Water Reclamation Facility, Moreno Valley Regional Water Reclamation Facility, Perris Valley Regional Water Reclamation Facility, Sun City Regional Water Reclamation Facility, and Temecula Valley Regional Water Reclamation Facility. Generated wastewater from the project site would be treated at the Moreno Valley Regional Water Reclamation Facility (MVRWRF).</p> <p>The typical daily flow at the MVRWRF is 10.6 million gallons per day with a current capacity of 16 million gallons per day, having a current excess capacity of approximately 5.4 million gallons per day.⁷² As stated previously, EMWD's 2015 average daily per capita water demand for commercial uses is 17.6 gallons per day,⁷³ and 39 employees and 4,925 patrons per day would demand approximately 87,366 gallons (0.268 acre-foot) of water per day.⁷⁴ As a worst case scenario, even if 100 percent of the project's anticipated water demand (87,366 gallons per day) were dedicated to wastewater, the project demand</p>				

⁶⁶ *Ibid.* Table 5-7 and Page 5-6.

⁶⁷ (39 employees + 4,925 patrons) × 17.6 gallons per capita per day = 87,366.4 gallons per day.

⁶⁸ The Metropolitan Water District of Southern California. *2015 Urban Water Management Plan*. Page ES-2. June 2016.

⁶⁹ *Ibid.* Tables 2-4, 2-5, and 2-6.

⁷⁰ City of Moreno Valley. *Moreno Valley General Plan Final Program EIR*. Sewer Services, Public Services and Utilities. July 2006.

⁷¹ Eastern Municipal Water District. *Wastewater Service*. <https://www.emwd.org/wastewater-service>. (Accessed July 1, 2020).

⁷² Eastern Municipal Water District. *Moreno Valley Regional Water Reclamation Facility*. <https://www.emwd.org/sites/main/files/file-attachments/mvrwrfactsheet.pdf?1537294991>. October 2016.

⁷³ *Ibid.* Table 5-7 and Page 5-6.

⁷⁴ (39 employees + 4,925 patrons) × 17.6 gallons per capita per day = 87,366.4 gallons per day.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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for wastewater treatment would represent 1.6 percent of the MVRWRF’s current excess wastewater treatment capacity of approximately 5.4 million gallons per day, which would be more than adequate to serve the project in addition to existing entitlements. Full buildout of the City’s General Plan will exceed the existing capacity of the MVRWRF; however, expansion of the MVRWRF is planned in and around the northern portion of the existing facility to accommodate future demand as buildout of the General Plan occurs. EMWD has prepared a Water Supply Strategic Plan and Recycled Water Strategic and Master Plan for its service area and levies connection charges on new development to finance the construction of necessary facilities, which would be subject to project- and site-specific environmental review under CEQA, to ensure adequate service. The proposed project’s incremental contribution to water and wastewater treatment demand would not in and of itself exceed the existing or planned capacity of the MVRWRF or require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects. Additionally, the proposed project would be required to comply with the applicable waste discharge prohibitions and water quality objectives established by the Santa Ana RWQCB. The project Applicant would also be required to satisfy City requirements related to the payment of fees and/or the provision of wastewater conveyance features, and installation and maintenance prior to the issuance of building permits. Adherence to requirements included in the NPDES permit, SWPPP, WQMP, and City wastewater conveyance standards would ensure potential impacts from wastewater treatment capacity remain **less than significant**. Mitigation is not required.

d) Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. Solid waste collection is a “demand-responsive” service, and current service levels can be expanded and funded through user fees. Solid waste generated within the proposed Project would be generated in the Riverside County Waste Management Department’s (RCWMD) Badlands Landfill located at 31125 Ironwood Avenue approximately 5-miles northeast of the project site. The Badlands Sanitary Landfill has a maximum daily permitted throughput of 4,800 tons per day and remaining capacity of 15,748,799 cubic yards.⁷⁵ Based on a solid waste generation of 0.9 pound per 100 square feet of service station per day and 17 pounds per employee of a fast-food restaurant per day,⁷⁶ the proposed project is anticipated to generate approximately 495.8 pounds of solid waste per day (90.48 tons/year).⁷⁷ The volume of solid waste anticipated by the proposed project per day represents approximately 0.005 percent of the current permitted throughput at the Badlands Sanitary Landfill. As adequate daily surplus capacity exists at the receiving landfill, development of the proposed project would not significantly affect current operations of the landfill serving the project area. A **less than significant** impact would occur, and mitigation is not required.

e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Response:
Less Than Significant Impact. The proposed project would be required to comply with applicable elements of AB 1327, Chapter 18 (California Solid Waste Reuse and Recycling Access Act of 1991) and other applicable local, State, and federal solid waste disposal standards, thereby ensuring that the solid waste stream to the Badlands Sanitary Landfill is reduced in accordance with existing regulations. The construction contractor must recycle/reuse at least 65 percent of the construction material (including, but not limited to, proposed aggregate base, soil, mulch, vegetation, concrete, lumber, metal, and cardboard) and use “Green Building Materials,” such as those materials that are rapidly renewable or resource efficient, and recycled and manufactured in an environmentally friendly way, for at least 10 percent of the project, in accordance with CalRecycle regulations. City Municipal Code Section 8.80.040 (On-site

⁷⁵ CalRecycle. *Facility/Site Summary Details: Badlands Sanitary Landfill*. 2020.
⁷⁶ CalRecycle. *Estimated Solid Waste Generation Rates*. <https://www2.calrecycle.ca.gov/WasteCharacterization/General/Rates#Industrial>. (Accessed July 1, 2020).
⁷⁷ 0.9 pounds of solid waste × (4,088 square feet ÷ 100) = 36.8 pounds per day for the service station. 17 pounds of solid waste × 27 restaurant employees = 459 pounds per day for the restaurant. (36.8 + 459 = 495.8 pounds per day of solid waste).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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practices) establishes protocols the construction contractor must follow to ensure construction materials that are recyclable and/or reusable are separated from the waste stream and quantified.

Pursuant to Assembly Bill 341, all commercial accounts with the City's solid waste provider, Waste Management of the Inland Empire, must implement a recycling program if it would generate at least four cubic yards of solid waste per week. Compliance with regulations related to solid waste disposal and diversion is required for all projects within the City as a matter of regulatory policy. Therefore, the Project would not conflict with statutes and regulations related to solid waste. A **less than significant** impact related to this issue would occur. Mitigation is not required.

Sources:

1. Moreno Valley General Plan, adopted July 11, 2006
 - Chapter 2 – Conservation Element – Section 2.4 – Utilities
 - Chapter 6 – Safety Element – Section 6.7 – Water Quality
 - Chapter 7 – Conservation Element – Section 7.3 – Solid Waste
 - Chapter 7 -- Conservation Element – Section 7.5—Water Resources
 - Figure 7-1 – Water Purveyor Service Area Map
2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006
 - Section 5.7 – Hydrology and Water Quality
 - Figure 5.7-1 – Storm Water Flows and Major Drainage Facilities
 - Figure 5.7-2 – Groundwater Basins
 - Section 5.13 – Public Services
 - Figure 5.13-1 – Locations of Public Facilities
3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code
4. Moreno Valley Municipal Code Chapter 8.10 Stormwater/Urban Runoff Management and Discharge Controls
5. Moreno Valley Municipal Code Section 8.21.170 National Pollutant Discharge Elimination System (NPDES).
6. Moreno Valley Municipal Code Chapter 8.80 – Recycling and Diversion of Construction and Demolition Waste

XX. WILDFIRE – If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a) Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Response:

No Impact. The project site is not located within any State Responsibility Area (SRA) for fire service,⁷⁸ and is not within a Very High Fire Hazard Severity Zone (VHFHSZ).⁷⁹ In addition, as noted in Response to Checklist Question 9.f, the proposed project would not impair the implementation of, or physically interfere with, and adopted emergency response plan, and **no impact** would occur. Mitigation is not required.

b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Response:

No Impact. Refer to Response to Checklist Question 20.a. Additionally, the project site is generally flat and is bound by existing development on all sides. Therefore, the proposed project would not exacerbate wildfire risks and expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire. **No impact** would occur. Mitigation is not required.

⁷⁸ California Board of Forestry and Fire Protection. *State Responsibility Area Viewer*. 2020. <https://bof.fire.ca.gov/projects-and-programs/state-responsibility-area-viewer/>. (Accessed June 2020).

⁷⁹ City of Moreno Valley. *Moreno Valley General Plan Final Program EIR*. July 11 2006.

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Response: <i>No Impact.</i> Refer to Response to Checklist Question 20.a. The project is not located within a SRA for fire service or VHFHSZ. Therefore, the project would not require the installation or maintenance of associated infrastructure. No impact would occur. Mitigation is not required.				
d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Response: <i>No Impact.</i> Refer to Response to Checklist Question 20.a and 20.b. The project site is not located within an SRA for fire service or VHFHSZ and is generally flat. Therefore, the proposed project would not expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes. No impact would occur. Mitigation is not required.				
Sources: <ol style="list-style-type: none"> 1. Moreno Valley General Plan, adopted July 11, 2006 <ul style="list-style-type: none"> • Chapter 6 – Safety Element – Section 6.2- Fire and Emergency Services – 6.2.8—Wildland Urban Interface 2. Final Environmental Impact Report City of Moreno Valley General Plan, certified July 11, 2006 <ul style="list-style-type: none"> • Section 5.5 – Hazards and Hazardous Materials <ul style="list-style-type: none"> - Figure 5.5-2 – Floodplains and High Fire Hazard Areas 3. Title 9 – Planning and Zoning of the Moreno Valley Municipal Code 4. Local Hazard Mitigation Plan, City of Moreno Valley Fire Department, adopted October 4, 2011, amended 2017, http://www.moval.org/city_hall/departments/fire/pdfs/haz-mit-plan.pdf <ul style="list-style-type: none"> • Chapter 5 – Wildland and Urban Fires <ul style="list-style-type: none"> - Figure 5-2 – Moreno Valley High Fire Area Map 2016 • Chapter 8 – Landslide <ul style="list-style-type: none"> - Figure 8-1 – Moreno Valley Slope Analysis 2016 5. Emergency Operations Plan, City of Moreno Valley, March 2009, http://www.moval.org/city_hall/departments/fire/pdfs/mv-eop-0309.pdf <ul style="list-style-type: none"> • Threat Assessment 3 – Wildfire 				
XXI. MANDATORY FINDINGS OF SIGNIFICANCE				
a) Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Response: <i>Less than Significant with Mitigation Incorporated.</i> The natural habitat of the site was cleared around the 1920s to establish a residence on the property (Appendix C). The site has been routinely disked since				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>at least 1966 for weed abatement, and the residence was removed by 2002.⁸⁰ The site remains highly disturbed and contains a variety of ornamental trees and ruderal vegetation as a result of seasonal weed abatement activities (Appendix B).</p> <p>Compliance with the NPDES and City's MS4 Permit ensures the State's mandatory standards for the maintenance of clean water and the federal minimums are met. Compliance with the provisions of the NPDES permit and implementation of the LID BMPs specified in the WQMP are regulatory requirements that apply to all development projects. These requirements are detailed as Mitigation Measures HYD-1 through HYD-3. A Final WQMP will be approved as a routine action during the processing of the Project by the City; therefore, the required measures and features detailed in the WQMP to safeguard water quality would be incorporated into the Project. Implementation of Mitigation Measures HYD-1 through HYD-3 and the requirements included in the NPDES permit, SWPPP, and WQMP would ensure impacts to water quality remain less than significant with mitigation incorporated.</p> <p>No riparian or sensitive natural community is located on site, and there is no designated critical habitat within or adjacent to the project site for any species (Appendix B). The project site does not include any federally protected wetlands or any drainage features, ponded areas, wetlands, or riparian habitat subject to jurisdiction by the CDFW, USACE, and/or RWQCB. The project-specific Biological Resources Assessment (Appendix B) included a literature search and pedestrian survey of the site and indicates the site does not contain suitable habitat for any threatened or endangered species. The project site is located within the Stephens' Kangaroo Rat Habitat Conservation Plan area and a fee payment will be required prior to issuance of a grading permit. The project will comply with the Habitat Conservation Plan for the Stephens' Kangaroo Rat in Western Riverside County (Riverside County Habitat Conservation Agency, February 1995) and fulfill its compensatory mitigation requirements under this plan through the payment of a fee</p> <p>Ornamental trees that provide suitable nesting habitat for common bird species are located on site, so the project would be conditioned via Mitigation Measure BIO-1 to ensure a qualified biologist conducts a pre-construction survey for nesting birds if construction activities occur during nesting bird season in accordance with Sections 3503–3801 of the California Fish and Game Code. The ornamental trees are expected to be removed and will be managed in accordance with Part E and Part G of 9.17.030 (Landscape and Irrigation Design Standards) of the City Municipal Code. Prior to the issuance of grading permits, the project Applicant must submit detailed grading plans prepared in conformance with applicable standards of the City's Landscape and Irrigation Design Standards. The City, at final plan check, would ensure that all improvements associated with the project are consistent with these City standards and requirements. Compliance with these standards is required of all projects in the City as a matter of regulatory policy (i.e., City Municipal Code) and therefore would not constitute mitigation in order to remove the trees. With implementation of Mitigation Measure BIO-1 for a pre-construction nesting bird survey, impacts to native resident or migratory fish or wildlife species, established native resident or migratory wildlife corridors, and native wildlife nursery sites would be reduced to less than significant with mitigation incorporated.</p> <p>The Project-specific Cultural Resources Assessment (Appendix C) identified and recorded remnants (a utility pole, structural slab, and a glass bottle fragment) of a residence constructed prior to 1930 within the project boundaries. The residence was demolished prior to 2002, and only the concrete slab foundation, utility pole, and glass bottle fragment remain. Demolition of the historic-era residence has eliminated the context required to convey any historical significance of the property. The remnant features (utility pole and slab) and glass bottle fragment are typical examples of regionally and locally ubiquitous features and artifacts with no unique character-defining features that could contribute to the significance of the property or to the features and artifacts themselves. Therefore, their removal during construction would not constitute a significant impact. Due to the former presence of a pre-Depression Era residence, the project area retains some potential for associated subsurface resources, and archaeological monitoring is prescribed during ground-disturbing activities. Mitigation Measures TCR-1 through TCR-6, although prescribed to address Tribal Cultural Resources, will serve to ensure archaeological and historic-era cultural resources inadvertently encountered during ground disturbing</p>				

⁸⁰ Nationwide Environmental Title Research, LLC. *Historic Aerials by NETRonline*. <https://historicaerials.com/viewer>. (Accessed July 1, 2020).

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>activities are managed pursuant to CEQA Guidelines Section 15064.5. Impacts related to historical and archaeological resources therefore would be less than significant with mitigation incorporated.</p> <p>Through consultation between the City and Native American tribes pursuant to SB 18 and AB 52, the City prescribes Mitigation Measures TCR-1 through TCR-6 to ensure the project would include provisions for Native American Monitoring of ground-disturbing activities and would be conditioned to cease excavation or construction activities if tribal cultural or archaeological resources are identified during execution. These measures would ensure further consultation with interested Native American Tribes for the appropriate treatment of Tribal Cultural Resources. Additionally, implementation of Mitigation Measures GEO-1 and GEO-2 would ensure unanticipated paleontological resources encountered during construction would be managed pursuant to applicable regulatory policy. Accordingly, impacts to important examples of major periods of California history or prehistory would be reduced to less than significant with mitigation incorporated.</p> <p>The proposed Project has either no impact, less than significant impact, or less than significant impact with implementation of mitigation with respect to all natural resources issues pursuant to CEQA. Due to the limited scope of physical impacts to the environment associated with the proposed Project, implementation of the Mitigation Measures described above would ensure impacts to the quality of the environment would be reduced to less than significant with mitigation incorporated.</p>				
<p>b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant with Mitigation Incorporated.</i> In evaluating the cumulative effects of the Project, Section 21100(e) of the <i>CEQA Guidelines</i> states that “previously approved land use documents including, but not limited to, general plans, specific plans, and local coastal plans, may be used in cumulative impact analysis.” The project Applicant indicates the project is expected to generate up to 39 employees for the restaurant and convenience store/service station. When compared to the planned use of the site for residential/office uses, the proposed changes in the land use designation would not substantially alter the housing and employment forecast in the City.</p> <p>As discussed in Checklist Response 3.b, construction and operation of the project would not generate criteria pollutants in excess of SCAQMD emissions thresholds. Therefore, the project would not contribute significantly to cumulative impacts for any air quality pollutants for which the region is in nonattainment. As for cumulative impacts to regional air quality, the discussion in Checklist Response 3.a indicates the proposed project would neither conflict with the SCAQMD’s AQMP nor jeopardize the region’s attainment of air quality standards. The SCAQMD uses the project-level significance thresholds to determine whether a project’s emissions are cumulatively considerable. Because the project’s emissions do not exceed the SCAQMD’s regional significance thresholds, as detailed in Checklist Response 3.b, the SCAQMD does not consider the project to contribute significantly to a cumulative air quality impact.</p> <p>It is anticipated that the proposed commercial retail project, as a small-scale service station with convenience store and drive-through restaurant, would serve primarily the residential units in the immediate vicinity of the site and provide them with the convenience of availing certain amenities close to home. Therefore, trip lengths would be shortened and vehicle travel would be reduced. This project has a much lower square footage (7,088 square feet for all uses combined) compared to the 50,000 square-foot threshold for VMT analysis of retail projects. Therefore, the project is considered a local serving retail land use that would not have a cumulatively considerable impact on VMT.</p> <p>Through implementation of Mitigation Measure TRA-1, the improvements at Perris Boulevard/Atwood Avenue and Perris Boulevard/ Cottonwood Avenue under the existing-with-project scenario, payment into the TUMP, and payment of fair share, as detailed in Table 17.B, would ensure the project would maintain the LOS standards outlined in the City’s General Plan under project-specific and cumulative</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>scenarios and therefore would not conflict with a program, plan, ordinance or policy addressing the circulation system.</p> <p>As detailed in Checklist Response 13.a, the project-related traffic noise increase on adjacent roadways would be up to 1.5 dBA. Noise level increases less than 3 dBA would not be perceptible to the human ear in an outdoor environment. Therefore, traffic noise impacts from cumulative Project-related traffic on off-site sensitive receptors would be less than significant.</p> <p>Finally, as detailed throughout Section 3.19, Utilities and Service Systems, sufficient utility facilities and resources are available to serve the Project in addition to existing entitlements.</p> <p>The Project has no impact, a less than significant impact, or a less than significant impact with implementation of mitigation with respect to all environmental issues. Therefore, a less than significant cumulative impact would occur with mitigation incorporated.</p>				
<p>c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Response: <i>Less than Significant with Mitigation Incorporated.</i> All development associated with the proposed project must comply with applicable provisions of the 2019 CBC and the City's building regulations. Accordingly, proper engineering design and construction in conformance with the 2019 CBC standards and project-specific geotechnical recommendations would ensure that the Project does not subject people to significant geologic hazards.</p> <p>The transport, use, and storage of hazardous materials during the construction and operation of the site would be conducted pursuant to all applicable local, State and federal laws, and in cooperation with the Riverside County Fire Department, Riverside County Department of Environmental Health, Hazardous Materials Division, Environmental Protection and Oversight Division, and California Occupational Safety and Health Administration. Additionally, the United States Department of Transportation Office of Hazardous Materials Safety prescribes strict regulations for the safe transportation of hazardous materials by truck and rail on State highways and rail lines, as described in Title 49 of the <i>Code of Federal Regulations</i>, and implemented by Title 13 of the California Code of Regulations.</p> <p>The proposed service station would utilize hazardous materials on a daily basis including gasoline, oil, solvents, and cleaning products. Two underground storage tanks (USTs) (one is 20,000 gallons and the other is 12,000 gallons/8,000 gallons combination tank) are proposed on the west side of the proposed canopy along with 6 MPDs (12 total fueling stations). Accordingly, the project would develop a Hazardous Materials Business Emergency Plan administered by the Riverside County Fire Department, as applicable, in accordance with California Health and Safety Code Section 25507 and other local, state, and federal standards, ordinances, and regulations. As required by Health and Safety Code Section 25507, a business shall establish and implement a Hazardous Materials Business Emergency Plan for emergency response to a release or threatened release of a hazardous material in accordance with the standards prescribed in the regulations adopted pursuant to Section 25503 if the business handles a hazardous material or a mixture containing a hazardous material that has a quantity at any one time above the thresholds described in Section 25507(a) (1) through (8).</p> <p>Depending on the specific tenants of the project site, the project would also be required to implement health and safety policies and procedures regarding hazardous materials used where employees would be expected to handle or work around hazardous materials. Pursuant to the Federal Hazard Communication Standard (29 CFR 1910.1200) and the Laboratory Standard (29 CFR 1910.1450), Safety Data Sheets (SDS) outlining procedures to address spills and leaks for individual chemicals would be used to conduct chemical safety training for all employees who work with chemicals in order to minimize the occurrence of accidental chemical releases and ensure that, when one does occur, it is handled in a safe manner. The underground fuel storage tanks would also require permitting and monitoring by the City Fire Department and the County Department of Environmental Health as the Certified Unified Program Agency (CUPA) for Riverside County.</p> <p>These regulations inherently safeguard life and property from the hazards of fire/explosion arising from the storage, handling, and use of hazardous substances, materials, and devices, as well as hazardous conditions due to the use or occupancy of buildings. Through compliance with all applicable federal,</p>				

ISSUES & SUPPORTING INFORMATION SOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>State, and local laws, impacts to the public or environment from hazardous materials would be less than significant.</p> <p>The project is required to comply with SCAQMD Rule 402, which states: “A person shall not discharge from any source whatsoever such quantities of air contaminants or other material which cause injury, detriment, nuisance, or annoyance to any considerable number of persons or to the public, or which endanger the comfort, repose, health or safety of any such persons or the public, or which cause, or have a natural tendency to cause, injury or damage to business or property.” Furthermore, SCAQMD Rule 461 - Gasoline Transfer and Dispensing, requires the installation of enhanced vapor recovery systems that would reduce the amount of vapor that would be emitted into the atmosphere by 95 to 98 percent from levels without such systems. SCAQMD published its <i>Final Localized Significance Threshold Methodology</i> in June 2003 and updated it in July 2008,⁸¹ recommending that all air quality analyses include an assessment of both construction and operational impacts on the air quality of nearby sensitive receptors. Localized significance thresholds (LSTs) represent the maximum emissions from a project site of up to 5 acres that are not expected to result in an exceedance of the National Ambient Air Quality Standards or California Ambient Air Quality Standards for CO, NO₂, PM₁₀ and PM_{2.5}. LSTs are based on the ambient concentrations of that pollutant within the project Source Receptor Area (SRA) and the distance to the nearest sensitive receptor. For this project, the appropriate SRA is the Perris Valley Area (SRA 24) according to the project air quality analysis included in Appendix A. As detailed in Tables 3.C and 3.D, construction and operation of the proposed project would not expose nearby sensitive receptors to substantial pollutant concentrations.</p> <p>As detailed in Checklist Responses 13.a and 13.b, construction and operation of the project would not generate a substantial temporary or permanent increase in ambient noise levels or generate vibration in the vicinity of the Project in excess of standards established in the local general plan or noise ordinance with implementation of Mitigation Measures NOI-1 and NOI-2.</p> <p>Through compliance with existing regulations and policy, as well as Mitigation Measures NOI-1 and NOI-2, substantial direct or indirect effects on human beings would be reduced to less than significant with mitigation incorporated.</p>				

REFERENCES

California Board of Forestry and Fire Protection. *State Responsibility Area Viewer*. 2020. <https://bof.fire.ca.gov/projects-and-programs/state-responsibility-area-viewer/>. (Accessed June 2020).

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California Department of Conservation, Farmland Mapping and Monitoring Program. *Riverside County Important Farmland 2016*. <https://www.conservation.ca.gov/dlrp/fmmp/Pages/Riverside.aspx> (Accessed June 4, 2019).

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⁸¹ South Coast Air Quality Management District. *Final Localized Significance Thresholds Methodology*. June 2003, Revised July 2008.

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Appendix A:

**Air Quality and Greenhouse Gas Analysis for the Commercial
Retail Project at Perris Boulevard and Dracaea Avenue in the City
of Moreno Valley, California**

MEMORANDUM

DATE: May 4, 2020

TO: Dave Runberg, Cadence Acquisition LLC
Joey Ly, TAIT & Associates

FROM: Michael Slavick, Senior Air Quality Specialist

SUBJECT: Air Quality and Greenhouse Gas Analysis for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California (LSA Project No. CAQ1901)

LSA has completed an Air Quality and Greenhouse Gas Analysis for the proposed Commercial Retail Project (proposed project) at the northeast corner of Perris Boulevard and Dracaea Avenue in the City of Moreno Valley (City) in Riverside County. This Air Quality and Greenhouse Gas Analysis has been prepared using methods and assumptions recommended in the *CEQA Air Quality Handbook* of the South Coast Air Quality Management District (SCAQMD). In keeping with these guidelines, this analysis describes existing air quality and potential impacts generated by the implementation of the proposed project related to generated criteria air pollutants and greenhouse gas (GHG) emissions. Mitigation measures to reduce or eliminate significant air quality impacts are identified, where appropriate.

PROJECT DESCRIPTION

The project includes development of a 3,000-square foot drive-through restaurant and a 4,088-square foot convenience store with fuel pumps. The fueling station will include 12 fueling positons (6 multi-product dispensers [MPDs]). Development of the project is proposed on a 2.30-acre site at the northeast corner of Perris Boulevard and Dracaea Avenue and encompasses Assessor’s Parcel Numbers 479-120-042, 027, 029, and 043.

Attached Figure 1 illustrates the regional and project location. Attached Figure 2 illustrates the conceptual site plan for the project. (All figures and tables are enclosed as Attachments A and B, respectively.)

BACKGROUND

This section provides background information on air pollutants and their health effects. It also provides information from the California Air Resources Board’s *Air Quality and Land Use Handbook*¹ (*CARB Handbook*), a description of the general health risks of toxics, and the significance criteria for project evaluation.

¹ California Air Resources Board (ARB), 2005. *Air Quality and Land Use Handbook: A Community Health Perspective*. April.

Air Pollutants and Health Effects

The project site is located in the City of Moreno Valley in the non-desert portion of the County of Riverside (County), California, which is part of the South Coast Air Basin (Basin) and is under the jurisdiction of the SCAQMD. The air quality assessment for the proposed project includes the estimation of the emissions associated with short-term construction and long-term operation of the proposed project.

Both State and federal governments have established health-based Ambient Air Quality Standards (AAQS) for six criteria air pollutants:² carbon monoxide (CO), ozone (O₃), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), lead (Pb), and suspended particulate matter (PM₁₀ and PM_{2.5}). In addition, the State has set standards for sulfates, hydrogen sulfide, vinyl chloride, and visibility-reducing particles. These standards are designed to protect the health and welfare of the populace with a reasonable margin of safety. Long-term exposure to elevated levels of criteria pollutants may result in adverse health effects. However, emissions thresholds established by an air district are used to manage total regional emissions within an air basin based on the air basin's attainment status for criteria pollutants. These emissions thresholds were established for individual projects that would contribute to regional emissions and pollutant concentrations and could adversely affect or delay the projected attainment target year for certain criteria pollutants.

Because of the conservative nature of the thresholds and the basin-wide context of individual project emissions, there is no direct correlation between a single project and localized air quality-related health effects. One individual project that generates emissions exceeding a threshold does not necessarily result in adverse health effects for residents in the project vicinity. This condition is especially true when the criteria pollutants exceeding thresholds are those with regional effects, such as ozone (O₃) precursors like nitrogen oxides (NOx) and reactive organic gases (ROG).

Occupants and users of facilities such as schools, daycare centers, parks and playgrounds, hospitals, and nursing and convalescent homes are considered to be more sensitive than the general public to air pollutants because these population groups have increased susceptibility to respiratory disease. Persons engaged in strenuous work or exercise also have increased sensitivity to poor air quality. Residential areas are considered more sensitive to air quality conditions, compared to commercial and industrial areas, because people generally spend longer periods of time at their residences, with greater associated exposure to ambient air quality conditions. Recreational uses are also considered sensitive compared to commercial and industrial uses due to greater exposure to ambient air quality conditions associated with exercise.

Greenhouse Gases and Global Climate Change

Global climate change is the observed increase in the average temperature of the Earth's atmosphere and oceans in recent decades. The Earth's average near-surface atmospheric temperature rose $0.6 \pm 0.2^\circ$ Celsius ($^\circ$ C) or $1.1 \pm 0.4^\circ$ Fahrenheit ($^\circ$ F) in the 20th century. The prevailing scientific opinion on climate change is that most of the warming observed over the last 50

² United States Environmental Protection Agency (EPA), 2014. Criteria pollutants are defined as those pollutants for which the federal and State governments have established ambient air quality standards, or criteria, for outdoor concentrations in order to protect public health.

years is attributable to human activities. The increased amounts of carbon dioxide (CO₂) and other greenhouse gases (GHGs) are the primary causes of the human-induced component of warming. GHGs are released by the burning of fossil fuels, land clearing, agriculture, and other activities, and lead to an increase in the greenhouse effect.³

GHGs are present in the atmosphere naturally, are released by natural sources, or are formed from secondary reactions taking place in the atmosphere. The gases that are widely seen as the principal contributors to human-induced global climate change are:

- Carbon dioxide (CO₂);
- Methane (CH₄);
- Nitrous oxide (N₂O);
- Hydrofluorocarbons (HFCs);
- Perfluorocarbons (PFCs); and
- Sulfur Hexafluoride (SF₆).

Over the last 200 years, humans have caused substantial quantities of GHGs to be released into the atmosphere. These extra emissions are increasing GHG concentrations in the atmosphere, and enhancing the natural greenhouse effect, which is believed to be causing global warming. While manmade GHGs include naturally-occurring GHGs such as CO₂, methane, and N₂O, some gases, like HFCs, PFCs, and SF₆ are completely new to the atmosphere.

Certain gases, such as water vapor, are short-lived in the atmosphere. Others remain in the atmosphere for significant periods of time, contributing to climate change in the long term. Water vapor is excluded from the list of GHGs above because it is short-lived in the atmosphere and its atmospheric concentrations are largely determined by natural processes, such as oceanic evaporation. For the purposes of this air quality analysis, the term “GHGs” will refer collectively to the six gases listed above only.

These gases vary considerably in terms of Global Warming Potential (GWP), which is a concept developed to compare the ability of each greenhouse gas to trap heat in the atmosphere relative to another gas. The global warming potential is based on several factors, including the relative effectiveness of a gas to absorb infrared radiation and length of time that the gas remains in the atmosphere (“atmospheric lifetime”). The GWP of each gas is measured relative to carbon dioxide, the most abundant GHG; the definition of GWP for a particular GHG is the ratio of heat trapped by one unit mass of the GHG to the ratio of heat trapped by one unit mass of CO₂ over a specified time period. GHG emissions are typically measured in terms of pounds or tons of “CO₂ equivalents” (CO₂e).

³ The temperature on Earth is regulated by a system commonly known as the “greenhouse effect.” Just as the glass in a greenhouse lets heat from sunlight in and reduces the heat escaping, greenhouse gases like carbon dioxide, methane, and nitrous oxide in the atmosphere keep the Earth at a relatively even temperature. Without the greenhouse effect, the Earth would be a frozen globe; thus, although an excess of greenhouse gas results in global warming, the *naturally occurring* greenhouse effect is necessary to keep our planet at a comfortable temperature.

Air Quality Regulatory Setting

South Coast Air Quality Management District

The SCAQMD has jurisdiction over most air quality matters in the South Coast Air Basin. This area includes all of Orange County, Los Angeles County except for the Antelope Valley, the non-desert portion of western San Bernardino County, and the western and Coachella Valley portions of Riverside County. Los Angeles County is a subregion of the SCAQMD jurisdiction. The SCAQMD is the agency principally responsible for comprehensive air pollution control in the Basin and is tasked with implementing certain programs and regulations required by the Federal Clean Air Act (CAA) and the California Clean Air Act (CCAA). The SCAQMD prepares plans to attain California and National Ambient Air Quality Standards (CAAQS and NAAQS, respectively). SCAQMD is directly responsible for reducing emissions from stationary (area and point) sources. The SCAQMD develops rules and regulations, establishes permitting requirements, inspects emissions sources, and enforces such measures through educational programs or fines, when necessary.

The proposed project could be subject to the following SCAQMD rules and regulations:

- **Regulation IV – Prohibitions:** This regulation sets forth the restrictions for visible emissions, odor nuisance, fugitive dust, various air pollutant emissions, fuel contaminants, start-up/shutdown exemptions and breakdown events.
 - **Rule 402 – Nuisance:** This rule restricts the discharge of any contaminant in quantities that cause or have a natural ability to cause injury, damage, nuisance, or annoyance to businesses, property or the public. The proposed project does not plan on discharging any contaminants in quantities that would cause injury to the public or property. Future development resulting from approval of the project will comply with Rule 402.
 - **Rule 403 – Fugitive Dust:** This rule requires the prevention, reduction, or mitigation of fugitive dust emissions from a project site. Rule 403 restricts visible fugitive dust to a project property line, restricts the net PM₁₀ emissions to less than 50 µg/m³ and restricts the tracking out of bulk materials onto public roads. Additionally, Rule 403 requires an applicant to utilize one or more of the best available control measures (identified in the tables within the rule). Mitigation measures may include adding freeboard to haul vehicles, covering loose material on haul vehicles, watering, using chemical stabilizers, and/or ceasing all activities during periods of high wind. Finally, Rule 403 requires that a contingency plan be prepared if so determined by the Environmental Protection Agency (EPA). Future development resulting from approval of the project will comply with Rule 403.
- **Regulation XI – Source-Specific Standards:** Regulation XI sets emissions standards for different sources.
 - **Rule 1113 – Architectural Coatings:** This rule limits the amount of volatile organic compounds (VOCs) from architectural coatings and solvents, which lowers the emissions of odorous compounds.

The SCAQMD is responsible for demonstrating regional compliance with ambient air quality standards but has limited indirect involvement in reducing emissions from fugitive, mobile, and natural sources. To that end, the SCAQMD works cooperatively with the CARB, the Southern

California Association of Governments (SCAG), county transportation commissions, local governments, and other federal and State government agencies. It has responded to this requirement by preparing a series of Air Quality Management Plans (AQMPs) to meet the CAAQS and NAAQS. SCAQMD and the SCAG are responsible for formulating and implementing the AQMP for the South Coast Air Basin. The main purpose of an AQMP is to bring the area into compliance with federal and State air quality standards. Every three years, SCAQMD prepares a new AQMP, updating the previous plan and 20-year horizon (SCAQMD 2016).

SCAQMD approved the 2016 AQMP on March 3, 2017, and submitted the plan to CARB on March 10, 2017. Key elements of the 2016 AQMP include the following:

- Calculating and taking credit for co-benefits from other planning efforts (e.g., climate, energy, and transportation);
- A strategy with fair-share emission reductions at the federal, State, and local levels;
- Investment in strategies and technologies meeting multiple air quality objectives;
- Seeking new partnerships and significant funding for incentives to accelerate deployment of zero-emission and near-zero emission technologies;
- Enhanced socioeconomic assessment, including an expanded environmental justice analysis;
- Attainment of the 24-hour PM_{2.5} standard in 2019 with no additional measures;
- Attainment of the annual PM_{2.5} standard by 2025 with implementation of a portion of the O₃ strategy; and
- Attainment of the 1-hour O₃ standard by 2022 with no reliance on “black box” future technology (FCAA Section 182(e)(5) measures).

County of Riverside Climate Action Plan

The County of Riverside Climate Action Plan (CAP) was adopted on December 8, 2015. The CAP establishes goals and policies that incorporate environmental responsibility into County’s daily management of residential, commercial and industrial growth, education, energy and water use, air quality, transportation, waste reduction, economic development, open space, and natural habitats to further its commitment. Following the State’s adopted AB 32 GHG reduction target, the County has set a goal to reduce emissions back to 1990 levels by the year 2020. The CAP describes a baseline for the County’s GHG emissions, projects how these emissions will grow, and includes strategies to reduce emissions to a level consistent with California’s emissions reduction target. These strategies complement the County’s General Plan policies and are consistent with the vision of the County for a more sustainable community.

City of Moreno Valley General Plan

The City of Moreno Valley General Plan adopted by the City in July 11, 2006, is designed to guide City Council members make land use decisions and shape priorities to allow the City to grow according to the vision of the plan. The plan helps City departments achieve the objectives and policies of the General Plan, and serve as development guidance for projects within the City. The air

quality element identifies the role the City can play in helping the South Coast Air Basin attain the goal of meeting federal and State air quality standards, as well as the function the City has in protecting the residents and businesses from the harmful air contaminants. To achieve these goals, the air quality element has set forth a number of provisions and programs to reduce current air pollutant emissions, while requiring new development to include measures to comply with air quality requirements and to address new stringent air quality standards. The air quality strategies listed in the air quality element include:

- Adopt land use policies that site polluting facilities away from sensitive receptors and vice versa; improve job-housing balance; reduce vehicle miles traveled (VMT) and length of work trips; and improve the flow of traffic.
- Reduce air pollution by reducing emissions from mobile sources. Investment in strategies and technologies meeting multiple air quality objectives.
- Prevent and reduce pollution from stationary sources, including point sources (such as power plants and refinery boilers) and area sources (including small emission sources such as residential water heaters and architectural coatings).
- Reduce particulate matter, as defined by the EPA, as either airborne photochemical precipitates air pollution.
- Increase energy efficiency and conservation in an effort to reduce air pollution.
- Develop a public education program committed to educating the general public on the issues of air pollution and mitigation measures that can be undertaken by businesses and residents to improve air quality.
- Support a regional approach to improving air quality through multi-jurisdictional cooperation.

Moreno Valley Energy Efficiency and Climate Action Strategy

The Energy Efficiency and Climate Action Strategy were adopted by the City on October 5, 2012. In 2014, Moreno Valley was one of 12 cities that collaborated with the Western Riverside Council of Governments (WRCOG) on a Subregional Climate Action Plan (Subregional CAP) that includes 36 measures to guide Moreno Valley's GHG reduction efforts through 2020. The Energy Efficiency and Climate Action Strategy is a policy document that identifies ways that the City of Moreno Valley can reduce energy and water consumption and greenhouse gas emissions. By using energy more efficiently, harnessing renewable energy to power buildings and vehicles, improving access to sustainable transportation modes, recycling more waste, conserving water, and building local food systems, the City can support the local economy, create new green jobs, and improve public health and community quality of life. The Energy Efficiency and Climate Action Strategy contains GHG reduction measures organized into four primary sectors, as defined by the following policy goals:

- Energy:
 - Energy measures designed to increase communitywide building and equipment efficiency and renewable energy use, and promote energy efficiency and renewable energy generation for use supporting municipal operations that support the community.

- Transportation and Land Use:
 - Transportation and land use measures that would reduce single-occupancy vehicle travel, increase non-motorized travel, improve public transit access, increase motor vehicle efficiency, encourage alternative fuel vehicles and promote sustainable growth patterns.
- Water:
 - Water measures that would conserve potable water and reduce water demand by the community and municipal operations.
- Solid Waste:
 - Solid waste measures that would reduce solid waste sent to landfills that is generated by the community and municipal operations.

THRESHOLDS OF SIGNIFICANCE

The State *CEQA Guidelines* indicate that a project would normally have a significant adverse air quality impact if project-generated pollutant emissions would:

- Conflict with or obstruct implementation of the applicable air quality plan;
- Violate any air quality standard or contribute substantially to an existing or projected air quality violation;
- Result in a cumulatively considerable net increase of any criteria pollutant for which the project is nonattainment under applicable federal or State ambient air quality standards (including releasing emissions which exceed quantitative thresholds for ozone precursors);
- Expose sensitive receptors to substantial pollutant concentrations; or
- Create objectionable odors affecting a substantial number of people.

SCAQMD has established daily emissions thresholds for construction and operation of a proposed project in the Basin. The emissions thresholds were established based on the attainment status of the Basin with regard to air quality standards for specific criteria pollutants. Because the concentration standards were set at a level that protects public health with an adequate margin of safety (EPA), these emissions thresholds are regarded as conservative and would overstate an individual project's contribution to health risks.

The following CEQA significance thresholds for construction emissions have been established for the Basin:

- 75 pounds per day (lbs/day) of VOC;
- 100 lbs/day of NO_x;
- 550 lbs/day of CO;
- 150 lbs/day of PM₁₀;
- 55 lbs/day of PM_{2.5}; and

- 150 lbs/day of sulfur oxides (SOx).

Projects in the Basin with construction-related emissions that exceed any of these emission thresholds are considered to be significant under SCAQMD guidelines.

The following CEQA significance thresholds for operational emissions have been established for the Basin:

- 55 lbs/day of VOCs;
- 55 lbs/day of NOx;
- 550 lbs/day of CO;
- 150 lbs/day of PM₁₀;
- 55 lbs/day of PM_{2.5}; and
- 150 lbs/day of SOx.

Projects in the Basin with operational emissions that exceed any of these emission thresholds are considered to be significant under SCAQMD guidelines.

In addition, the SCAQMD published its *Final Localized Significance Threshold Methodology* in July 2008, recommending that all air quality analyses include an assessment of air quality impacts to nearby sensitive receptors.⁴ This guidance was used to analyze potential localized air quality impacts associated with construction of a proposed project. Localized significance thresholds (LSTs) are developed based on the size or total area of the emission source, the ambient air quality in the source receptor area, and the distance to the project. The SCAQMD defines structures that house persons (e.g., children, the elderly, persons with pre-existing respiratory or cardiovascular illness, and athletes and others who engage in frequent exercise) or places where they gather as sensitive receptors (e.g., residences, schools, playgrounds, childcare centers, convalescent centers, retirement homes, and athletic fields).

LSTs are based on the ambient concentrations of that pollutant within the project Source Receptor Area (SRA) and the distance to the nearest sensitive receptor. For the proposed project, the appropriate SRA for the LST is the nearby Perris Valley area (SRA 24). SCAQMD provides LST screening tables for 25, 50, 100, 200, and 500-meter source-receptor distances.

The State *CEQA Guidelines* indicate that a project would normally have a significant adverse GHG emissions impact if the project would:

- Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment; or
- Conflict with an applicable plan, policy or regulation adopted for the purpose of reduction the emissions of greenhouse gases.

⁴ SCAQMD. 2008. *Final Localized Significance Threshold Methodology*. July.

To provide guidance to local lead agencies on determining significance for GHG emissions in their CEQA documents, SCAQMD convened a GHG CEQA Significance Threshold Stakeholder Working Group (Working Group).⁵ Based on the last Working Group meeting (Meeting No. 15), held in September 2010, SCAQMD proposed an analysis methodology using a tiered approach for the evaluation of GHG emissions for development projects where SCAQMD is not the lead agency (SCAQMD 2010). The applicable tier for this commercial development project is Tier 3 (if GHG emissions are less than 3,000 metric tons of CO₂e per year, project-level and cumulative GHG emissions are less than significant).

IMPACTS AND MITIGATION MEASURES

The proposed project would release emissions over the short term as a result of construction activities, and over the long term from traffic generation and operation of the project. Emissions would include criteria air pollutants and GHG emissions. The sections below describe the proposed project's consistency with applicable air quality plans, estimated project emissions, and the significance of impacts with respect to SCAQMD thresholds.

Air Quality Impacts

Consistency with Applicable Air Quality Plans

A consistency determination plays an essential role in local agency project review by linking local planning and unique individual projects to the air quality plans. A consistency determination fulfills the CEQA goal of fully informing local agency decision-makers of the environmental costs of the project under consideration at a stage early enough to ensure that air quality concerns are addressed. Only new or amended General Plan elements, Specific Plans, and significantly unique projects need to undergo a consistency review due to the air quality plan strategy being based on projections from local General Plans.

Projects are considered consistent with, and would not conflict with or obstruct implementation of the AQMP, if the growth in socioeconomic factors (e.g., population, employment) is consistent with the underlying regional plans used to develop the AQMP. The future emissions forecasts are primarily based on demographic and economic growth projections provided by SCAG. Thus, demographic growth forecasts for various socioeconomic categories (e.g., population, housing, and employment by industry) developed by SCAG for its 2016 Regional Transportation Plan (SCAG 2016) were used to estimate future emissions in the Final 2016 AQMP (SCAQMD 2016).

Pursuant to the methodology provided in Chapter 12 of the SCAQMD *CEQA Air Quality Handbook* (1993), consistency with the 2016 AQMP is affirmed when a project (1) does not increase the frequency or severity of an air quality standards violation or cause a new violation and (2) is consistent with the growth assumptions in the AQMP. Consistency review is presented as follows:

⁵ South Coast Air Quality Management District. Greenhouse Gases (GHG) CEQA Significance Thresholds. Website: <http://www.aqmd.gov/home/regulations/ceqa/air-quality-analysis-handbook/ghg-significance-thresholds/>, accessed November 2018.

1. The proposed project would result in short-term construction and long-term operational pollutant emissions that are all less than the CEQA significance emissions thresholds established by the SCAQMD, as demonstrated above; therefore, the proposed project could not result in an increase in the frequency or severity of any air quality standards violation and will not cause a new air quality standard violation.
2. The *CEQA Air Quality Handbook* (1993) indicates that consistency with AQMP growth assumptions must be analyzed for new or amended General Plan elements, Specific Plans, and significant projects. Significant projects include airports, electrical generating facilities, petroleum and gas refineries, designation of oil drilling districts, water ports, solid waste disposal sites, and offshore drilling facilities; therefore, the proposed project is not defined as a significant project.

The proposed project would construct a 3,000-square foot drive through restaurant and a 4,088-square foot convenience store with 12 fuel pumps. The project site is designated as a residential office land use in the City's General Plan Land Use Map and is zoned as office commercial. The proposed land uses are not allowed under the existing zoning designation of the site; therefore, the project includes a Zone Change and General Plan Amendment to designate the project site as Community Commercial (CC). The changes in the land use designation would not alter the housing and employment forecast in the City. In addition, there are community commercial uses in the General Plan that may be reallocated within the City and its housing and employment forecast are already included in the AQMP assumptions. As such, the proposed project is not anticipated to exceed the AQMP assumptions for the project site and is found to be consistent with the AQMP for the Basin. In addition, as discussed below, construction of the proposed project would not result in the generation of criteria air pollutants that would exceed SCAQMD thresholds of significance. Operational emissions associated with the proposed project would also not exceed SCAQMD established significance thresholds for VOC, NO_x, CO, SO₂, PM₁₀, or PM_{2.5} emissions. Based on the consistency analysis presented above, the proposed project would be consistent with the current regional AQMP and would not conflict with or obstruct implementation of the 2016 AQMP.

Short-Term Construction Emissions

During construction, short-term degradation of air quality may occur due to the release of particulate emissions generated by grading, paving, building, and other activities. Emissions from construction equipment are also anticipated and would include CO, NO_x, ROG, directly-emitted particulate matter (PM_{2.5} and PM₁₀), and Toxic Air Contaminants (TACs) such as diesel exhaust particulate matter (DPM).

Project construction activities would include grading, paving, and building activities. Construction-related effects on air quality from the proposed project would be greatest during the site preparation phase due to the disturbance of soils. If not properly controlled, these activities would temporarily generate particulate emissions. Sources of fugitive dust would include disturbed soils at the construction site. Unless properly controlled, vehicles leaving the site would deposit dirt and mud on local streets, which could be an additional source of airborne dust after it dries. PM₁₀ emissions would vary from day to day, depending on the nature and magnitude of construction activity and local weather conditions. PM₁₀ emissions would depend on soil moisture, silt content of soil, wind speed, and the amount of operating equipment. Larger dust particles would settle near

the source, while fine particles would be dispersed over greater distances from the construction site.

Water or other soil stabilizers can be used to control dust, resulting in emission reductions of 50 percent or more. The SCAQMD has established Rule 403: Fugitive Dust, which would require the applicant to implement measures that would reduce the amount of particulate matter generated during the construction period.

In addition to dust-related PM₁₀ emissions, heavy trucks and construction equipment powered by gasoline and diesel engines would generate CO, SO₂, NO_x, VOCs and some soot particulate (PM_{2.5} and PM₁₀) in exhaust emissions. If construction activities were to increase traffic congestion in the area, CO and other emissions from traffic would increase slightly while those vehicles idle in traffic. These emissions would be temporary in nature and limited to the immediate area surrounding the construction site.

Construction emissions were estimated for the project using the California Emissions Estimator Model (CalEEMod) version 2016.3.2, consistent with SCAQMD recommendations. For purposes of this analysis, the construction schedule was assumed to be approximately six months. The proposed project will be constructed modular portable buildings and the majority of the modular building manufacturing and interior painting will be completed off site. Other precise details of construction activities are unknown at this time; therefore, default assumptions (e.g., construction duration and fleet activities) from CalEEMod were assumed. Table A summarizes construction-related emissions. CalEEMod output sheets are enclosed as Attachment C.

Fugitive Dust. Fugitive dust emissions are generally associated with land clearing and exposure of soils to the air and wind, as well as cut-and-fill grading operations. Dust generated during construction varies substantially on a project-by-project basis, depending on the level of activity, the specific operations, and weather conditions at the time of construction. The proposed project will be required to comply with SCAQMD Rule 403 to control fugitive dust.

Architectural Coatings. Architectural coatings contain VOCs that are part of the O₃ precursors. Based on the proposed project, it is estimated that application of the architectural coatings for the proposed peak construction day will result in a peak of 9.98 lbs/day of VOC. Therefore, VOC emissions from architectural coating application would not exceed the SCAQMD VOC threshold of 75.00 lbs/day.

Construction Emissions Results. As shown in Table A, construction emissions associated with the project would be less than significant for ROG, NO_x, CO, SO_x, PM_{2.5}, and PM₁₀ exhaust emissions. Additionally, construction activities associated with the project would be required to comply with SCAQMD Rule 403: Fugitive Dust, which would require the implementation of measures that would reduce the amount of particulate matter generated during the construction period.

Long-Term Operational Emissions

Long-term air pollutant emission impacts are those associated with area sources and mobile sources related to the proposed project. In addition to the short-term construction emissions, the project

would also generate long-term air pollutant emissions, such as those associated with changes in permanent use of the project site. These long-term emissions are primarily mobile source emissions that would result from vehicle trips associated with the proposed project. Area sources, such as natural gas heaters, landscape equipment, and use of consumer products, would also result in pollutant emissions.

PM₁₀ emissions result from running exhaust, tire and brake wear, and the entrainment of dust into the atmosphere from vehicles traveling on paved roadways. Entrainment of PM₁₀ occurs when vehicle tires pulverize small rocks and pavement and the vehicle wakes generate airborne dust. The contribution of tire and brake wear is small compared to the other PM emission processes. Gasoline-powered engines have small rates of particulate matter emissions compared with diesel-powered vehicles.

Energy source emissions result from activities in buildings for which electricity and natural gas are used. The quantity of emissions is the product of usage intensity (i.e., the amount of electricity or natural gas) and the emission factor of the fuel source. Major sources of energy demand include building mechanical systems, such as heating and air conditioning, lighting, and plug-in electronics, such as refrigerators or computers. Greater building or appliance efficiency reduces the amount of energy for a given activity and thus lowers the resultant emissions. The emission factor is determined by the fuel source, with cleaner energy sources, like renewable energy, producing fewer emissions than conventional sources. Area source emissions associated with the project would include emissions from water heating and the use of landscaping equipment.

Emission estimates for operation of the project were calculated using CalEEMod and Table B shows model results. Trip generation rates for the project were based on the project's trip generation estimates, as identified in Table 5-A of the Traffic Impact Analysis report.⁶ Based on the Traffic Impact Analysis report, the proposed project would generate approximately 4,925 average daily trips (i.e., ADTs without internal capture and pass-by trips), with approximately 416 trips occurring in the a.m. peak hour and approximately 298 trips occurring in the p.m. peak hour.

The primary emissions associated with the project are regional in nature, meaning that air pollutants are rapidly dispersed on release or, in the case of vehicle emissions associated with the project; emissions are released in other areas of the Basin. Table B identifies the daily emissions associated with project operational trip generation, energy, and area sources for ROG, NO_x, CO, SO_x, PM₁₀, and PM_{2.5}.

The results shown in Table B indicate the project would not exceed the significance criteria for daily ROG, NO_x, CO, SO_x, PM₁₀ or PM_{2.5} emissions; therefore, the proposed project would not have a significant effect on regional air quality and mitigation would not be required.

Localized Significance Analysis

Project construction emissions were compared to the LST screening tables in SRA 24, based on a 25-meter source-receptor distance and a 2.30-acre project size. The results of the LST analysis,

⁶ *Traffic Impact Analysis for Commercial Retail Project at Perris Boulevard and Dracaea Avenue*. LSA August 2019.

summarized in Table C and Table D, indicate that the project would not result in an exceedance of the SCAQMD LST during project construction or operation. Therefore, the proposed project would result in less than significant localized air quality impacts during project construction and operation.

Cumulative Air Quality Impacts

In analyzing cumulative impacts from a proposed project, the analysis must specifically evaluate a project's contribution to the cumulative increase in pollutants for which the Basin is listed as nonattainment for the State and federal ambient air quality standards. The proposed project would have a cumulatively considerable impact if project-generated emissions would exceed thresholds for NO_x, VOC, PM₁₀, and/or PM_{2.5}. If the proposed project does not exceed thresholds and is determined to have less than significant project-specific impacts, it may still have a cumulatively considerable impact on air quality and GHG if the emissions from the project, in combination with emissions from other proposed or reasonably foreseeable future projects, are in excess of established thresholds. However, the proposed project would be considered to have a cumulative impact only if its contribution accounts for a significant portion of the cumulative total emissions.

The geographic extent for the analysis of cumulative impacts related to air quality includes the central area of the South Coast Air Basin. Due to the nonattainment status of the Basin, the primary air pollutants of concern would be NO_x and VOCs, which are ozone precursors, and PM₁₀ and PM_{2.5}. Project-related NO_x and VOCs are primarily emitted from motor vehicles and construction equipment, while PM₁₀ and PM_{2.5} are emitted primarily as fugitive dust during construction. Because of the nature of ozone as a regional air pollutant, emissions from the entire geographic area for this cumulative impact analysis would tend to be important, although maximum ozone impacts generally occur downwind of the area in which the ozone precursors are released. PM₁₀ and PM_{2.5} impacts, on the other hand, would tend to occur locally; thus, projects occurring in the same general area and in the same time period would tend to create cumulative air quality impacts.

The project would contribute criteria pollutants to the area during project construction. A number of individual projects in the area may be under construction simultaneously with the proposed project. Depending on construction schedules and actual implementation of projects in the area, generation of fugitive dust and pollutant emissions during construction could result in substantial short-term increases in air pollutants. However, each project would be required to comply with the SCAQMD's standard construction measures. The proposed project's short-term construction CO, NO₂, PM₁₀, and PM_{2.5} emissions would not exceed the LSTs. Therefore, construction of the proposed project would have a less than significant impact with regard to regional and localized emissions and impacts would not be cumulatively considerable.

Sensitive Receptors

Sensitive receptors are defined as people that have an increased sensitivity to air pollution or environmental contaminants. Sensitive receptor locations include schools, parks and playgrounds, day care centers, nursing homes, hospitals, and residential dwelling units. The project site is located within an area characterized by a mix of land uses that includes single-family residential uses to the north, east, and south and general commercial/retail uses to the west and southwest. The nearest sensitive receptors to the project site are the off-site single-family residences approximately 50 feet from the northern and eastern border of the project site.

Construction of the proposed project may expose surrounding sensitive receptors to airborne particulates, as well as a small quantity of construction equipment pollutants (i.e., usually diesel-fueled vehicles and equipment). However, construction contractors would be required to implement measures to reduce or eliminate emissions by following SCAQMD standard construction practices. As shown in Table C and Table D, the project would not result in significant localized emissions during project construction or operation. Therefore, once the project is constructed, the project would not be a source of substantial pollutant emissions and sensitive receptors would not be exposed to substantial pollutant concentrations during project construction and operation.

Objectionable Odors

During project construction, some odors may be present due to diesel exhaust. However, these odors would be temporary and limited to the construction period. The proposed project would not include any activities or operations that would generate objectionable odors and once operational, the project would not be a source of odors. Therefore, the proposed project would not create objectionable odors affecting a substantial number of people.

Greenhouse Gas Impacts

This section discusses the proposed project's potential impacts related to the release of GHG emissions for both construction and project operation.

Construction Greenhouse Gas Impacts

Construction activities associated with the proposed project would produce combustion emissions from various sources. During construction, GHGs would be emitted through the operation of construction equipment and from worker and builder supply vendor vehicles, each of which typically uses fossil-based fuels to operate. The combustion of fossil-based fuels creates GHGs such as CO₂, CH₄, and N₂O. Furthermore, CH₄ is emitted during the fueling of heavy equipment. Exhaust emissions from on-site construction activities would vary daily as construction activity levels change.

The SCAQMD does not have an adopted threshold of significance for construction-related greenhouse gas emissions. However, lead agencies are required to quantify and disclose greenhouse gas emissions that would occur during construction. The SCAQMD then requires the construction GHG emissions to be amortized over the life of the project, defined as 30 years, added to the operational emissions, and compared to the applicable interim GHG significance threshold tier.

Using CalEEMod, it is estimated that the project would generate approximately 127.01 metric tons of CO₂e during construction of the project. When annualized over the 30-year life of the project, annual emissions would be 4.23 metric tons CO₂e.

Operational Greenhouse Gas Impacts

Long-term operation of the proposed project would generate GHG emissions from area and mobile sources as well as indirect emissions from sources associated with energy consumption. Mobile-source GHG emissions would include project-generated vehicle trips associated with trips to the proposed project. Area-source emissions would be associated with activities such as landscaping and maintenance on the project site, and other sources.

Following guidance from the SCAQMD, GHG emissions were estimated using CalEEMod. Table E shows the calculated GHG emissions for the proposed project. Motor vehicle emissions are the largest source of GHG emissions for the project at approximately 95 percent of the total. Energy use is the next largest category at 4 percent. Water and waste are about 1 percent of the total emissions. Additional calculation details are included in Attachment C.

As discussed above, according to SCAQMD, a project would have less than significant GHG emissions if it would result in operational-related GHG emissions of less than 3,000 metric tons of CO₂e per year. Based on the analysis results, the proposed project would result in approximately 2,584 metric tons of CO₂e per year, which would be well below the SCAQMD's numeric threshold of 3,000 metric tons of CO₂e per year. Therefore, operation of the proposed project would not generate significant GHG emissions that would have a significant effect on the environment.

Consistency with Greenhouse Gas Reduction Plans

As discussed above, the Energy Efficiency and Climate Action Strategy outlines a programmatic approach to review the potential GHG-related impacts associated with new development. As detailed in Table F, the project would be consistent with the Energy Efficiency and Climate Action Strategy and applicable State regulations.

As shown in Table F, the project would be consistent with all applicable GHG reduction strategies of the Energy Efficiency and Climate Action Strategy. Some of the measures are not applicable to the project. Furthermore, the project would be consistent with applicable land use and zoning re-designations, would not conflict with any State and City regulations intended to reduce GHG emissions statewide, and would be consistent with applicable plans and programs designed to reduce GHG emissions. Therefore, the project would have a less than significant impact.

Cumulative Greenhouse Gas Impacts

As climate change impacts are cumulative in nature, no typical single project can result in emissions of such a magnitude that it, in and by itself, would be significant on a project basis. The proposed project has incorporated sustainability design measures in accordance with regulatory requirements as provided throughout the analysis and to reduce the proposed project's potential impact with respect to GHG emissions. As GHG emissions would not exceed the SCAQMD Tier 4 threshold and project design measures will be applied to lower the GHG emissions, the proposed project would result in a less than significant cumulative impact related to GHG emissions. The proposed project's GHG reduction measures make it consistent with AB 32, 2016 RTP/SCS, and the City's CAP. Therefore, the proposed project would not conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the GHG emissions. Given this consistency, it is concluded that the proposed project's impacts are not cumulatively considerable.

MITIGATION MEASURES

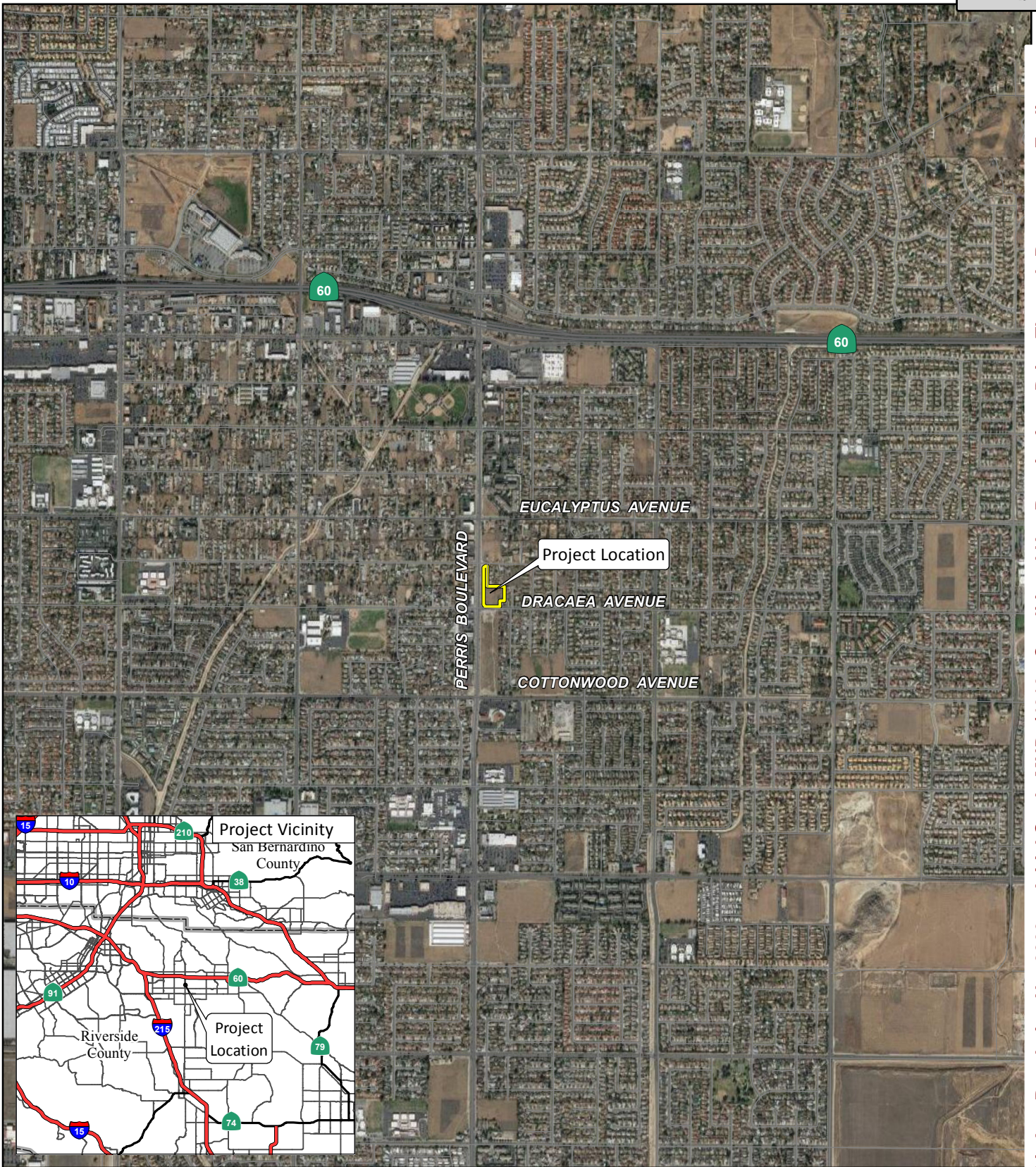
No mitigation measures are required.

ATTACHMENTS

- A: Figures

- Figure 1: Regional and Project Location
- Figure 2: Conceptual Site Plan
- B: Tables
 - Table A: Project Construction Emissions in Pounds Per Day
 - Table B: Project Operational Emissions in Pounds Per Day
 - Table C: Project Localized Construction Emissions in Pounds Per Day
 - Table D: Project Localized Operational Emissions in Pounds Per Day
 - Table E: GHG Emissions (Metric Tons Per Year)
 - Table F: Moreno Valley Energy Efficiency and Climate Action Strategy Consistency
- C: CalEEMod Output Files

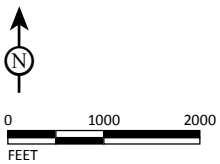
ATTACHMENT A
FIGURES



Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

FIGURE 1

LSA



SOURCE: ESRI Streetmap, 2013; Google Earthl, 2018.

I:\CAQ1901\Reports\Traffic\fig1-1_RegLoc.mxd (7/19/2019)

Perris/Dracaea Commercial Project

Regional and Project Location

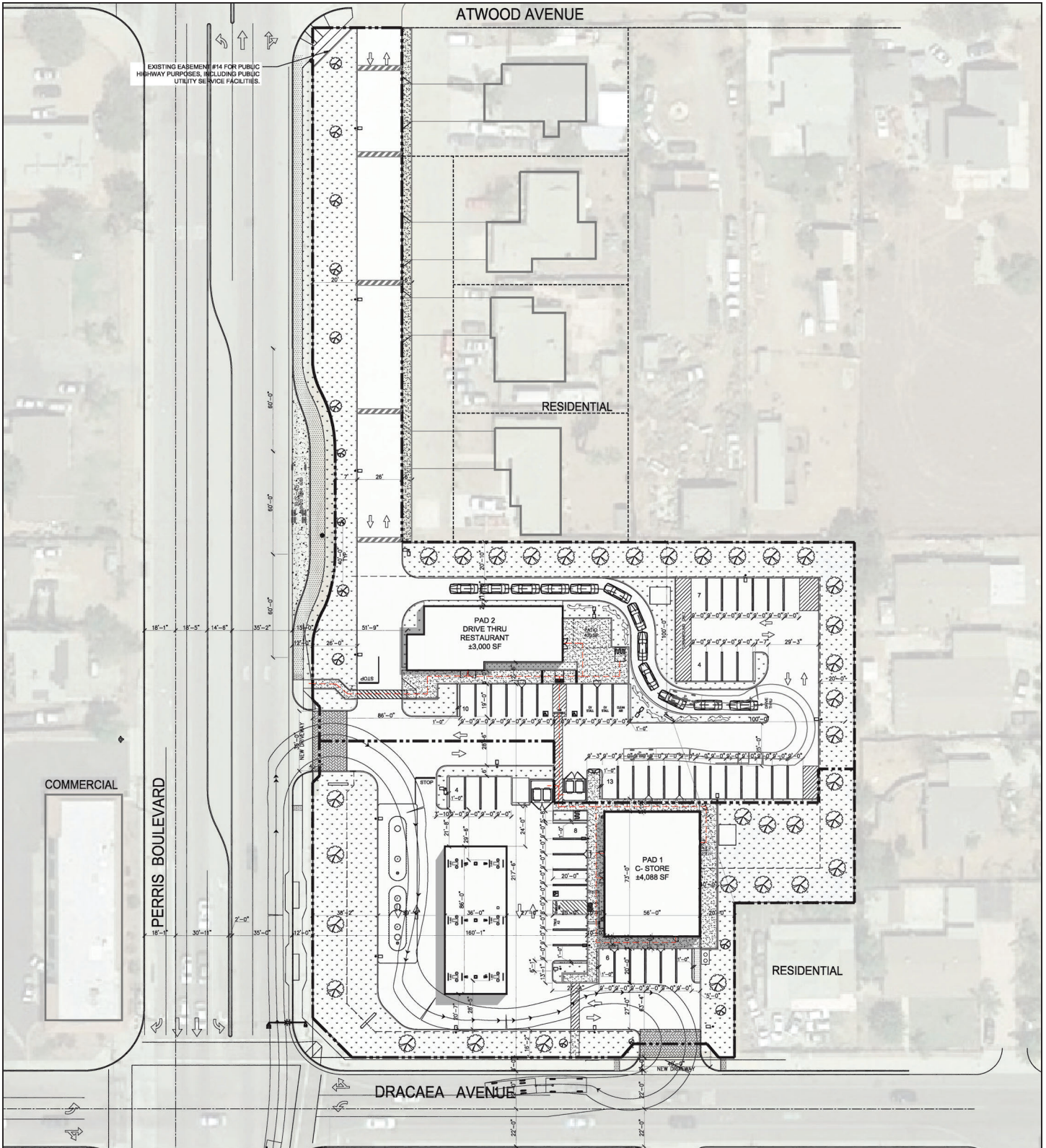
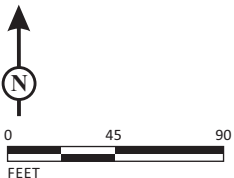


FIGURE 2

LSA



SOURCE: Cadence Capital Investments LLC; June 2020

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Commercial Retail Project at Perris Boulevard/Dracaea Avenue

Site Plan

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

ATTACHMENT B
TABLES

Table A: Project Construction Emissions in Pounds Per Day

Project Construction	ROG	NOx	CO	SOx	PM ₁₀	PM _{2.5}
Site Preparation	1.70	20.20	11.75	0.03	1.05	0.77
Grading	1.63	24.74	11.27	0.05	2.37	1.12
Building Construction	2.35	17.49	16.87	0.03	1.12	0.98
Paving	2.19	9.42	10.37	0.02	0.31	0.52
Architectural Coating	9.98	1.69	1.91	0.00	0.13	0.12
Maximum (pounds per day)	9.98	24.74	16.87	0.05	2.37	1.12
SCAQMD Threshold	75.00	100.00	550.00	150.00	150.00	55.00
Exceeds?	No	No	No	No	No	No

Source: LSA (April 2020).

Table B: Project Operational Emissions in Pounds Per Day

Source	ROG	NOx	CO	SOx	PM ₁₀	PM _{2.5}
Area Sources	0.16	<0.01	<0.01	0	<0.01	<0.01
Energy Sources	0.02	0.22	0.19	<0.01	0.02	0.02
Mobile Sources	6.03	36.81	36.38	0.16	9.67	2.64
Total Emissions	6.22	37.03	36.57	0.16	9.69	2.66
SCAQMD Threshold	55.00	55.00	550.00	150.00	150.00	55.00
Exceeds?	No	No	No	No	No	No

Source: LSA (April 2020).

Table C: Project Localized Construction Emissions in Pounds Per Day

Source	NOx	CO	PM ₁₀	PM _{2.5}
On-Site Project Emissions	20.2	16.4	1.5	0.9
Localized Significance Threshold	170.0	833.0	7.0	4.0
Exceeds?	No	No	No	No

Source: LSA (April 2020).

Table D: Project Localized Operational Emissions in Pounds Per Day

Source	NOx	CO	PM ₁₀	PM _{2.5}
On-Site Project Emissions	1.8	1.8	0.5	0.1
Localized Significance Threshold	170.0	833.0	2.0	1.0
Exceeds?	No	No	No	No

Source: LSA (April 2020).

Table E: GHG Emissions (Metric Tons Per Year)

Emissions Source	Operational Emissions				
	CO ₂	CH ₄	N ₂ O	CO ₂ e	Percent of Total
Area Source Emissions	<0.01	0	0	<0.01	0.0
Energy Source Emissions	108.41	<0.01	<0.01	108.91	4.2
Mobile Source Emissions	2,443.16	0.18	0	2,447.57	94.80
Waste Source Emissions	7.02	0.41	0	17.38	0.7
Water Source Emissions	5.11	0.03	<0.01	6.20	0.3
Total Operational Emissions				2,580.07	100%
Amortized Construction Emissions				4.23	—
Total Annual Emissions				2,584.30	—
SCAQMD Threshold				3,000.00	—
Exceed?				No	—

Source: LSA (April 2020).

Table F: Moreno Valley Energy Efficiency and Climate Action Strategy Consistency

Measure/Regulation	Project Consistency
State and Regional Regulations	
Energy	
California Building Energy Efficiency Standards (Title 24, Part 6). Maximize energy efficiency building and appliance standards, and pursue additional efficiency efforts including new technologies, and new policy and implementation mechanisms. Pursue comparable investment in energy efficiency from all retail providers of electricity in California (including both investor-owned and publicly owned utilities).	Consistent. The proposed project will comply with the requirements of the 2020 California Building Energy Efficiency Standards (Title 24, Part 6) including measures to incorporate energy-efficient building design features.
Water	
Water Use Efficiency. Reduce per capita water use by 20% by 2020. SB X7-7 is part of a California legislative package passed in 2009 that requires urban retail water suppliers to reduce per-capita water use by 10% from a baseline level by 2015, and to reduce per capita water use by 20% by 2020. Green Accountability Performance (GAP) Goal 16 directly aligns with SB X7-7. In Southern California, energy costs and GHG emissions associated with the transport, treatment, and delivery of water from outlying regions are high. Therefore, the region has extra incentive to reduce water consumption. While this is considered a State measure, it is up to the local water retailers, jurisdictions, and water users to meet these targets.	Consistent. The proposed project will comply with the requirements of Title 19 – Article VIII – Chapter 19.570 – Water Efficient Landscaping and Irrigation, including measures to increase water use efficiency. Water efficient irrigation systems and devices and drought-tolerant landscaping will be installed on the project site.
Solid Waste	
Construction and Demolition (C&D) Waste Diversion. Meet mandatory requirement to divert 50% of C&D waste from landfills by 2020 and exceed requirement by diverting 90% of C&D waste from landfills by 2035.	Consistent. In compliance with CalGreen requirements, at least 65 percent of all nonhazardous construction waste generated by the proposed project would be recycled and/or salvaged (including, but not limited to, soil, vegetation, concrete, lumber, metal, and cardboard). Furthermore, 100 percent of excavated soil shall be reused or recycled.

Table F: Moreno Valley Energy Efficiency and Climate Action Strategy Consistency

Measure/Regulation	Project Consistency
Transportation	
<p>Pavley and Low Carbon Fuel Standard (LCFS). The California Air Resources Board identified this measure as a Discrete Early Action Measure. This measure would reduce the carbon intensity of California’s transportation fuels by at least 10 percent by 2020.</p>	<p>Consistent. The project does not involve the manufacture, sale, or purchase of vehicles. However, vehicles that operate within and access the project site will comply with Pavley and Low Carbon Fuel Standard. No feature of the project will interfere with implementation of these requirements and programs.</p>
Moreno Valley Energy Efficiency and CAS Measures	
Transportation	
<p>R2-T1: Land Use Based Trips and VMT Reduction Policies. Encourage the development of Transit Priority Projects along High Quality Transit Corridors identified in the SCAG Sustainable Communities Plan, to allow a reduction in vehicle miles traveled.</p>	<p>Not Applicable. This objective is aimed at government agencies, not private developers.</p>
<p>R2-T3: Employment-Based Trip Reductions. Require a Transportation Demand Management (TDM) program for new development to reduce automobile travel by encouraging ride-sharing, carpooling, and alternative modes of transportation.</p>	<p>Consistent. The project will reduce vehicle miles traveled and emissions associated with by implementing the pedestrian and bicycle connections to the surrounding areas and implementing a voluntary trip reduction programs in consistent with the City’s General Plan.</p>
Energy	
<p>R2-E1: New Construction Residential Energy Efficiency Requirements. Require energy efficient design for all new residential buildings to be 10 percent beyond the current Title 24 standards.</p>	<p>Not Applicable. This measure applies to residential projects.</p>
<p>R2-E2: New Construction Residential Renewable Energy. Facilitate the use of renewable energy (such as solar (photovoltaic) panels or small wind turbines) for new residential developments. An alternative approach would be the purchase of renewable energy resources off site.</p>	<p>Not Applicable. This measure applies to residential projects.</p>
<p>R2-E5: New Construction Commercial Energy Efficiency Requirements. Require energy efficient design for all new commercial buildings to be 10% beyond the 2008 Title 24 standards (which were in effect at the time the CAP was adopted).</p>	<p>Consistent. 2013 Title 24 requirements would achieve greater reduction than envisioned by the City’s Climate Action Strategy. The project would comply with applicable energy efficiency requirements detailed in the Green Building Standards Code (Title 24, California Code of Regulations). Further, the project would be required to comply with any adopted Municipal Code requirements set forth by the City of Moreno Valley.</p>
<p>R3-E1: Energy Efficient Development, and Renewable Energy Deployment Facilitation and Streamlining. Updating of codes and zoning requirements and guidelines to further implement green building practices. This could include incentives for energy-efficient projects.</p>	<p>Not Applicable. This policy is not applicable on a project level.</p>
<p>R3-L2: Heat Island Plan. Develop measures that address “heat islands.” Potential measures include using strategically placed shade trees, using paving materials with a Solar Reflective Index of at least 29, an open grid pavement system, or covered parking</p>	<p>Consistent. The project will comply with the City of Moreno Valley’s landscaping requirements.</p>

Table F: Moreno Valley Energy Efficiency and Climate Action Strategy Consistency

Measure/Regulation	Project Consistency
Water	
<p>R2-W1: Water Use Reduction Initiative. Consider adopting a per capita water use reduction goal, which mandates the reduction of water use of 20 percent per capita with requirements applicable to new development and with cooperative support of the water agencies.</p>	<p>Consistent. California Green Building Standards Code, Chapter 5, Division 5.3, Section 5.303.2 requires that indoor water use be reduced by 20 percent. Section 5.304.3 requires irrigation controllers and sensors</p>
<p>R3-W1: Water Efficiency Training and Education. Work with EMWD and local water companies to implement a public information and education program that promotes water conservation.</p>	<p>Not Applicable. This policy is not applicable at a project level.</p>
Solid Waste	
<p>R2-S1: City Diversion Program. For solid waste, consider a target of increasing the waste diverted from the landfill to a total of 75 percent by 2020.</p>	<p>Consistent. The project will comply with the City of Moreno Valley’s citywide goal of solid waste reduction. Additionally the project will be compliant with the City of Moreno Valley’s Municipal Code 8.80.030 by implementing a Waste Management Plan.</p>

Source: LSA (April 2020).

ATTACHMENT C
CALEEMOD OUTPUT FILES

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

Dracaea/Perris Commercial Project
Riverside-South Coast County, Winter

1.0 Project Characteristics

1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Parking Lot	52.00	Space	2.14	20,800.00	0
Fast Food Restaurant with Drive Thru	3.00	1000sqft	0.07	3,000.00	0
Convenience Market With Gas Pumps	12.00	Pump	0.09	4,088.00	0

1.2 Other Project Characteristics

Urbanization	Urban	Wind Speed (m/s)	2.4	Precipitation Freq (Days)	28
Climate Zone	10			Operational Year	2024
Utility Company	Southern California Edison				
CO2 Intensity (lb/MW hr)	702.44	CH4 Intensity (lb/MW hr)	0.029	N2O Intensity (lb/MW hr)	0.006

1.3 User Entered Comments & Non-Default Data

Project Characteristics -

Land Use - Project site area is 2.3 acres.

Construction Phase -

Grading - Project site is approx. 2.30 acres. Assume fuel tanks installation requires soil excavation of approx. 700 cy to be exported.

Vehicle Trips - Trip generation rates without pass-by trips were obtained from Table 5-A of the Traffic Impact Analysis report (LSA 2020)

Construction Off-road Equipment Mitigation - Fugitive dust controled with on-site watering at least 3 times daily - SCAQMD Rule 403.

Area Mitigation -

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

Table Name	Column Name	Default Value	New Value
tblArchitecturalCoating	ConstArea_Nonresidential_Exterior	3,544.00	2,347.00
tblArchitecturalCoating	ConstArea_Nonresidential_Interior	10,632.00	7,041.00
tblArchitecturalCoating	ConstArea_Parking	1,248.00	1,104.00
tblAreaCoating	Area_Nonresidential_Exterior	3544	2347
tblAreaCoating	Area_Nonresidential_Interior	10632	7041
tblAreaCoating	Area_Parking	1248	1104
tblAreaMitigation	UseLowVOCPaintParkingCheck	False	True
tblConstructionPhase	NumDays	10.00	5.00
tblConstructionPhase	NumDays	220.00	100.00
tblConstructionPhase	NumDays	6.00	2.00
tblConstructionPhase	NumDays	10.00	5.00
tblConstructionPhase	NumDays	3.00	1.00
tblGrading	AcresOfGrading	1.00	2.30
tblGrading	AcresOfGrading	1.50	0.50
tblGrading	MaterialExported	0.00	700.00
tblLandUse	LandUseSquareFeet	1,694.10	4,088.00
tblLandUse	LotAcreage	0.47	2.14
tblLandUse	LotAcreage	0.04	0.09
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	1.00	4.00
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	2.00	1.00
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	1.00	2.00
tblOffRoadEquipment	UsageHours	8.00	6.00
tblOffRoadEquipment	UsageHours	8.00	4.00
tblOffRoadEquipment	UsageHours	7.00	6.00
tblOffRoadEquipment	UsageHours	8.00	7.00
tblOffRoadEquipment	UsageHours	8.00	7.00

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

tblOffRoadEquipment	UsageHours	8.00	1.00
tblOffRoadEquipment	UsageHours	6.00	8.00
tblOffRoadEquipment	UsageHours	7.00	6.00
tblOffRoadEquipment	UsageHours	8.00	7.00
tblOffRoadEquipment	UsageHours	7.00	8.00
tblTripsAndVMT	VendorTripNumber	5.00	4.00
tblTripsAndVMT	WorkerTripNumber	8.00	5.00
tblTripsAndVMT	WorkerTripNumber	13.00	10.00
tblTripsAndVMT	WorkerTripNumber	11.00	10.00
tblTripsAndVMT	WorkerTripNumber	20.00	18.00
tblVehicleTrips	PB_TP	65.00	59.00
tblVehicleTrips	PR_TP	14.00	20.00
tblVehicleTrips	WD_TR	542.60	205.36
tblVehicleTrips	WD_TR	496.12	820.38

2.0 Emissions Summary

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

2.1 Overall Construction (Maximum Daily Emission)

Unmitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	lb/day										lb/day					
2020	9.9782	24.7392	16.8084	0.0523	2.8982	0.9823	3.6019	0.7928	0.9404	1.4574	0.0000	5,354.0319	5,354.0319	0.7808	0.0000	5,370.5084
Maximum	9.9782	24.7392	16.8084	0.0523	2.8982	0.9823	3.6019	0.7928	0.9404	1.4574	0.0000	5,354.0319	5,354.0319	0.7808	0.0000	5,370.5084

Mitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	lb/day										lb/day					
2020	9.9782	24.7392	16.8084	0.0523	1.6680	0.9823	2.3718	0.4560	0.9404	1.1206	0.0000	5,354.0319	5,354.0319	0.7808	0.0000	5,370.5084
Maximum	9.9782	24.7392	16.8084	0.0523	1.6680	0.9823	2.3718	0.4560	0.9404	1.1206	0.0000	5,354.0319	5,354.0319	0.7808	0.0000	5,370.5084

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	42.45	0.00	34.15	42.49	0.00	23.11	0.00	0.00	0.00	0.00	0.00	0.00

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

2.2 Overall Operational

Unmitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Area	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Energy	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199
Mobile	4.8881	36.0464	34.8420	0.1460	9.5850	0.0859	9.6709	2.5639	0.0799	2.6439		15,016.8251	15,016.8251	1.1788		15,046.2941
Total	5.0743	36.2693	35.0359	0.1473	9.5850	0.1029	9.6879	2.5639	0.0969	2.6608		15,284.1710	15,284.1710	1.1839	4.9000e-003	15,315.2296

Mitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Area	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Energy	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199
Mobile	4.8881	36.0464	34.8420	0.1460	9.5850	0.0859	9.6709	2.5639	0.0799	2.6439		15,016.8251	15,016.8251	1.1788		15,046.2941
Total	5.0743	36.2693	35.0359	0.1473	9.5850	0.1029	9.6879	2.5639	0.0969	2.6608		15,284.1710	15,284.1710	1.1839	4.9000e-003	15,315.2296

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	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

3.0 Construction Detail

Construction Phase

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Site Preparation	Site Preparation	6/1/2020	6/1/2020	5	1	
2	Grading	Grading	6/2/2020	6/3/2020	5	2	
3	Building Construction	Building Construction	6/4/2020	10/21/2020	5	100	
4	Paving	Paving	10/22/2020	10/28/2020	5	5	
5	Architectural Coating	Architectural Coating	10/29/2020	11/4/2020	5	5	

Acres of Grading (Site Preparation Phase): 0.5

Acres of Grading (Grading Phase): 2.3

Acres of Paving: 2.14

Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 7,041; Non-Residential Outdoor: 2,347; Striped Parking Area: 1,104 (Architectural Coating – sqft)

OffRoad Equipment

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

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Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Site Preparation	Graders	1	8.00	187	0.41
Site Preparation	Tractors/Loaders/Backhoes	1	8.00	97	0.37
Grading	Concrete/Industrial Saws	1	8.00	81	0.73
Grading	Rubber Tired Dozers	1	1.00	247	0.40
Grading	Tractors/Loaders/Backhoes	2	6.00	97	0.37
Building Construction	Cranes	1	4.00	231	0.29
Building Construction	Forklifts	2	6.00	89	0.20
Building Construction	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Paving	Cement and Mortar Mixers	4	6.00	9	0.56
Paving	Pavers	1	7.00	130	0.42
Paving	Rollers	1	7.00	80	0.38
Paving	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Architectural Coating	Air Compressors	1	6.00	78	0.48
Building Construction	Generator Sets	1	8.00	84	0.74
Grading	Graders	1	8.00	187	0.41
Paving	Paving Equipment	1	8.00	132	0.36
Site Preparation	Scrapers	1	8.00	367	0.48
Building Construction	Welders	3	8.00	46	0.45

Trips and VMT

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Site Preparation	3	5.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	5	10.00	0.00	88.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	9	10.00	4.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	8	18.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	2.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

3.1 Mitigation Measures Construction

Water Exposed Area

3.2 Site Preparation - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					0.5303	0.0000	0.5303	0.0573	0.0000	0.0573			0.0000			0.0000
Off-Road	1.6782	20.1828	11.5528	0.0249		0.7937	0.7937		0.7302	0.7302		2,410.502 3	2,410.502 3	0.7796		2,429.992 4
Total	1.6782	20.1828	11.5528	0.0249	0.5303	0.7937	1.3240	0.0573	0.7302	0.7875		2,410.502 3	2,410.502 3	0.7796		2,429.992 4

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

3.2 Site Preparation - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0249	0.0156	0.1631	5.0000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		49.4118	49.4118	1.2300e-003		49.4425
Total	0.0249	0.0156	0.1631	5.0000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		49.4118	49.4118	1.2300e-003		49.4425

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					0.2068	0.0000	0.2068	0.0223	0.0000	0.0223			0.0000			0.0000
Off-Road	1.6782	20.1828	11.5528	0.0249		0.7937	0.7937		0.7302	0.7302	0.0000	2,410.5023	2,410.5023	0.7796		2,429.9924
Total	1.6782	20.1828	11.5528	0.0249	0.2068	0.7937	1.0005	0.0223	0.7302	0.7525	0.0000	2,410.5023	2,410.5023	0.7796		2,429.9924

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3.2 Site Preparation - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0249	0.0156	0.1631	5.0000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		49.4118	49.4118	1.2300e-003		49.4425
Total	0.0249	0.0156	0.1631	5.0000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		49.4118	49.4118	1.2300e-003		49.4425

3.3 Grading - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					2.0167	0.0000	2.0167	0.5522	0.0000	0.5522			0.0000			0.0000
Off-Road	1.3432	14.1984	9.4370	0.0186		0.6694	0.6694		0.6317	0.6317		1,789.9539	1,789.9539	0.4248		1,800.5732
Total	1.3432	14.1984	9.4370	0.0186	2.0167	0.6694	2.6861	0.5522	0.6317	1.1839		1,789.9539	1,789.9539	0.4248		1,800.5732

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

3.3 Grading - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.2380	10.5097	1.5056	0.0327	0.7697	0.0337	0.8034	0.2110	0.0322	0.2432		3,465.254 4	3,465.254 4	0.2318		3,471.050 3
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0498	0.0311	0.3262	9.9000e-004	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		98.8236	98.8236	2.4500e-003		98.8849
Total	0.2879	10.5408	1.8318	0.0337	0.8815	0.0344	0.9158	0.2406	0.0328	0.2735		3,564.078 0	3,564.078 0	0.2343		3,569.935 3

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					0.7865	0.0000	0.7865	0.2154	0.0000	0.2154			0.0000			0.0000
Off-Road	1.3432	14.1984	9.4370	0.0186		0.6694	0.6694		0.6317	0.6317	0.0000	1,789.953 9	1,789.953 9	0.4248		1,800.573 2
Total	1.3432	14.1984	9.4370	0.0186	0.7865	0.6694	1.4559	0.2154	0.6317	0.8471	0.0000	1,789.953 9	1,789.953 9	0.4248		1,800.573 2

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3.3 Grading - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.2380	10.5097	1.5056	0.0327	0.7697	0.0337	0.8034	0.2110	0.0322	0.2432		3,465.2544	3,465.2544	0.2318		3,471.0503
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0498	0.0311	0.3262	9.9000e-004	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		98.8236	98.8236	2.4500e-003		98.8849
Total	0.2879	10.5408	1.8318	0.0337	0.8815	0.0344	0.9158	0.2406	0.0328	0.2735		3,564.0780	3,564.0780	0.2343		3,569.9353

3.4 Building Construction - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375		2,348.4458	2,348.4458	0.4838		2,360.5411
Total	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375		2,348.4458	2,348.4458	0.4838		2,360.5411

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3.4 Building Construction - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0118	0.4094	0.0882	1.0100e-003	0.0256	2.3700e-003	0.0280	7.3700e-003	2.2700e-003	9.6400e-003		106.0171	106.0171	9.1900e-003		106.2470
Worker	0.0498	0.0311	0.3262	9.9000e-004	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		98.8236	98.8236	2.4500e-003		98.8849
Total	0.0616	0.4405	0.4143	2.0000e-003	0.1374	3.0500e-003	0.1404	0.0370	2.8900e-003	0.0399		204.8407	204.8407	0.0116		205.1319

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375	0.0000	2,348.4458	2,348.4458	0.4838		2,360.5411
Total	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375	0.0000	2,348.4458	2,348.4458	0.4838		2,360.5411

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3.4 Building Construction - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0118	0.4094	0.0882	1.0100e-003	0.0256	2.3700e-003	0.0280	7.3700e-003	2.2700e-003	9.6400e-003		106.0171	106.0171	9.1900e-003		106.2470
Worker	0.0498	0.0311	0.3262	9.9000e-004	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		98.8236	98.8236	2.4500e-003		98.8849
Total	0.0616	0.4405	0.4143	2.0000e-003	0.1374	3.0500e-003	0.1404	0.0370	2.8900e-003	0.0399		204.8407	204.8407	0.0116		205.1319

3.5 Paving - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	0.9790	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654		1,429.9208	1,429.9208	0.4292		1,440.6504
Paving	1.1214					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Total	2.1004	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654		1,429.9208	1,429.9208	0.4292		1,440.6504

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3.5 Paving - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0897	0.0560	0.5871	1.7900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		177.8824	177.8824	4.4200e-003		177.9929
Total	0.0897	0.0560	0.5871	1.7900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		177.8824	177.8824	4.4200e-003		177.9929

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	0.9790	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654	0.0000	1,429.9208	1,429.9208	0.4292		1,440.6504
Paving	1.1214					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Total	2.1004	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654	0.0000	1,429.9208	1,429.9208	0.4292		1,440.6504

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

3.5 Paving - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0897	0.0560	0.5871	1.7900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		177.8824	177.8824	4.4200e-003		177.9929
Total	0.0897	0.0560	0.5871	1.7900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		177.8824	177.8824	4.4200e-003		177.9929

3.6 Architectural Coating - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Archit. Coating	9.7261					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.2422	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109		281.4481	281.4481	0.0218		281.9928
Total	9.9683	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109		281.4481	281.4481	0.0218		281.9928

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

3.6 Architectural Coating - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	9.9700e-003	6.2300e-003	0.0652	2.0000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		19.7647	19.7647	4.9000e-004		19.7770
Total	9.9700e-003	6.2300e-003	0.0652	2.0000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		19.7647	19.7647	4.9000e-004		19.7770

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Archit. Coating	9.7261					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.2422	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109	0.0000	281.4481	281.4481	0.0218		281.9928
Total	9.9683	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109	0.0000	281.4481	281.4481	0.0218		281.9928

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

3.6 Architectural Coating - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	9.9700e-003	6.2300e-003	0.0652	2.0000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		19.7647	19.7647	4.9000e-004		19.7770
Total	9.9700e-003	6.2300e-003	0.0652	2.0000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		19.7647	19.7647	4.9000e-004		19.7770

4.0 Operational Detail - Mobile

4.1 Mitigation Measures Mobile

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

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	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Mitigated	4.8881	36.0464	34.8420	0.1460	9.5850	0.0859	9.6709	2.5639	0.0799	2.6439		15,016.8251	15,016.8251	1.1788		15,046.2941
Unmitigated	4.8881	36.0464	34.8420	0.1460	9.5850	0.0859	9.6709	2.5639	0.0799	2.6439		15,016.8251	15,016.8251	1.1788		15,046.2941

4.2 Trip Summary Information

Land Use	Average Daily Trip Rate			Unmitigated	Mitigated
	Weekday	Saturday	Sunday	Annual VMT	Annual VMT
Convenience Market With Gas Pumps	2,464.32	2,453.64	2002.56	1,853,599	1,853,599
Fast Food Restaurant with Drive Thru	2,461.14	2,166.09	1628.16	2,420,484	2,420,484
Parking Lot	0.00	0.00	0.00		
Total	4,925.46	4,619.73	3,630.72	4,274,083	4,274,083

4.3 Trip Type Information

Land Use	Miles			Trip %			Trip Purpose %		
	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Convenience Market With Gas	16.60	8.40	6.90	0.80	80.20	19.00	20	21	59
Fast Food Restaurant with Drive	16.60	8.40	6.90	2.20	78.80	19.00	29	21	50
Parking Lot	16.60	8.40	6.90	0.00	0.00	0.00	0	0	0

4.4 Fleet Mix

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Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Convenience Market With Gas Pumps	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840
Fast Food Restaurant with Drive Thru	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840
Parking Lot	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
NaturalGas Mitigated	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199
NaturalGas Unmitigated	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

5.2 Energy by Land Use - Natural Gas

Unmitigated

	Natural Gas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	lb/day										lb/day					
Convenience Market With Gas Pumps	24.864	2.7000e-004	2.4400e-003	2.0500e-003	1.0000e-005		1.9000e-004	1.9000e-004		1.9000e-004	1.9000e-004		2.9252	2.9252	6.0000e-005	5.0000e-005	2.9426
Fast Food Restaurant with Drive Thru	2247.45	0.0242	0.2203	0.1851	1.3200e-003		0.0168	0.0168		0.0168	0.0168		264.4061	264.4061	5.0700e-003	4.8500e-003	265.9774
Parking Lot	0	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000
Total		0.0245	0.2228	0.1871	1.3300e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1300e-003	4.9000e-003	268.9199

Mitigated

	Natural Gas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	lb/day										lb/day					
Convenience Market With Gas Pumps	0.024864	2.7000e-004	2.4400e-003	2.0500e-003	1.0000e-005		1.9000e-004	1.9000e-004		1.9000e-004	1.9000e-004		2.9252	2.9252	6.0000e-005	5.0000e-005	2.9426
Fast Food Restaurant with Drive Thru	2.24745	0.0242	0.2203	0.1851	1.3200e-003		0.0168	0.0168		0.0168	0.0168		264.4061	264.4061	5.0700e-003	4.8500e-003	265.9774
Parking Lot	0	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000
Total		0.0245	0.2228	0.1871	1.3300e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1300e-003	4.9000e-003	268.9199

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Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

6.0 Area Detail

6.1 Mitigation Measures Area

Use Low VOC Paint - Non-Residential Interior

Use Low VOC Paint - Non-Residential Exterior

Use only Natural Gas Hearths

Use Low VOC Cleaning Supplies

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Mitigated	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Unmitigated	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

6.2 Area by SubCategory

Unmitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	lb/day										lb/day					
Architectural Coating	0.0133					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Consumer Products	0.1477					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Landscaping	6.3000e-004	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Total	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156

Mitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	lb/day										lb/day					
Architectural Coating	0.0133					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Consumer Products	0.1477					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Landscaping	6.3000e-004	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Total	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156

7.0 Water Detail

Dracaea/Perris Commercial Project - Riverside-South Coast County, Winter

7.1 Mitigation Measures Water

8.0 Waste Detail

8.1 Mitigation Measures Waste

9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
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10.0 Stationary Equipment

Fire Pumps and Emergency Generators

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
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Boilers

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type
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User Defined Equipment

Equipment Type	Number
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11.0 Vegetation

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

Dracaea/Perris Commercial Project
Riverside-South Coast County, Summer

1.0 Project Characteristics

1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Parking Lot	52.00	Space	2.14	20,800.00	0
Fast Food Restaurant with Drive Thru	3.00	1000sqft	0.07	3,000.00	0
Convenience Market With Gas Pumps	12.00	Pump	0.09	4,088.00	0

1.2 Other Project Characteristics

Urbanization	Urban	Wind Speed (m/s)	2.4	Precipitation Freq (Days)	28
Climate Zone	10			Operational Year	2024
Utility Company	Southern California Edison				
CO2 Intensity (lb/MWhr)	702.44	CH4 Intensity (lb/MWhr)	0.029	N2O Intensity (lb/MWhr)	0.006

1.3 User Entered Comments & Non-Default Data

Project Characteristics -

Land Use - Project site area is 2.3 acres.

Construction Phase -

Grading - Project site is approx. 2.30 acres. Assume fuel tanks installation requires soil excavation of approx. 700 cy to be exported.

Vehicle Trips - Trip generation rates without pass-by trips were obtained from Table 5-A of the Traffic Impact Analysis report (LSA 2020)

Construction Off-road Equipment Mitigation - Fugitive dust controled with on-site watering at least 3 times daily - SCAQMD Rule 403.

Area Mitigation -

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Table Name	Column Name	Default Value	New Value
tblArchitecturalCoating	ConstArea_Nonresidential_Exterior	3,544.00	2,347.00
tblArchitecturalCoating	ConstArea_Nonresidential_Interior	10,632.00	7,041.00
tblArchitecturalCoating	ConstArea_Parking	1,248.00	1,104.00
tblAreaCoating	Area_Nonresidential_Exterior	3544	2347
tblAreaCoating	Area_Nonresidential_Interior	10632	7041
tblAreaCoating	Area_Parking	1248	1104
tblAreaMitigation	UseLowVOCPaintParkingCheck	False	True
tblConstructionPhase	NumDays	10.00	5.00
tblConstructionPhase	NumDays	220.00	100.00
tblConstructionPhase	NumDays	6.00	2.00
tblConstructionPhase	NumDays	10.00	5.00
tblConstructionPhase	NumDays	3.00	1.00
tblGrading	AcresOfGrading	1.00	2.30
tblGrading	AcresOfGrading	1.50	0.50
tblGrading	MaterialExported	0.00	700.00
tblLandUse	LandUseSquareFeet	1,694.10	4,088.00
tblLandUse	LotAcreage	0.47	2.14
tblLandUse	LotAcreage	0.04	0.09
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	1.00	4.00
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	2.00	1.00
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	1.00	2.00
tblOffRoadEquipment	UsageHours	8.00	6.00
tblOffRoadEquipment	UsageHours	8.00	4.00
tblOffRoadEquipment	UsageHours	7.00	6.00
tblOffRoadEquipment	UsageHours	8.00	7.00
tblOffRoadEquipment	UsageHours	8.00	7.00

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tblOffRoadEquipment	UsageHours	8.00	1.00
tblOffRoadEquipment	UsageHours	6.00	8.00
tblOffRoadEquipment	UsageHours	7.00	6.00
tblOffRoadEquipment	UsageHours	8.00	7.00
tblOffRoadEquipment	UsageHours	7.00	8.00
tblTripsAndVMT	VendorTripNumber	5.00	4.00
tblTripsAndVMT	WorkerTripNumber	8.00	5.00
tblTripsAndVMT	WorkerTripNumber	13.00	10.00
tblTripsAndVMT	WorkerTripNumber	11.00	10.00
tblTripsAndVMT	WorkerTripNumber	20.00	18.00
tblVehicleTrips	PB_TP	65.00	59.00
tblVehicleTrips	PR_TP	14.00	20.00
tblVehicleTrips	WD_TR	542.60	205.36
tblVehicleTrips	WD_TR	496.12	820.38

2.0 Emissions Summary

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

2.1 Overall Construction (Maximum Daily Emission)

Unmitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	lb/day										lb/day					
2020	9.9784	24.6471	16.8726	0.0532	2.8982	0.9823	3.6015	0.7928	0.9404	1.4569	0.0000	5,454.289 3	5,454.289 3	0.7810	0.0000	5,470.274 3
Maximum	9.9784	24.6471	16.8726	0.0532	2.8982	0.9823	3.6015	0.7928	0.9404	1.4569	0.0000	5,454.289 3	5,454.289 3	0.7810	0.0000	5,470.274 3

Mitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	lb/day										lb/day					
2020	9.9784	24.6471	16.8726	0.0532	1.6680	0.9823	2.3713	0.4560	0.9404	1.1201	0.0000	5,454.289 3	5,454.289 3	0.7810	0.0000	5,470.274 3
Maximum	9.9784	24.6471	16.8726	0.0532	1.6680	0.9823	2.3713	0.4560	0.9404	1.1201	0.0000	5,454.289 3	5,454.289 3	0.7810	0.0000	5,470.274 3

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	42.45	0.00	34.16	42.49	0.00	23.12	0.00	0.00	0.00	0.00	0.00	0.00

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2.2 Overall Operational

Unmitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Area	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Energy	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199
Mobile	6.0288	36.8075	36.3798	0.1597	9.5850	0.0845	9.6695	2.5639	0.0786	2.6425		16,427.5172	16,427.5172	1.0851		16,454.6436
Total	6.2150	37.0303	36.5737	0.1611	9.5850	0.1015	9.6865	2.5639	0.0956	2.6595		16,694.8631	16,694.8631	1.0902	4.9000e-003	16,723.5792

Mitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Area	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Energy	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199
Mobile	6.0288	36.8075	36.3798	0.1597	9.5850	0.0845	9.6695	2.5639	0.0786	2.6425		16,427.5172	16,427.5172	1.0851		16,454.6436
Total	6.2150	37.0303	36.5737	0.1611	9.5850	0.1015	9.6865	2.5639	0.0956	2.6595		16,694.8631	16,694.8631	1.0902	4.9000e-003	16,723.5792

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	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

3.0 Construction Detail

Construction Phase

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Site Preparation	Site Preparation	6/1/2020	6/1/2020	5	1	
2	Grading	Grading	6/2/2020	6/3/2020	5	2	
3	Building Construction	Building Construction	6/4/2020	10/21/2020	5	100	
4	Paving	Paving	10/22/2020	10/28/2020	5	5	
5	Architectural Coating	Architectural Coating	10/29/2020	11/4/2020	5	5	

Acres of Grading (Site Preparation Phase): 0.5

Acres of Grading (Grading Phase): 2.3

Acres of Paving: 2.14

Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 7,041; Non-Residential Outdoor: 2,347; Striped Parking Area: 1,104 (Architectural Coating – sqft)

OffRoad Equipment

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Site Preparation	Graders	1	8.00	187	0.41
Site Preparation	Tractors/Loaders/Backhoes	1	8.00	97	0.37
Grading	Concrete/Industrial Saws	1	8.00	81	0.73
Grading	Rubber Tired Dozers	1	1.00	247	0.40
Grading	Tractors/Loaders/Backhoes	2	6.00	97	0.37
Building Construction	Cranes	1	4.00	231	0.29
Building Construction	Forklifts	2	6.00	89	0.20
Building Construction	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Paving	Cement and Mortar Mixers	4	6.00	9	0.56
Paving	Pavers	1	7.00	130	0.42
Paving	Rollers	1	7.00	80	0.38
Paving	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Architectural Coating	Air Compressors	1	6.00	78	0.48
Building Construction	Generator Sets	1	8.00	84	0.74
Grading	Graders	1	8.00	187	0.41
Paving	Paving Equipment	1	8.00	132	0.36
Site Preparation	Scrapers	1	8.00	367	0.48
Building Construction	Welders	3	8.00	46	0.45

Trips and VMT

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Site Preparation	3	5.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	5	10.00	0.00	88.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	9	10.00	4.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	8	18.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	2.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

3.1 Mitigation Measures Construction

Water Exposed Area

3.2 Site Preparation - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					0.5303	0.0000	0.5303	0.0573	0.0000	0.0573			0.0000			0.0000
Off-Road	1.6782	20.1828	11.5528	0.0249		0.7937	0.7937		0.7302	0.7302		2,410.5023	2,410.5023	0.7796		2,429.9924
Total	1.6782	20.1828	11.5528	0.0249	0.5303	0.7937	1.3240	0.0573	0.7302	0.7875		2,410.5023	2,410.5023	0.7796		2,429.9924

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.2 Site Preparation - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0254	0.0151	0.2016	5.5000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		55.0797	55.0797	1.4100e-003		55.1150
Total	0.0254	0.0151	0.2016	5.5000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		55.0797	55.0797	1.4100e-003		55.1150

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					0.2068	0.0000	0.2068	0.0223	0.0000	0.0223			0.0000			0.0000
Off-Road	1.6782	20.1828	11.5528	0.0249		0.7937	0.7937		0.7302	0.7302	0.0000	2,410.5023	2,410.5023	0.7796		2,429.9924
Total	1.6782	20.1828	11.5528	0.0249	0.2068	0.7937	1.0005	0.0223	0.7302	0.7525	0.0000	2,410.5023	2,410.5023	0.7796		2,429.9924

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.2 Site Preparation - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0254	0.0151	0.2016	5.5000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		55.0797	55.0797	1.4100e-003		55.1150
Total	0.0254	0.0151	0.2016	5.5000e-004	0.0559	3.4000e-004	0.0562	0.0148	3.1000e-004	0.0151		55.0797	55.0797	1.4100e-003		55.1150

3.3 Grading - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					2.0167	0.0000	2.0167	0.5522	0.0000	0.5522			0.0000			0.0000
Off-Road	1.3432	14.1984	9.4370	0.0186		0.6694	0.6694		0.6317	0.6317		1,789.9539	1,789.9539	0.4248		1,800.5732
Total	1.3432	14.1984	9.4370	0.0186	2.0167	0.6694	2.6861	0.5522	0.6317	1.1839		1,789.9539	1,789.9539	0.4248		1,800.5732

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.3 Grading - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.2263	10.4186	1.2855	0.0335	0.7697	0.0332	0.8029	0.2110	0.0318	0.2428		3,554.1760	3,554.1760	0.2118		3,559.4711
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0509	0.0301	0.4032	1.1100e-003	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		110.1595	110.1595	2.8200e-003		110.2301
Total	0.2772	10.4487	1.6887	0.0346	0.8815	0.0339	0.9154	0.2406	0.0324	0.2730		3,664.3354	3,664.3354	0.2146		3,669.7012

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Fugitive Dust					0.7865	0.0000	0.7865	0.2154	0.0000	0.2154			0.0000			0.0000
Off-Road	1.3432	14.1984	9.4370	0.0186		0.6694	0.6694		0.6317	0.6317	0.0000	1,789.9539	1,789.9539	0.4248		1,800.5732
Total	1.3432	14.1984	9.4370	0.0186	0.7865	0.6694	1.4559	0.2154	0.6317	0.8471	0.0000	1,789.9539	1,789.9539	0.4248		1,800.5732

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.3 Grading - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.2263	10.4186	1.2855	0.0335	0.7697	0.0332	0.8029	0.2110	0.0318	0.2428		3,554.1760	3,554.1760	0.2118		3,559.4711
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0509	0.0301	0.4032	1.1100e-003	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		110.1595	110.1595	2.8200e-003		110.2301
Total	0.2772	10.4487	1.6887	0.0346	0.8815	0.0339	0.9154	0.2406	0.0324	0.2730		3,664.3354	3,664.3354	0.2146		3,669.7012

3.4 Building Construction - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375		2,348.4458	2,348.4458	0.4838		2,360.5411
Total	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375		2,348.4458	2,348.4458	0.4838		2,360.5411

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Dracaena/Perris Commercial Project - Riverside-South Coast County, Summer

3.4 Building Construction - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0112	0.4116	0.0753	1.0400e-003	0.0256	2.3400e-003	0.0280	7.3700e-003	2.2400e-003	9.6100e-003		110.1564	110.1564	8.2600e-003		110.3629
Worker	0.0509	0.0301	0.4032	1.1100e-003	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		110.1595	110.1595	2.8200e-003		110.2301
Total	0.0620	0.4417	0.4785	2.1500e-003	0.1374	3.0200e-003	0.1404	0.0370	2.8600e-003	0.0399		220.3158	220.3158	0.0111		220.5930

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375	0.0000	2,348.4458	2,348.4458	0.4838		2,360.5411
Total	2.2870	17.0449	16.3941	0.0256		0.9793	0.9793		0.9375	0.9375	0.0000	2,348.4458	2,348.4458	0.4838		2,360.5411

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.4 Building Construction - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0112	0.4116	0.0753	1.0400e-003	0.0256	2.3400e-003	0.0280	7.3700e-003	2.2400e-003	9.6100e-003		110.1564	110.1564	8.2600e-003		110.3629
Worker	0.0509	0.0301	0.4032	1.1100e-003	0.1118	6.8000e-004	0.1125	0.0296	6.2000e-004	0.0303		110.1595	110.1595	2.8200e-003		110.2301
Total	0.0620	0.4417	0.4785	2.1500e-003	0.1374	3.0200e-003	0.1404	0.0370	2.8600e-003	0.0399		220.3158	220.3158	0.0111		220.5930

3.5 Paving - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	0.9790	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654		1,429.9208	1,429.9208	0.4292		1,440.6504
Paving	1.1214					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Total	2.1004	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654		1,429.9208	1,429.9208	0.4292		1,440.6504

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.5 Paving - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0916	0.0542	0.7258	1.9900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		198.2870	198.2870	5.0800e-003		198.4141
Total	0.0916	0.0542	0.7258	1.9900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		198.2870	198.2870	5.0800e-003		198.4141

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Off-Road	0.9790	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654	0.0000	1,429.9208	1,429.9208	0.4292		1,440.6504
Paving	1.1214					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Total	2.1004	9.3679	9.6472	0.0153		0.5022	0.5022		0.4654	0.4654	0.0000	1,429.9208	1,429.9208	0.4292		1,440.6504

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

3.5 Paving - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0916	0.0542	0.7258	1.9900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		198.2870	198.2870	5.0800e-003		198.4141
Total	0.0916	0.0542	0.7258	1.9900e-003	0.2012	1.2200e-003	0.2024	0.0534	1.1200e-003	0.0545		198.2870	198.2870	5.0800e-003		198.4141

3.6 Architectural Coating - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Archit. Coating	9.7261					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.2422	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109		281.4481	281.4481	0.0218		281.9928
Total	9.9683	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109		281.4481	281.4481	0.0218		281.9928

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

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3.6 Architectural Coating - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0102	6.0200e-003	0.0806	2.2000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		22.0319	22.0319	5.6000e-004		22.0460
Total	0.0102	6.0200e-003	0.0806	2.2000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		22.0319	22.0319	5.6000e-004		22.0460

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Archit. Coating	9.7261					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.2422	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109	0.0000	281.4481	281.4481	0.0218		281.9928
Total	9.9683	1.6838	1.8314	2.9700e-003		0.1109	0.1109		0.1109	0.1109	0.0000	281.4481	281.4481	0.0218		281.9928

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3.6 Architectural Coating - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0102	6.0200e-003	0.0806	2.2000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		22.0319	22.0319	5.6000e-004		22.0460
Total	0.0102	6.0200e-003	0.0806	2.2000e-004	0.0224	1.4000e-004	0.0225	5.9300e-003	1.2000e-004	6.0500e-003		22.0319	22.0319	5.6000e-004		22.0460

4.0 Operational Detail - Mobile

4.1 Mitigation Measures Mobile

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Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Mitigated	6.0288	36.8075	36.3798	0.1597	9.5850	0.0845	9.6695	2.5639	0.0786	2.6425		16,427.51 72	16,427.51 72	1.0851		16,454.64 36
Unmitigated	6.0288	36.8075	36.3798	0.1597	9.5850	0.0845	9.6695	2.5639	0.0786	2.6425		16,427.51 72	16,427.51 72	1.0851		16,454.64 36

4.2 Trip Summary Information

Land Use	Average Daily Trip Rate			Unmitigated	Mitigated
	Weekday	Saturday	Sunday	Annual VMT	Annual VMT
Convenience Market With Gas Pumps	2,464.32	2,453.64	2002.56	1,853,599	1,853,599
Fast Food Restaurant with Drive Thru	2,461.14	2,166.09	1628.16	2,420,484	2,420,484
Parking Lot	0.00	0.00	0.00		
Total	4,925.46	4,619.73	3,630.72	4,274,083	4,274,083

4.3 Trip Type Information

Land Use	Miles			Trip %			Trip Purpose %		
	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Convenience Market With Gas	16.60	8.40	6.90	0.80	80.20	19.00	20	21	59
Fast Food Restaurant with Drive	16.60	8.40	6.90	2.20	78.80	19.00	29	21	50
Parking Lot	16.60	8.40	6.90	0.00	0.00	0.00	0	0	0

4.4 Fleet Mix

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Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Convenience Market With Gas Pumps	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840
Fast Food Restaurant with Drive Thru	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840
Parking Lot	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
NaturalGas Mitigated	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199
NaturalGas Unmitigated	0.0245	0.2228	0.1871	1.3400e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1200e-003	4.9000e-003	268.9199

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

5.2 Energy by Land Use - Natural Gas

Unmitigated

	Natural Gas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	lb/day										lb/day					
Convenience Market With Gas Pumps	24.864	2.7000e-004	2.4400e-003	2.0500e-003	1.0000e-005		1.9000e-004	1.9000e-004		1.9000e-004	1.9000e-004		2.9252	2.9252	6.0000e-005	5.0000e-005	2.9426
Fast Food Restaurant with Drive Thru	2247.45	0.0242	0.2203	0.1851	1.3200e-003		0.0168	0.0168		0.0168	0.0168		264.4061	264.4061	5.0700e-003	4.8500e-003	265.9774
Parking Lot	0	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000
Total		0.0245	0.2228	0.1871	1.3300e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1300e-003	4.9000e-003	268.9199

Mitigated

	Natural Gas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	lb/day										lb/day					
Convenience Market With Gas Pumps	0.024864	2.7000e-004	2.4400e-003	2.0500e-003	1.0000e-005		1.9000e-004	1.9000e-004		1.9000e-004	1.9000e-004		2.9252	2.9252	6.0000e-005	5.0000e-005	2.9426
Fast Food Restaurant with Drive Thru	2.24745	0.0242	0.2203	0.1851	1.3200e-003		0.0168	0.0168		0.0168	0.0168		264.4061	264.4061	5.0700e-003	4.8500e-003	265.9774
Parking Lot	0	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000
Total		0.0245	0.2228	0.1871	1.3300e-003		0.0169	0.0169		0.0169	0.0169		267.3313	267.3313	5.1300e-003	4.9000e-003	268.9199

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6.0 Area Detail

6.1 Mitigation Measures Area

Use Low VOC Paint - Non-Residential Interior

Use Low VOC Paint - Non-Residential Exterior

Use only Natural Gas Hearths

Use Low VOC Cleaning Supplies

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	lb/day										lb/day					
Mitigated	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Unmitigated	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

6.2 Area by SubCategory

Unmitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	lb/day										lb/day					
Architectural Coating	0.0133					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Consumer Products	0.1477					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Landscaping	6.3000e-004	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Total	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156

Mitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	lb/day										lb/day					
Architectural Coating	0.0133					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Consumer Products	0.1477					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Landscaping	6.3000e-004	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156
Total	0.1617	6.0000e-005	6.8300e-003	0.0000		2.0000e-005	2.0000e-005		2.0000e-005	2.0000e-005		0.0147	0.0147	4.0000e-005		0.0156

7.0 Water Detail

Dracaea/Perris Commercial Project - Riverside-South Coast County, Summer

7.1 Mitigation Measures Water

8.0 Waste Detail

8.1 Mitigation Measures Waste

9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
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10.0 Stationary Equipment

Fire Pumps and Emergency Generators

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
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Boilers

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type
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User Defined Equipment

Equipment Type	Number
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11.0 Vegetation

Dracaea/Perris Commercial Project - Riverside-South Coast County, Annual

Dracaea/Perris Commercial Project
Riverside-South Coast County, Annual

1.0 Project Characteristics

1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Parking Lot	52.00	Space	2.14	20,800.00	0
Fast Food Restaurant with Drive Thru	3.00	1000sqft	0.07	3,000.00	0
Convenience Market With Gas Pumps	12.00	Pump	0.09	4,088.00	0

1.2 Other Project Characteristics

Urbanization	Urban	Wind Speed (m/s)	2.4	Precipitation Freq (Days)	28
Climate Zone	10			Operational Year	2024
Utility Company	Southern California Edison				
CO2 Intensity (lb/MW hr)	702.44	CH4 Intensity (lb/MW hr)	0.029	N2O Intensity (lb/MW hr)	0.006

1.3 User Entered Comments & Non-Default Data

Project Characteristics -

Land Use - Project site area is 2.3 acres.

Construction Phase -

Grading - Project site is approx. 2.30 acres. Assume fuel tanks installation requires soil excavation of approx. 700 cy to be exported.

Vehicle Trips - Trip generation rates without pass-by trips were obtained from Table 5-A of the Traffic Impact Analysis report (LSA 2020)

Construction Off-road Equipment Mitigation - Fugitive dust controled with on-site watering at least 3 times daily - SCAQMD Rule 403.

Area Mitigation -

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Table Name	Column Name	Default Value	New Value
tblArchitecturalCoating	ConstArea_Nonresidential_Exterior	3,544.00	2,347.00
tblArchitecturalCoating	ConstArea_Nonresidential_Interior	10,632.00	7,041.00
tblArchitecturalCoating	ConstArea_Parking	1,248.00	1,104.00
tblAreaCoating	Area_Nonresidential_Exterior	3544	2347
tblAreaCoating	Area_Nonresidential_Interior	10632	7041
tblAreaCoating	Area_Parking	1248	1104
tblAreaMitigation	UseLowVOCPaintParkingCheck	False	True
tblConstructionPhase	NumDays	10.00	5.00
tblConstructionPhase	NumDays	220.00	100.00
tblConstructionPhase	NumDays	6.00	2.00
tblConstructionPhase	NumDays	10.00	5.00
tblConstructionPhase	NumDays	3.00	1.00
tblGrading	AcresOfGrading	1.00	2.30
tblGrading	AcresOfGrading	1.50	0.50
tblGrading	MaterialExported	0.00	700.00
tblLandUse	LandUseSquareFeet	1,694.10	4,088.00
tblLandUse	LotAcreage	0.47	2.14
tblLandUse	LotAcreage	0.04	0.09
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	1.00	4.00
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	2.00	1.00
tblOffRoadEquipment	OffRoadEquipmentUnitAmount	1.00	2.00
tblOffRoadEquipment	UsageHours	8.00	6.00
tblOffRoadEquipment	UsageHours	8.00	4.00
tblOffRoadEquipment	UsageHours	7.00	6.00
tblOffRoadEquipment	UsageHours	8.00	7.00
tblOffRoadEquipment	UsageHours	8.00	7.00

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tblOffRoadEquipment	UsageHours	8.00	1.00
tblOffRoadEquipment	UsageHours	6.00	8.00
tblOffRoadEquipment	UsageHours	7.00	6.00
tblOffRoadEquipment	UsageHours	8.00	7.00
tblOffRoadEquipment	UsageHours	7.00	8.00
tblTripsAndVMT	VendorTripNumber	5.00	4.00
tblTripsAndVMT	WorkerTripNumber	8.00	5.00
tblTripsAndVMT	WorkerTripNumber	13.00	10.00
tblTripsAndVMT	WorkerTripNumber	11.00	10.00
tblTripsAndVMT	WorkerTripNumber	20.00	18.00
tblVehicleTrips	PB_TP	65.00	59.00
tblVehicleTrips	PR_TP	14.00	20.00
tblVehicleTrips	WD_TR	542.60	205.36
tblVehicleTrips	WD_TR	496.12	820.38

2.0 Emissions Summary

Dracaea/Perris Commercial Project - Riverside-South Coast County, Annual

2.1 Overall Construction

Unmitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	tons/yr										MT/yr					
2020	0.1501	0.9375	0.8884	1.5000e-003	0.0105	0.0518	0.0622	2.8000e-003	0.0495	0.0523	0.0000	126.4031	126.4031	0.0244	0.0000	127.0139
Maximum	0.1501	0.9375	0.8884	1.5000e-003	0.0105	0.0518	0.0622	2.8000e-003	0.0495	0.0523	0.0000	126.4031	126.4031	0.0244	0.0000	127.0139

Mitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	tons/yr										MT/yr					
2020	0.1501	0.9375	0.8884	1.5000e-003	9.0900e-003	0.0518	0.0609	2.4400e-003	0.0495	0.0519	0.0000	126.4030	126.4030	0.0244	0.0000	127.0137
Maximum	0.1501	0.9375	0.8884	1.5000e-003	9.0900e-003	0.0518	0.0609	2.4400e-003	0.0495	0.0519	0.0000	126.4030	126.4030	0.0244	0.0000	127.0137

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	13.35	0.00	2.23	12.86	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00

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Quarter	Start Date	End Date	Maximum Unmitigated ROG + NOX (tons/quarter)	Maximum Mitigated ROG + NOX (tons/quarter)
1	6-1-2020	8-31-2020	0.6571	0.6571
2	9-1-2020	9-30-2020	0.2125	0.2125
		Highest	0.6571	0.6571

2.2 Overall Operational
Unmitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Area	0.0295	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003
Energy	4.4700e-003	0.0407	0.0342	2.4000e-004		3.0900e-003	3.0900e-003		3.0900e-003	3.0900e-003	0.0000	108.4146	108.4146	3.5000e-003	1.3600e-003	108.9071
Mobile	0.8474	6.3606	6.0490	0.0262	1.6313	0.0147	1.6460	0.4370	0.0137	0.4507	0.0000	2,443.1597	2,443.1597	0.1765	0.0000	2,447.5726
Waste						0.0000	0.0000		0.0000	0.0000	7.0154	0.0000	7.0154	0.4146	0.0000	17.3803
Water						0.0000	0.0000		0.0000	0.0000	0.3287	4.7765	5.1052	0.0340	8.4000e-004	6.2039
Total	0.8814	6.4013	6.0840	0.0264	1.6313	0.0178	1.6491	0.4370	0.0168	0.4538	7.3441	2,556.3524	2,563.6965	0.6286	2.2000e-003	2,580.0656

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2.2 Overall Operational

Mitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Area	0.0295	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003
Energy	4.4700e-003	0.0407	0.0342	2.4000e-004		3.0900e-003	3.0900e-003		3.0900e-003	3.0900e-003	0.0000	108.4146	108.4146	3.5000e-003	1.3600e-003	108.9071
Mobile	0.8474	6.3606	6.0490	0.0262	1.6313	0.0147	1.6460	0.4370	0.0137	0.4507	0.0000	2,443.1597	2,443.1597	0.1765	0.0000	2,447.5726
Waste						0.0000	0.0000		0.0000	0.0000	7.0154	0.0000	7.0154	0.4146	0.0000	17.3803
Water						0.0000	0.0000		0.0000	0.0000	0.3287	4.7765	5.1052	0.0340	8.4000e-004	6.2039
Total	0.8814	6.4013	6.0840	0.0264	1.6313	0.0178	1.6491	0.4370	0.0168	0.4538	7.3441	2,556.3524	2,563.6965	0.6286	2.2000e-003	2,580.0656

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

3.0 Construction Detail

Construction Phase

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Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Site Preparation	Site Preparation	6/1/2020	6/1/2020	5	1	
2	Grading	Grading	6/2/2020	6/3/2020	5	2	
3	Building Construction	Building Construction	6/4/2020	10/21/2020	5	100	
4	Paving	Paving	10/22/2020	10/28/2020	5	5	
5	Architectural Coating	Architectural Coating	10/29/2020	11/4/2020	5	5	

Acres of Grading (Site Preparation Phase): 0.5

Acres of Grading (Grading Phase): 2.3

Acres of Paving: 2.14

Residential Indoor: 0; Residential Outdoor: 0; Non-Residential Indoor: 7,041; Non-Residential Outdoor: 2,347; Striped Parking Area: 1,104 (Architectural Coating – sqft)

OffRoad Equipment

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Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Site Preparation	Graders	1	8.00	187	0.41
Site Preparation	Tractors/Loaders/Backhoes	1	8.00	97	0.37
Grading	Concrete/Industrial Saws	1	8.00	81	0.73
Grading	Rubber Tired Dozers	1	1.00	247	0.40
Grading	Tractors/Loaders/Backhoes	2	6.00	97	0.37
Building Construction	Cranes	1	4.00	231	0.29
Building Construction	Forklifts	2	6.00	89	0.20
Building Construction	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Paving	Cement and Mortar Mixers	4	6.00	9	0.56
Paving	Pavers	1	7.00	130	0.42
Paving	Rollers	1	7.00	80	0.38
Paving	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Architectural Coating	Air Compressors	1	6.00	78	0.48
Building Construction	Generator Sets	1	8.00	84	0.74
Grading	Graders	1	8.00	187	0.41
Paving	Paving Equipment	1	8.00	132	0.36
Site Preparation	Scrapers	1	8.00	367	0.48
Building Construction	Welders	3	8.00	46	0.45

Trips and VMT

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Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Site Preparation	3	5.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	5	10.00	0.00	88.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	9	10.00	4.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	8	18.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	2.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

3.1 Mitigation Measures Construction

Water Exposed Area

3.2 Site Preparation - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					2.7000e-004	0.0000	2.7000e-004	3.0000e-005	0.0000	3.0000e-005	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	8.4000e-004	0.0101	5.7800e-003	1.0000e-005		4.0000e-004	4.0000e-004		3.7000e-004	3.7000e-004	0.0000	1.0934	1.0934	3.5000e-004	0.0000	1.1022
Total	8.4000e-004	0.0101	5.7800e-003	1.0000e-005	2.7000e-004	4.0000e-004	6.7000e-004	3.0000e-005	3.7000e-004	4.0000e-004	0.0000	1.0934	1.0934	3.5000e-004	0.0000	1.1022

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3.2 Site Preparation - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.0000e-005	1.0000e-005	9.0000e-005	0.0000	3.0000e-005	0.0000	3.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0230	0.0230	0.0000	0.0000	0.0230
Total	1.0000e-005	1.0000e-005	9.0000e-005	0.0000	3.0000e-005	0.0000	3.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0230	0.0230	0.0000	0.0000	0.0230

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					1.0000e-004	0.0000	1.0000e-004	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	8.4000e-004	0.0101	5.7800e-003	1.0000e-005		4.0000e-004	4.0000e-004		3.7000e-004	3.7000e-004	0.0000	1.0934	1.0934	3.5000e-004	0.0000	1.1022
Total	8.4000e-004	0.0101	5.7800e-003	1.0000e-005	1.0000e-004	4.0000e-004	5.0000e-004	1.0000e-005	3.7000e-004	3.8000e-004	0.0000	1.0934	1.0934	3.5000e-004	0.0000	1.1022

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3.2 Site Preparation - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.0000e-005	1.0000e-005	9.0000e-005	0.0000	3.0000e-005	0.0000	3.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0230	0.0230	0.0000	0.0000	0.0230
Total	1.0000e-005	1.0000e-005	9.0000e-005	0.0000	3.0000e-005	0.0000	3.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0230	0.0230	0.0000	0.0000	0.0230

3.3 Grading - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					2.0200e-003	0.0000	2.0200e-003	5.5000e-004	0.0000	5.5000e-004	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	1.3400e-003	0.0142	9.4400e-003	2.0000e-005		6.7000e-004	6.7000e-004		6.3000e-004	6.3000e-004	0.0000	1.6238	1.6238	3.9000e-004	0.0000	1.6335
Total	1.3400e-003	0.0142	9.4400e-003	2.0000e-005	2.0200e-003	6.7000e-004	2.6900e-003	5.5000e-004	6.3000e-004	1.1800e-003	0.0000	1.6238	1.6238	3.9000e-004	0.0000	1.6335

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3.3 Grading - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	2.3000e-004	0.0107	1.3800e-003	3.0000e-005	7.6000e-004	3.0000e-005	7.9000e-004	2.1000e-004	3.0000e-005	2.4000e-004	0.0000	3.1904	3.1904	2.0000e-004	0.0000	3.1954
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	5.0000e-005	3.0000e-005	3.4000e-004	0.0000	1.1000e-004	0.0000	1.1000e-004	3.0000e-005	0.0000	3.0000e-005	0.0000	0.0920	0.0920	0.0000	0.0000	0.0920
Total	2.8000e-004	0.0107	1.7200e-003	3.0000e-005	8.7000e-004	3.0000e-005	9.0000e-004	2.4000e-004	3.0000e-005	2.7000e-004	0.0000	3.2824	3.2824	2.0000e-004	0.0000	3.2874

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					7.9000e-004	0.0000	7.9000e-004	2.2000e-004	0.0000	2.2000e-004	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	1.3400e-003	0.0142	9.4400e-003	2.0000e-005		6.7000e-004	6.7000e-004		6.3000e-004	6.3000e-004	0.0000	1.6238	1.6238	3.9000e-004	0.0000	1.6335
Total	1.3400e-003	0.0142	9.4400e-003	2.0000e-005	7.9000e-004	6.7000e-004	1.4600e-003	2.2000e-004	6.3000e-004	8.5000e-004	0.0000	1.6238	1.6238	3.9000e-004	0.0000	1.6335

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3.3 Grading - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	2.3000e-004	0.0107	1.3800e-003	3.0000e-005	7.6000e-004	3.0000e-005	7.9000e-004	2.1000e-004	3.0000e-005	2.4000e-004	0.0000	3.1904	3.1904	2.0000e-004	0.0000	3.1954
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	5.0000e-005	3.0000e-005	3.4000e-004	0.0000	1.1000e-004	0.0000	1.1000e-004	3.0000e-005	0.0000	3.0000e-005	0.0000	0.0920	0.0920	0.0000	0.0000	0.0920
Total	2.8000e-004	0.0107	1.7200e-003	3.0000e-005	8.7000e-004	3.0000e-005	9.0000e-004	2.4000e-004	3.0000e-005	2.7000e-004	0.0000	3.2824	3.2824	2.0000e-004	0.0000	3.2874

3.4 Building Construction - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	0.1144	0.8523	0.8197	1.2800e-003		0.0490	0.0490		0.0469	0.0469	0.0000	106.5237	106.5237	0.0220	0.0000	107.0723
Total	0.1144	0.8523	0.8197	1.2800e-003		0.0490	0.0490		0.0469	0.0469	0.0000	106.5237	106.5237	0.0220	0.0000	107.0723

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

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3.4 Building Construction - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	5.7000e-004	0.0208	4.0700e-003	5.0000e-005	1.2600e-003	1.2000e-004	1.3800e-003	3.6000e-004	1.1000e-004	4.8000e-004	0.0000	4.9178	4.9178	3.9000e-004	0.0000	4.9276
Worker	2.3000e-003	1.6100e-003	0.0172	5.0000e-005	5.5000e-003	3.0000e-005	5.5300e-003	1.4600e-003	3.0000e-005	1.4900e-003	0.0000	4.5980	4.5980	1.2000e-004	0.0000	4.6008
Total	2.8700e-003	0.0224	0.0213	1.0000e-004	6.7600e-003	1.5000e-004	6.9100e-003	1.8200e-003	1.4000e-004	1.9700e-003	0.0000	9.5157	9.5157	5.1000e-004	0.0000	9.5284

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	0.1144	0.8522	0.8197	1.2800e-003		0.0490	0.0490		0.0469	0.0469	0.0000	106.5236	106.5236	0.0220	0.0000	107.0722
Total	0.1144	0.8522	0.8197	1.2800e-003		0.0490	0.0490		0.0469	0.0469	0.0000	106.5236	106.5236	0.0220	0.0000	107.0722

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3.4 Building Construction - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	5.7000e-004	0.0208	4.0700e-003	5.0000e-005	1.2600e-003	1.2000e-004	1.3800e-003	3.6000e-004	1.1000e-004	4.8000e-004	0.0000	4.9178	4.9178	3.9000e-004	0.0000	4.9276
Worker	2.3000e-003	1.6100e-003	0.0172	5.0000e-005	5.5000e-003	3.0000e-005	5.5300e-003	1.4600e-003	3.0000e-005	1.4900e-003	0.0000	4.5980	4.5980	1.2000e-004	0.0000	4.6008
Total	2.8700e-003	0.0224	0.0213	1.0000e-004	6.7600e-003	1.5000e-004	6.9100e-003	1.8200e-003	1.4000e-004	1.9700e-003	0.0000	9.5157	9.5157	5.1000e-004	0.0000	9.5284

3.5 Paving - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	2.4500e-003	0.0234	0.0241	4.0000e-005		1.2600e-003	1.2600e-003		1.1600e-003	1.1600e-003	0.0000	3.2430	3.2430	9.7000e-004	0.0000	3.2673
Paving	2.8000e-003					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	5.2500e-003	0.0234	0.0241	4.0000e-005		1.2600e-003	1.2600e-003		1.1600e-003	1.1600e-003	0.0000	3.2430	3.2430	9.7000e-004	0.0000	3.2673

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3.5 Paving - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.1000e-004	1.4000e-004	1.5500e-003	0.0000	4.9000e-004	0.0000	5.0000e-004	1.3000e-004	0.0000	1.3000e-004	0.0000	0.4138	0.4138	1.0000e-005	0.0000	0.4141
Total	2.1000e-004	1.4000e-004	1.5500e-003	0.0000	4.9000e-004	0.0000	5.0000e-004	1.3000e-004	0.0000	1.3000e-004	0.0000	0.4138	0.4138	1.0000e-005	0.0000	0.4141

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	2.4500e-003	0.0234	0.0241	4.0000e-005		1.2600e-003	1.2600e-003		1.1600e-003	1.1600e-003	0.0000	3.2430	3.2430	9.7000e-004	0.0000	3.2673
Paving	2.8000e-003					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	5.2500e-003	0.0234	0.0241	4.0000e-005		1.2600e-003	1.2600e-003		1.1600e-003	1.1600e-003	0.0000	3.2430	3.2430	9.7000e-004	0.0000	3.2673

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3.5 Paving - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.1000e-004	1.4000e-004	1.5500e-003	0.0000	4.9000e-004	0.0000	5.0000e-004	1.3000e-004	0.0000	1.3000e-004	0.0000	0.4138	0.4138	1.0000e-005	0.0000	0.4141
Total	2.1000e-004	1.4000e-004	1.5500e-003	0.0000	4.9000e-004	0.0000	5.0000e-004	1.3000e-004	0.0000	1.3000e-004	0.0000	0.4138	0.4138	1.0000e-005	0.0000	0.4141

3.6 Architectural Coating - 2020

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Archit. Coating	0.0243					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	6.1000e-004	4.2100e-003	4.5800e-003	1.0000e-005		2.8000e-004	2.8000e-004		2.8000e-004	2.8000e-004	0.0000	0.6383	0.6383	5.0000e-005	0.0000	0.6396
Total	0.0249	4.2100e-003	4.5800e-003	1.0000e-005		2.8000e-004	2.8000e-004		2.8000e-004	2.8000e-004	0.0000	0.6383	0.6383	5.0000e-005	0.0000	0.6396

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3.6 Architectural Coating - 2020

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.0000e-005	2.0000e-005	1.7000e-004	0.0000	5.0000e-005	0.0000	6.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0460	0.0460	0.0000	0.0000	0.0460
Total	2.0000e-005	2.0000e-005	1.7000e-004	0.0000	5.0000e-005	0.0000	6.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0460	0.0460	0.0000	0.0000	0.0460

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Archit. Coating	0.0243					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	6.1000e-004	4.2100e-003	4.5800e-003	1.0000e-005		2.8000e-004	2.8000e-004		2.8000e-004	2.8000e-004	0.0000	0.6383	0.6383	5.0000e-005	0.0000	0.6396
Total	0.0249	4.2100e-003	4.5800e-003	1.0000e-005		2.8000e-004	2.8000e-004		2.8000e-004	2.8000e-004	0.0000	0.6383	0.6383	5.0000e-005	0.0000	0.6396

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3.6 Architectural Coating - 2020

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.0000e-005	2.0000e-005	1.7000e-004	0.0000	5.0000e-005	0.0000	6.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0460	0.0460	0.0000	0.0000	0.0460
Total	2.0000e-005	2.0000e-005	1.7000e-004	0.0000	5.0000e-005	0.0000	6.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0460	0.0460	0.0000	0.0000	0.0460

4.0 Operational Detail - Mobile

4.1 Mitigation Measures Mobile

Dracaea/Perris Commercial Project - Riverside-South Coast County, Annual

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Mitigated	0.8474	6.3606	6.0490	0.0262	1.6313	0.0147	1.6460	0.4370	0.0137	0.4507	0.0000	2,443.1597	2,443.1597	0.1765	0.0000	2,447.5726
Unmitigated	0.8474	6.3606	6.0490	0.0262	1.6313	0.0147	1.6460	0.4370	0.0137	0.4507	0.0000	2,443.1597	2,443.1597	0.1765	0.0000	2,447.5726

4.2 Trip Summary Information

Land Use	Average Daily Trip Rate			Unmitigated	Mitigated
	Weekday	Saturday	Sunday	Annual VMT	Annual VMT
Convenience Market With Gas Pumps	2,464.32	2,453.64	2002.56	1,853,599	1,853,599
Fast Food Restaurant with Drive Thru	2,461.14	2,166.09	1628.16	2,420,484	2,420,484
Parking Lot	0.00	0.00	0.00		
Total	4,925.46	4,619.73	3,630.72	4,274,083	4,274,083

4.3 Trip Type Information

Land Use	Miles			Trip %			Trip Purpose %		
	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Convenience Market With Gas	16.60	8.40	6.90	0.80	80.20	19.00	20	21	59
Fast Food Restaurant with Drive	16.60	8.40	6.90	2.20	78.80	19.00	29	21	50
Parking Lot	16.60	8.40	6.90	0.00	0.00	0.00	0	0	0

4.4 Fleet Mix

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Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Convenience Market With Gas Pumps	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840
Fast Food Restaurant with Drive Thru	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840
Parking Lot	0.551648	0.035769	0.187848	0.110184	0.013450	0.004660	0.017552	0.070120	0.001413	0.001134	0.004476	0.000905	0.000840

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Electricity Mitigated						0.0000	0.0000		0.0000	0.0000	0.0000	64.1549	64.1549	2.6500e-003	5.5000e-004	64.3844
Electricity Unmitigated						0.0000	0.0000		0.0000	0.0000	0.0000	64.1549	64.1549	2.6500e-003	5.5000e-004	64.3844
Natural Gas Mitigated	4.4700e-003	0.0407	0.0342	2.4000e-004		3.0900e-003	3.0900e-003		3.0900e-003	3.0900e-003	0.0000	44.2597	44.2597	8.5000e-004	8.1000e-004	44.5227
Natural Gas Unmitigated	4.4700e-003	0.0407	0.0342	2.4000e-004		3.0900e-003	3.0900e-003		3.0900e-003	3.0900e-003	0.0000	44.2597	44.2597	8.5000e-004	8.1000e-004	44.5227

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5.2 Energy by Land Use - NaturalGas

Unmitigated

	NaturalGas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	tons/yr										MT/yr					
Convenience Market With Gas Pumps	9075.36	5.0000e-005	4.4000e-004	3.7000e-004	0.0000		3.0000e-005	3.0000e-005		3.0000e-005	3.0000e-005	0.0000	0.4843	0.4843	1.0000e-005	1.0000e-005	0.4872
Fast Food Restaurant with Drive Thru	820320	4.4200e-003	0.0402	0.0338	2.4000e-004		3.0600e-003	3.0600e-003		3.0600e-003	3.0600e-003	0.0000	43.7754	43.7754	8.4000e-004	8.0000e-004	44.0355
Parking Lot	0	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total		4.4700e-003	0.0407	0.0342	2.4000e-004		3.0900e-003	3.0900e-003		3.0900e-003	3.0900e-003	0.0000	44.2597	44.2597	8.5000e-004	8.1000e-004	44.5227

Mitigated

	NaturalGas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	tons/yr										MT/yr					
Convenience Market With Gas Pumps	9075.36	5.0000e-005	4.4000e-004	3.7000e-004	0.0000		3.0000e-005	3.0000e-005		3.0000e-005	3.0000e-005	0.0000	0.4843	0.4843	1.0000e-005	1.0000e-005	0.4872
Fast Food Restaurant with Drive Thru	820320	4.4200e-003	0.0402	0.0338	2.4000e-004		3.0600e-003	3.0600e-003		3.0600e-003	3.0600e-003	0.0000	43.7754	43.7754	8.4000e-004	8.0000e-004	44.0355
Parking Lot	0	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total		4.4700e-003	0.0407	0.0342	2.4000e-004		3.0900e-003	3.0900e-003		3.0900e-003	3.0900e-003	0.0000	44.2597	44.2597	8.5000e-004	8.1000e-004	44.5227

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Dracaea/Perris Commercial Project - Riverside-South Coast County, Annual

5.3 Energy by Land Use - Electricity

Unmitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr	MT/yr			
Convenience Market With Gas Pumps	51631.4	16.4509	6.8000e-004	1.4000e-004	16.5097
Fast Food Restaurant with Drive Thru	142440	45.3844	1.8700e-003	3.9000e-004	45.5468
Parking Lot	7280	2.3196	1.0000e-004	2.0000e-005	2.3279
Total		64.1549	2.6500e-003	5.5000e-004	64.3844

Mitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr	MT/yr			
Convenience Market With Gas Pumps	51631.4	16.4509	6.8000e-004	1.4000e-004	16.5097
Fast Food Restaurant with Drive Thru	142440	45.3844	1.8700e-003	3.9000e-004	45.5468
Parking Lot	7280	2.3196	1.0000e-004	2.0000e-005	2.3279
Total		64.1549	2.6500e-003	5.5000e-004	64.3844

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6.0 Area Detail

6.1 Mitigation Measures Area

Use Low VOC Paint - Non-Residential Interior

Use Low VOC Paint - Non-Residential Exterior

Use only Natural Gas Hearths

Use Low VOC Cleaning Supplies

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Mitigated	0.0295	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003
Unmitigated	0.0295	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003

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6.2 Area by SubCategory

Unmitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	tons/yr										MT/yr					
Architectural Coating	2.4300e-003					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Consumer Products	0.0270					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Landscaping	8.0000e-005	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003
Total	0.0295	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003

Mitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	tons/yr										MT/yr					
Architectural Coating	2.4300e-003					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Consumer Products	0.0270					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Landscaping	8.0000e-005	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003
Total	0.0295	1.0000e-005	8.5000e-004	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	1.6600e-003	1.6600e-003	0.0000	0.0000	1.7700e-003

7.0 Water Detail

Dracaea/Perris Commercial Project - Riverside-South Coast County, Annual

7.1 Mitigation Measures Water

	Total CO2	CH4	N2O	CO2e
Category	MT/yr			
Mitigated	5.1052	0.0340	8.4000e-004	6.2039
Unmitigated	5.1052	0.0340	8.4000e-004	6.2039

7.2 Water by Land Use

Unmitigated

	Indoor/Outdoor Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal	MT/yr			
Convenience Market With Gas Pumps	0.125486 / 0.0769109	0.8327	4.1200e-003	1.0000e-004	0.9665
Fast Food Restaurant with Drive Thru	0.910601 / 0.0581235	4.2725	0.0298	7.3000e-004	5.2374
Parking Lot	0 / 0	0.0000	0.0000	0.0000	0.0000
Total		5.1052	0.0340	8.3000e-004	6.2039

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7.2 Water by Land Use

Mitigated

	Indoor/Outdoor Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal	MT/yr			
Convenience Market With Gas Pumps	0.125486 / 0.0769109	0.8327	4.1200e-003	1.0000e-004	0.9665
Fast Food Restaurant with Drive Thru	0.910601 / 0.0581235	4.2725	0.0298	7.3000e-004	5.2374
Parking Lot	0 / 0	0.0000	0.0000	0.0000	0.0000
Total		5.1052	0.0340	8.3000e-004	6.2039

8.0 Waste Detail

8.1 Mitigation Measures Waste

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Category/Year

	Total CO2	CH4	N2O	CO2e
	MT/yr			
Mitigated	7.0154	0.4146	0.0000	17.3803
Unmitigated	7.0154	0.4146	0.0000	17.3803

8.2 Waste by Land Use

Unmitigated

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons	MT/yr			
Fast Food Restaurant with Drive Thru	34.56	7.0154	0.4146	0.0000	17.3803
Parking Lot	0	0.0000	0.0000	0.0000	0.0000
Total		7.0154	0.4146	0.0000	17.3803

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8.2 Waste by Land Use

Mitigated

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons	MT/yr			
Fast Food Restaurant with Drive Thru	34.56	7.0154	0.4146	0.0000	17.3803
Parking Lot	0	0.0000	0.0000	0.0000	0.0000
Total		7.0154	0.4146	0.0000	17.3803

9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
----------------	--------	-----------	-----------	-------------	-------------	-----------

10.0 Stationary Equipment

Fire Pumps and Emergency Generators

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
----------------	--------	-----------	------------	-------------	-------------	-----------

Boilers

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type
----------------	--------	----------------	-----------------	---------------	-----------

User Defined Equipment

Equipment Type	Number
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11.0 Vegetation

Dracaea/Perris Commercial Project - Riverside-South Coast County, Annual

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Appendix B:

**Western Riverside County Multiple Species Habitat Conservation
Plan Consistency Analysis and Biology Report, Commercial Retail
Project at Perris Boulevard/Dracaea Avenue, City of Moreno Valley,
Riverside County, California**

WESTERN RIVERSIDE COUNTY MULTIPLE SPECIES HABITAT CONSERVATION PLAN CONSISTENCY ANALYSIS AND BIOLOGY REPORT

COMMERCIAL RETAIL PROJECT AT PERRIS BOULEVARD/DRACAEA AVENUE
CITY OF MORENO VALLEY
RIVERSIDE COUNTY, CALIFORNIA



September 2020

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

**WESTERN RIVERSIDE COUNTY MULTIPLE SPECIES
HABITAT CONSERVATION PLAN CONSISTENCY
ANALYSIS AND BIOLOGY REPORT**

**COMMERCIAL RETAIL PROJECT AT PERRIS BOULEVARD/DRACAEA AVENUE
CITY OF MORENO VALLEY
RIVERSIDE COUNTY, CALIFORNIA**

MSHCP Permittee:

City of Moreno Valley

Prepared for:

Cadence Acquisition LLC
6400 S. Fiddlers Green Circle, Suite 1820
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Prepared by:

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LSA Project No. CAQ1901



September 2020

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1.0 EXECUTIVE SUMMARY

LSA Associates Inc., doing business as LSA, was retained by TAIT & Associates (TAIT) to conduct a Western Riverside County Multiple Species Habitat Conservation Plan (MSHCP) consistency analysis and general biological study of the approximately 2.3-acre Commercial Retail Project at Perris Boulevard/Dracaea Avenue (project) in the City of Moreno Valley, Riverside County, California.

The project is not located within the MSHCP Criteria Area, Public/Quasi-Public Lands, or conservation areas.

The project site does not contain suitable habitat for any threatened or endangered species. No drainage features, ponded areas, or riparian habitat potentially subject to jurisdiction by the California Department of Fish and Wildlife (CDFW), U.S. Army Corps of Engineers (USACE), or the Regional Water Quality Control Board (RWQCB) were found within the project site.

The project area is not within the MSHCP Narrow Endemic Plant Species Area, burrowing owl (*Athene cunicularia hypugaea*) survey area, or any other species survey areas. Therefore, no surveys for MSHCP survey species will be required. In addition, "species not adequately conserved" by the MSHCP were not found within the project area.

The project will not be subject to MSHCP Urban/Wildlands interface requirements because the project area is not within or adjacent to an identified Conservation Area. In addition, because the project is not located within a Criteria Area or Public/Quasi-Public Lands, it is not subject to best management practices (BMPs) specified in Appendix C of the MSHCP.

The project site is located within the Stephens' Kangaroo Rat Habitat Conservation Plan area and a fee payment will be required prior to issuance of a grading permit.

2.0 INTRODUCTION

TAIT retained LSA to conduct a Western Riverside County Multiple Species Habitat Conservation Plan (MSHCP) Consistency Analysis and general biological study for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, Riverside County, California. The purpose of this Consistency Analysis report is to summarize the biological data for the proposed project and its consistency with the goals and objectives of the MSHCP.

2.1 PROJECT AREA

The 2.3-acre project site consists of an undeveloped corner lot surrounded by existing roads and residential and commercial development at the northeast corner of Perris Boulevard and Dracaea Avenue (Figure 1; see Appendix A for all figures) on Assessor's Parcel Numbers 479-120-042, 479-120-027, 479-120-029, and 479-120-043. The entire project site is proposed for development. The project site is mostly devoid of vegetation since the lot has been historically plowed or disked for weed abatement compliance. Historical aerial imagery of the project site revealed that the project site has remained mostly undeveloped since 1966.

3.0 RESERVE ASSEMBLY ANALYSIS

3.1 CELL AND CRITERIA ANALYSIS

The MSHCP provides for the assembly of a Conservation Area consisting of Core Areas and Linkages for the conservation of covered species. The Conservation Area is to be assembled from portions of the MSHCP Criteria Area, which consist of quarter-section (i.e., approximately 160-acre) Criteria Cells, each with specific criteria for the species conservation within that cell.

The project is not located within an MSHCP defined criteria cell or special survey area.

3.2 PUBLIC QUASI-PUBLIC LANDS ANALYSIS

The project area is not located within or adjacent to public or quasi-public lands.

4.0 VEGETATION MAPPING

The project site does not contain any natural plant communities since it is regularly disked for weed abatement compliance (Figure 3). There are several dead and live ornamental trees along the eastern perimeter. Table A shows the results of an on-site tree inventory. The project site was recently plowed; however, some evidence of non-native grassland is still present, likely mouse barley (*Hordeum murinum*), shortpod mustard (*Hirschfeldia incana*), stork's bill (*Erodium* spp.), and red brome (*Bromus madritensis* ssp. *rubens*). A complete list of plant species observed on site is included in Appendix B.

Table A: Tree Inventory

Tree	Species (Trees 4" caliper or 15' tall)	Height (ft)	Caliper at 2'	Rating/ Transplantable	Condition Notes
T1	black locust (<i>Robinia pseudoacacia</i>)	24	8	Poor/No	Poor structure (codominant leaders, pleaching)
T2	black locust (<i>Robinia pseudoacacia</i>)	18	9.5	Poor/No	Poor structure (codominant leaders, pleaching); unhealed old branch cuts
T3	black locust (<i>Robinia pseudoacacia</i>)	15	6	Poor/No	Poor structure (codominant leaders, pleaching)
T4	eucalyptus (<i>Eucalyptus</i> sp.)	35	14	Poor/No	Poor structure (codominant leaders, pleaching); unhealed old branch cuts

5.0 PROTECTION OF SPECIES ASSOCIATED WITH RIPARIAN/RIVERINE AREAS AND VERNAL POOLS (MSHCP SECTION 6.1.2)

Section 6.1.2 of the MSHCP requires assessment of impacts to riparian habitats, riverine areas, and vernal pools, including focused surveys for sensitive riparian bird and fairy shrimp species when suitable habitat is present. The intent of the assessment requirement is to provide for the protection of resources used by MSHCP-covered species, as well as existing and future downstream conservation areas. Riverine/riparian areas and vernal pools are defined in Section 6.1.2 of the MSHCP as follows:

Riparian/Riverine Areas are lands which contain Habitat dominated by trees, shrubs, persistent emergents, or emergent mosses and lichens, which occur close to or which depend upon soil moisture from a nearby fresh water source; or areas with fresh water flow during all or a portion of the year.

Vernal pools are seasonal wetlands that occur in depression areas that have wetlands indicators of all three parameters (soils, vegetation and hydrology) during the wetter portion of the growing season but normally lack wetlands indicators of hydrology and/or vegetation during the drier portion of the growing season. Obligate hydrophytes and facultative wetlands plant species are normally dominant during the wetter portion of the growing season, while upland species (annuals) may be dominant during the drier portion of the growing season. The determination that an area exhibits vernal pool characteristics, and the definition of the watershed supporting vernal pool hydrology, must be made on a case-by-case basis. Such determinations should consider the length of the time the area exhibits upland and wetland characteristics and the manner in which the area fits into the overall ecological system as a wetland. Evidence concerning the persistence of an area's wetness can be obtained from its history, vegetation, soils, and drainage characteristics, uses to which it has been subjected, and weather and hydrologic records.

Fairy Shrimp. For Riverside, vernal pool and Santa Rosa fairy shrimp, mapping of stock ponds, ephemeral pools and other features shall also be undertaken as determined appropriate by a qualified biologist.

With the exception of wetlands created for the purpose of providing wetlands Habitat or resulting from human actions to create open waters or from the alteration of natural stream courses, areas demonstrating characteristics as described above which are artificially created are not included in these definitions.

5.1 RIPARIAN/RIVERINE AREAS

5.1.1 Methods

LSA Senior Biologist Anthony Greco assessed the project area for riparian/riverine areas on April 12, 2019. LSA surveyed the study area on foot and with small Unmanned Aerial Systems (sUAS) to evaluate all areas of potential jurisdiction according to USACE, CDFW, and MSHCP criteria. Data were recorded using a global positioning system (GPS) unit with submeter accuracy. The assessment

included identification and mapping of plant communities within the project area as well as any drainage features.

5.1.2 Existing Conditions and Results

No drainage features, ponded areas, or riparian habitat subject to jurisdiction by the CDFW, USACE, and/or RWQCB were found within the project site. A review of the *Sunnymead, California* U.S. Geological Survey (USGS) quadrangle and historic aerial photographs (NETROnline) did not reveal any previously mapped drainage features.

5.2 VERNAL POOLS

5.2.1 Methods

LSA Senior Biologist Anthony Greco assessed the project area for vernal pools during the site visit on April 12, 2019. The assessment included a search for depressions, indicators of wetland hydrology, suitable soils, and hydrophytic vegetation. The assessment also included a review of seasonally appropriate aerial photographs (Google Earth) from 2002 through 2018.

5.2.2 Existing Conditions and Results

There are no vernal pools or other ponded areas suitable for sensitive fairy shrimp species on the project site. Soils in the project area are well drained from repeated plowing and have been previously mapped by the Natural Resources Conservation Service (NRCS) as Ramona sandy loam, with 0 to 2 percent slopes (Figure 4). No hydrophytic vegetation occurs in the project area.

5.3 FAIRY SHRIMP

5.3.1 Methods

LSA Senior Biologist Anthony Greco performed the general biological resources survey and jurisdictional delineation fieldwork on April 12, 2019. LSA prepared a field map of the area to be surveyed using a 2019 aerial photograph base at a scale of 1 inch = 100 feet. Mr. Greco surveyed the study area on foot and evaluated all areas of potential jurisdiction according to USACE, CDFW, and MSHCP criteria. Data were recorded using a GPS unit with submeter accuracy and by marking directly on the aerial photograph.

5.3.2 Existing Conditions and Results

There are no vernal pools or other fairy shrimp habitat suitable for sensitive fairy shrimp species on the project site. Soils in the project area are well drained and have been previously mapped by the NRCS as Ramona sandy loam, with 0 to 2 percent slopes.

5.4 RIPARIAN BIRDS

5.4.1 Methods

Habitat suitability for riparian birds, including least Bell's vireo (LBVI; *Vireo bellii pusillus*), southwestern willow flycatcher (SWFL; *Empidonax traillii extimus*), and yellow-billed cuckoo (YBCU; *Coccyzus americanus*) was assessed in conjunction with the assessment for riverine/riparian areas.

5.4.2 Existing Conditions and Results

The project site does not contain any suitable riparian vegetation or habitat for special-status riparian birds. No additional surveys or mitigation are recommended.

6.0 PROTECTION OF NARROW ENDEMIC PLANT SPECIES (MSHCP SECTION 6.1.3)

The project is not located within a Section 6.1.3. Narrow Endemic Plant Species Survey Area (NEPSSA). The project site does not contain suitable vegetation communities, hydrology, or soils to support the narrow endemic plant species.

7.0 ADDITIONAL SURVEY NEEDS AND PROCEDURES (MSHCP SECTION 6.3.2)

The project is not located within a mapped survey area described in MSHCP Section 6.3.2.

7.1 CRITERIA AREA PLANT SPECIES

The project does not fall within a mapped survey area for Criteria Area plant species. The project site does not contain suitable vegetation communities, hydrology, or soils to support the MSHCP Criteria Area Plant Survey Species.

7.2 AMPHIBIANS

The project does not fall within a mapped survey area for amphibian species.

7.3 BURROWING OWL

The project site is not located within or adjacent to any burrowing owl survey areas.

7.3.1 Methods

The burrowing owl habitat assessment was conducted in accordance with guidelines (CDFW's 2012 *Staff Report on Burrowing Owl Mitigation and Burrowing Owl Survey Instructions for the Western Riverside County Multiple Species Habitat Conservation Plan Area*, Riverside County Environmental Programs Department, March 29, 2006).

The habitat assessment was conducted on April 12, 2019, between 9:00 and 11:00 a.m. by LSA Biologist Anthony Greco. Weather conditions during the survey were sunny with wind speeds of 5 to 8 miles per hour and temperatures ranging from 65 to 75 degrees.

The survey was conducted by walking throughout all suitable habitat on the project site. The survey area was approximately 2.3 acres and consisted of the project site plus a 500-foot buffer. Transects were spaced at 20 meters, which allowed for 100 percent visual coverage of the ground surface (Figure 6).

7.3.2 Existing Conditions and Results

The project site is a highly disturbed inholding. Soils within the project site are loose, primarily from repeated plowing or other weed abatement compliance methods. No ground squirrel burrows, or other man-made potential burrows were observed within the project site or surrounding survey buffer and none would be expected to occur given the isolation from existing habitat, the presence of avian predator perch sites, and generally high levels of urban activity. Therefore, no suitable habitat for the burrowing owl was found to be present on site and the species is not discussed further in this document.

7.4 MAMMALS

The project site does not fall within a mapped survey area for special-status mammal species.

8.0 INFORMATION ON OTHER SPECIES

8.1 DELHI SANDS FLOWER-LOVING FLY

The project site does not contain Delhi soils nor are any mapped within the MSHCP baseline data. Soils in the project area are well drained and have been previously mapped by the NRCS as Ramona sandy loam, with 0 to 2 percent slopes (Figure 4).

8.2 SPECIES NOT ADEQUATELY CONSERVED

Species identified in MSHCP Table 9-3 are not considered adequately conserved under the MSHCP. Other species with limited coverage or with no take authorization under the MSHCP include Santa Rosa Plateau fairy shrimp (*Linderiella santarosae*), bald eagle (*Haliaeetus leucocephalus*), golden eagle (*Aquila chrysaetos*), peregrine falcon (*Falco peregrinus anatum*), and white-tailed kite (*Elanus leucurus*).

No species identified on MSHCP Table 9-3 would be expected to occur on the project site, due to the level of disturbance and a lack of suitable vegetation communities, soils, and hydrology.

9.0 GUIDELINES PERTAINING TO THE URBAN/WILDLANDS INTERFACE (MSHCP SECTION 6.1.4)

The project site has no connection to any existing conservation lands or lands described for conservation. No additional guidelines pertaining to MSHCP Section 6.1.4 apply to the project site.

10.0 POTENTIAL JURISDICTIONAL WATERS AND STREAMBEDS

No drainage features, ponded areas, or riparian habitat potentially subject to jurisdiction by the CDFW or USACE were found within the project area.

11.0 NESTING BIRDS

During the bird breeding season (typically February 1 through August 31), large trees on or adjacent to the project area may be used by hawks, ravens, or other large birds for nesting. Trees, shrubs, and other vegetation may provide nest sites for smaller birds, and burrowing owls may nest in ground squirrel burrows, pipes, or similar features. Nesting bird species, with potential to occur are protected by California Fish and Game Code Sections 3503, 3503.5, and 3800, and by the Migratory Bird Treaty Act (MBTA) (16 USC 703–711). These laws regulate the take, possession, or destruction of the nest or eggs of any migratory bird or bird of prey. However, the USFWS has recently determined that the MBTA should apply only to “... affirmative actions that have as their purpose the taking or killing of migratory birds, their nests, or their eggs” and will not be applied to incidental take of migratory birds pursuant to otherwise lawful activities.

To avoid potential effects to fully protected raptors, special-status bird species, and other nesting birds protected by the California Fish and Game Code, and for compliance with MSHCP Incidental Take Permit Condition 5, the following measures will be implemented:

- A nesting bird pre-construction survey will be conducted by a qualified biologist three days prior to ground-disturbing activities. Should nesting birds be found, an exclusionary buffer will be established by the qualified biologist. The buffer may be up to 500 feet in diameter depending on the species of nesting bird found. This buffer will be clearly marked in the field by construction personnel under guidance of the qualified biologist and construction or clearing will not be conducted within this zone until the qualified biologist determines that the young have fledged or the nest is no longer active. Nesting bird habitat within the project area will be resurveyed during bird breeding season if there is a lapse in construction activities longer than seven days.

12.0 REFERENCES

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13.0 CERTIFICATION STATEMENT

I hereby certify that the statements furnished in this report present the data and information required for this biological evaluation and the facts, statements, and information presented are true and correct to the best of my knowledge and belief.

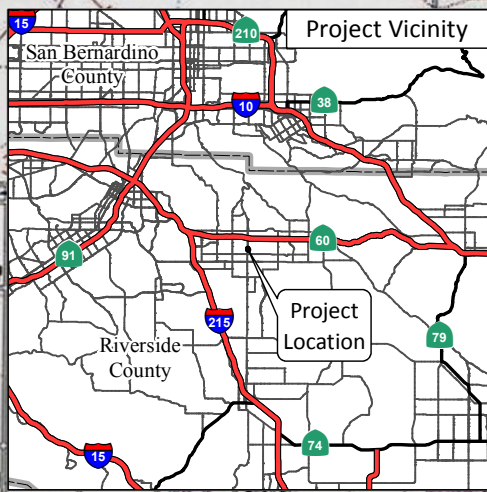
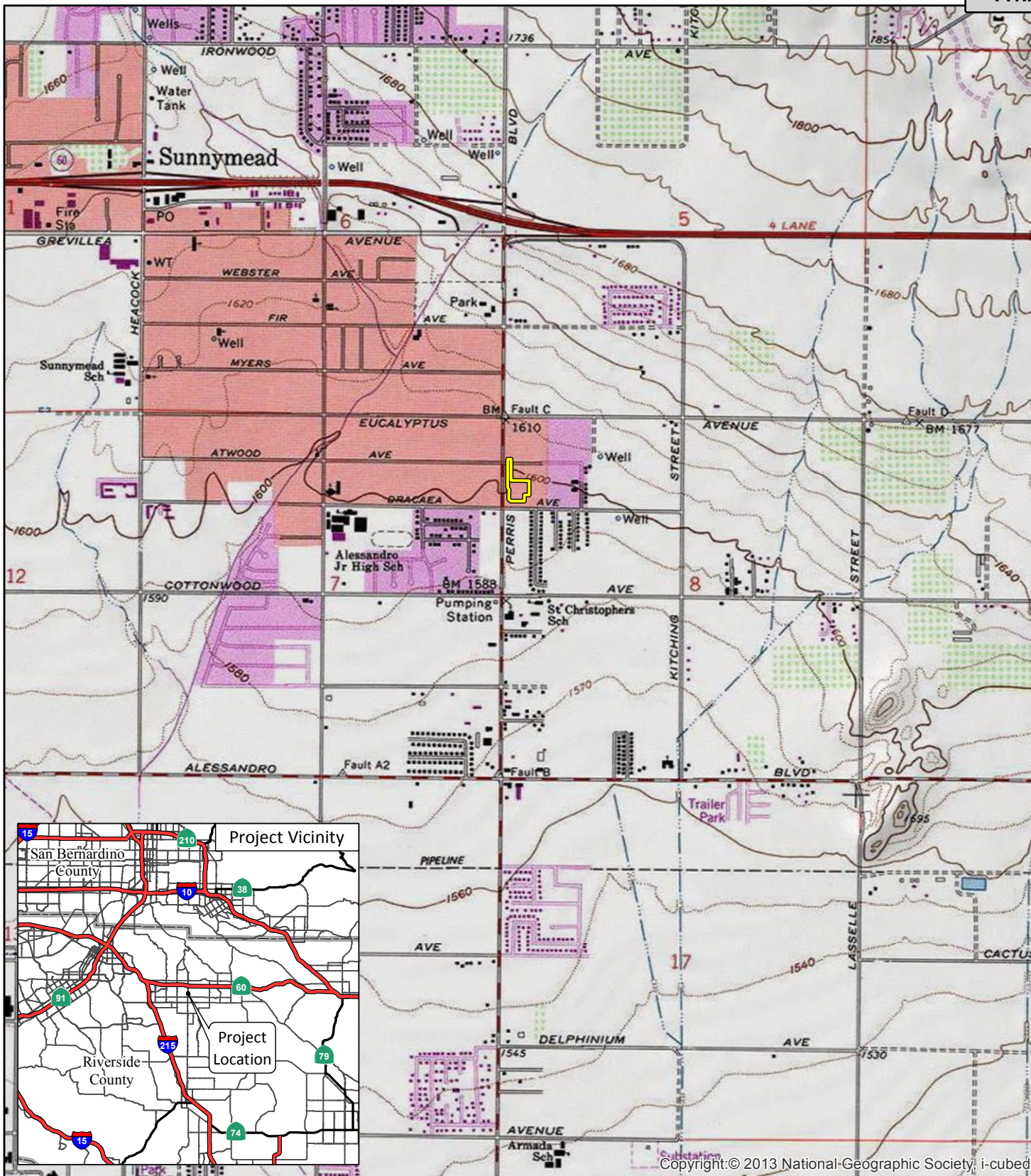
Date: September 1, 2020 Signature: *Anthony Gress*

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling


APPENDIX A

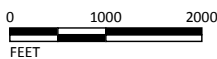
FIGURES

- Figure 1: Regional and Project Location
- Figure 2: Project Site
- Figure 3: Soils
- Figure 4: Site Photographs
- Figure 5: Burrowing Owl Habitat Assessment



LSA

LEGEND
 Project Location



SOURCE: USGS 7.5' Quad., Sunnymead, CA (1980)

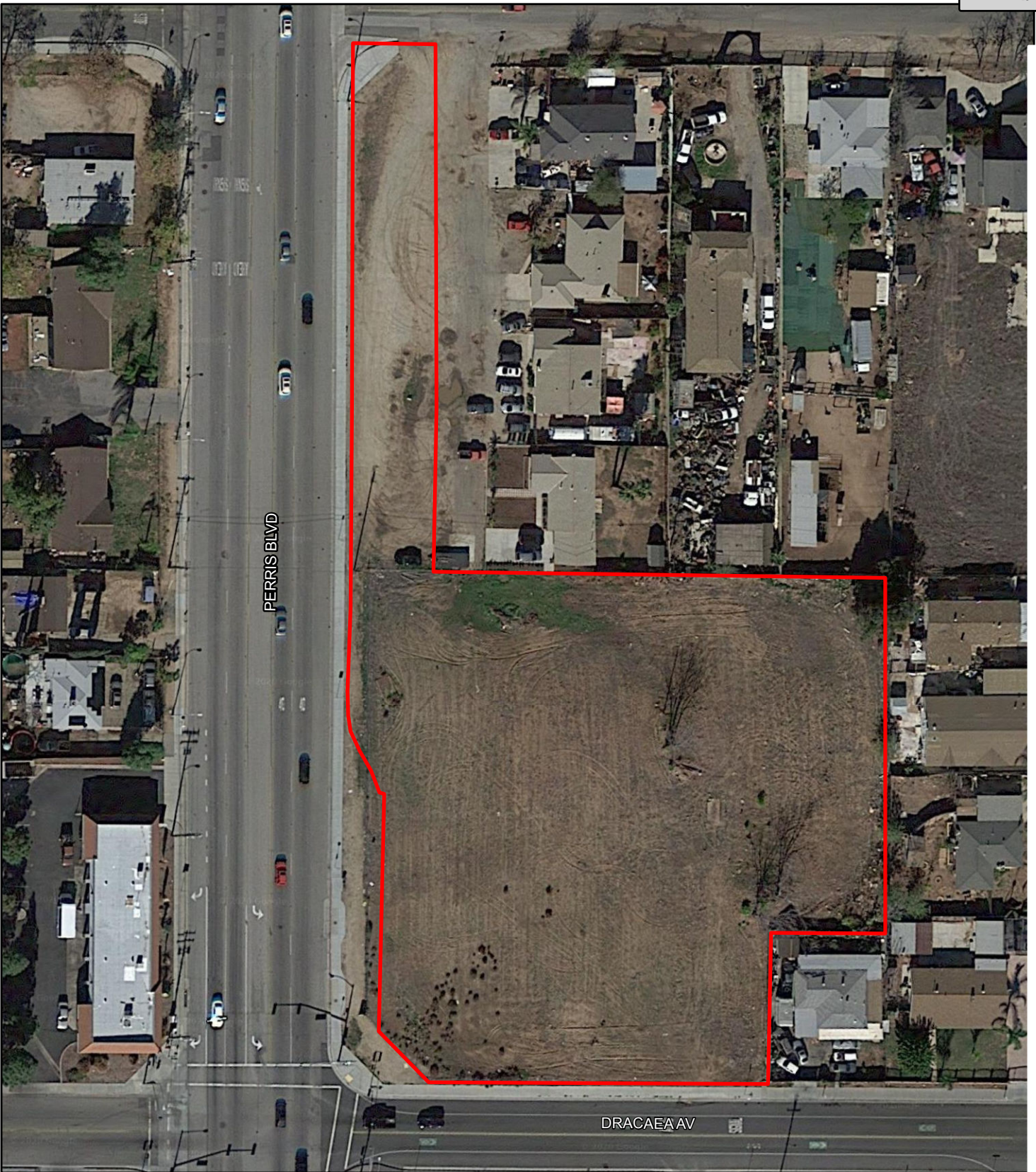
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FIGURE :

Commercial Retail Project at Perris Boulevard/Dracaea Avenue

Project Location



PERRIS BLVD

DRACAEA AV

LSA

LEGEND
 Project Site



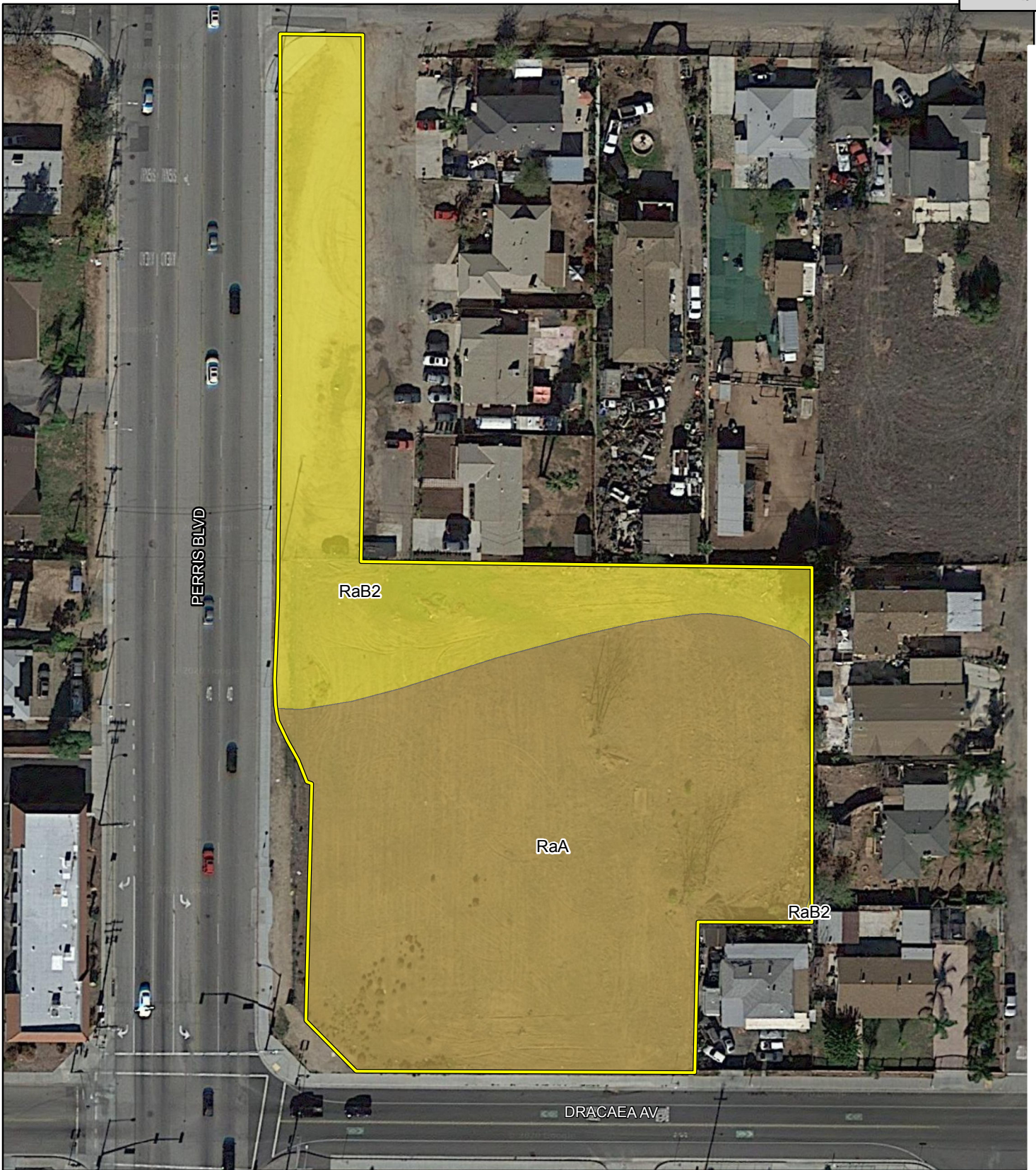
SOURCE: Google (2019)

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FIGURE :

Commercial Retail Project at Perris Boulevard/Dracaea Avenue
 Project Site

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



PERRIS BLVD

RaB2

RaA

RaB2

DRACAEA AV

LSA

LEGEND

Project Site

Soil Type

Ramona sandy loam, 0 to 2 percent slopes, MLRA 19 (RaA)

Ramona sandy loam, 2 to 5 percent slopes, eroded (RaB2)



SOURCE: Google (2018); SSURGO (accessed 2017)

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FIGURE 4

Commercial Retail Project at Perris Boulevard/Dracaea Avenue

Soils

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

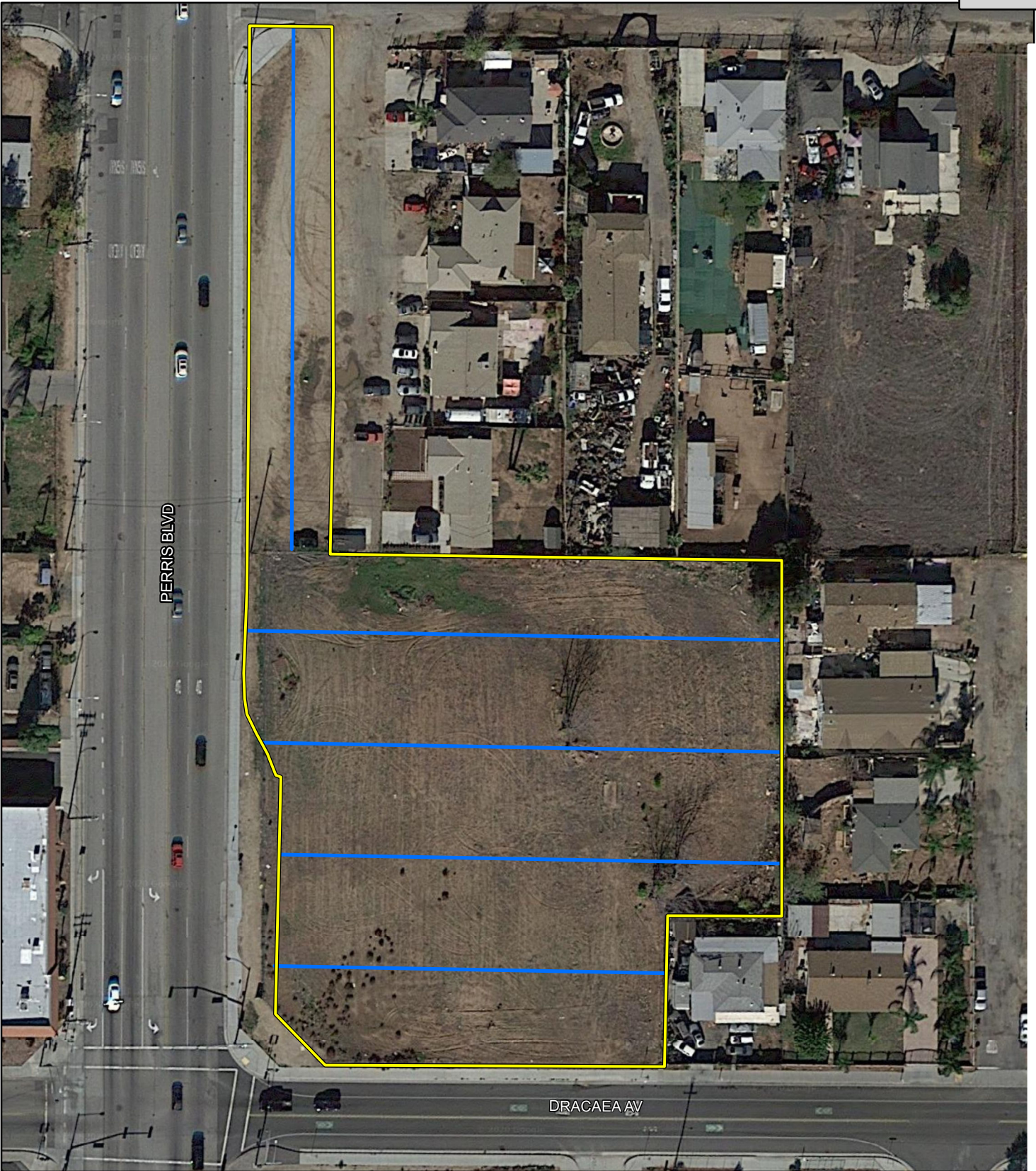


Photograph 1. Aerial view of the project site, facing north.



Photograph 2. Aerial view of project site, facing northwest.

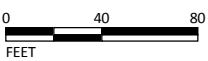
Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Thru Restaurant, Fueling



LSA

LEGEND

- Project Site
- 20 Meter Transects



SOURCE: Google (2019)

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FIGURE !

Commercial Retail Project at Perris Boulevard/Dracaea Avenue
 Burrowing Owl

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

APPENDIX B

PLANT AND ANIMAL SPECIES OBSERVED

Plant and Animal Species Observed

Scientific Name	Common Name
MAGNOLIID FLOWERING PLANTS	
Brassicaceae	Mustard family
<i>Hirschfeldia incana</i> (non-native species)	Shortpod mustard
Chenopodiaceae	Saltbush family
<i>Salsola tragus</i> (non-native species)	Russian thistle
Fabaceae	Fabaceae family
<i>Robinia pseudoacacia</i> (non-native species)	Black locust
Geraniaceae	Geranium family
<i>Erodium cicutarium</i> (non-native species)	Redstem stork's bill
Myrtaceae	Myrtaceae family
<i>Eucalyptus sp.</i>	Eucalyptus
Simaroubaceae	Quassia family
<i>Ailanthus altissima</i> (non-native species)	Tree of heaven
MONOCOTS FLOWERING PLANTS	
Poaceae	Grass family
<i>Bromus diandrus</i> (non-native species)	Ripgut brome
<i>Hordeum murinum</i> (non-native species)	Mouse barley
BIRDS	
Corvidae	Crows and Jays
<i>Corvus brachyrhynchos</i>	American crow
Passeridae	Old World Sparrows
<i>Passer domesticus</i> (non-native species)	House sparrow
Fringillidae	Finches
<i>Haemorhous mexicanus</i>	House finch

Appendix C:

**Cultural Resources Assessment, Perris Boulevard and Dracaea
Avenue Commercial Retail Project, City of Moreno Valley, Riverside
County, California**

CULTURAL RESOURCES ASSESSMENT

PERRIS BOULEVARD AND DRACAEA AVENUE COMMERCIAL RETAIL PROJECT

CITY OF MORENO VALLEY

RIVERSIDE COUNTY, CALIFORNIA

The logo for LSA, consisting of the letters 'L', 'S', and 'A' in a bold, blue, sans-serif font.

August 2020

CULTURAL RESOURCES ASSESSMENT

PERRIS BOULEVARD AND DRACAEA AVENUE COMMERCIAL RETAIL PROJECT

CITY OF MORENO VALLEY

RIVERSIDE COUNTY, CALIFORNIA

Prepared for:

Cadence Acquisition LLC
6400 S. Fiddlers Green Circle, Suite 1820
Greenwood Village, Colorado 80111

Prepared by:

Riordan Goodwin
LSA Associates, Inc.
1500 Iowa Avenue, Suite 200
Riverside, California 92507

LSA Project No. CAQ1901

National Archaeological Data Base Information:

Type of Study: Reconnaissance Survey

Sites Recorded: 33-028824/CA-RIV-012934

USGS 7.5' Quadrangle: Sunnymead, California

Acreage: ~2.3 acres

Keywords: Phase I, previously unsurveyed, positive results

The logo for LSA Associates, Inc. consists of the letters 'LSA' in a bold, blue, sans-serif font.

August 2020

MANAGEMENT SUMMARY

LSA was retained by Cadence Acquisition to conduct a cultural resources assessment for the proposed Perris Boulevard and Dracaea Avenue Commercial Retail Project located in Moreno Valley, Riverside County, California. This cultural resources assessment was completed pursuant to the California Environmental Quality Act (CEQA).

A cultural resources records search, additional research, and a field survey were conducted for the project area. No cultural resources were previously documented within or near the project parcels by the records search, and no prehistoric resources are recorded within one mile. However, a residence constructed prior to 1930 was formerly located within the project boundaries and associated features (a utility pole and slab) along with a glass bottle fragment dating to the historic period were identified during the survey. While these are (collectively) not a significant resource per se (typical examples of regionally and locally ubiquitous features and artifacts), they are temporal markers and remnant features suggesting limited disturbance. Their cultural resource value has been realized by recordation and their removal does not constitute a significant impact to cultural resources. However, due to the former presence of a pre-Depression Era residence, the project area retains some sensitivity for associated subsurface resources. As archaeological monitoring is part of the standard conditions for the project, no mitigation measures are required.

In the event previously undocumented archaeological resources are identified during earthmoving activities, further work in the area should be halted until the nature and significance of the find can be assessed by a qualified archaeologist.

If human remains are encountered, State Health and Safety Code Section 7050.5. states that no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to State Public Resources Code Section 5097.98. The County Coroner must be notified of the find immediately. If the remains are determined to be Native American, the County Coroner will notify the Native American Heritage Commission (NAHC), which will determine and notify a Most Likely Descendant (MLD). With the permission of the landowner or his/her authorized representative, the MLD may inspect the site of the discovery. The MLD shall complete the inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site. The MLD recommendations may include scientific removal and nondestructive analysis of human remains and items associated with Native American burials, preservation of Native American human remains and associated items in place, relinquishment of Native American human remains and associated items to the descendants for treatment, or any other culturally appropriate treatment.

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Figure 1: Project Regional and Project Location 2



Table

Table A: Cultural Resources Within One Mile with DPR Documentation 6

INTRODUCTION

LSA was retained by Cadence Acquisition to conduct a cultural resources assessment for the Perris Boulevard and Dracaea Avenue Commercial Retail Project located in Moreno Valley, Riverside County, California. This cultural resources assessment was completed per the California Environmental Quality Act (CEQA), Public Resources Code Chapter 2.6, Section 21083.2, and California Code of Regulations Title 14, Chapter 3, Article 5, Section 15064.5. The research and field survey was conducted to determine whether the proposed project would adversely affect any resources considered historical resources per CEQA.

PERSONNEL

LSA staff that worked on this project included Senior Cultural Resources Manager/Archaeologist Riordan Goodwin, who conducted the research and survey and authored the report; Archaeological Technician Alegria Garcia, who conducted the records search; and Lloyd Sample, who provided principal review.

PROJECT LOCATION AND DESCRIPTION

The proposed project includes approximately 2.3 acres (Assessor's Parcel Numbers [APNs] 479-120-027, 029, 043, and 049 on the northeast corner of Perris Boulevard and Dracaea Avenue. This is depicted on the United States Geological Survey (USGS) *Sunnymead, California* topographic quadrangle map in Township 3 South, Range 3 West in Section 8, San Bernardino Baseline and Meridian (USGS 1968; photo revised 1980) (Figure 1). The project is a proposed commercial retail development.

NATURAL SETTING

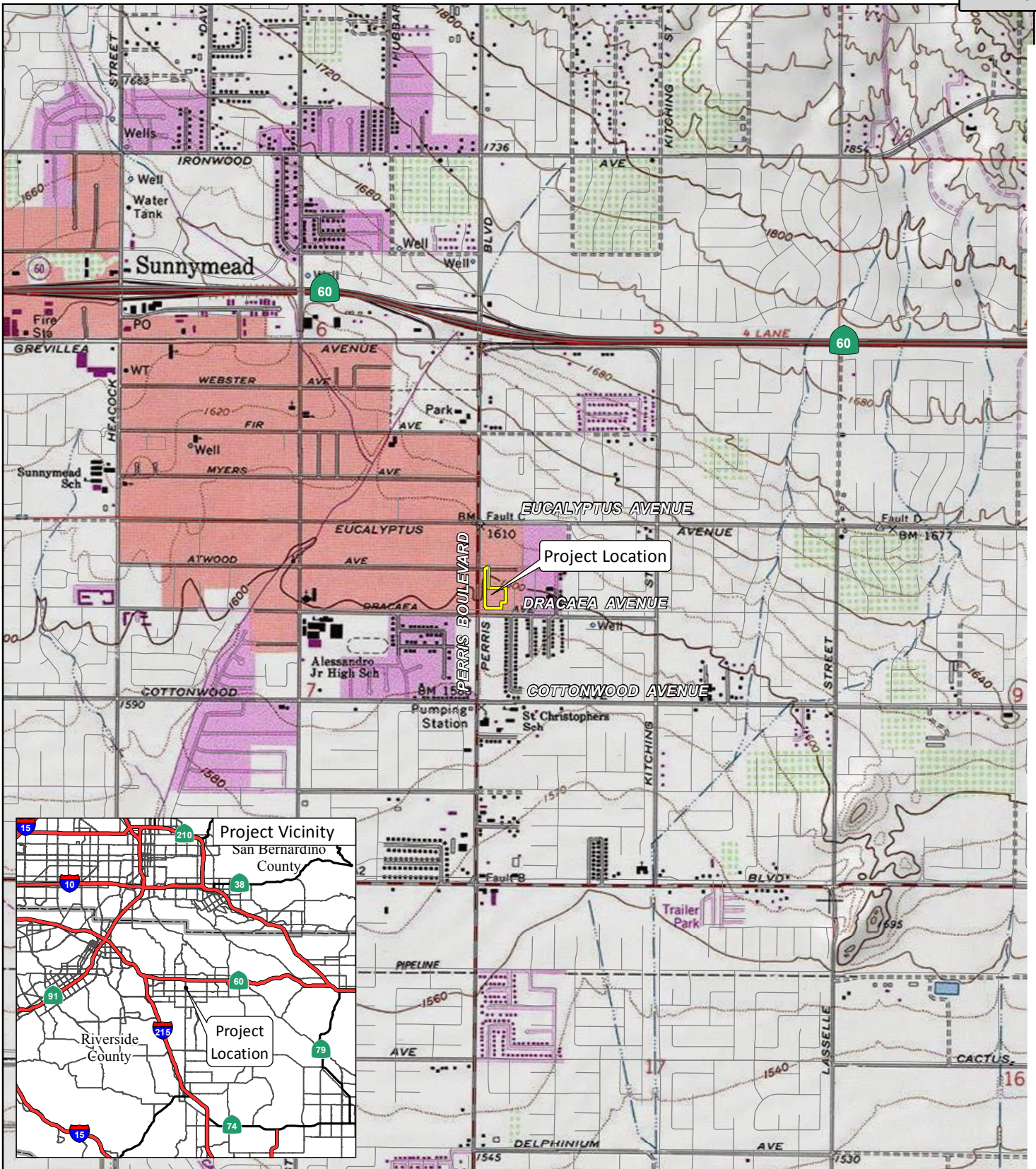
The natural setting of the project vicinity is presented based on the underlying theoretical assumption that humans and human societies are in continual interaction with the physical environment. Being an integral and major part of the ecological system, humans adapt to the environment through technological and behavioral changes. Locations of archaeological sites are based on the constraints of these adaptations, whether it is proximity to a particular resource, topographical restrictions, or shelter and protection. Sites will also contain an assemblage of artifacts and ecofacts consistent with the particular interaction.

Climate and Watershed

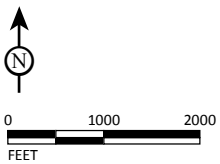
The project region is characterized by a temperate climate, with dry, hot summers and moderate winters. Rainfall ranges from 12 to 16 inches annually (Beck and Haase 1974). Precipitation usually occurs in the form of winter rain, with warm monsoonal showers in summer. The project was once bracketed by two drainages within a mile, both of which drained southwest.

Biology

At an elevation of approximately 1,600 feet above mean sea level (amsl), the project is within the Lower Sonoran Life Zone of California (Schoenherr 1992), which ranges from below sea level to 3,500 feet amsl. Pioneer species such as hare oats, mallow, mustard, red brome, Russian thistle,



LSA



SOURCE: USGS 7.5' Quad - Sunnymead (1980); ESRI Streetmap, 2013.

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FIGURE :

Perris Boulevard and Dracaea Avenue Commercial Retail
Regional and Project Location

eucalyptus and tree of heaven were noted on the property. Common animals of this region include rodents, rabbits, coyotes, raptors, reptiles, vultures, and insects.

Geology

The project is located at the northern end of the Peninsular Ranges Geomorphic Province that extends from the Transverse Ranges to the tip of Baja California and includes the Los Angeles Basin (California Geological Survey 2002; Norris and Webb 1976). This region is characterized by a series of mountain ranges separated by northwest-trending valleys subparallel to faults branching from the San Andreas Fault. The geology of this province is similar to that of the Sierra Nevada, with numerous rock outcroppings useful to the Native Americans for resource milling, shelter, and ceremonial art.

CULTURAL SETTING

Prehistory

Chronologies of prehistoric cultural change in Southern California area have been attempted numerous times, and several are reviewed in Moratto (2004). No single description is universally accepted as the various chronologies are based primarily on material developments identified by researchers familiar with sites in a particular region and variation exists essentially due to the differences in those items found at the sites. Small differences occur over time and space, which combine to form patterns that are variously interpreted.

Currently, two primary regional culture chronology syntheses are commonly referenced in the archaeological literature. The first, Wallace (1955), describes four cultural horizons or time periods: Horizon I – Early Man (9000–6000 BC), Horizon II – Milling Stone Assemblages (6000–3000 BC), Horizon III – Intermediate Cultures (3000 BC–AD 500), and Horizon IV – Late Prehistoric Cultures (AD 500–historic contact). This chronology was refined (Wallace 1978) using absolute chronological dates obtained after 1955.

The second cultural chronology (Warren 1968) is based broadly on Southern California prehistoric cultures and was also revised (Warren 1984; Warren and Crabtree 1986). Warren's (1984) chronology includes five periods in prehistory: Lake Mojave (7000–5000 BC), Pinto (5000–2000 BC), Gypsum (2000 BC–AD 500), Saratoga Springs (AD 500–1200), and Protohistoric (AD 1200–historic contact). Changes in settlement pattern and subsistence focus are viewed as cultural adaptations to a changing environment, which begins with gradual environmental warming in the late Pleistocene, continues with the desiccation of the desert lakes, followed by a brief return to pluvial conditions, and concludes with a general warming and drying trend, with periodic reversals that continue to the present (Warren and Crabtree 1986).

After AD 500, there was an influx of Native American groups from the eastern deserts into southern California. These groups brought changes in subsistence focus and associated technologies, as well as burial practices. These cultural changes along with the group migrations are known as the Shoshonean Intrusion or Shoshonean Wedge (Kroeber 1925; Koerper 1979) and the Takic Wedge (Bergin and Ferraro 1999). The term Takic Wedge refers to the wedge of Takic culture groups that moved to the coast, displacing tribes of the Hokan and Yuman language stocks to the north and

south (Shiple 1978). The ethnographically recorded Luiseño, Juaneño, and Gabrielino are thought to be the descendants of prehistoric Takic populations that settled along the coast during the Late Prehistoric Period, or perhaps even earlier. The Serrano and Cahuilla, more distant from the coast, are also Takic-speaking tribes within this wedge.

Ethnography

The project area is situated near the boundary of the traditional territories of the Cahuilla (Kroeber 1925; Bean 1978) and Luiseño (Kroeber 1925; Drucker 1939; Heizer and Whipple 1951; Smith and Freers 1994; Earp-Escobar 2018). Tribal territorial boundaries were somewhat fluid and changed over time. The first written accounts of the Cahuilla are attributed to the mission fathers. Later documentation was by Barrows (1900), Hooper (1920), and Strong (1929) among others.

Cahuilla

The territory of the Cahuilla ranged from the San Bernardino Mountains south to Borrego Springs and the Chocolate Mountains, from Orocopia Mountain to the east, to the San Jacinto Plain and Palomar Mountain to the west (Bean 1978). Cahuilla territory lies within the geographic center of Southern California and encompasses diverse environments ranging from inland river valleys and foothills to mountains and desert (Bean and Shipek 1978).

Cahuilla villages, generally located near water sources within canyons or near alluvial fans, comprised groups of related individuals, generally from a single lineage, and the territory around the village was owned by the villagers (Bean 1978). Like other Native American groups in Southern California, the Cahuilla were semi-nomadic peoples leaving their villages and utilizing temporary campsites to exploit seasonably available plant and animal resources (James 1960).

Cahuilla subsistence was based primarily on acorns, honey mesquite, screw beans, piñon nuts, and cactus fruit, supplemented by a variety of wild fruits and berries, tubers, roots, and greens (Kroeber 1925; Heizer and Elsasser 1980). Hunting deer, rabbit, antelope, bighorn sheep, reptiles, small rodents, quail, doves, ducks, and reptiles by means of bows, throwing sticks, traps, and communal drives is documented (James 1960).

Luiseño

The territory of the Luiseño “reached as far northeast as the Santa Ana River and the Box Springs Mountain Range, as far east as Mount San Jacinto, as far southeast as Lake Henshaw, and to the west including the Southern Channel Islands,” encompassing an extremely diverse environment that included beaches, lagoons, marshes, inland river valleys, foothills, and mountain groves of oaks and evergreens (Bean and Shipek 1978; Kroeber 1925; Drucker 1939; Heizer and Whipple 1951; Smith and Freers 1994; Earp-Escobar 2018).

The Luiseño lived in small communities, which were the focus of family life. Patrilineally linked, extended families occupied each village (Kroeber 1925; Bean and Shipek 1978). Luiseño villages were politically independent and were administered by a chief who inherited his position from his father. Luiseño villages generally were located in valley bottoms, along streams, or along coastal

strands near mountain ranges sheltered in coves or canyons, near a water source, and in a location that was easily defended.

The Luiseño took advantage of the varied resources available. Luiseño subsistence was based primarily on seeds (e.g., acorns, grass seed, manzanita, sunflower, sage, chía, and pine nuts) that were dried and ground to be cooked into a mush. Their diet also included game animals (e.g., deer, rabbit, jackrabbit, wood rat, mice, antelope, and many types of birds) (Bean and Shipek 1978). They established seasonal camps along the coast and near bays and estuaries to gather shellfish and hunt waterfowl; and they utilized fire for crop management and engaged in communal rabbit drives (Bean and Shipek 1978).

The first written accounts of the Luiseño are attributed to the mission fathers. Later documentation was authored by Sparkman (1908), Kroeber (1925), White (1963), Oxendine (1983), and others.

History

In California, the historic era is generally divided into three periods: the Spanish Period (1769 to 1821), the Mexican Period (1821 to 1848), and the American Period (1848 to present). Early exploration of the Riverside County area was slow until Lieutenant Pedro Fages, then the military governor of San Diego, crossed through the San Jacinto Valley in 1772.

Riverside County

The Southern Pacific Railroad completed its line from Los Angeles through the San Geronio Pass in 1876. The trains were eventually used to transport settlers into the area, creating a period of agricultural and land development, ultimately resulting in the establishment of Riverside County in 1893. Transportation, agriculture, and the control of water have continued to be central themes in the settlement, development, and growth of Riverside County (Robinson 1979).

Moreno Valley

Originally platted as “New Haven,” the community of Moreno Valley was renamed Moreno (Spanish for “brown”) in honor of real estate entrepreneur/founder Frank E. Brown who had helped organize the Bear Valley Land and Water Company and instituted an irrigation district that fostered large-scale grain and fruit farming (Holtzclaw et al. 2007). The community thrived during its first few years in the late 1880s and, by 1893, it included a hotel, weekly newspaper, pharmacy, livery stable, stores, offices, two churches, and a nursery; and the surrounding farmland became known as Moreno Valley (Gunther 1984; Brown 1985). The town’s prosperity was short-lived, however, and a drought, combined with the City of Redlands’ water rights claim along the same Bear Valley Pipeline, precipitated its decline in the final years of the 19th century (Brown 1985; Holtzclaw et al. 2007). Many settlers relocated, homes and all, to nearby Riverside (Brown 1985).

Subsequent attempts at municipal revival of the Moreno Valley area in the 20th century were unsuccessful until 1973, when locals created Lake Perris to retain water transported from the Feather River. The new lake provided recreational infrastructure in addition to a reliable water

supply, and stimulated growth throughout Moreno Valley. The communities of Moreno, Sunnymead, and Edgemont were incorporated as the City of Moreno Valley in 1984 (Gunther 1984).

METHODS

Records Search

On March 28, 2019, Ms. Garcia conducted a cultural resources records search for the project area at the Eastern Information Center (EIC) at the University of California, Riverside. The EIC houses the pertinent archaeological site and survey information necessary to determine whether cultural resources are recorded within the study area and which specific areas have been previously surveyed. The research included a review of all recorded historic and prehistoric archaeological sites within one mile of the project, as well as a review of known cultural resource survey and excavation reports. In addition, LSA examined the California State Historic Property Data File (HPD), which includes the National Register of Historic Places (National Register), California Historical Landmarks (CHL), and California Points of Historical Interest (CPHI), various local historic registers, and historic maps.

Additional Research

In March 2019, Mr. Goodwin reviewed historic period maps and aerial photographs and conducted additional online research.

Sacred Lands File Search

On March 18, 2019, a request was sent to the Native American Heritage Commission (NAHC) requesting a Sacred Lands File (SLF) search for this project.

Field Survey

On April 25, 2019, Mr. Goodwin conducted the survey of the project area by walking transects spaced at 10 meters, with particular attention given to rodent burrows and back dirt.

RESULTS

Records Search

Results of the records search at the EIC indicate there have been 15 previous cultural resources studies conducted within a one-mile radius, none of which included any portion of the project area. Although no cultural resources have been documented within the project area, five historic period residences occur within one mile, all eligible for listing in the National Register (Table A). The nearest resource (33-007280) is approximately 0.5 mile to the southwest.

Table A: Cultural Resources Within One Mile with DPR Documentation

Primary #	OHP#	Site Description	Status Code
33-007276	062617	25780 Alessandro Boulevard; c. 1920 residence	3S
33-007279	062620	24771 Bay Avenue/D.C. Hield House; c. 1896 residence	3S
33-007280	062621	24685 Cottonwood Avenue/Rosa More House; c. 1880 residence	3S

Table A: Cultural Resources Within One Mile with DPR Documentation

Primary #	OHP#	Site Description	Status Code
33-007282	062623	26010 Eucalyptus Avenue; c. 1910 residence	3S
33-007284	062625	24638 Fir Avenue; c. 1915 residence	3S

Additional Research

Review of historic period maps and online research indicated there was formerly a residence on the one of the currently undeveloped parcels (APN 479-120-027) constructed prior to 1930 and removed by 2002 (HistoricAerials.com var.). Appendix A contains the Records Search Bibliography.

Sacred Lands File Search

The results of the SLF were negative, i.e., no Traditional Cultural Properties (TCPs) are documented within or near the project parcels (Appendix B).

Field Survey

The field survey revealed that the project area is almost completely obscured by spring growth vegetation and visibility was exceptionally poor throughout the parcels at less than 5 percent. Modern refuse was noted on the surface. Soils are medium to fine sandy alluvial silt.

33-028824/CA-RIV-012934

A utility pole with a 1930 inspection nail, a small (15 feet × 6 feet) slab and a glass bottle fragment dating to the historic period (1910s–1920s) were identified during the survey (Lindsey 2019a, 2019b). These were associated with the house formerly located on APN 479-120-027 (see site record in Appendix C).

RECOMMENDATIONS

A cultural resources records search, additional research, and a field survey were conducted for the project area. No cultural resources were previously documented within or near the project parcels by the records search, and no prehistoric resources are recorded within one mile. However, a residence constructed prior to 1930 was formerly located within the project boundaries and associated features (a utility pole and slab) and a glass bottle fragment dating to the historic period were identified during the survey. While these are (collectively) not a significant resource per se (typical examples of regionally and locally ubiquitous features and artifacts), they are temporal markers and remnant features suggesting limited disturbance. Their cultural resource value has been realized by recordation and their removal does not constitute a significant impact to cultural resources. However, due to the former presence of a pre-Depression Era residence, the project area retains some sensitivity for associated subsurface resources. As archaeological monitoring is part of the standard conditions for the project, no mitigation measures are required.

In the event previously undocumented archaeological resources are identified during earthmoving activities, further work in the area should be halted until the nature and significance of the find can be assessed by a qualified archaeologist.

If human remains are encountered, State Health and Safety Code Section 7050.5. states that no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to State Public Resources Code Section 5097.98. The County Coroner must be notified of the find immediately. If the remains are determined to be Native American, the County Coroner will notify the NAHC, which will determine and notify an MLD. With the permission of the landowner or his/her authorized representative, the MLD may inspect the site of the discovery. The MLD shall complete the inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site. The MLD recommendations may include scientific removal and nondestructive analysis of human remains and items associated with Native American burials, preservation of Native American human remains and associated items in place, relinquishment of Native American human remains and associated items to the descendants for treatment, or any other culturally appropriate treatment.

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2019a Bottle/Glass Colors. BLM Klamath Falls, OR (<https://sha.org/bottle/colors.htm>).

2019b Bottle Body Characteristics and Mold Seams. BLM Klamath Falls, OR (<https://sha.org/bottle/body.htm#Body%20Irregularities>).

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APPENDIX A

RECORDS SEARCH BIBLIOGRAPHY

Report List

Report No.	Other IDs	Year	Author(s)	Title	Affiliation	Resources
RI-00002	NADB-R - 1080003; Voided - MF-0003	1953	Malcolm J. Rogers	Miscellaneous Field Notes - Riverside County. San Diego Museum of Man	San Diego Museum of Man	
RI-00133	NADB-R - 1080150; Voided - MF-0114	1974	Thomas F. King, Mary A. Brown, Gerrit Fenenge, and Claudia Nissley	Archaeological Impact Evaluation: Southern California Edison Company's Devers-Vista 220 KV Transmission Line, Riverside County, California	Archaeological Research Unit, U.C. Riverside	
RI-00137	NADB-R - 1080155; Voided - MF-0117	1974	James F. O'Connell, Philip J. Wilke, Thomas F., King, and Carol L. Mix	Perris Reservoir Archaeology, Late Prehistoric Demographic Change in Southeastern California	Archaeological Research Unit, U.C. Riverside	33-000012, 33-000021, 33-000062, 33-000202, 33-000331, 33-000419, 33-000452, 33-000455, 33-000463, 33-000464
RI-00161	NADB-R - 1080200; Voided - MF-0144	1975	Roberta S. Greenwood	Paleontological, Archaeological, Historical, and Cultural Resources, West Coast-Midwest Pipeline Project, Long Beach to Colorado River	Greenwood and Associates	
RI-01665	NADB-R - 1081956; Voided - MF-1759	1983	Wirth Associates	Devers-Serrano-Villa Park Transmission System Supplement to the Cultural Resources Technical Report - Public Review Document and Confidential Appendices	Wirth Associates	33-002529, 33-002530, 33-002531, 33-002591, 33-002592, 33-013336, 33-013366, 33-013545
RI-01786	NADB-R - 1082137; Voided - MF-1930	1983	SCIENTIFIC RESOURCE SURVEYS, INC.	CULTURAL RESOURCE REPORT ON TRACTS 12608, 12606-2 AND 11410 LOCATED IN THE SUNNYMEAD AREA, RIVERSIDE COUNTY, CALIFORNIA	AUTHOR(S)	
RI-01955	NADB-R - 1082349; Voided - MF-2124	1977	HELLER, ROD, TIM TETHEROW, and C. WHITE	AN OVERVIEW OF THE SUNDESERT NUCLEAR PROJECT TRANSMISSION SYSTEM CULTURAL RESOURCE INVESTIGATION	WIRTH ASSOCIATES	

Report List

Report No.	Other IDs	Year	Author(s)	Title	Affiliation	Resources
RI-02171	NADB-R - 1082753; Submitter - 0870; Voided - MF-2358	1987	MCCARTHY, DANIEL F.	CULTURAL RESOURCES INVENTORY FOR THE CITY OF MORENO VALLEY, RIVERSIDE COUNTY, CALIFORNIA	ARCHAEOLOGICAL RESEARCH UNIT, U.C. RIVERSIDE	33-000361, 33-000395, 33-000497, 33-000857, 33-000860, 33-001063, 33-001064, 33-003223, 33-003224, 33-003225, 33-003226, 33-003227, 33-003228, 33-003229, 33-003230, 33-003231, 33-003232, 33-003233, 33-003234, 33-003235, 33-003236, 33-003237, 33-003238, 33-003239, 33-003240, 33-003241, 33-003242, 33-003243, 33-003244, 33-003245, 33-003246, 33-003247, 33-003248, 33-003249, 33-003250, 33-003254, 33-003258, 33-003259, 33-003260, 33-003261, 33-003262, 33-003263, 33-003264, 33-003265, 33-003266, 33-003267, 33-003268, 33-003269, 33-003270, 33-003271, 33-003272, 33-003273, 33-003304, 33-003305, 33-003306, 33-003341, 33-003342, 33-003343, 33-003344, 33-003345, 33-003346, 33-003347, 33-003351, 33-003352, 33-003353
RI-03604	NADB-R - 1084327; Voided - MF-3878	1992	Carleton S. Jones	The Development of Cultural Complexity Among the Luiseno: A Thesis Presented to the Department of Anthropology, California State University, Long Beach in Partial Fulfillment of the Requirements for the Degree, Master of Arts	California State University, Long Beach	
RI-04762	NADB-R - 1083854; Voided - MF-3495	1990	BARKER, LEO R. and ANN E. HUSTON, EDITORS	DEATH VALLEY TO DEADWOOD; KENNECOTT TO CRIPPLE CREEK. PROCEEDINGS OF THE HISTORIC MINING CONFERENCE, JANUARY 23-27, 1989, DEATH VALLEY NATIONAL MONUMENT	Division of National Register Programs National Park Service	
RI-04992	NADB-R - 1086354; Submitter - 10-04-10-1001	2004	MCKENNA ET AL.	AN ARCHITECTURAL EVALUATION OF STRUCTURES LOCATED WITHIN ASSESSOR PARCEL NUMBERS 482-090-009-0, -010-0, AND 033-0, WITHIN THE CITY OF MORENO VALLEY, RIVERSIDE COUNTY, CALIFORNIA.	MCKENNA ET AL.	
RI-05088	NADB-R - 1086450	2005	CULTURAL SYSTEMS RESEARCH, INC.	ETHNOGRAPHIC OVERVIEW INLAND FEEDER PIPELINE PROJECT	CULTURAL SYSTEMS RESEARCH, INC.	

Report List

Report No.	Other IDs	Year	Author(s)	Title	Affiliation	Resources
RI-07539	Submitter - LA2356B Cultural Rpt	2007	Bonner, Wayne H. and Marnie Aislin-Kay	Letter Report: Cultural Resources Records Search and Site Visit for Royal Street Telecommunications, LLC Candidate LA2356B (Sunnymead Plaza), 24903 Sunnymead Boulevard, Moreno Valley, Riverside County, California	Michael Brandman Associates	
RI-08235		2001	James E. Workman	Cupules A Type of Petroglyphic Rock Art. A Study of the Pitted Boulders in the San Jacinto Wildlife Area and the Lake Perris State Recreational Area	Indian Rock Art Specialist	33-000012, 33-000062, 33-000202, 33-000331, 33-000395, 33-000419, 33-000421, 33-000428, 33-000452, 33-000464
RI-08554	Submitter - CRM TECH Project No. 2507; Submitter - CRM TECH Project No. 2507	2011	Michael Hogan, Bai "Tom" Tang, John Goodman, and Daniel Ballester	California Living Moreno Valley Project	CRM TECH	33-007280, 33-007284, 33-007285, 33-007289, 33-007297, 33-017202, 33-017203

APPENDIX B

SACRED LANDS FILE SEARCH RESULTS

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

NATIVE AMERICAN HERITAGE COMMISSION
Cultural and Environmental Department
1550 Harbor Blvd., Suite 100
West Sacramento, CA 95691
Phone: (916) 373-3710
Email: nahc@nahc.ca.gov
Website: <http://www.nahc.ca.gov>
Twitter: @CA_NAHC



April 5, 2019

Rory Goodwin
LSA

VIA Email to: rory.goodwin@lsa.net

RE: Perris Blvd. and Dracaea Ave. Commercial Retail Project, Riverside County

Dear Mr. Goodwin:

A record search of the Native American Heritage Commission (NAHC) Sacred Lands File (SLF) was completed for the information you have submitted for the above referenced project. The results were negative. However, the absence of specific site information in the SLF does not indicate the absence of cultural resources in any project area. Other sources of cultural resources should also be contacted for information regarding known and recorded sites.

Attached is a list of Native American tribes who may also have knowledge of cultural resources in the project area. This list should provide a starting place in locating areas of potential adverse impact within the proposed project area. I suggest you contact all of those indicated; if they cannot supply information, they might recommend others with specific knowledge. By contacting all those listed, your organization will be better able to respond to claims of failure to consult with the appropriate tribe. If a response has not been received within two weeks of notification, the Commission requests that you follow-up with a telephone call or email to ensure that the project information has been received.

If you receive notification of change of addresses and phone numbers from tribes, please notify the NAHC. With your assistance, we can assure that our lists contain current information. If you have any questions or need additional information, please contact me at my email address: steven.quinn@nahc.ca.gov.

Sincerely,

A handwritten signature in blue ink that reads "Steven Quinn".

Steven Quinn
Associate Governmental Program Analyst

Attachment

Native American Heritage Commission
Native American Contact List
Riverside County
4/5/2019

F.1.b

**Agua Caliente Band of Cahuilla
Indians**

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Cahuilla

**Los Coyotes Band of Cahuilla
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Fax: (760) 782-2730
Cahuilla

**Agua Caliente Band of Cahuilla
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**Augustine Band of Cahuilla
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Cahuilla

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Serrano

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Cahuilla Band of Indians

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Cahuilla

This list is current only as of the date of this document. Distribution of this list does not relieve any person of statutory responsibility as defined in Section 7050.5 of the Health and Safety Code, Section 5097.94 of the Public Resource Section 5097.98 of the Public Resources Code.

This list is only applicable for contacting local Native Americans with regard to cultural resources assessment for the proposed Perris Blvd. and Dracaea Ave. Commercial Retail Project, Riverside County.

Native American Heritage Commission
Native American Contact List
Riverside County
4/5/2019

F.1.b

Ramona Band of Cahuilla

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**Santa Rosa Band of Cahuilla
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**Soboba Band of Luiseno
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San Jacinto, CA, 92583 Luiseno
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**Soboba Band of Luiseno
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**Torres-Martinez Desert Cahuilla
Indians**

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mmirelez@tmdci.org

This list is current only as of the date of this document. Distribution of this list does not relieve any person of statutory responsibility as defined in Section 7050.5 of the Health and Safety Code, Section 5097.94 of the Public Resource Section 5097.98 of the Public Resources Code.

This list is only applicable for contacting local Native Americans with regard to cultural resources assessment for the proposed Perris Blvd. and Dracaea Ave. Commercial Retail Project, Riverside County.

APPENDIX C
DPR SITE RECORD

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # 33-028824
 HRI #
 Trinomial CA-RIV- 012934
 NRHP Status Code

Other Listings
 Review Code Reviewer Date

Page 1 of 4 *Resource Name or #: LSA-CAQ1901-S-1

P1. Other Identifier:

*P2. Location: Not for Publication Unrestricted *a. County: Riverside, California

and (P2b and P2c or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad: *Sunnymead* Date: 1980 T 3S; R 3W; NW ¼ of NW ¼ of Sec 8; M.D. SBB.M.

c. Address: Perris Boulevard and Dracaea Avenue City: Moreno Valley Zip: 92557

d. UTM: Zone: 11 ; 4791717 mE/ 3754275 mN (G.P.S. NAD 83)

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate) APN 479-120-027

Access to this site from State Route 60 is via Perris Boulevard south (approximately .72 miles)- the resource is approximately 180 feet east of the road.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) Utility pole and slab features along with a glass bottle fragment (see Archaeological Site Record for details).

*P3b. Resource Attributes: (List attributes and codes) AH2 (Foundation/slab) AH16 (power pole, artifact)

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)



P5b. Description of Photo: (View, date, accession #) View northwest 4/25/2019; Frame #2175

*P6. Date Constructed/Age and Sources: Historic Prehistoric Both
 1910s-20s/ Lindsey 2019

*P7. Owner and Address:
 Cadence Acquisition LLC
 6400 S. Fiddlers Green Circle, St. 1820
 Greenwood Village CO 80111

*P8. Recorded by: (Name, affiliation, and address)
 Riordan Goodwin
 LSA Associates, Inc.
 1500 Iowa Avenue, Suite 200
 Riverside, California 92507

*P9. Date Recorded:
 April 25, 2019

*P10. Survey Type: (Describe)
 Intensive pedestrian

*P11. Report Citation: (Cite survey report and other sources, or enter "none.") Goodwin, Riordan 2019: Perris Boulevard and Dracaea Avenue Commercial Retail Project, City of Moreno Valley, Riverside County, California.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List):
 DPR 523A (1/95) *Required information

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
ARCHAEOLOGICAL SITE RECORD

Primary # 33-028824
 Trinomial CA-RIV-012934

Page 2 of 4

*Resource Name or #: LSA-CAQ1901-S-1

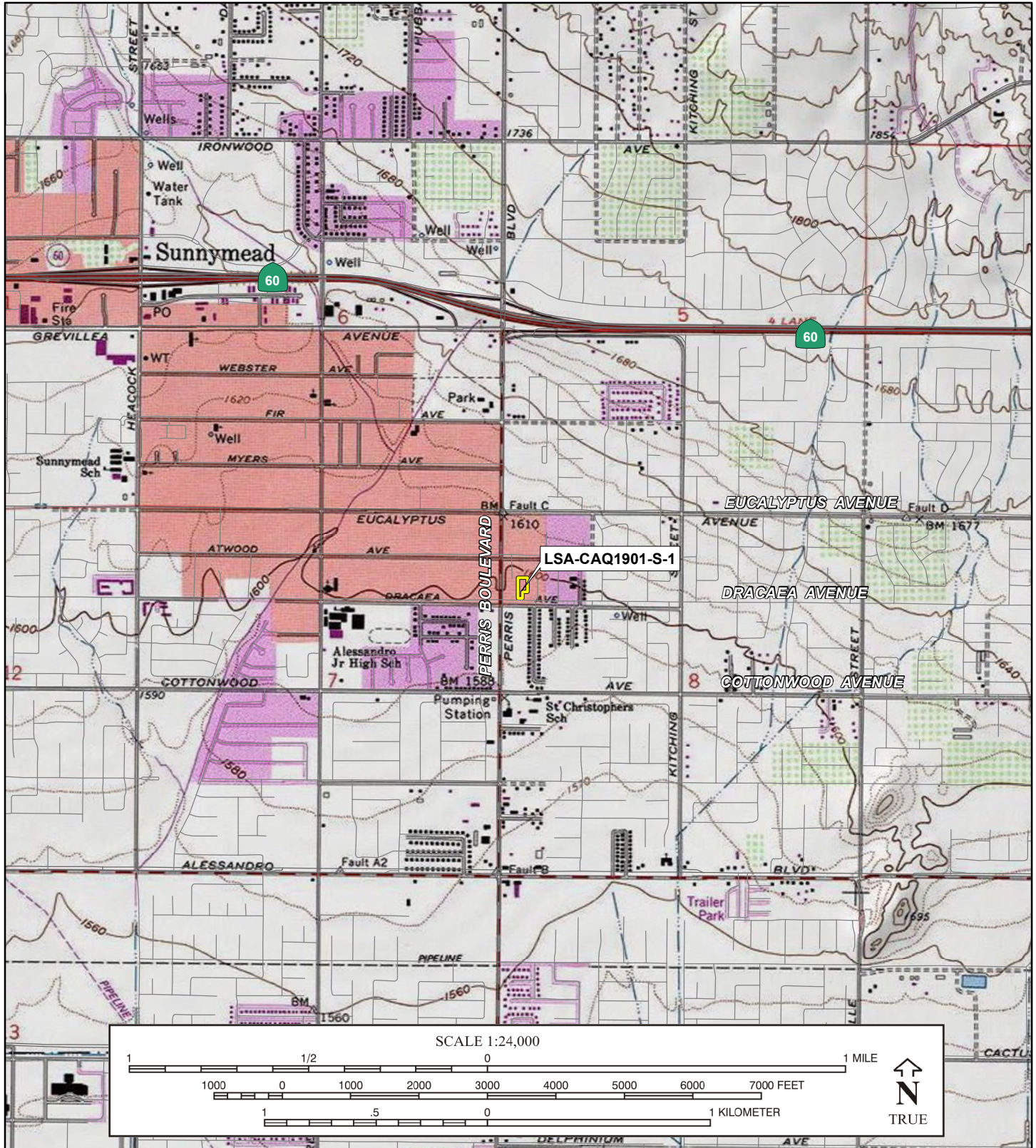
- *A1. Dimensions:** a. **Length:** ~220 feet N-S x **Width:** ~140 feet E-W
Method of Measurement: Paced Taped Visual estimate Other: Google Earth Pro
Method of Determination (Check any that apply.): Artifacts Features Soil Vegetation Topography
 Cut bank Animal burrow Excavation Property boundary Other (Explain):
Reliability of Determination: High Medium Low Explain:
Limitations (Check any that apply): Restricted access Paved/built over Site limits incompletely defined
 Disturbances Vegetation Other (Explain):
- A2. Depth:** None Unknown **Method of Determination:**
- *A3. Human Remains:** Present Absent Possible Unknown (Explain):
- *A4. Features** (Number, briefly describe, indicate size, list associated cultural constituents, and show location of each feature on sketch map.):
 15' x 6' remnant slab, utility pole (# 215924) with 1930 and 1947 inspection nails.
- *A5. Cultural Constituents** (Describe and quantify artifacts, ecofacts, cultural residues, etc., not associated with features.):
 Single clear glass bottle fragment with seed bubbles.
- *A6. Were Specimens Collected?** No Yes (If yes, attach Artifact Record or catalog and identify where specimens are curated.)
- *A7. Site Condition:** Good Fair Poor (Describe disturbances.): House footings/foundations and any related features absent.
- *A8. Nearest Water** (Type, distance, and direction.): San Antonio Creek is located approximately 1750 east.
- *A9. Elevation:** approximately 1,600 feet AMSL
- A10. Environmental Setting** (Describe culturally relevant variables such as vegetation, fauna, soils, geology, landform, slope, aspect, exposure, etc.): The site is on a vacant lot with extensive modern and temporally ambiguous refuse and debris, mallow, mustard, eucalyptus, red brome, Russian thistle, tree of heaven and xeric grasses were noted.
- A11. Historical Information:** A residence once stood on this parcel and the associated utility pole has a 1930 inspection nail.
- *A12. Age:** Prehistoric Protohistoric 1542-1769 1769-1848 1848-1880 1880-1914 1914-1945
 Post 1945 Undetermined **Describe position in regional prehistoric chronology or factual historic dates if known:**
 Likely 1910s-1920s
- A13. Interpretations** (Discuss data potential, function[s], ethnic affiliation, and other interpretations):
 It is a typical example of a common resource type: a remnant building foundation and related feature lacking substantial associated surface artifacts or significant historical association. However, it dates prior to the Depression Era and therefore retains some potential for subsurface features or artifacts (i.e. a privy or refuse deposit).
- A14. Remarks:** There was only one artifact identified in *potential* associated with these features.
- A15. References** (Documents, informants, maps, and other references):
 Lindsey, Bill
 2019a Bottle/Glass Colors. BLM Klamath Falls OR (<https://sha.org/bottle/colors.htm>)
 2019b Bottle Body Characteristics and Mold Seams. BLM Klamath Falls OR (<https://sha.org/bottle/body.htm#Body%20Irregularities>)
- A16. Photographs** (List subjects, direction of view, and accession numbers or attach a Photograph Record.):
 Frames 2157- 2160, 2165-2169, 2173-2177
- Original Media/Negatives Kept at:** LSA Associates, Inc., 1500 Iowa Avenue, Suite 200, Riverside, CA 92507
- *A17. Form Prepared by:** Riordan Goodwin **Date:** 5/8/19
Affiliation and Address: LSA Associates, Inc., 1500 Iowa Avenue, Suite 200, Riverside, CA 92507

DPR 523C (1/95)

*Required information

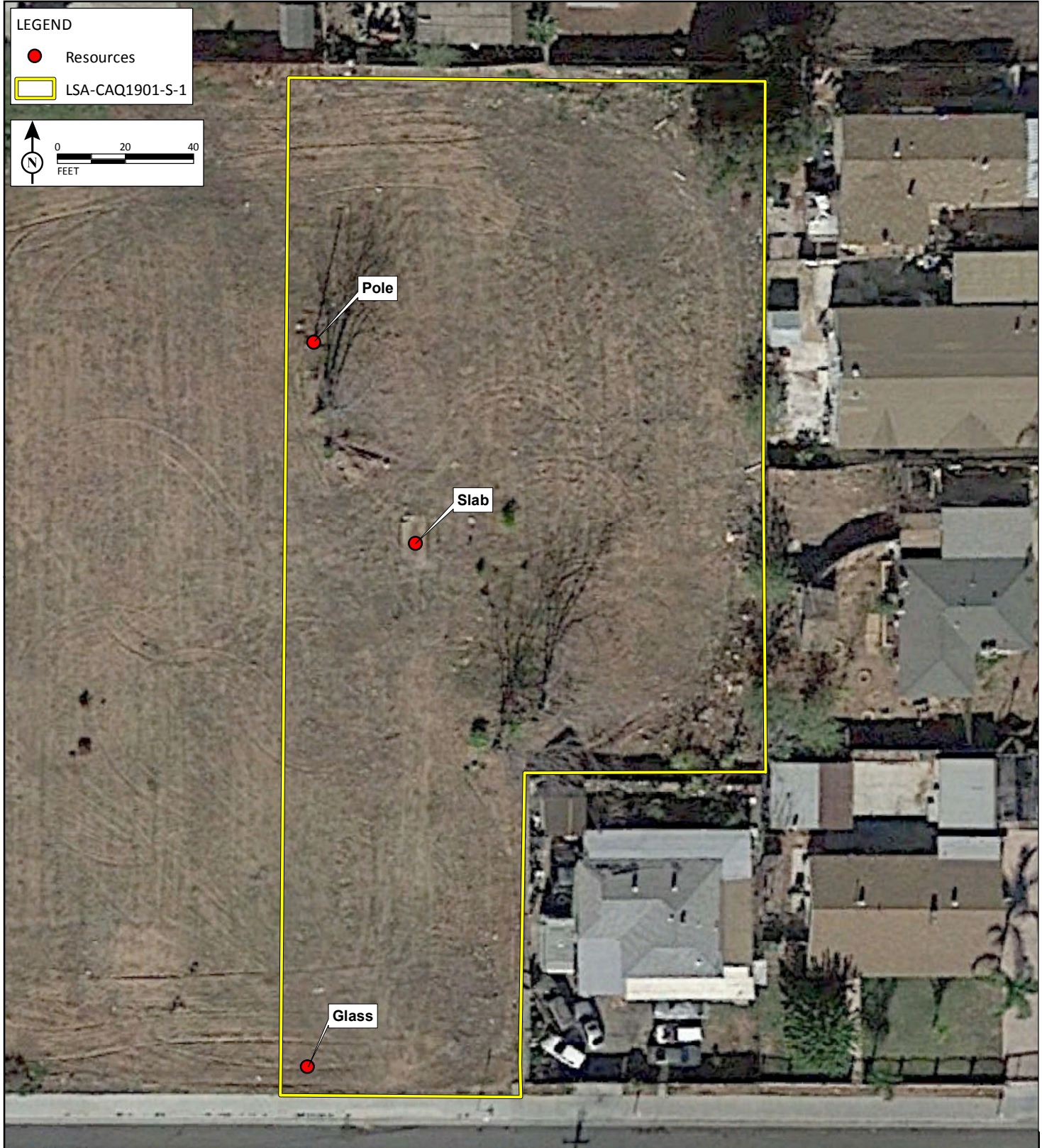
State of California - Resource Agency
DEPARTMENT OF PARKS AND RECREATION
LOCATION MAP

Primary # 33-028824
HRI # _____
Trinomial CA-RIV-012934



Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

State of California - The Resources Agency DEPARTMENT OF PARKS AND RECREATION SITE MAP	Primary # 33-028824 HRI # _____ Trinomial CA-RIV-012934
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Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Appendix D:

**Noise and Vibration Impact Analysis Memorandum for the
Commercial Retail Project at Perris Boulevard and Dracaea Avenue
in the City of Moreno Valley, California**

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

MEMORANDUM

DATE: September 11, 2020

TO: Dave Runberg, Cadence Acquisition LLC
Joey Ly, TAIT & Associates

FROM: Jason Lui, Associate/Senior Noise Specialist

SUBJECT: Noise and Vibration Impact Analysis Memorandum for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California

INTRODUCTION

This memorandum has been prepared to evaluate potential noise and vibration impacts associated with the proposed Commercial Retail Project at Perris Boulevard and Dracaea Avenue in Moreno Valley, California (project). This report provides a project-specific noise and vibration impact analysis by examining the impacts of the proposed project on nearby sensitive uses.

PROJECT DESCRIPTION

The project includes development of a 3,000-square-foot restaurant with drive through lane and a 4,088-square-foot convenience store with fuel pumps. The fueling station will include 12 fueling positions (6 multi-product dispensers [MPDs]). The convenience store and fuel pumps would operate 24 hours. Development of the project is proposed on a 2.3-acre site at the northeast corner of Perris Boulevard and Dracaea Avenue and encompasses Assessor's Parcel Numbers 479-120-042, 027, 029, and 043.

Access to the project site would be provided via two driveways: one on Perris Boulevard and the other on Dracaea Avenue. The project will also have an access from Atwood Avenue via Sunset Lane. Figure 1 illustrates the project's location, and Figure 2 illustrates the conceptual site plan for the project (all figures are provided in Attachment A of this document).

CHARACTERISTICS OF SOUND

Sound is increasing to such disagreeable levels in the environment that it can threaten quality of life. Noise is usually defined as unwanted sound. Noise consists of any sound that may produce physiological or psychological damage and/or interfere with communication, work, rest, recreation, and sleep.

To the human ear, sound has two significant characteristics: pitch and loudness. Pitch is generally an annoyance, while loudness can affect the ability to hear. Pitch is the number of complete vibrations, or cycles per second, of a wave resulting in the tone's range from high to low. Loudness is the strength of a sound that describes a noisy or quiet environment and is measured by the amplitude of the sound wave. Loudness is determined by the intensity of the sound waves combined with the

reception characteristics of the human ear. Sound intensity is the average rate of sound energy transmitted through a unit area perpendicular to the direction in which the sound waves are traveling. This characteristic of sound can be precisely measured with instruments. The analysis of a project defines the noise environment of the project area in terms of sound intensity and its effect on adjacent sensitive land uses.

Measurement of Sound

Sound intensity is measured through the A-weighted scale to correct for the relative frequency response of the human ear. That is, an A-weighted noise level de-emphasizes low and very high frequencies of sound similar to the human ear's de-emphasis of these frequencies. Unlike the linear scale (e.g., inches or pounds), decibels are measured on a logarithmic scale representing points on a sharply rising curve.

For example, 10 decibels (dB) is 10 times more intense than 0 dB, 20 dB is 100 times more intense than 0 dB, and 30 dB is 1,000 times more intense than 0 dB. Thirty decibels (30 dB) represents a 1,000 times as much acoustic energy as 0 dB. The decibel scale increases on a logarithmic scale, representing the sound pressure energy. A sound as soft as human breathing is about 10 times greater than 0 dB. The decibel system of measuring sound gives a rough connection between the physical intensity of sound and its perceived loudness to the human ear. A 10 dB increase in sound level is perceived by the human ear as only a doubling of the loudness of the sound. Ambient sounds generally range from 30 dB (very quiet) to 100 dB (very loud).

Sound levels are generated from a source, and their decibel level decreases as the distance from that source increases. Sound dissipates exponentially with increasing distance from the noise source. For a single point source, sound levels decrease approximately 6 dB for each doubling of distance from the source. This drop-off rate is appropriate for noise generated by stationary equipment. If noise is produced by a line source (e.g., highway traffic or railroad operations) the sound decreases 3 dB for each doubling of distance in a hard site environment. Line source (noise in a relatively flat environment with absorptive vegetation) decreases 4.5 dB for each doubling of distance.

There are many ways to rate noise for various time periods, but an appropriate rating of ambient noise affecting humans also accounts for the annoying effects of sound. The equivalent continuous sound level (L_{eq}) is the total sound energy of time-weighted average noise over a sample period. However, the predominant rating scales for human communities in the State of California are the L_{eq} and Community Noise Equivalent Level (CNEL) or the day-night average noise level (L_{dn}) based on A-weighted decibels (dBA). CNEL is the time-varying noise over a 24-hour period, with a 5 dBA weighting factor applied to the hourly L_{eq} for noises occurring from 7:00 p.m. to 10:00 p.m. (defined as relaxation hours), and a 10 dBA weighting factor applied to noises occurring from 10:00 p.m. to 7:00 a.m. (defined as sleeping hours). L_{dn} is similar to the CNEL scale but without the adjustment for events occurring during the evening hours. CNEL and L_{dn} are within 1 dBA of each other and are normally interchangeable. The City uses the CNEL noise scale for long-term noise impact assessment.

Other noise rating scales of importance when assessing the annoyance factor include the maximum instantaneous noise level (L_{max}), which is the highest sound level that occurs during a stated time

period. The noise environments discussed in this analysis for short-term noise impacts are specified in terms of maximum levels denoted by L_{max} , which reflects peak operating conditions and addresses the annoying aspects of intermittent noise. It is often used together with another noise scale, or noise standards in terms of percentile noise levels, in noise ordinances for enforcement purposes. For example, the L_{10} noise level represents the noise level exceeded 10 percent of the time during a stated period. The L_{50} noise level represents the median noise level. Half the time the noise level exceeds this level, and half the time it is less than this level. The L_{90} noise level represents the noise level exceeded 90 percent of the time and is considered the background noise level during a monitoring period. For a relatively constant noise source, the L_{eq} and L_{50} are approximately the same.

Noise impacts can be described in three categories. The first category includes audible impacts that refer to increases in noise levels noticeable to humans. Audible increases in noise levels generally refer to a change of 3 dB or greater because this level has been found to be barely perceptible in exterior environments. The second category, potentially audible, refers to a change in the noise level between 1 dB and 3 dB. This range of noise levels has been found to be noticeable only in laboratory environments. The last category includes changes in noise levels of less than 1 dB, which are inaudible to the human ear. Only audible changes in existing ambient or background noise levels are considered potentially significant.

Physiological Effects of Noise

Physical damage to human hearing begins at prolonged exposure to noise levels higher than 85 dBA. Exposure to high noise levels affects the entire system, with prolonged noise exposure in excess of 75 dBA increasing body tensions, thereby affecting blood pressure and functions of the heart and the nervous system. In comparison, extended periods of noise exposure above 90 dBA would result in permanent cell damage. When the noise level reaches 120 dBA, a tickling sensation occurs in the human ear, even with short-term exposure. This level of noise is called the threshold of feeling. As the sound reaches 140 dBA, the tickling sensation is replaced by the feeling of pain in the ear (the threshold of pain). A sound level of 160–165 dBA will result in dizziness or loss of equilibrium. The ambient or background noise problem is widespread and generally more concentrated in urban areas than in outlying, less developed area. Table A lists definitions of acoustical terms, and Table B shows common sound levels and their sources.

FUNDAMENTALS OF VIBRATION

Vibration refers to ground-borne noise and perceptible motion. Ground-borne vibration is almost exclusively a concern inside buildings and is rarely perceived as a problem outdoors, where the motion may be discernible, but without the effects associated with the shaking of a building there is less adverse reaction. Vibration energy propagates from a source through intervening soil and rock layers to the foundations of nearby buildings. The vibration then propagates from the foundation throughout the remainder of the structure. Building vibration may be perceived by occupants as the motion of building surfaces, the rattling of items sitting on shelves or hanging on walls, or a low-frequency rumbling noise. The rumbling noise is caused by the vibration of walls, floors, and ceilings that radiate sound waves. Annoyance from vibration often occurs when the vibration exceeds the threshold of perception by 10 dB or less. This is an order of magnitude below the damage threshold for normal buildings.

Table A: Definitions of Acoustical Terms

Term	Definitions
Decibel, dB	A unit of measurement that denotes the ratio between two quantities that are proportional to power; the number of decibels is 10 times the logarithm (to the base 10) of this ratio.
Frequency, Hz	Of a function periodic in time, the number of times that the quantity repeats itself in 1 second (i.e., number of cycles per second).
A-Weighted Sound Level, dBA	The sound level obtained by use of A-weighting. The A-weighting filter deemphasizes the very low- and very high-frequency components of the sound in a manner similar to the frequency response of the human ear and correlates well with subjective reactions to noise. (All sound levels in this report are A-weighted, unless reported otherwise.)
L_{01} , L_{10} , L_{50} , L_{90}	The fast A-weighted noise levels that are equaled or exceeded by a fluctuating sound level 1%, 10%, 50%, and 90% of a stated time period.
Equivalent Continuous Noise Level, L_{eq}	The level of a steady sound that, in a stated time period and at a stated location, has the same A-weighted sound energy as the time-varying sound.
Community Noise Equivalent Level, CNEL	The 24-hour A-weighted average sound level from midnight to midnight, obtained after the addition of 5 dBA to sound levels occurring in the evening from 7:00 PM to 10:00 PM and after the addition of 10 dBA to sound levels occurring in the night between 10:00 PM and 7:00 AM.
Day/Night Noise Level, L_{dn}	The 24-hour A-weighted average sound level from midnight to midnight, obtained after the addition of 10 dBA to sound levels occurring in the night between 10:00 PM and 7:00 AM.
L_{max} , L_{min}	The maximum and minimum A-weighted sound levels measured on a sound level meter, during a designated time interval, using fast time averaging.
Ambient Noise Level	The all-encompassing noise associated with a given environment at a specified time; usually a composite of sound from many sources at many directions, near and far; no particular sound is dominant.
Intrusive	The noise that intrudes over and above the existing ambient noise at a given location. The relative intrusiveness of a sound depends upon its amplitude, duration, frequency, and time of occurrence and tonal or informational content, as well as the prevailing ambient noise level.

Source: *Handbook of Acoustical Measurements and Noise Control* (Harris 1991).

Table B: Common Sound Levels and Their Noise Sources

Noise Source	A-Weighted Sound Level in Decibels	Noise Environments	Subjective Evaluations
Near Jet Engine	140	Deafening	128 times as loud
Civil Defense Siren	130	Threshold of Pain	64 times as loud
Hard Rock Band	120	Threshold of Feeling	32 times as loud
Accelerating Motorcycle at a Few Feet Away	110	Very Loud	16 times as loud
Pile Driver; Noisy Urban Street/Heavy City Traffic	100	Very Loud	8 times as loud
Ambulance Siren; Food Blender	95	Very Loud	—
Garbage Disposal	90	Very Loud	4 times as loud
Freight Cars; Living Room Music	85	Loud	—
Pneumatic Drill; Vacuum Cleaner	80	Loud	2 times as loud
Busy Restaurant	75	Moderately Loud	—
Near Freeway Auto Traffic	70	Moderately Loud	—
Average Office	60	Quiet	One-half as loud
Suburban Street	55	Quiet	—
Light Traffic; Soft Radio Music in Apartment	50	Quiet	One-quarter as loud
Large Transformer	45	Quiet	—
Average Residence without Stereo Playing	40	Faint	One-eighth as loud
Soft Whisper	30	Faint	—
Rustling Leaves	20	Very Faint	—
Human Breathing	10	Very Faint	Threshold of Hearing
—	0	Very Faint	—

Source: Compiled by LSA Associates, Inc. (2015).

Typical sources of ground-borne vibration are construction activities (e.g., blasting, pile-driving, and operating heavy-duty earthmoving equipment), steel-wheeled trains, and occasional traffic on rough roads. Problems with both ground-borne vibration and noise from these sources are usually localized to areas within approximately 100 feet (ft) from the vibration source, although there are examples of ground-borne vibration causing interference out to distances greater than 200 ft (Federal Transit Authority's [FTA] 2018 *Transit Noise and Vibration Impact Assessment Manual*). When roadways are smooth, vibration from traffic, even heavy trucks, is rarely perceptible. It is assumed for most projects that the roadway surface will be smooth enough that ground-borne vibration from street traffic will not exceed the impact criteria; however, both construction of the project and the freight train operations could result in ground-borne vibration that may be perceptible and annoying.

Ground-borne noise is not likely to be a problem because noise arriving via the normal airborne path will usually be greater than ground-borne noise. Ground-borne vibration has the potential to disturb people and damage buildings. Although it is very rare for train-induced ground-borne vibration to cause even cosmetic building damage, it is not uncommon for construction processes such as

blasting and pile-driving to cause vibration of sufficient amplitudes to damage nearby buildings (FTA 2018). Ground-borne vibration is usually measured in terms of vibration velocity, either the root-mean-square (RMS) velocity or peak particle velocity (PPV). The RMS is best for characterizing human response to building vibration, and PPV is used to characterize potential for damage. Decibel notation acts to compress the range of numbers required to describe vibration. Vibration velocity level in decibels is defined as:

$$L_v = 20 \log_{10} [V/V_{ref}]$$

where: “L_v” is the vibration velocity in decibels (VdB),
 “V” is the RMS velocity amplitude, and
 “V_{ref}” is the reference velocity amplitude, or
 1 x 10⁻⁶ inches/second (in/sec) used in the United States.

Table C illustrates human response to various vibration levels, as described in the *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018).

Table C: Human Response to Different Levels of Ground-Borne Noise and Vibration

Vibration Velocity Level	Noise Level		Human Response
	Low Frequency ¹	Mid Frequency ²	
65 VdB	25 dBA	40 dBA	Approximate threshold of perception for many humans. Low-frequency sound is usually inaudible; mid-frequency sound is excessive for quiet sleeping areas.
75 VdB	35 dBA	50 dBA	Approximate dividing line between barely perceptible and distinctly perceptible. Many people find transit vibration at this level unacceptable. Low-frequency noise is acceptable for sleeping areas; mid-frequency noise is annoying in most quiet occupied areas.
85 VdB	45 dBA	60 dBA	Vibration is acceptable only if there are an infrequent number of events per day. Low-frequency noise is unacceptable for sleeping areas; mid-frequency noise is unacceptable even for infrequent events with institutional land uses, such as schools and churches.

Source: *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018).

¹ Approximate noise level when vibration spectrum peak is near 30 Hz.

² Approximate noise level when vibration spectrum peak is near 60 Hz.

dBA = A-weighted decibels

FTA = Federal Transit Administration

Hz = Hertz

VdB = vibration velocity decibels

Factors that influence ground-borne vibration and noise include the following:

- **Vibration Source:** Vehicle suspension, wheel types and condition, railroad track/roadway surface, railroad track support system, speed, transit structure, and depth of vibration source.
- **Vibration Path:** Soil type, rock layers, soil layering, depth to water table, and frost depth.
- **Vibration Receiver:** Foundation type, building construction, and acoustical absorption.

Among the factors listed above, there are significant differences in the vibration characteristics when the source is underground compared to at the ground surface. In addition, soil conditions are known to have a strong influence on the levels of ground-borne vibration. Among the most important factors are the stiffness and internal damping of the soil and the depth to bedrock.

Experience with ground-borne vibration indicates: (1) vibration propagation is more efficient in stiff, clay soils than in loose, sandy soils; and (2) shallow rock seems to concentrate the vibration energy close to the surface and can result in ground-borne vibration problems at large distances from a railroad track. Factors such as layering of the soil and the depth to the water table can have significant effects on the propagation of ground-borne vibration. Soft, loose, sandy soils tend to attenuate more vibration energy than hard, rocky materials. Vibration propagation through groundwater is more efficient than through sandy soils.

REGULATORY SETTING

Federal Regulations

Federal Transit Administration

Vibration standards included in the FTA's *Transit Noise and Vibration Impact Assessment Manual* (2018) are used in this analysis for ground-borne vibration impacts on human annoyance, as shown in Table D. Table D provides the criteria for assessing the potential for interference or annoyance from vibration levels in a building.

Table D: Interpretation of Vibration Criteria for Detailed Analysis

Land Use	Max L _v (VdB) ¹	Description of Use
Workshop	90	Distinctly feelable vibration. Appropriate to workshops and non-sensitive areas.
Office	84	Feelable vibration. Appropriate to offices and non-sensitive areas.
Residential Day	78	Feelable vibration. Appropriate for computer equipment and low-power optical microscopes (up to 20X).
Residential Night and Operating Rooms	72	Vibration not feelable, but ground-borne noise may be audible inside quiet rooms. Suitable for medium-power microscopes (100X) and other equipment of low sensitivity.

Source: *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018).

¹ As measured in 1/3-Octave bands of frequency over the frequency range 8 to 80 Hertz.

FTA = Federal Transit Administration

L_v = velocity in decibels

VdB = vibration velocity decibels

The criteria for environmental impact from ground-borne vibration and noise are based on the maximum levels for a single event. Table E lists the potential vibration building damage criteria associated with construction activities, as suggested in the *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018). FTA guidelines show that a vibration level of up to 102 VdB (equivalent to 0.5 in/sec in PPV) (FTA 2018) is considered safe for buildings consisting of reinforced concrete, steel, or timber (no plaster) and would not result in any construction vibration damage. For a non-engineered timber and masonry building, the construction building vibration damage criterion is 94 VdB (0.2 in/sec in PPV).

Table E: Construction Vibration Damage Criteria

Building Category	PPV (in/sec)	Approximate L _v (VdB) ¹
Reinforced concrete, steel, or timber (no plaster)	0.50	102
Engineered concrete and masonry (no plaster)	0.30	98
Non-engineered timber and masonry buildings	0.20	94
Buildings extremely susceptible to vibration damage	0.12	90

Source: *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018).

¹ RMS vibration velocity in decibels (VdB) re 1 μin/sec.

μin/sec = microinches per second

FTA = Federal Transit Administration

in/sec = inch/inches per second

L_v = velocity in decibels

PPV = peak particle velocity

RMS = root-mean-square

VdB = vibration velocity decibels

Local Regulations

City of Moreno Valley

General Plan Safety Element. The City of Moreno Valley addresses noise in the City’s Safety Element of the General Plan. The goals, objectives, and policies in the City’s General Plan are designed to provide noise compatible land use relationships by establishing noise standards utilized for design and siting purposes and minimize noise impacts from significant noise generators. The following policies are applicable to the proposed project: **Policy 6.4.1:** Site, landscape and architectural design features shall be encouraged to mitigate noise impacts for new developments, with a preference for noise barriers that avoid freeway sound barrier walls.

- **Policy 6.4.2:** Construction activities shall be operated in a manner that limits noise impacts on surrounding uses.
- **Policy 6.5.1:** New commercial and industrial activities (including the placement of mechanical equipment) shall be evaluated and designed to mitigate noise impacts on adjacent uses.

Municipal Code. Section 8.21.050(O) states that grading and equipment operations shall only be completed between the hours of 7:00 a.m. to 7:00 p.m. Monday through Friday, excluding holidays and from 8:00 a.m. to 4:00 p.m. on Saturday.

Section 9.09.080(C)(1) states that a drive-in, drive-through, fast food or take-out restaurant shall not open prior to 6:00 a.m., nor remain open after 10:00 p.m when located adjacent to or separated by an alley from any residentially zoned property. In addition, Section 9.09.080(C)(6) states that any drive-up or drive-through speaker system shall not be detectable above daytime ambient noise levels beyond the property boundaries. The system shall be designed to compensate for ambient noise levels in the immediate area, and shall not be located within one hundred (100) ft of any residential district or any property used for residential uses. It should be noted that Section 9.09.020 of the City’s Municipal Code allows the decision-making body discretion to allow alternative drive-through operations that “adequately protect[s] the public health, safety and welfare.”

Section 11.80.030(C) of the City’s Municipal Code establishes limits on non-impulsive noise where no person shall maintain, create, operate, or cause noise on private property to not exceed the noise standards shown in Table F for the source land use category when measured at a distance of

200 ft from the property line of the source of the noise, if the noise occurs on privately owned property, or from the source of the sound, if the sound occurs on public right-of-way, public space or other publicly owned property. Noise levels that exceed the noise standards in Table G shall be deemed to be a noise disturbance.

Table F: Maximum Sound Levels for Source Land Uses

Residential		Commercial	
Daytime ¹	Nighttime ²	Daytime ¹	Nighttime ²
60 dBA ³	55 dBA ³	65 dBA ³	60 dBA ³

Source: Section 11.80.030(C) of the City of Moreno Valley *Municipal Code*.

¹ Daytime means 8:00 a.m. to 10:00 p.m.

² Nighttime means 10:01 p.m. to 7:59 a.m.

³ Noise levels that are non-impulsive are interrupted to be in equivalent continuous sound level (L_{eq}). Noise level standard when measured at a distance of 200 ft from the property line of the source of the noise.

dBA = A-weighted decibels

Table G: Long-Term Ambient Noise Monitoring Results

Monitoring No.	Location	Start Date	Start Time	Duration (hrs)	Daytime Noise Level (dBA L _{eq})	Evening Noise Level (dBA L _{eq})	Nighttime Noise Level (dBA L _{eq})	dBA CNEL
LT-1	Northern project boundary at 13182 Sunset Lane, Moreno Valley, California.	7/22/19	10:00 am	48	54-61	55-59	50-61	63
LT-2	Eastern project boundary at 13182 Sunset Lane, Moreno Valley, California.	10/9/18	10:00 am	48	49-59	55-58	50-59	62

Source: Compiled by LSA Associates, Inc. (2020).

CNEL = Community Noise Equivalent Level

dBA = A-weighted decibels

hrs = hours

L_{eq} = equivalent continuous sound level

Section 11.80.030(D)(7) of the City’s Municipal Code limits construction and demolition activities to between the hours of 7:00 a.m. and 8:00 p.m. every day. No person shall operate or allow the operation of any tools or equipment used in construction, drilling, repair, or alteration or demolition work outside of these hours to prevent noise disturbances.

Section 9.10.170 of the Municipal Code prohibits vibration that can be felt at or beyond the property line. However, construction activity is exempt from Section 9.10.170 pursuant to Section 9.10.030, which states temporary construction, maintenance, or demolition activities between the hours of 7:00 a.m. and 7:00 p.m. are exempt from the provisions of Chapter 9.10 (Performance Standards) of the City Municipal Code.

EXISTING SETTING

Overview of the Existing Noise Environment

The primary existing noise sources in the project area are transportation facilities. Traffic on Perris Boulevard and Dracaea Avenue is a steady source of ambient noise.

Existing Sensitive Land Uses in the Project Vicinity

The project site is surrounded by single-family residences, vacant land, and commercial uses. Single-family residences are located north, east, and west of the project site. Commercial uses and vacant land are located west and south, respectively. The closest residence is located within 50 ft of the boundary of the proposed project site.

Ambient Noise Measurements

Long-Term Noise Level Measurement

Two long-term (48-hour) noise level measurements were conducted from July 22, 2019, to July 24, 2019, using Larson Davis Spark Model 706RC Dosimeters. Table G shows the equivalent continuous sound level (L_{eq}) range during daytime, evening, and nighttime hours as well as the calculated CNEL levels. The hourly L_{eq} results from the long-term measurements are shown in Attachment B. As shown in Table G, the calculated CNEL levels range from 62 to 63 dBA CNEL. Figure 3 in Attachment A shows the long-term noise monitoring locations.

Existing Aircraft Noise

Airport-related noise levels are primarily associated with aircraft engine noise made while aircraft are taking off, landing, or running their engines while still on the ground. The closest airport to the proposed project site is March Air Force Base (RIV) located approximately 2.8 miles southwest of the project site. Based on the Riverside County Airport Land Use Compatibility Plan (Riverside County, November 2014) the project is located outside of the 60 dBA CNEL noise contour of the airport. In addition, there are no private airstrips or heliports within 2 miles of the project site.

Existing Traffic Noise

The Federal Highway Administration (FHWA) Highway Traffic Noise Prediction Model (FHWA-RD-77-108) was used to evaluate traffic-related noise conditions along roadway segments in the project vicinity. This model requires various parameters, including traffic volumes, vehicle mix, vehicle speed, and roadway geometry to compute typical equivalent noise levels during daytime, evening, and nighttime hours. The resultant noise levels are weighted and summed over 24-hour periods to determine the CNEL values. The existing average daily traffic (ADT) volumes were calculated based on existing ADT volumes obtained from the *Transportation Impact Analysis for the Perris/Dracaea Commercial Project* (LSA 2020b). The standard vehicle mix for Southern California roadways was used for traffic on these roadway segments. Table H provides the existing traffic noise levels in the project vicinity. These traffic noise levels are representative of a worst-case scenario that assumes a flat terrain and no shielding between the traffic and the noise contours. Attachment C provides the specific assumptions used in developing these noise levels and model printouts.

Table H: Existing Without Project Traffic Noise Levels

Roadway Segment	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane
Perris Boulevard between Eucalyptus Avenue and Atwood Avenue	31,829	71	136	286	68.5
Perris Boulevard between Atwood Avenue and Dracaea Avenue	30,997	70	134	281	68.4
Perris Boulevard between Dracaea Avenue and Cottonwood Avenue	29,577	66	129	272	68.4
Dracaea Avenue between Perris Boulevard and Project Driveway 2	4,912	< 50	< 50	67	60.4
Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5

Source: Compiled by LSA Associates, Inc. (2020).

Notes: Traffic noise within 50 ft of the roadway centerline should be evaluated with site-specific information.

ADT = average daily traffic

dBA = A-weighted decibels

CNEL = Community Noise Equivalent Level

ft = foot/feet

IMPACTS

Short-Term Construction Noise Impacts

Two types of short-term noise impacts would occur during construction on the project site. First, construction crew commutes and the transport of construction equipment to the project site would incrementally increase noise levels on access roads leading to the site. There would be a relatively high single-event noise exposure potential causing intermittent noise from large trucks passing at 50 ft that would generate up to a maximum of 84 dBA. The grading phase would generate the highest daily construction vehicle trips based on the California Emission Estimator Model (Version 2016.3.2) output, shown in Attachment C of the *Air Quality and Greenhouse Gas Analysis for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue* (LSA 2020a) prepared for the proposed project. Project construction vehicle trips associated with construction crew commute trips and transport of construction equipment are estimated to reach up to 26 vehicles per hour or 201 vehicles per day (roundtrip) during the grading construction phase. Perris Boulevard and Dracaea Avenue would be used to access the project site, which has estimated existing hourly/daily traffic volumes of 2,958/29,577 and 491/4,912, respectively. Construction-related traffic would increase hourly traffic noise levels by up to 0.2 dBA and would not increase daily traffic noise levels along Perris Boulevard and Dracaea Avenue. A noise level increase of less than 3 dBA would not be perceptible to the human ear in an outdoor environment. Therefore, no noise impacts from short-term construction-related traffic associated with worker commutes and equipment transport to the project site would occur.

The second type of short-term noise impact is related to noise generated during construction activities on the project site. Construction is completed in discrete steps, each of which has its own mix of equipment, and consequently, its own noise characteristics. These various sequential phases would change the character of noise generated on the project site. Therefore, the noise levels vary as construction progresses. Despite the variety in the type and size of construction equipment,

similarities in the dominant noise sources and patterns of operation allow construction-related noise ranges to be categorized by work phase. Table I lists the typical construction equipment noise levels (L_{max}) recommended for noise impact assessments, based on a distance of 50 ft between the equipment and a noise receptor.

Table I: Typical Construction Equipment Noise Levels

Equipment Description	Acoustical Usage Factor (%)	Maximum Noise Level (L_{max}) at 50 ft ¹
Concrete/Industrial Saw	20	90
Dozers	40	85
Dump Trucks	40	84
Excavators	40	85
Flat Bed Trucks	40	84
Graders	40	85
Front-End Loaders	40	80
Man Lifts	20	85
Pickup Trucks	40	55
Scrapers	40	85

Source: FHWA Highway Construction Noise Handbook, Table 9.1 (FHWA 2006).

Note: Noise levels reported in this table are rounded to the nearest whole number.

¹. Maximum noise levels were developed based on Spec 721.560 from the Central Artery/Tunnel program to be consistent with the City of Boston's Noise Code for the "Big Dig" project.

ft = foot/feet

L_{max} = maximum instantaneous sound level

Spec = specification

Typical noise levels range up to 88 dBA L_{max} at 50 ft during the noisiest construction phases. The site preparation phase, which includes excavation and grading of the site, tends to generate the highest noise levels because the noisiest construction equipment is earthmoving equipment. Earthmoving equipment includes excavating machinery such as backfillers, bulldozers, draglines, and front loaders. Earthmoving and compacting equipment includes compactors, scrapers, and graders.

Site preparation is expected to require the use of bulldozers, graders, and water trucks/pickup trucks. Noise associated with the use of construction equipment is estimated to be between 55 and 85 dBA L_{max} at a distance of 50 ft from the active construction area for the site preparation phase. As shown in Table I, the maximum noise level generated by each grader and bulldozer is assumed to be approximately 85 dBA L_{max} at 50 ft. The maximum noise level generated by water trucks/pickup trucks is approximately 55 dBA L_{max} at 50 ft from these vehicles. Each doubling of the sound sources with equal strength increases the noise level by 3 dBA. Assuming that each piece of construction equipment operates at some distance from the other equipment, the worst-case combined noise level during this phase of construction would be 88 dBA L_{max} at a distance of 50 ft from the active construction area. Based on a usage factor of 40 percent, the worst-case combined noise level during this phase of construction would be 84 dBA L_{eq} at a distance of 50 ft from the active construction area.

The closest residential property lines are located within 50 ft immediately adjacent to the northern and eastern project construction boundary and would be subject to short-term construction noise levels of 88 dBA L_{max} (84 dBA L_{eq}) or higher when construction occurs at the project construction boundary. Although noise generated by project construction activities would result in a temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project, the compliance with the construction hours specified in the City's Municipal Code in Sections 8.21.050(O) and 11.80.030(D)(7), the use of construction equipment with noise mufflers that are properly operating and maintained, placing construction staging areas away from off-site sensitive uses, and placing all stationary construction equipment so that the emitted noise is directed away from sensitive receptors would minimize construction noise impacts. Therefore, no construction noise impacts would occur with the implementation of the minimization measures described above.

Short-Term Construction Vibration Impacts

This construction vibration impact analysis discusses the level of human annoyance using vibration levels in VdB and will assess the potential for building damage using vibration levels in PPV (in/sec). As shown in Table E, the FTA guidelines indicate that a vibration level up to 102 VdB (equivalent to 0.5 PPV [in/sec]) is considered safe for buildings consisting of reinforced concrete, steel, or timber (no plaster), and would not result in any construction vibration damage (FTA 2018). For a non-engineered timber and masonry building, the construction vibration damage criterion is 94 VdB (0.2 PPV [in/sec]). For a fragile building, the construction vibration damage criterion is 90 VdB (0.12 PPV [in/sec]).

Table J shows the PPV and VdB values at a distance of 25 ft from the construction vibration source. As shown in Table J, large bulldozers and other heavy-tracked construction equipment (except for pile drivers and vibratory rollers) generate approximately 87 VdB of ground-borne vibration when measured at a distance of 25 ft, based on the *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018). Project construction is expected to use a small bulldozer, large bulldozer, and a loaded truck, which would generate 58 VdB (0.003 PPV [in/sec]), 87 VdB (0.089 PPV [in/sec]), and 86 VdB (0.076 PPV [in/sec]) at 25 ft, respectively. The greatest levels of vibration are anticipated to occur during the site preparation and grading phase. All other phases are expected to result in lower vibration levels. The distance to the nearest buildings for vibration impact analysis is measured between the nearest off-site buildings and the project boundary (assuming the construction equipment would be used at or near the project boundary) because vibration impacts occur normally within the buildings. An exception to this would be the location of loaded trucks because they would be limited to a certain areas on the project site and would not operate at the project construction boundary.

The formula for vibration transmission is provided below.

$$L_{\text{vdB}}(D) = L_{\text{vdB}}(25 \text{ feet}) - 30 \text{ Log}(D/25)$$

$$PPV_{\text{equip}} = PPV_{\text{ref}} \times (25/D)^{1.5}$$

Table J: Vibration Source Amplitudes for Construction Equipment

Equipment	Reference PPV/L _v at 25 ft	
	PPV (in/sec)	L _v (VdB) ¹
Hoe Ram	0.089	87
Large Bulldozer	0.089	87
Caisson Drilling	0.089	87
Loaded Trucks	0.076	86
Jackhammer	0.035	79
Small Bulldozer	0.003	58

Source: *Transit Noise and Vibration Impact Assessment Manual* (FTA 2018).

¹ RMS VdB re 1 µin/sec.

µin/sec = microinches per second

ft = foot/feet

FTA = Federal Transit Administration

in/sec = inches per second

L_v = velocity in decibels

PPV = peak particle velocity

RMS = root-mean-square

VdB = vibration velocity in decibels

Table K lists the projected vibration levels from various construction equipment expected to be used on the project site to the nearest buildings in the project vicinity. As shown in Table K, residential structures located east of the project would experience the highest construction vibration levels of up to 94 VdB (0.191 PPV [in/sec]). This vibration level would have the potential to result in community annoyance and because vibration levels would exceed the FTA's community annoyance threshold of 78 VdB for residential uses. However, this vibration level would not have the potential to damage residential structures because vibration levels would not exceed the FTA's damage threshold of 94 VdB (0.2 PPV [in/sec]) for residential structures constructed of non-engineered timber and masonry. Other residential and commercial structures in the project area shown in Table K would experience lower vibration levels due to either the use of a small bulldozer or greater distance from the project construction boundary.

Table K: Summary of Construction Vibration Levels

Land Use	Direction	Equipment/ Activity	Reference Vibration Level (VdB) at 25 ft	Reference Vibration Level (PPV) at 25 ft	Distance ¹ (ft)	Maximum Vibration Level (VdB)	Maximum Vibration Level (PPV)
Residential	North	Small Bulldozer	58	0.003	8	73	0.017
		Loaded Trucks	86	0.076	15	93	0.164
Residential	East	Large Bulldozer	87	0.089	15	94	0.191
		Loaded Trucks	86	0.076	15	93	0.164
Residential	Southeast	Small Bulldozer	58	0.003	8	73	0.017
		Loaded Trucks	86	0.076	15	93	0.164
Residential/ Commercial	West	Large Bulldozer	87	0.089	130	66	0.008
		Loaded Trucks	86	0.076	130	65	0.006

Source: Compiled by LSA Associates, Inc. (2020).

¹ Distance reflects the nearest structure to the nearest project construction boundary.

ft = foot/feet

PPV = peak particle velocity

VdB = vibration velocity decibels

As the project is expected to use both small and large bulldozers on the project site, the project will be conditioned to operate only small bulldozers within 15 ft of the residential structures immediately north and southeast of the project to ensure vibration levels would not exceed the FTA's damage threshold of 94 VdB (0.2 PPV [in/sec]) for structures constructed of non-engineered timber and masonry. This condition would be incorporated onto the project grading plans, and directional signage would be placed on the construction site to direct equipment operators. Established City procedures for plan check, permit issuance, and construction inspection would ensure project implementation consistent with the conditions of approval. The resulting vibration levels, and therefore community annoyance, along the adjacent property lines to the north and southeast would be temporary and sporadic, and they would cease upon completion of grading activities. In addition, compliance with Sections 8.21.050(O) and 9.10.030 of the City's Municipal Code to ensure grading activities are restricted to within the permissible hours of operation (between the hours of 7:00 a.m. and 7:00 p.m. Monday through Friday and between the hours of 8:00 a.m. and 4:00 p.m. on Saturday) would be required. Therefore, no vibration impacts from construction activities would occur and no vibration reduction measures are required.

Long-Term Aircraft Noise Impacts

The proposed project is approximately 2.8 miles from the nearest airport. Based on the Riverside County Airport Land Use Compatibility Plan (Riverside County, November 2014) the project is located outside of the 60 dBA CNEL noise contour of the airport. In addition, there are no private airstrips or heliports within 2 miles of the project site. The project would not expose people residing or working in the project area to excessive noise levels from aircraft noise. Therefore, no noise impacts would occur. No noise reduction measures are required.

Long Term Traffic Noise Impacts

The FHWA Highway Traffic Noise Prediction Model (FHWA-RD-77 108) was used to evaluate traffic-related noise conditions along roadway segments in the project vicinity. This model requires various parameters, including traffic volumes, vehicle mix, vehicle speed, and roadway geometry, to compute typical equivalent noise levels during daytime, evening, and nighttime hours. The resultant noise levels are weighted and summed over 24-hour periods to determine the CNEL values. The existing, 2024, 2024 cumulative, and 2040 (General Plan Buildout) without and with project ADT volumes were obtain from the *Transportation Impact Analysis for the Perris/Dracaea Commercial Project* (LSA 2020b). The standard vehicle mix for Southern California roadways was used for traffic on these roadway segments. Tables L, M, and N provide the traffic noise levels for the existing, 2022, and 2040 (General Plan Buildout) without and with project, respectively. These noise levels represent the worst-case scenario, which assumes that no shielding is provided between traffic and the location where the noise contours are drawn. Attachment C provides the specific assumptions used in developing these noise levels and model printouts.

Tables L, M, and N show that the project-related traffic noise increase would be up to 0.2 dBA along Perris Boulevard, 1.6 dBA along Dracaea Avenue, and 13.2 dBA along Sunset Lane. As shown in Tables L, M, and N, traffic noise on Sunset Lane would increase substantially due to access to the project from Sunset Lane. However, the overall traffic noise increase for the residences along Sunset Lane would be 0.2 dBA when traffic noise on Perris Boulevard is factored in. The detailed noise calculations are provided in Attachment D. Noise level increases less than 3 dBA would not be



Table L: Existing Without and With Project Traffic Noise Levels

Roadway Segment	Without Project Traffic Conditions					With Project Traffic Conditions					
	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane	Increase from Baseline Conditions
Perris Boulevard between Eucalyptus Avenue and Atwood Avenue	31,829	71	136	286	68.5	32,699	72	139	291	68.6	0.1
Perris Boulevard between Atwood Avenue and Dracaea Avenue	30,997	70	134	281	68.4	31,282	70	135	283	68.4	0.0
Perris Boulevard between Dracaea Avenue and Cottonwood Avenue	29,577	66	129	272	68.4	30,621	67	132	279	68.6	0.2
Dracaea Avenue between Perris Boulevard and Project Driveway 2	4,912	< 50	< 50	67	60.4	7,251	< 50	< 50	87	62.0	1.6
Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5	795	< 50	< 50	< 50	49.7	13.2 (0.2) ¹

Source: Compiled by LSA Associates, Inc. (2020).

Note: Traffic noise within 50 ft of the roadway centerline should be evaluated with site-specific information.

¹ The overall traffic noise increase for the residences along Sunset Lane would be 0.2 dBA when traffic noise on Perris Boulevard is factored in. The detailed noise calculations are provided in Attachment D.

ADT = average daily traffic

CNEL = Community Noise Equivalent Level

dBA = A-weighted decibels

ft = foot/feet



Table M: 2022 Without and With Project Traffic Noise Levels

Roadway Segment	Without Project Traffic Conditions					With Project Traffic Conditions					
	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane	Increase from Baseline Conditions
Perris Boulevard between Eucalyptus Avenue and Atwood Avenue	36,447	76	149	313	69.1	37,317	77	151	318	69.2	0.1
Perris Boulevard between Atwood Avenue and Dracaea Avenue	35,559	75	146	308	69.0	35,844	75	147	310	69.0	0.0
Perris Boulevard between Dracaea Avenue and Cottonwood Avenue	34,036	71	141	299	69.0	35,080	72	144	305	69.2	0.2
Dracaea Avenue between Perris Boulevard and Project Driveway 2	5,207	< 50	< 50	70	60.6	7,546	< 50	< 50	89	62.2	1.6
Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5	795	< 50	< 50	< 50	49.7	13.2 (0.2) ¹

Source: Compiled by LSA Associates, Inc. (2020).

Note: Traffic noise within 50 ft of the roadway centerline should be evaluated with site-specific information.

¹ The overall traffic noise increase for the residences along Sunset Lane would be 0.2 dBA when traffic noise on Perris Boulevard is factored in. The detailed noise calculations are provided in Attachment D.

ADT = average daily traffic

CNEL = Community Noise Equivalent Level

dBA = A-weighted decibels

ft = foot/feet



Table N: 2040 (General Plan Buildout) Traffic Noise Levels Without and With Project

Roadway Segment	Without Project Traffic Conditions					With Project Traffic Conditions					
	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane	ADT	Centerline to 70 dBA CNEL (ft)	Centerline to 65 dBA CNEL (ft)	Centerline to 60 dBA CNEL (ft)	CNEL (dBA) 50 ft from Centerline of Outermost Lane	Increase from Baseline Conditions
Perris Boulevard between Eucalyptus Avenue and Atwood Avenue	45,861	86	172	364	70.1	46,731	87	174	369	70.1	0.0
Perris Boulevard between Atwood Avenue and Dracaea Avenue	46,776	87	174	369	70.1	47,061	87	175	370	70.2	0.1
Perris Boulevard between Dracaea Avenue and Cottonwood Avenue	42,958	81	164	348	70.1	44,003	82	167	354	70.2	0.1
Dracaea Avenue between Perris Boulevard and Project Driveway 2	6,735	< 50	< 50	83	61.7	9,074	< 50	< 50	100	63.0	1.3
Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5	795	< 50	< 50	< 50	49.7	13.2(0.2) ¹

Source: Compiled by LSA Associates, Inc. (2020).

Note: Traffic noise within 50 ft of the roadway centerline should be evaluated with site-specific information.

¹ The overall traffic noise increase for the residences along Sunset Lane would be 0.2 dBA when traffic noise on Perris Boulevard is factored in. The detailed noise calculations are provided in Attachment D.

ADT = average daily traffic

CNEL = Community Noise Equivalent Level

dBA = A-weighted decibels

ft = foot/feet

perceptible to the human ear in an outdoor environment. Therefore, no traffic noise impacts from project-related traffic on off-site sensitive receptors would occur. No noise reduction measures are required.

Long-Term Off-Site Stationary Noise Impacts

Truck Deliveries and Truck Unloading Activities

Noise levels generated by delivery trucks would be similar to noise readings from truck loading and unloading activities, which generate a noise level of 65 dBA L_{eq} at 50 ft. Although a typical truck unloading process takes an average of 15–20 minutes, this maximum noise level occurs in a much shorter period of time (less than 5 minutes). Table O shows the noise level generated by truck delivery and unloading activities at 200 ft. As Table O shows, noise levels generated by truck delivery and unloading activities would not exceed the City’s exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} , respectively, at a distance of 200 ft for noise sources generated on commercial land uses. In addition, the closest residences are located approximately 100 ft from the truck deliveries and would be exposed to a noise level of 59 dBA L_{eq} as shown in Table O. A noise level of 59 dBA L_{eq} would not exceed the City’s equivalent exterior daytime and nighttime noise standard of 71 dBA L_{eq} and 66 dBA L_{eq} , respectively, at a distance of 100 ft. Therefore, no noise impacts would occur, and no noise reduction measures are required.

Table O: Truck Delivery and Unloading Activities

Activity	Reference Noise Level (dBA L_{eq})	Reference Distance (ft)	Noise Attenuation at 100/200 ft (dBA)	Noise Level at 100/200 ft (dBA L_{eq})
Truck Delivery and Unloading Activities	65	50	6/12	59/53

Source: Compiled by LSA Associates, Inc. (2020).
 dBA = A-weighted decibels
 ft = foot/feet
 L_{eq} = equivalent continuous sound level

HVAC Equipment

The project would construct a new convenience store and a restaurant with a drive-through lane that would have up to four rooftop HVAC units on each building. The HVAC equipment could potentially operate 24 hours per day. Rooftop HVAC equipment would generate noise levels of 66.5 dBA L_{eq} at 5 ft. Four rooftop HVAC units operating together would generate a noise level of 73 dBA L_{eq} at 5 ft. Table P shows the noise level generated by four HVAC units at 200 ft. As Table P shows, noise levels generated by four HVAC units would not exceed the City’s exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} , respectively, at a distance of 200 ft for noise sources generated on commercial land uses. In addition, the closest residences are located approximately 45 ft from the HVAC equipment and would be exposed to a noise level of 54 dBA L_{eq} as shown in Table P. A noise level of 54 dBA L_{eq} would not exceed the City’s equivalent exterior daytime and nighttime noise standard of 78 dBA L_{eq} and 73 dBA L_{eq} , respectively, at a distance of 45 ft. Therefore, no noise impacts would occur, and no noise reduction measures are required.

Table P: HVAC Equipment

Activity	No. of HVAC Units	Reference Noise Level (dBA L _{eq})	Reference Distance (ft)	Noise Attenuation at 45/200 ft (dBA)	Noise Level at 45/200 ft (dBA L _{eq})
HVAC Equipment	4	73	5	19/32	54/41

Source: Compiled by LSA Associates, Inc. (2020).
 dBA = A-weighted decibels
 ft = foot/feet

Speakerphone Noise

The proposed project would construct a coffee shop with a drive-through speakerphone that is part of the menu board. Noise generated from speakerphones is approximately 81 dBA at 1 ft. Table Q shows the noise level generated by the speakerphone at 200 ft. As Table Q shows, noise levels generated by speakerphone would not exceed the City’s exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq}, respectively, at a distance of 200 ft for noise sources generated on commercial land uses. As the proposed project would construct a 6 ft high wall along the northern, eastern, and southern boundaries, noise levels generated by the drive-through speakerphone would be lower than 35 dBA L_{eq}. Compliance with Section 9.09.080 in the City’s Municipal Code or approval to allow an alternative that “adequately protects the public health, safety, and welfare” based on Section 9.09.020 of the City’s Municipal would be required. Section 9.09.080 of the City’s Municipal Code specifies the hours of operation and that the drive through speakerphone system shall be designed to compensate for ambient noise levels in the immediate area and the drive through speakerphone shall not be located within 100 ft of any residential district or any property used for residential uses. Section 9.09.020 allows the decision-making body discretion to allow an alternative that “adequately protects the public health, safety and welfare.” Based on the site plan shown in Figure 2, the closest residential property to the speakerphone is located at 100 ft. The proposed drive through restaurant and drive through would potentially operate 24 hours per day. The closest residences are located approximately 100 ft from the speakerphone and would be exposed to a noise level of 41 dBA L_{eq} as shown in Table Q. The noise level of 41 dBA L_{eq} would not exceed the City’s equivalent exterior daytime and nighttime noise standard of 71 dBA L_{eq} and 66 dBA L_{eq}, respectively, at a distance of 100 ft. Therefore, no noise impacts would occur with the compliance of the hours of operation or approval to allow an alternative that “adequately protects the public health, safety, and welfare” and implementation of a drive through speakerphone system designed to compensate for ambient noise levels in the immediate area.

Table Q: Speakerphone Noise

Activity	Reference Noise Level (dBA)	Reference Distance (ft)	Noise Attenuation at 100/200 ft (dBA)	Noise Level at 100/200 ft (dBA L _{eq})
Speakerphone Operations	81	1	40/46	41/35

Source: Compiled by LSA Associates, Inc. (2020).
 dBA = A-weighted decibels
 ft = foot/feet
 L_{eq} = equivalent continuous sound level

Parking Activities

The project would include parking areas on the project site. Noise generated from parking activities includes vehicles traveling at slow speeds, engine start-up noise, car door slams, car horns, car alarms, and tire squeals. Representative parking activities would generate approximately 60 to 70 dBA L_{max} at 50 ft. Table R shows the noise level generated by the parking activities at 200 ft. Although parking activities generate high intermittent noise levels of 58 dBA L_{max} , the L_{eq} noise level would be much lower over an hour period, and the L_{eq} noise level would not exceed the City’s exterior daytime and nighttime noise standard of 65 dBA L_{eq} and 60 dBA L_{eq} , respectively, at a distance of 200 ft for noise sources generated on commercial land uses. In addition, the closest residences are located approximately 25 ft from the parking activities and would be exposed to a noise level of 76 dBA L_{max} as shown in Table R. A noise level of 76 dBA L_{max} would not exceed the City’s equivalent exterior daytime and nighttime noise standard of 83 dBA L_{eq} and 78 dBA L_{eq} , respectively, at a distance of 25 ft. Therefore, no noise impacts would occur, and no noise reduction measures are required.

Table R: Parking Lot Activities

Activity	Reference Noise Level (dBA)	Reference Distance (ft)	Noise Attenuation at 25/200 ft (dBA)	Noise Level at 25/200 ft (dBA L_{max})
Truck Delivery and Unloading Activities	70	50	-6/12	76/58

Source: Compiled by LSA Associates, Inc. (2020).
 dBA = A-weighted decibels
 ft = foot/feet
 L_{eq} = equivalent continuous sound level

Long-Term Vibration Impacts

The proposed project would not generate vibration. In addition, vibration levels generated from project-related traffic on the adjacent roadways (Perris Boulevard, Dracaea Avenue, and local roadways leading to the project site) are unusual for on-road vehicles because the rubber tires and suspension systems of on-road vehicles provide vibration isolation. Vibration generated from project-related traffic on the adjacent roadways would be less than significant. No vibration reduction measures are required.

MINIMIZATION MEASURES

The following would minimize short-term construction-related noise and vibration impacts resulting from the proposed project:

- The construction contractor shall limit the operation of any tools or equipment used in construction, drilling, repair, alteration or demolition work, to between the hours of 7:00 a.m. and 8:00 p.m. every day.

- The construction contractor shall limit all grading-related activities, including operation of grading equipment, to between the hours of 7:00 a.m. and 7:00 p.m. Monday through Friday and between the hours of 8:00 a.m. and 4:00 p.m. on Saturday.
- During all project site excavation and grading, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers consistent with manufacturers' standards.
- The project contractor shall place all stationary construction equipment so that emitted noise is directed away from the relatively more sensitive receptors nearest the project site.
- The construction contractor shall locate equipment staging in areas that will create the greatest distance between construction-related noise sources and relatively more noise-sensitive receptors nearest the project site during all project construction.

CONDITIONS OF APPROVAL

Short-Term Construction Noise Impacts

No noise reduction measures are required.

Short-Term Construction Vibration Impacts

The following measure would reduce construction-related vibration impacts resulting from the proposed project:

- The project will be conditioned to operate only small bulldozers within 15 ft of the residential structures immediately north and southeast of the project site to ensure vibration levels would not exceed the FTA's damage threshold of 94 VdB (0.2 PPV [in/sec]) for buildings constructed of non-engineered timber and masonry. This condition would be incorporated onto the project grading plans, and directional signage would be placed on the construction site to direct equipment operators. Established City procedures for plan check, permit issuance, and construction inspection would ensure project implementation consistent with the conditions of approval.

Long-Term Aircraft Noise Impacts

No noise reduction measures are required.

Long-Term Traffic Noise Impacts

No noise reduction measures are required.

Long-Term Stationary Noise Impacts

The following measures would reduce long-term operational noise impacts resulting from the proposed project:

- Pursuant to Section 9.09.080(C)(1) of the City Municipal Code, the fast-food restaurant shall not open prior to 6:00 a.m. nor remain open after 10:00 p.m. unless the Project Applicant obtains approval from a decision-making body of the City to allow an alternative that “adequately protects the public health, safety and welfare” in accordance with Section 9.09.020 of the City Municipal Code.
- The drive through speakerphone system shall be designed to compensate for ambient noise levels in the immediate area.

Long-Term Vibration Impacts

No vibration reduction measures are required.

REFERENCES

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_____. 2006. Noise Element of the Moreno Valley General Plan. July 11.

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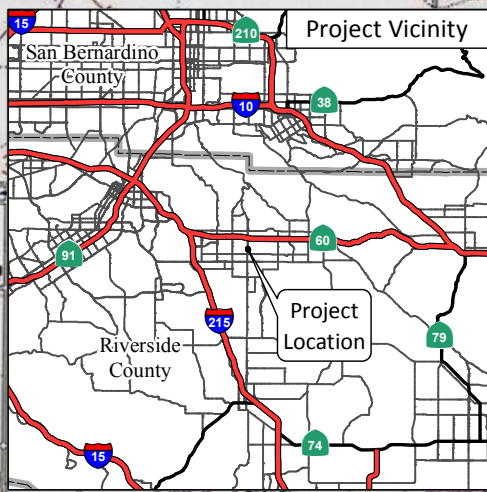
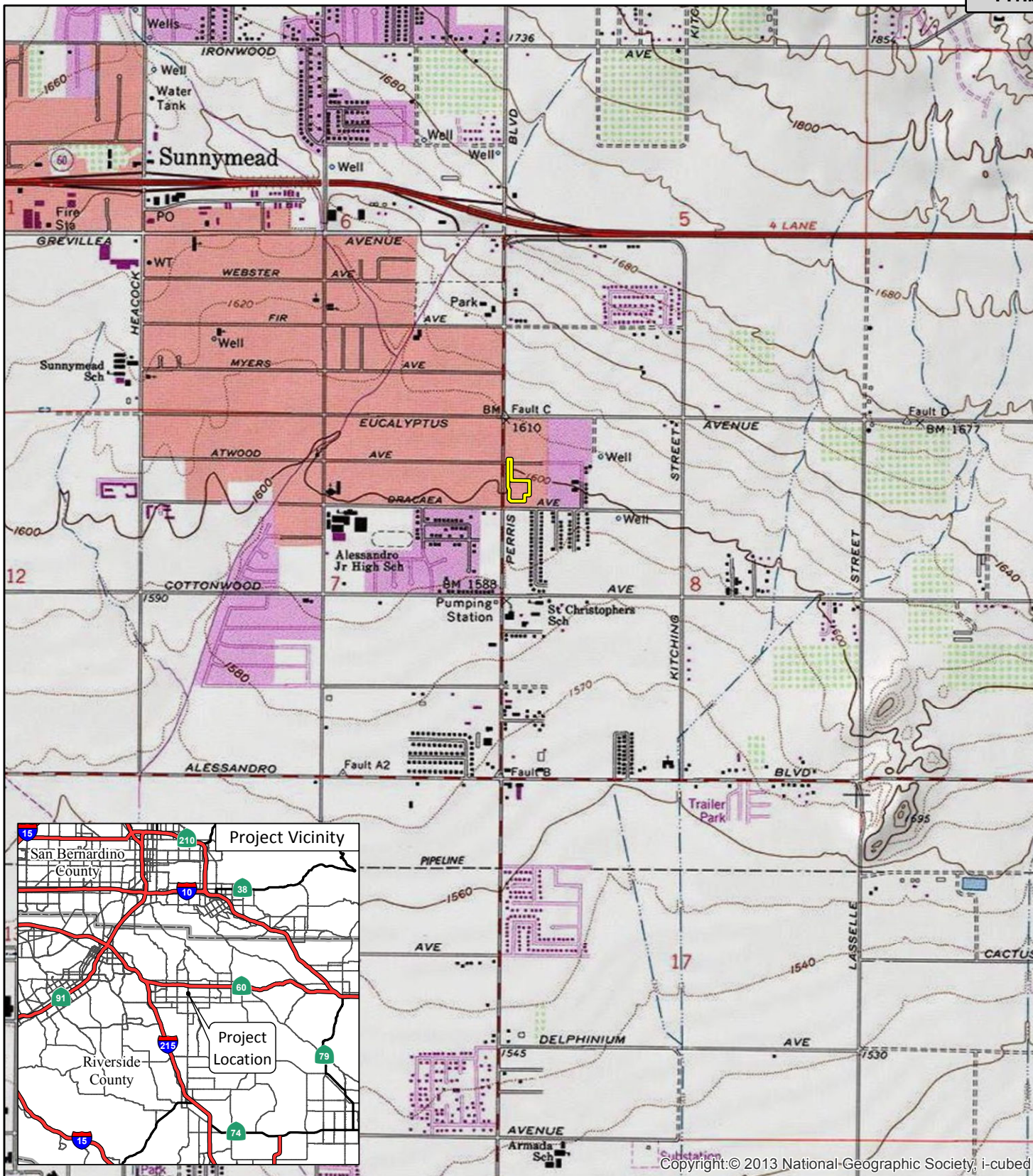
LSA Associates, Inc. (LSA). 2020a. *Air Quality and Greenhouse Gas Analysis for the Commercial Retail Project at Perris Boulevard and Dracaea Avenue in the City of Moreno Valley, California*. May 4.

_____. 2020b. *Transportation Impact Analysis for the Perris/Dracaea Commercial Project*. September.


Attachments: A: Figures
 B: Long-Term Noise Level Measurement Results
 C: FHWA Highway Traffic Noise Model Printouts
 D: Detailed Noise Calculations

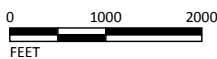
ATTACHMENT A

FIGURES



LSA

LEGEND
 Project Location



SOURCE: USGS 7.5' Quad., Sunnymead, CA (1980)

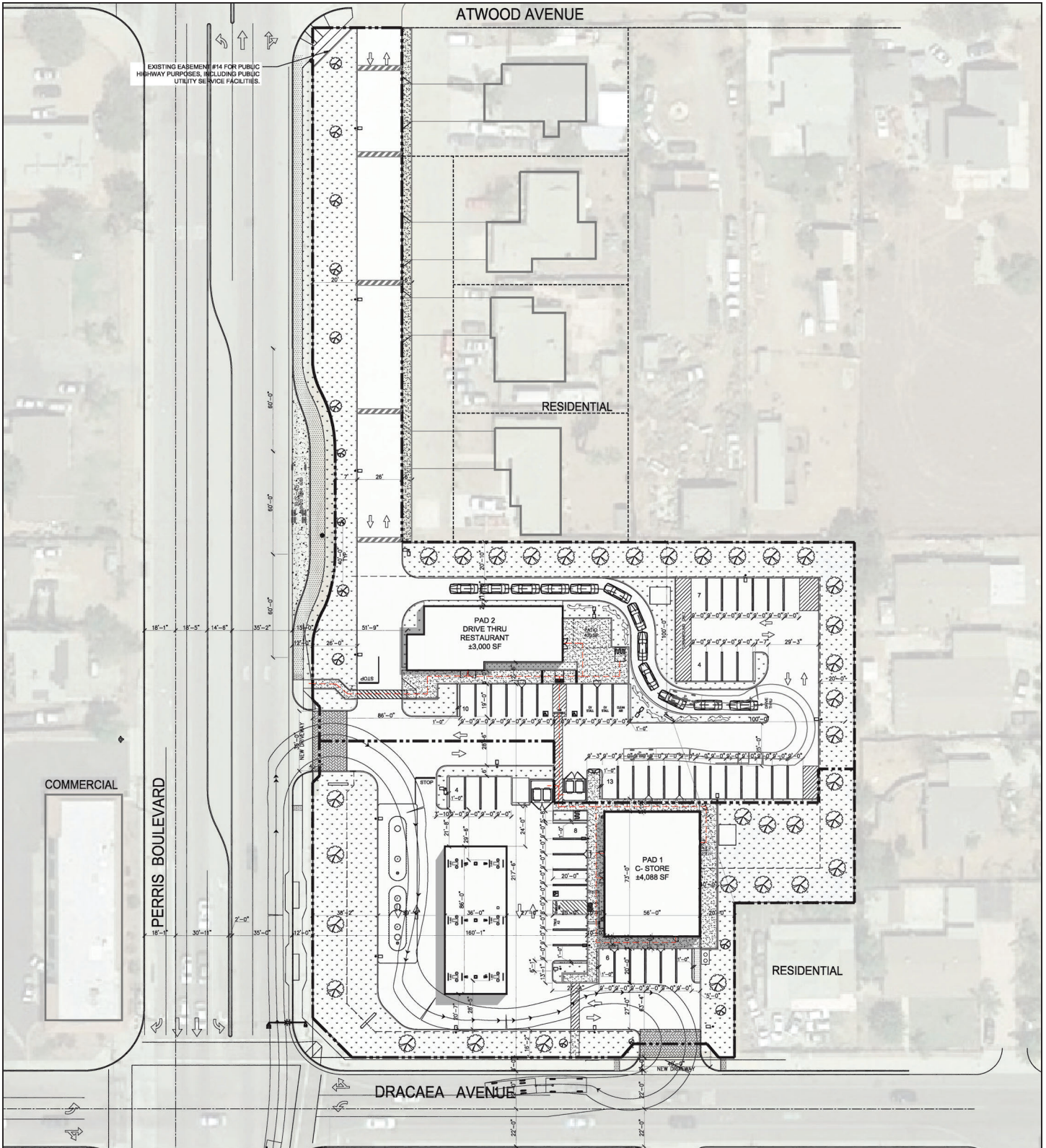
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Copyright: © 2013 National Geographic Society, i-cubed

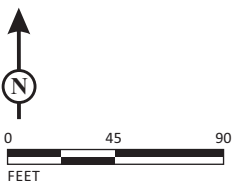
FIGURE :

Commercial Retail Project at Perris Boulevard/Dracaea Avenue

Project Location



LSA



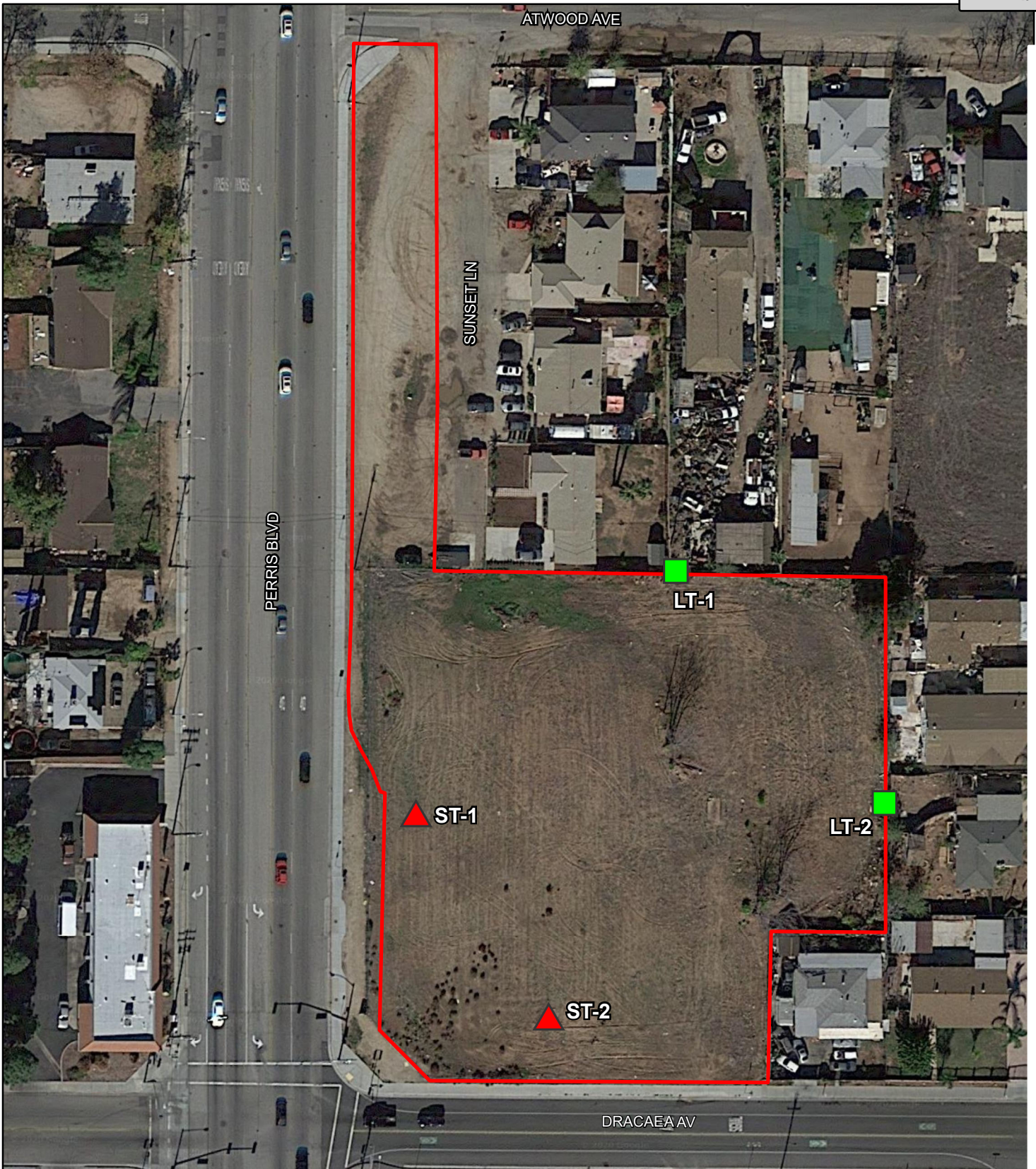
SOURCE: Cadence Capital Investments LLC; June 2020

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Commercial Retail Project at Perris Boulevard/Dracaea Avenue

Site Plan

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



LSA

LEGEND

- Project Site
- Long Term Monitoring Locations
- ▲ Short Term Monitoring Locations



SOURCE: Google (2019)

I:\CAQ1901\G\Noise_Locations.ai (5/1/2020)

FIGURE

Commercial Retail Project at Perris Boulevard/Dracaea Avenue
 Noise Monitoring Locations

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

ATTACHMENT B

LONG-TERM NOISE LEVEL MEASUREMENT RESULTS

Table A-1: Long-Term (24-Hour) Noise Level Measurement Results at LT-1

	Start Time	Day 1			Day 2	
		Date	Noise Level (dBA L _{eq})		Date	Noise Level (dBA L _{eq})
1	10:00 AM	7/22/19	57	25	7/23/19	56
2	11:00 AM	7/22/19	58	26	7/23/19	56
3	12:00 PM	7/22/19	59	27	7/23/19	55
4	1:00 PM	7/22/19	56	28	7/23/19	55
5	2:00 PM	7/22/19	58	29	7/23/19	57
6	3:00 PM	7/22/19	59	30	7/23/19	58
7	4:00 PM	7/22/19	58	31	7/23/19	61
8	5:00 PM	7/22/19	60	32	7/23/19	61
9	6:00 PM	7/22/19	60	33	7/23/19	60
10	7:00 PM	7/22/19	59	34	7/23/19	58
11	8:00 PM	7/22/19	57	35	7/23/19	58
12	9:00 PM	7/22/19	56	36	7/23/19	55
13	10:00 PM	7/22/19	57	37	7/23/19	54
14	11:00 PM	7/22/19	54	38	7/23/19	55
15	12:00 AM	7/23/19	51	39	7/23/19	55
16	1:00 AM	7/23/19	51	40	7/23/19	53
17	2:00 AM	7/23/19	50	41	7/23/19	55
18	3:00 AM	7/23/19	53	42	7/23/19	56
19	4:00 AM	7/23/19	56	43	7/23/19	59
20	5:00 AM	7/23/19	57	44	7/23/19	60
21	6:00 AM	7/23/19	57	45	7/23/19	61
22	7:00 AM	7/23/19	58	46	7/23/19	57
23	8:00 AM	7/23/19	57	47	7/23/19	55
24	9:00 AM	7/23/19	54	48	7/23/19	56

Source: Compiled by LSA Associates, Inc. (2020).

dBA L_{eq} = equivalent continuous sound level measured in A-weighted decibels

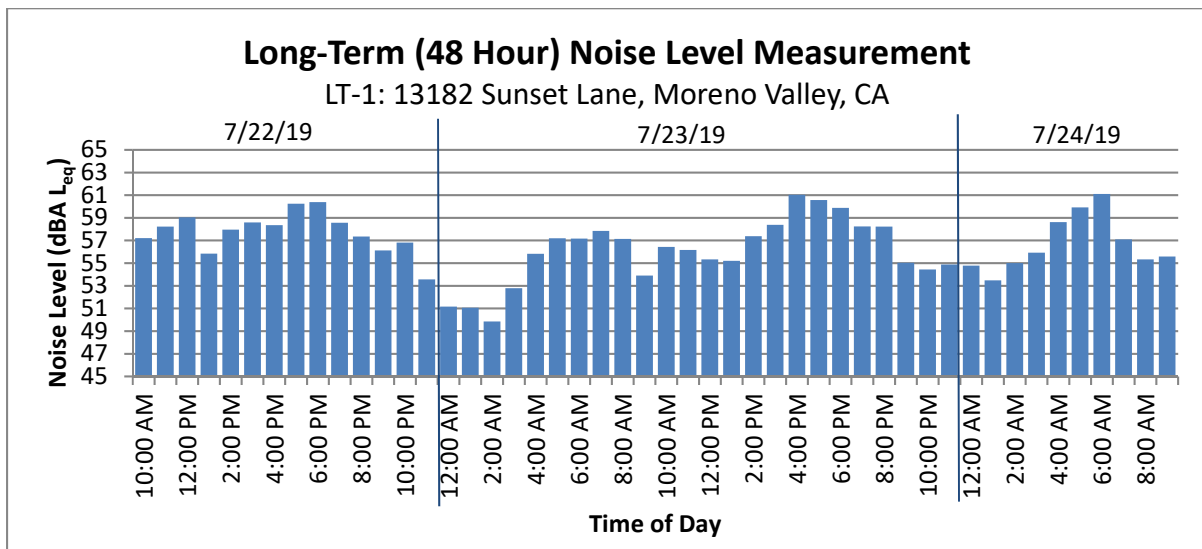
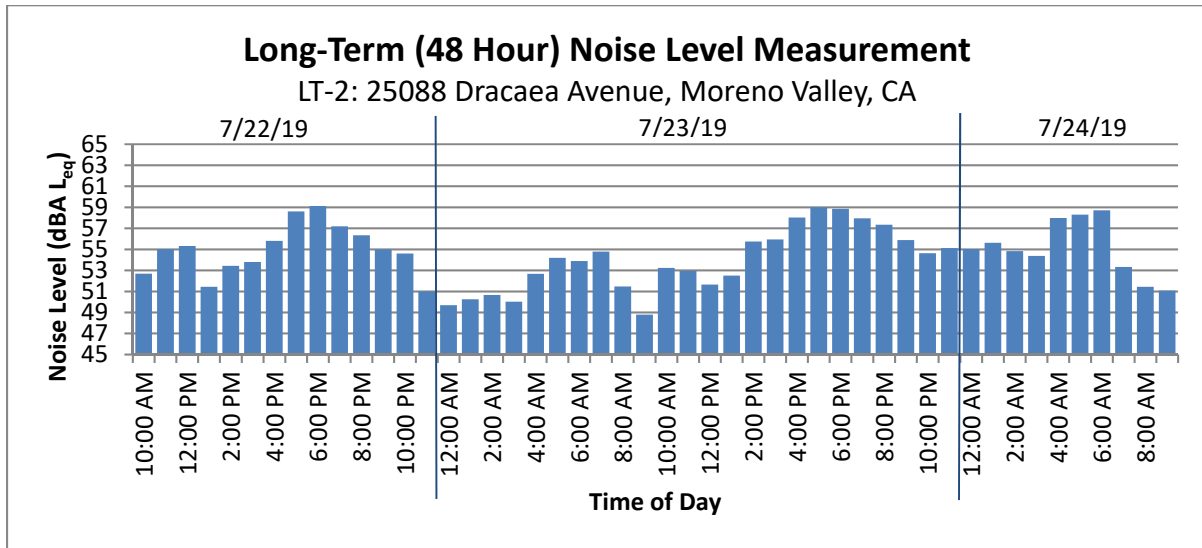


Table A-2: Long-Term (24-Hour) Noise Level Measurement Results at LT-2

	Start Time	Day 1			Day 2	
		Date	Noise Level (dBA L _{eq})		Date	Noise Level (dBA L _{eq})
1	10:00 AM	7/22/19	53	25	7/23/19	53
2	11:00 AM	7/22/19	55	26	7/23/19	53
3	12:00 PM	7/22/19	55	27	7/23/19	52
4	1:00 PM	7/22/19	51	28	7/23/19	52
5	2:00 PM	7/22/19	53	29	7/23/19	56
6	3:00 PM	7/22/19	54	30	7/23/19	56
7	4:00 PM	7/22/19	56	31	7/23/19	58
8	5:00 PM	7/22/19	59	32	7/23/19	59
9	6:00 PM	7/22/19	59	33	7/23/19	59
10	7:00 PM	7/22/19	57	34	7/23/19	58
11	8:00 PM	7/22/19	56	35	7/23/19	57
12	9:00 PM	7/22/19	55	36	7/23/19	56
13	10:00 PM	7/22/19	55	37	7/23/19	55
14	11:00 PM	7/22/19	51	38	7/23/19	55
15	12:00 AM	7/23/19	50	39	7/23/19	55
16	1:00 AM	7/23/19	50	40	7/23/19	56
17	2:00 AM	7/23/19	51	41	7/23/19	55
18	3:00 AM	7/23/19	50	42	7/23/19	54
19	4:00 AM	7/23/19	53	43	7/23/19	58
20	5:00 AM	7/23/19	54	44	7/23/19	58
21	6:00 AM	7/23/19	54	45	7/23/19	59
22	7:00 AM	7/23/19	55	46	7/23/19	53
23	8:00 AM	7/23/19	51	47	7/23/19	51
24	9:00 AM	7/23/19	49	48	7/23/19	51

Source: Compiled by LSA Associates, Inc. (2020).

dBA L_{eq} = equivalent continuous sound level measured in A-weighted decibels



ATTACHMENT C

FHWA HIGHWAY TRAFFIC NOISE MODEL PRINTOUTS

TABLE Existing No Project-01
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Eucalyptus Avenue and Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 31829 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.47

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
70.5	136.5	286.3	613.1

TABLE Existing No Project-02
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Atwood Avenue and Dracaea Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 30997 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.36

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
69.6	134.3	281.4	602.4

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE Existing No Project-03
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Dracaea Avenue and Cottonwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 29577 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 30 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.44

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
65.6	129.2	272.4	583.9

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE Existing No Project-04
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Dracaea Avenue between Perris Boulevard and Project Driveway 2
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 4912 SPEED (MPH): 35 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 14 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 60.35

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	67.4	142.6

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE Existing No Project-05
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Sunset Lane South of Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing
No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 38 SPEED (MPH): 25 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 6 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 36.52

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	0.0	0.0

TABLE Existing Project-01
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Eucalyptus Avenue and Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 32699 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.59

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
71.5	138.8	291.4	624.2

TABLE Existing Project-02
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Atwood Avenue and Dracaea Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 31282 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.40

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
69.9	135.0	283.1	606.1

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE Existing Project-03
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Dracaea Avenue and Cottonwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 30621 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 30 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.59

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
66.8	132.1	278.7	597.5

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE Existing Project-04
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Dracaea Avenue between Perris Boulevard and Project Driveway 2
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 7251 SPEED (MPH): 35 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 14 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 62.04

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	86.6	184.6

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE Existing Project-05
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Sunset Lane South of Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - Existing Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 795 SPEED (MPH): 25 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 6 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 49.73

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	0.0	0.0

TABLE 2022 No Project-01
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Eucalyptus Avenue and Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 36447 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 69.06

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
75.6	148.6	313.0	670.9

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE 2022 No Project-02
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Atwood Avenue and Dracaea Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 35559 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.95

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
74.7	146.3	307.9	660.0

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE 2022 No Project-03
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Dracaea Avenue and Cottonwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 34036 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 30 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 69.05

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
70.7	141.2	298.8	641.1

TABLE 2022 No Project-04
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Dracaea Avenue between Perris Boulevard and Project Driveway 2
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 5207 SPEED (MPH): 35 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 14 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 60.61

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	69.9	148.2

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE 2022 No Project-05
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Sunset Lane South of Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 38 SPEED (MPH): 25 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 6 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 36.52

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	0.0	0.0

TABLE 2022 Project-01
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Eucalyptus Avenue and Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 37317 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 69.16

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
76.6	150.8	317.9	681.5

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TABLE 2022 Project-02
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Atwood Avenue and Dracaea Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 35844 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 68.99

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
75.0	147.0	309.6	663.5

TABLE 2022 Project-03
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Dracaea Avenue and Cottonwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 35080 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 30 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 69.18

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
71.9	144.0	304.8	654.1

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TABLE 2022 Project-04
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Dracaea Avenue between Perris Boulevard and Project Driveway 2
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 7546 SPEED (MPH): 35 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 14 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 62.22

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	88.9	189.5

TABLE 2022 Project-05
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Sunset Lane South of Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2022 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 795 SPEED (MPH): 25 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 6 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 49.73

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	0.0	0.0

TABLE 2040 No Project-01
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Eucalyptus Avenue and Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 45861 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 70.06

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
85.6	171.9	364.2	781.6

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TABLE 2040 No Project-02
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Atwood Avenue and Dracaea Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 46776 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 70.14

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
86.6	174.1	369.0	792.0

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TABLE 2040 No Project-03
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Dracaea Avenue and Cottonwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 42958 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 30 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 70.06

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
80.6	164.0	348.5	748.5

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TABLE 2040 No Project-04
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Dracaea Avenue between Perris Boulevard and Project Driveway 2
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 6735 SPEED (MPH): 35 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 14 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 61.72

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	82.5	175.8

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TABLE 2040 No Project-05
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Sunset Lane South of Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 No Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 38 SPEED (MPH): 25 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 6 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 36.52

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	0.0	0.0

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TABLE 2040 Project-01
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Eucalyptus Avenue and Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 46731 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 70.14

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
86.5	174.0	368.7	791.5

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

TABLE 2040 Project-02
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Atwood Avenue and Dracaea Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 47061 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 35 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 70.17

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
86.8	174.8	370.5	795.2

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TABLE 2040 Project-03
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Perris Boulevard between Dracaea Avenue and Cottonwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 44003 SPEED (MPH): 40 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 30 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 70.16

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
81.7	166.5	354.1	760.5

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TABLE 2040 Project-04
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Dracaea Avenue between Perris Boulevard and Project Driveway 2
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 9074 SPEED (MPH): 35 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 14 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 63.02

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	100.2	214.2

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TABLE 2040 Project-05
FHWA ROADWAY NOISE LEVEL ANALYSIS

RUN DATE: 09/03/2020
ROADWAY SEGMENT: Sunset Lane South of Atwood Avenue
NOTES: Perris Boulevard and Dracaea Avenue Commercial Retail - 2040 Project

* * ASSUMPTIONS * *

AVERAGE DAILY TRAFFIC: 795 SPEED (MPH): 25 GRADE: .5

	TRAFFIC DISTRIBUTION PERCENTAGES		
	DAY	EVENING	NIGHT
	---	-----	-----
AUTOS	75.51	12.57	9.34
M-TRUCKS	1.56	0.09	0.19
H-TRUCKS	0.64	0.02	0.08

ACTIVE HALF-WIDTH (FT): 6 SITE CHARACTERISTICS: SOFT

* * CALCULATED NOISE LEVELS * *

CNEL AT 50 FT FROM NEAR TRAVEL LANE CENTERLINE (dB) = 49.73

DISTANCE (FEET) FROM ROADWAY CENTERLINE TO CNEL			
70 CNEL	65 CNEL	60 CNEL	55 CNEL
-----	-----	-----	-----
0.0	0.0	0.0	0.0

ATTACHMENT D

DETAILED NOISE CALCULATIONS

Commercial Retail Project at Perris Boulevard and Dracaea Avenue in Moreno Valley, California

Traffic Noise Calculations

Scenario	Roadway	ADT	Center-line to 70 CNEL (feet)	Center-line to 65 CNEL (feet)	Center-line to 60 CNEL (feet)	CNEL (dBA) 50 feet from Centerline of Outermost Lane	Distance from Roadway Centerline to Receptor (ft)	Exterior Noise Level (dBA CNEL)	Combined Noise Level (dBA CNEL)	Noise Level Increase (dBA)
Existing NP	Perris Boulevard between Atwood Avenue and Dracaea Avenue	30,997	70	134	281	68.4	194.74	62.6	62.6	
Existing NP	Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5	61.75	35.9		
Existing WP	Perris Boulevard between Atwood Avenue and Dracaea Avenue	31,282	70	135	283	68.4	194.74	62.6	62.8	0.2
Existing WP	Sunset Lane South of Atwood Avenue	795	< 50	< 50	< 50	49.7	61.75	49.1		
2022 NP	Perris Boulevard between Atwood Avenue and Dracaea Avenue	35,559	75	146	308	69.0	194.74	63.1	63.1	
2022 NP	Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5	61.75	35.9		
2022 P	Perris Boulevard between Atwood Avenue and Dracaea Avenue	35,844	75	147	310	69.0	194.74	63.2	63.3	0.2
2022 P	Sunset Lane South of Atwood Avenue	795	< 50	< 50	< 50	49.7	61.75	49.1		
2040 NP	Perris Boulevard between Atwood Avenue and Dracaea Avenue	46,776	87	174	369	70.1	194.74	64.3	64.3	
2040 NP	Sunset Lane South of Atwood Avenue	38	< 50	< 50	< 50	36.5	61.75	35.9		
2040 P	Perris Boulevard between Atwood Avenue and Dracaea Avenue	47,061	87	175	370	70.2	194.74	64.3	64.4	0.1
2040 P	Sunset Lane South of Atwood Avenue	795	< 50	< 50	< 50	49.7	61.75	49.1		

Appendix E:

Traffic Impact Analysis, Perris/Dracaean Commercial Project, City of Moreno Valley, Riverside County, California

TRANSPORTATION IMPACT ANALYSIS

PERRIS/DRACAEA COMMERCIAL PROJECT
CITY OF MORENO VALLEY
RIVERSIDE COUNTY, CALIFORNIA



September 2020

TRANSPORTATION IMPACT ANALYSIS

PERRIS/DRAECAE COMMERCIAL PROJECT

CITY OF MORENO VALLEY

RIVERSIDE COUNTY, CALIFORNIA

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Project No. CAQ1901



September 2020

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1.0 EXECUTIVE SUMMARY

The proposed Perris/Dracaea Commercial Project will include a gasoline station with 12 fueling positions, a 4,088-square foot convenience store, and a 3,000-square foot drive-through restaurant. The project will be located at the northeast corner of Perris Boulevard and Dracaea Avenue in the City of Moreno Valley (City). The project will require a General Plan Amendment (GPA) and Zone Change (ZC).

Access to the project site would be provided via three driveways: one on Perris Boulevard, one on Dracaea Avenue, and one on Atwood Avenue via Sunset Lane. While the driveway on Perris Boulevard (Project Driveway 1) will be restricted to a right-in/right-out (RIRO) only driveway by a raised concrete median on Perris Boulevard, the driveway on Dracaea Avenue (Project Driveway 2) and the access via Sunset lane will operate as full-access driveways.

The project will replace the two-way left-turn lane (TWLTL) on Perris Boulevard from south of Pedro's Taco Shop driveway to the Dracaea Avenue and create turn pockets in the median for northbound and southbound left-turn movements at the intersection of Perris Boulevard/Dracaea Avenue, with storage lengths of 150 feet (ft) and 125 ft, respectively. Additionally, the project will extend the southbound left-turn pocket at the intersection of Perris Boulevard/Dracaea Avenue by 5 ft. Further, the project will add a bus bay on the east side of Perris Boulevard, north of the intersection of Perris Boulevard/Auto Center Driveway-Project Driveway 1.

The project is forecast to generate 324 net trips in the a.m. peak hour, 204 net trips in the p.m. peak hour, and 3,471 net daily trips. Table 1-A, at the end of this chapter, illustrates the recommended improvements for study intersections and funding mechanism under all scenarios.

The study area for the project was finalized based on the criteria defined in the City of Moreno Valley Transportation Engineering Division *Transportation Impact Analysis Preparation Guide for Vehicle Miles Traveled and Level of Service Assessment*, dated June 2020, as well as the requirements for the disclosure of potential impacts and mitigation measures pursuant to the California Environmental Quality Act (CEQA). Based on discussions with City staff during the scoping agreement process, the study area includes seven intersections and four roadway segments.

Traffic conditions were examined for the weekday daily, a.m., and p.m. peak hour conditions under the following scenarios:

- Existing conditions;
- Project Completion (2022) without project conditions;
- Project Completion (2022) with project conditions;
- General Plan Build-out (2040) without project conditions; and
- General Plan Build-out (2040) with project conditions.

1.1 EXISTING CONDITIONS SUMMARY

Based on the criteria as discussed in the “Level of Service Procedures and Thresholds” section of this report, the intersection of Perris Boulevard/Atwood Avenue operates at an unsatisfactory Level of Service (LOS) under existing conditions. All other study intersections and roadway segments are forecast to operate at a satisfactory LOS under existing conditions.

Queues for some of the turn movements at study area intersections are forecast to exceed the existing available turn-pocket storage lengths under existing conditions.

1.2 PROJECT COMPLETION (2022) CONDITIONS SUMMARY

Based on the criteria discussed in the “Level of Service Procedures and Thresholds” section of this report, the intersection of Perris Boulevard/Atwood Avenue operates at an unsatisfactory LOS under project completion without project conditions, but operates at a satisfactory LOS under project completion with project conditions. All other study intersections are forecast to operate at a satisfactory LOS under project completion without and with project conditions.

All the roadway segments on Perris Boulevard are forecast to operate at an unsatisfactory LOS under project completion without and without project conditions. However, the project increases the volume-to capacity (V/C) ratio by less than 0.05 at these segments. Therefore, pursuant to the City’s Transportation Impact Analysis (TIA) guidelines, improvements are not required for any of these segments. The segment of Dracaea Avenue between Perris Boulevard and Birchwood Drive is forecast to operate at a satisfactory LOS.

Queues for some of the turn movements at study area intersections are forecast to exceed the existing available turn-pocket storage lengths under project completion without and with project conditions. Table 1-A lists improvements recommended at these locations and the corresponding funding mechanisms.

1.3 GENERAL PLAN BUILD-OUT (2040) CONDITIONS SUMMARY

Based on the criteria discussed in the “Level of Service Procedures and Thresholds” section of this report, the intersection of Perris Boulevard/Atwood Avenue operates at an unsatisfactory LOS under General Plan build-out without project conditions, but operates at a satisfactory LOS under General Plan build-out with project conditions. However, the intersection of Perris Boulevard/Dracaea Avenue operates at an unsatisfactory LOS under both General Plan build-out without and with project conditions. Additionally, the project increases the delay at this intersection by more than 5.0 seconds. Therefore, pursuant to the City’s TIA guidelines, the project needs to identify improvements to offset the delay at this intersection. With the implementation of the improvements listed in Table 1-A, this intersection is forecast to operate at a satisfactory LOS. All other study intersections are forecast to operate at a satisfactory LOS under General Plan build-out without and with project conditions.

All the roadway segments on Perris Boulevard are forecast to operate at an unsatisfactory LOS under project completion without and without project conditions. However, the project increases the V/C ratio by less than 0.05 at these segments. Therefore, pursuant to the City’s TIA guidelines,

improvements are not required for any of these segments. The segment of Dracaea Avenue between Perris Boulevard and Birchwood Drive is forecast to operate at a satisfactory LOS.

Queues for some of the turn movements at study area intersections are forecast to exceed the existing available turn-pocket storage lengths under General Plan build-out without and with project conditions. Table 1-A lists improvements recommended at these locations and the corresponding funding mechanisms.

1.4 VMT EVALUATION SUMMARY

Pursuant to the City's Vehicle Miles Traveled (VMT) analysis guidelines, retail projects having an area of less than 50,000 square feet (sf), which serve the local community and have the potential to reduce VMT, are exempted from a VMT assessment. Since this project has a much lower square footage (7,088 sf for all uses combined) than the 50,000-square foot threshold, a detailed VMT analysis is not required for this project. Additionally, the project can be considered as a local-serving retail and will not have a significant VMT impact.

1.5 LIST OF CHAPTER 1.0 TABLES

- Table 1-A: Recommended Improvements for Intersections and Funding Mechanism

Table 1-A - Recommended Improvements for Intersections and Funding Mechanism

Intersection	Project Completion (2022) with Project Improvements	General Plan Build-Out (2040) with Project Improvements	Funding Mechanism	Improvements Covered by TUMF	Improvements Covered by Fair Share	Fair Share Percentage
1 . Perris Boulevard/Eucalyptus Avenue	Extend storage length for NBL turn lane from 95 feet to 280 feet, Extend storage length for WBL turn lane from 100 feet to 135 feet.	Project Completion Improvements + Extend EBR turn lane from 50 feet to 155 feet.	Fair Share		Extend storage length for NBL turn lane from 95 feet to 280 feet, Extend EBR turn lane from 50 feet to 155 feet, Extend storage length for WBL turn lane from 100 feet to 135 feet.	5.89%
4 . Perris Boulevard/Dracaea Avenue	-	Add a EBL turn lane with storage length of 100 feet, Add a WBL turn lane with storage length of 135 feet.	Fair Share		Add a EBL turn lane with storage length of 100 feet, Add a WBL turn lane with storage length of 135 feet.	14.86%
5 . Perris Boulevard/Cottonwood Avenue	Extend storage length for SBL turn lane from 95 feet to 190 feet, Extend storage length for EBL turn lane from 115 feet to 185 feet.	Project Completion Improvements	Fair Share		Extend storage length for SBL turn lane from 95 feet to 190 feet, Extend storage length for EBL turn lane from 115 feet to 185 feet.	8.41%

Notes:

EB = Eastbound, WB = Westbound

L = Left

TUMF refers to the Transportation Uniform Mitigation Fee program.

Project Fair Share Percentage is the highest fair share value of the AM and PM peak hour when both peak hours are impacted by the project, or only in the peak hour where the project has an impact.

2.0 INTRODUCTION

The TIA has been prepared to assess the potential circulation impacts associated with the proposed Perris/Dracaea Commercial Project (Case No. PPA18-0018) to be located at the northeast corner of Perris Boulevard and Dracaea Avenue in the City. Figure 2-1 illustrates the regional and project location. (Figures and tables are located at the end of each chapter).

This report is intended to satisfy the requirements established by the City of Moreno Valley Transportation Engineering Division *Transportation Impact Analysis Preparation Guide for Vehicle Miles Traveled and Level of Service Assessment*, dated June 2020, as well as the requirements for the disclosure of potential impacts and mitigation measures pursuant to CEQA. The scope of work for this TIA, including trip generation, trip distribution, study area, and analysis methodologies, has been approved by City staff via the Scoping Agreement process. A copy of the Scoping Agreement is included as Appendix A.

This study examines traffic operations in the vicinity of the proposed project under the following scenarios:

- Existing Conditions;
- Project Completion (2022) without Project Conditions;
- Project Completion (2022) with Project Conditions;
- General Plan Build-out (2040) without Project Conditions; and
- General Plan Build-out (2040) with Project Conditions.

Traffic conditions were examined for the weekday daily, a.m., and p.m. peak hour conditions. The a.m. peak hour is defined as the one hour of highest traffic volumes occurring between 7:00 and 9:00 a.m. The p.m. peak hour is the one hour of highest traffic volumes occurring between 4:00 and 6:00 p.m. Roadway segments were analyzed using daily volume counts and comparisons were made to the daily service volume standards provided in the City's TIA guidelines.

2.1 PROJECT DESCRIPTION

The proposed project will include a gasoline station with 12 fueling positions, a 4,088-square foot convenience store, and a 3,000-square foot drive-through restaurant. Figure 1-2 illustrates the conceptual site plan for the project.

The land use for the site in the City's General Plan is Residential/Office and the Zoning is Office. The proposed land use for the site is Commercial. The proposed Zoning is Commercial. Therefore, the project will require a GPA and ZC. The project will be completed in a single phase.

Access to the project site would be provided via three driveways: one on Perris Boulevard, one on Dracaea Avenue, and one on Atwood Avenue via Sunset Lane. While the driveway on Perris Boulevard (Project Driveway 1) will be restricted to a RIRO only driveway by a raised concrete median on Perris Boulevard, the driveway on Dracaea Avenue (Project Driveway 2) and the access

via Sunset lane will operate as full-access driveways. Figure 2-2 illustrates the conceptual site plan for the project.

The City's TIA guidelines require that the project completion year should be considered at a minimum of two years from existing conditions. Therefore, for purposes of this analysis, the project completion year is 2022.

2.2 STUDY AREA

The study area was approved by City staff via the City's scoping agreement process (Appendix A). Based on the TIA guidelines, the TIA is required to analyze all intersections of "Collector" or higher classification streets, at which the project will add 50 or more peak hour trips. Additionally, the City recommended inclusion of additional intersections in this analysis during the scoping agreement process. The study area includes the following intersections and roadway segments.

2.2.1 Study Intersections

1. Perris Boulevard/Eucalyptus Avenue;
2. Perris Boulevard/Atwood Avenue;
3. Perris Boulevard/Auto Center Driveway – Project Driveway 1;
4. Perris Boulevard/Dracaea Avenue;
5. Perris Boulevard/Cottonwood Avenue;
6. Project Driveway 2/Dracaea Avenue; and
7. Sunset Lane/Atwood Avenue.

All study intersections are located in the City of Moreno Valley.

Figure 2-3 illustrates the locations of all analysis intersections.

2.2.2 Roadway Segments

1. Perris Boulevard, between Eucalyptus Avenue and Atwood Avenue;
2. Perris Boulevard, between Atwood Avenue and Dracaea Avenue;
3. Perris Boulevard, between Dracaea Avenue and Cottonwood Avenue; and
4. Dracaea Avenue, between Perris Boulevard and Birchwood Drive.

2.3 LIST OF CHAPTER 2.0 FIGURES

- Figure 2-1: Regional and Project Location
- Figure 2-2: Conceptual Site Plan
- Figure 2-3: Study Area Intersections

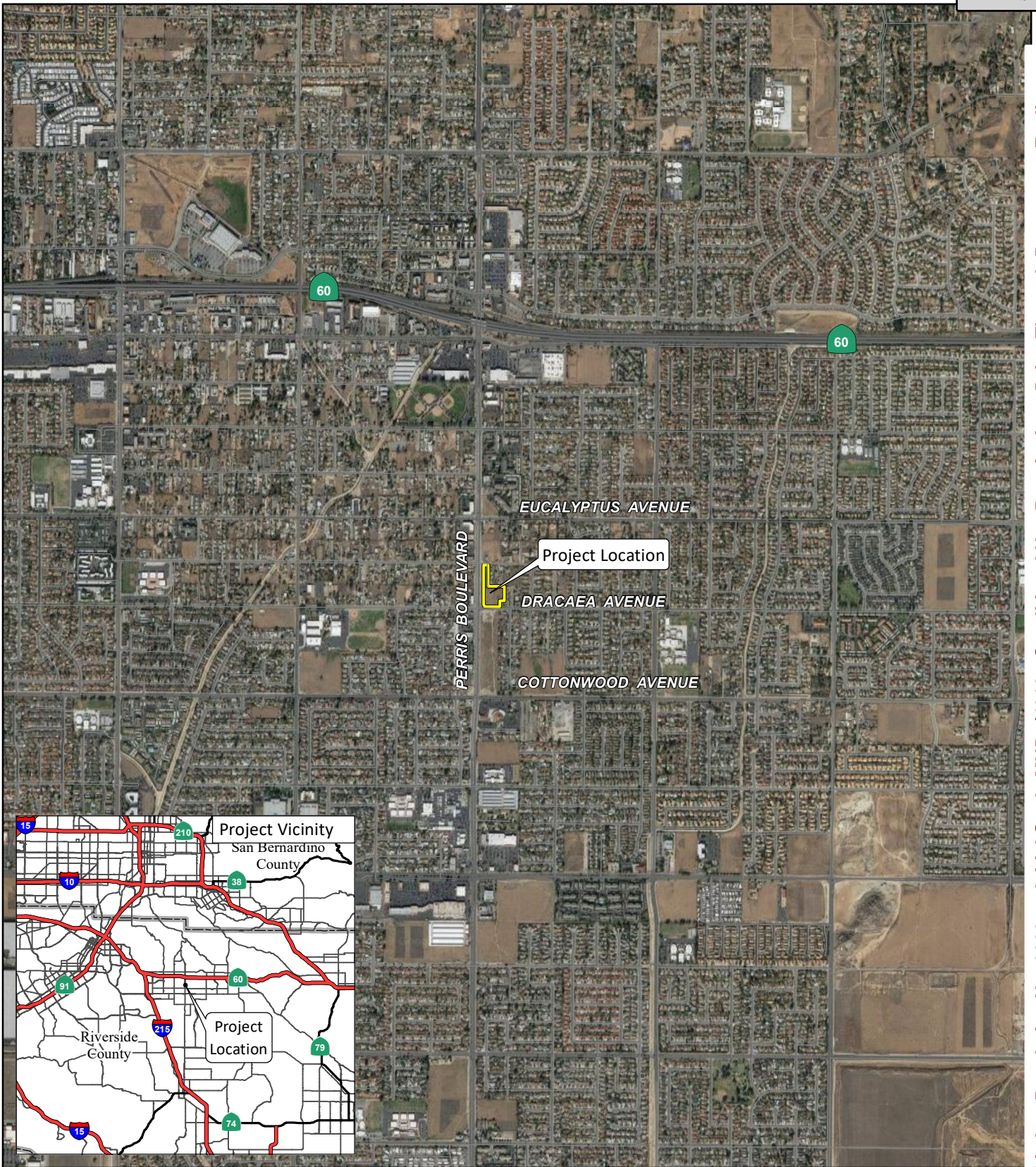
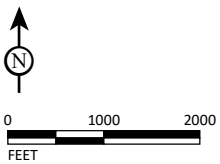


FIGURE 2-:

LSA



SOURCE: ESRI Streetmap, 2013; Google Earth, 2018.

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Perris/Dracaea Commercial Project
Transportation Impact Analysis
Regional and Project Location

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

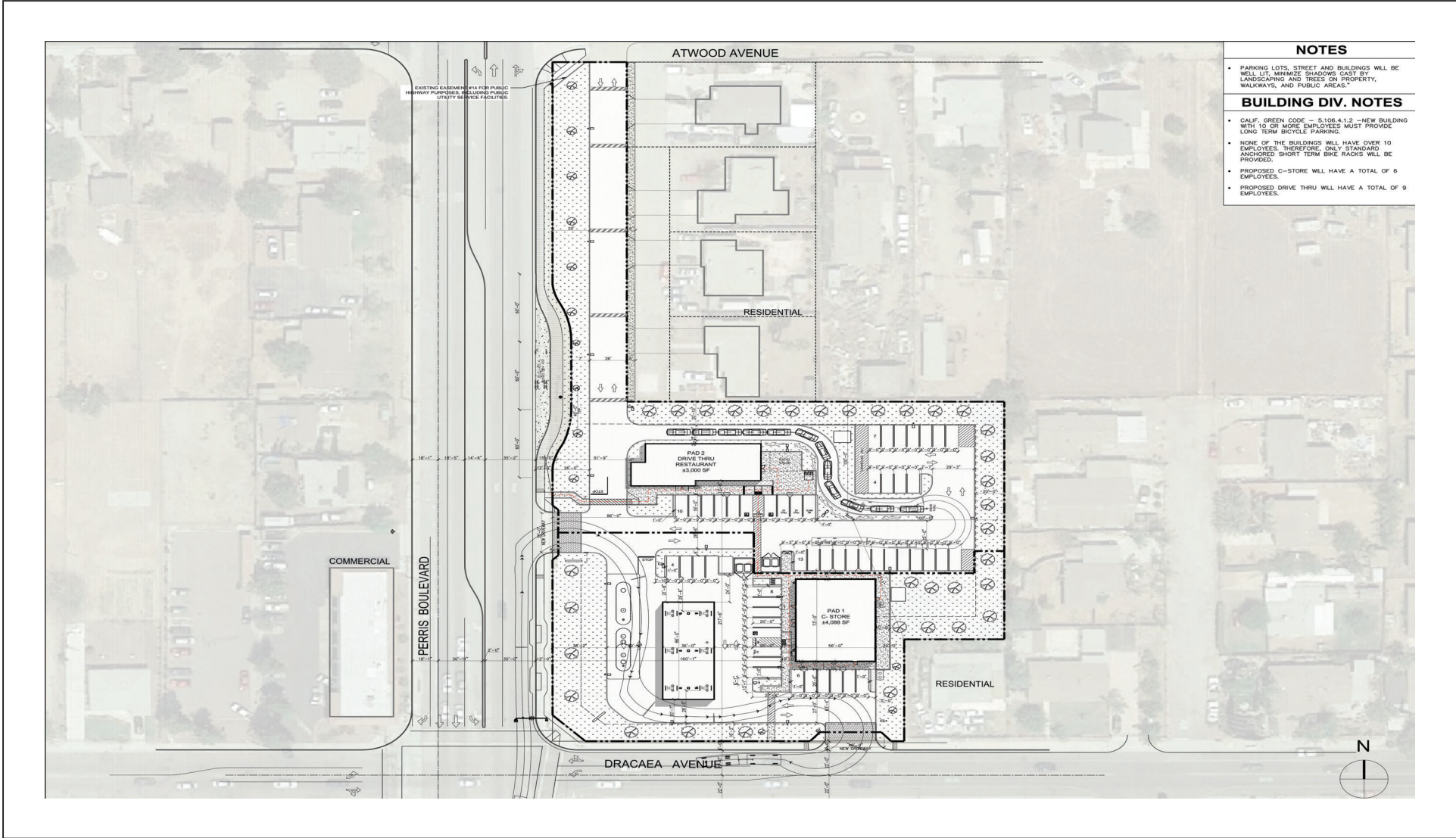
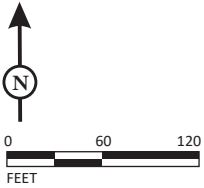


FIGURE 2-2

LSA

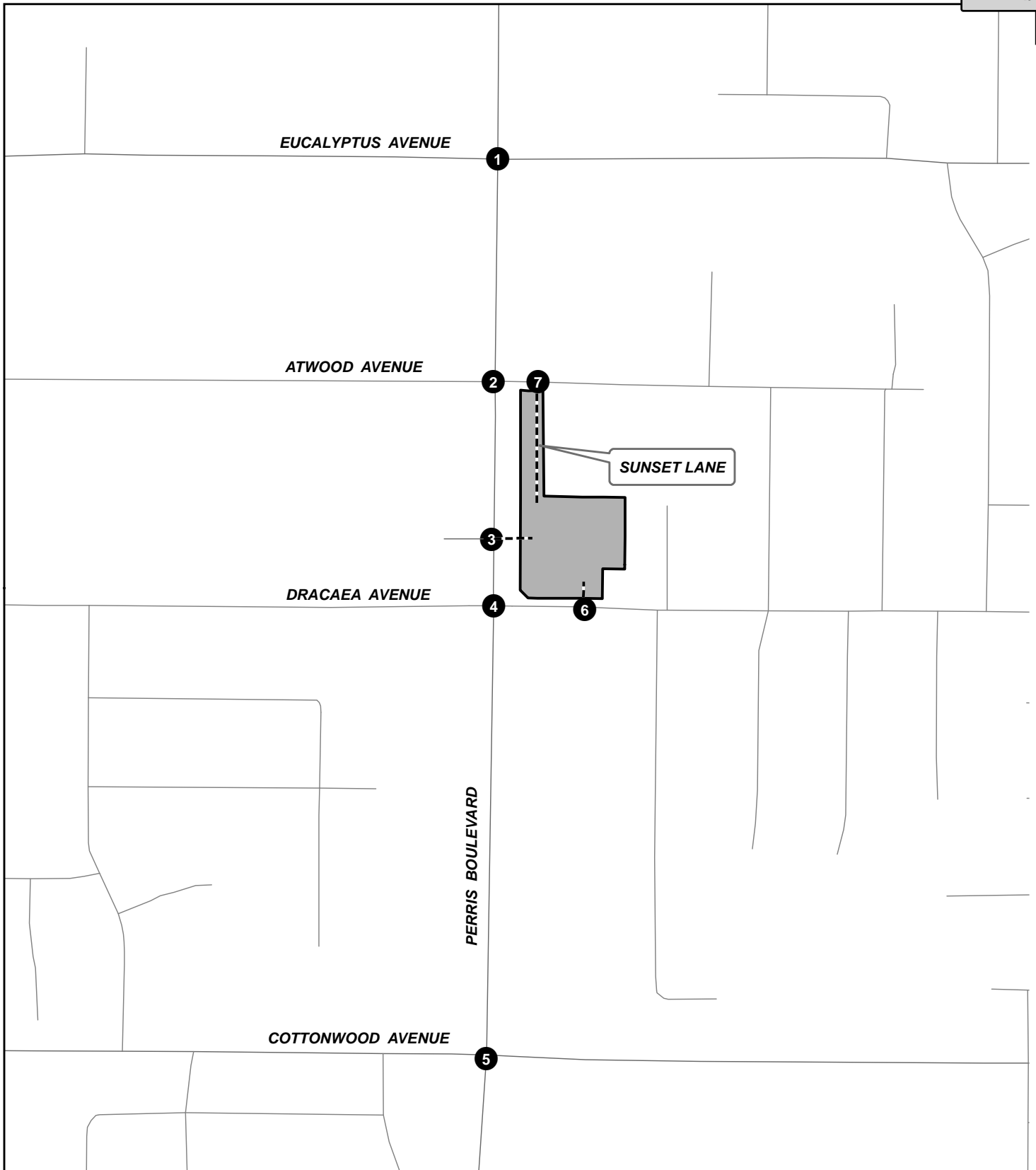


Perris/Dracaea Commercial Project
Transportation Impact Analysis

Conceptual Site Plan

SOURCE: Cadence Capital Investments LLC; September, 2020
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

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a



Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



LEGEND

-  Project Site
-  Driveway
-  Study Intersection



SOURCE: ESRI Streetmap, 2013

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FIGURE 2-:

Perris/Dracaea Commercial Project
 Transportation Impact Analysis
 Study Area Intersections

3.0 LEVEL OF SERVICE ANALYSIS METHODOLOGY

3.1 LEVEL OF SERVICE DEFINITIONS

LOS can be characterized for the whole intersection, each intersection approach, and by each lane group. Control delay alone is used to characterize LOS for the entire intersection. Control delay quantifies the increase in travel time due to the traffic signal control, and is a surrogate measure of driver discomfort and fuel consumption.

A complete description of the meaning of LOS can be found in the Transportation Research Board Special Report 209, *Highway Capacity Manual* (HCM). The HCM establishes LOS A through F for intersections. A description of LOS for signalized and unsignalized intersections is summarized in Table 3-A. A description of LOS for roadway segments is summarized in Table 3-B.

Table 3-C shows the LOS criteria for unsignalized and signalized intersections. Table 3-D summarizes the LOS criteria used to evaluate roadway segments based on the daily capacity for each functional classification as per the City's TIA guidelines. The daily traffic volumes represent the total vehicles (both directions) traveling on a roadway segment within 24 hours.

For all study area intersections, the *Highway Capacity Manual 6th Edition* (HCM 6) analysis methodologies were used to determine intersection LOS. Intersection LOS was calculated using Synchro 10 software, which uses the HCM 6 methodologies.

3.2 LEVEL OF SERVICE PROCEDURES AND STANDARDS

Study intersections and roadway segments analyzed in this report are under the jurisdiction of the City of Moreno Valley. The City uses both LOS C and LOS D as its minimum level of service criteria for intersections and roadway segments. As stated in both the *City of Moreno Valley General Plan* (dated July 11, 2006) and the City's TIA Guidelines, LOS D is applicable to intersections and roadway segments adjacent to employment-generating land uses while LOS C is applicable to all other areas. Figure 3-1 illustrates the LOS standards for intersections and roadway segments within the City.

At study intersections and roadway segments under the jurisdiction of the City, the operating requirements pursuant to the City's TIA guidelines are as follows:

3.2.1 Study Intersections, Signalized

- Any signalized study intersection operating at acceptable LOS without project traffic at which the addition of project traffic causes the intersection to degrade to unacceptable LOS shall identify improvements to provide acceptable LOS.
- Any signalized study intersection that is operating at unacceptable LOS without project traffic where the project increases delay by 5.0 or more seconds shall identify improvements to offset the increase in delay.

3.2.2 Study Intersections, Unsignalized

An operational improvement is required if the study determines that either section a) or both sections b) and c) occur:

- a) The addition of project-related traffic causes the intersection to degrade from an acceptable LOS to unacceptable LOS.

OR

- b) The project adds 5.0 seconds or more of delay to an intersection that is already projected to operate without project traffic at unacceptable LOS,

AND

- c) The intersection meets the peak hour traffic signal warrant after the addition of project traffic.

If the conditions above are satisfied, improvements should be identified that achieve the following:

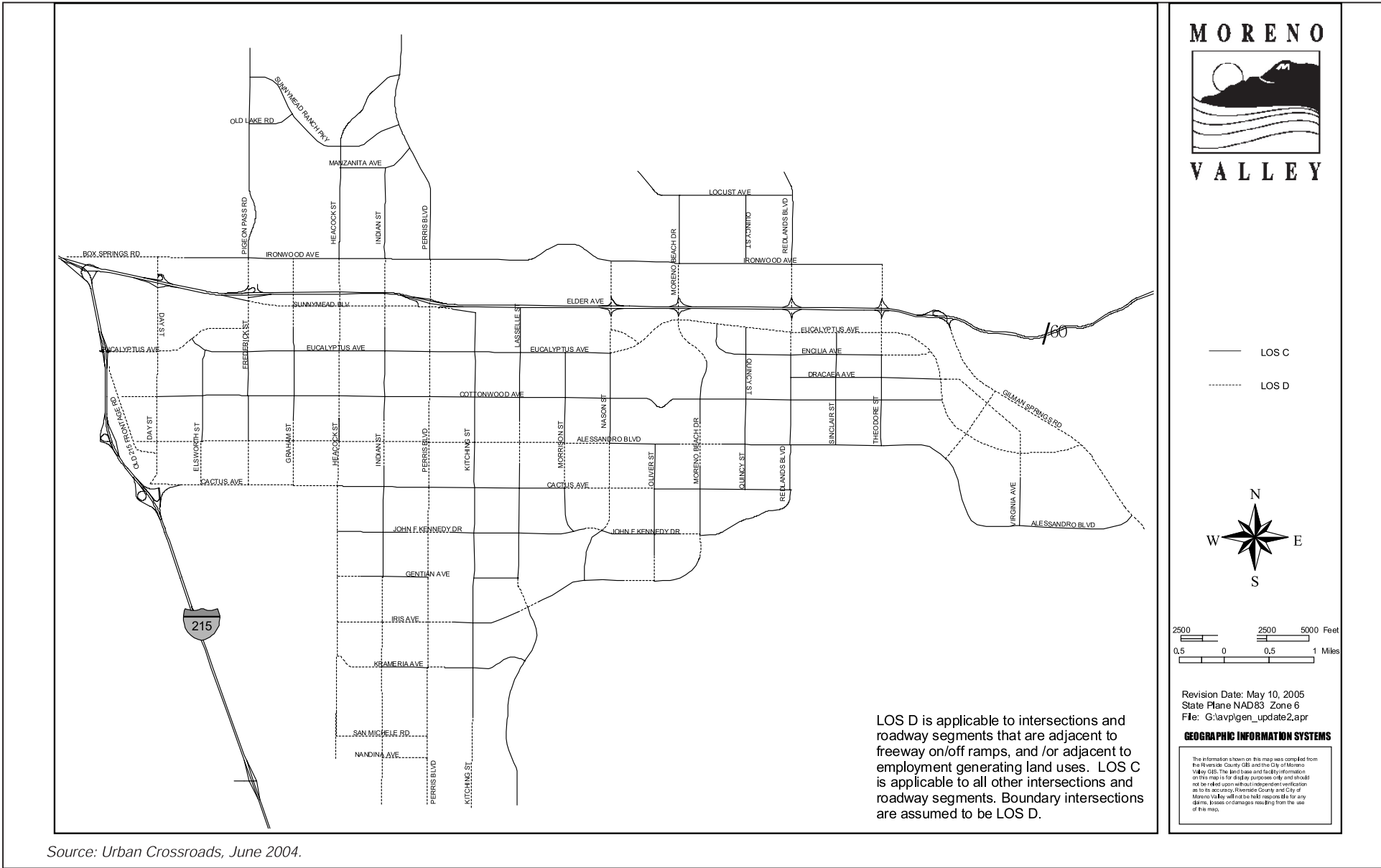
- LOS D or better for case a) above or to pre-project LOS and delay for case b) and c) above.

3.2.3 Roadway Segments

- Any study roadway segment operating at acceptable LOS without project traffic on which the addition of project traffic causes the segment to degrade to unacceptable LOS should identify improvements to achieve acceptable LOS.
- Any roadway segment that operates at unacceptable LOS in the no project scenario where the project adds traffic in excess of 5 percent of the roadway capacity (i.e., a V/C ratio increase of 0.05) should identify improvements to add capacity to the segment.

3.3 LIST OF CHAPTER 3.0 FIGURES AND TABLES

- Figure 3-1: City of Moreno Valley LOS Standards
- Table 3-A: Intersection Level of Service Definitions
- Table 3-B: Roadway Segments/Urban Segments Level of Service Definitions
- Table 3-C: Level of Service Criteria for Unsignalized and Signalized Intersections
- Table 3-D: Roadway Segment Capacity and Levels of Service



Source: Urban Crossroads, June 2004.



FIGURE 3-1

Perris/Dracaea Commercial Project
Transportation Impact Analysis

City of Moreno Valley LOS Standards

Table 3-A: Intersection Level of Service Definitions

LOS	Description
A	Traffic operations with a control delay of 10 seconds per vehicle or less and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is low and either progression is exceptionally favorable or the cycle length is very short. If LOS A is the result of favorable progression, most vehicles arrive during the green indication and travel through the intersection without stopping.
B	Traffic operations with control delay between 10 seconds per vehicle and 20 seconds per vehicle and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is low and either progression is highly favorable or the cycle length is short. More vehicles stop than with LOS A.
C	Traffic operations with control delay between 20 and 35 seconds per vehicle and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when progression is favorable or the cycle length is moderate. Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of the insufficient capacity during the cycle) may begin to appear at this level. The number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.
D	Traffic operations with control delay between 35 and 55 seconds per vehicle and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is high and either progression is ineffective or the cycle length is long. Many vehicles stop and individual cycle failures are noticeable.
E	Traffic operations with control delay between 55 and 80 seconds per vehicle and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when volume-to-capacity ratio is high, progression is unfavorable, and the cycle length is long. Individual cycle failures are frequent.
F	Traffic operations with control delay exceeding 80 seconds per vehicle or a volume-to-capacity ratio greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue.

Source: *Highway Capacity Manual* (6th Edition)**Table 3-B: Roadway Segment Level of Service Definitions**

LOS	Description
A	Describes primarily free-flow operation. Vehicles are completely unimpeded in their ability to maneuver within the traffic stream. Control Delay at the boundary intersection is minimal. The travel speed exceeds 80% of the base free-flow speed, and the volume-to-capacity ratio is no greater than 1.0.
B	Describes reasonably unimpeded operation. The ability to maneuver within the traffic stream is only slightly restricted, and control delay at the boundary is not significant. The travel speed is between 67% and 80% of the base free-flow speed, and the volume-to-capacity ratio is no greater than 1.0.
C	Describes stable operation. The ability to maneuver and change lanes at mid-segment locations may be more restricted than at LOS B. Longer queues at the boundary intersection may contribute to lower travel speeds. The travel speed is between 50% and 67% of the base free-flow speed, and the volume-to-capacity ratio is no greater than 1.0.
D	Indicates a less stable condition in which small increases in flow may cause substantial increases in delay and decreases in travel speed. This operation may be due to adverse signal progression, high volume, or inappropriate signal timing at the boundary intersections. The travel speed is between 40% and 50% of the base free-flow speed, and the volume-to-capacity ratio is no greater than 1.0.
E	Characterized by unstable operation and significant delay. Such operations may be due to some combination of adverse progression, high volume, and inappropriate signal timing at the boundary intersections. The travel speed is between 30% and 40% of the base free-flow speed, and the volume-to-capacity ratio is no greater than 1.0.
F	Characterized by flow at extremely low speed. Congestion is likely occurring at the boundary intersections, as indicated by high delay and extensive queuing. The travel speed is between 30% or less of the base free-flow speed, and the volume-to-capacity ratio is greater than 1.0.

Source: *Highway Capacity Manual* (6th Edition)

Table 3-C: Level of Service Criteria for Unsignalized and Signalized Intersections

Level of Service	Unsignalized Intersection Average Delay per Vehicle (sec.)	Signalized Intersection Average Delay per Vehicle (sec.)
A	≤ 10	≤ 10
B	> 10 and ≤ 15	> 10 and ≤ 20
C	> 15 and ≤ 25	> 20 and ≤ 35
D	> 25 and ≤ 35	> 35 and ≤ 55
E	> 35 and ≤ 50	> 55 and ≤ 80
F	> 50	> 80

Source: *Highway Capacity Manual* (6th Edition)**Table 3-D: Roadway Segment Capacity and Levels of Service**

Type of Roadway	Level of Service*				
	A	B	C	D	E
Six-Lane Divided Arterial	33,900	39,400	45,000	50,600	56,300
Four-Lane Divided Arterial	22,500	26,300	30,000	33,800	37,500
Four-Lane Undivided Arterial	15,000	17,500	20,000	22,500	25,000
Two-Lane Industrial Collector	7,500	8,800	10,000	11,300	12,500
Two-Lane Undivided Residential	N/A	N/A	N/A	N/A	2,000

*Maximum Average Daily Traffic (ADT)

Source: City of Moreno Valley Transportation Engineering Division *Transportation Impact Analysis Preparation Guide for Vehicle Miles Traveled and Level of Service Assessment*, June 2020

4.0 EXISTING CONDITIONS

4.1 EXISTING ROADWAY NETWORK

Figure 4-1 illustrates existing study intersection geometrics and traffic control. Within the City of Moreno Valley, all major roadways are classified based on the City's Circulation Plan. Figure 4-2 illustrates roadway classifications defined by the City's Circulation Plan. Table 4-A summarizes the classifications and the number of mid-block arterial lanes on major roadways in the TIA study area. Following is a brief description of these roadways:

- **Perris Boulevard:** Based on the City's Circulation Plan, Perris Boulevard is designated as a "Divided Arterial – 6 lane" in the study area. Between Eucalyptus Avenue and Cottonwood Avenue, Perris Boulevard is a four-lane divided road with a two-way-left-turn median. There are no bike lanes in this segment. There is also no provision for on-street parking.
- **Atwood Avenue:** Atwood Avenue is not designated in the City's Circulation Plan. Between Perris Boulevard and Birchwood Drive, Atwood Avenue is a two-lane undivided road. There are no bike lanes in this segment. There is also no provision for on-street parking.
- **Dracaea Avenue:** Dracaea Avenue is not designated in the City's Circulation Plan. Between Perris Boulevard and Birchwood Drive, Dracaea Avenue is a two-lane undivided road. There are no bike lanes in this segment. However, there is provision for on-street parking on both sides of the segment.

4.2 PROJECT DESIGN FEATURES

The following project design features will be implemented as improvements for the striping plan along project frontage:

- Remove the existing TWLTL on Perris Boulevard, from south of Pedro's Taco Shop Driveway to Dracaea Avenue;
- Replace the TWLTL with the raised median along the entire project frontage on Perris Boulevard extending up to south of the Pedro's Taco Shop driveway;
- Create left-turn pockets in the raised median for the northbound and southbound movements at the intersection of Perris Boulevard/Atwood Avenue, with storage lengths of 150 ft and 125 ft, respectively;
- Extend the southbound left-turn pocket at the intersection of Perris Boulevard/Dracaea Avenue by 5 ft; and
- Add a bus bay on the east side of Perris Boulevard, north of the intersection of Perris Boulevard/Auto Center Driveway-Project Driveway 1.

Due to the proposed raised median, Auto Center Driveway will operate as a RIRO driveway. Additionally, eastbound and westbound left-turn and through movements will be prohibited at the intersection of Perris Boulevard/Atwood Avenue under with project conditions. Figure 4-3 illustrates

the project design features. Figure 4-4 illustrates study intersection geometrics and traffic control under with project conditions.

4.3 EXISTING BICYCLE FACILITIES

Figure 4-5 illustrates the existing and proposed bikeways in the City, per the City of Moreno Valley Bicycle Master Plan, dated November 2014. At present, bike lanes do not exist along Perris Boulevard or Dracaea Avenue within the study area. However, a bicycle boulevard has been proposed along Dracaea Avenue between Elsworth Street and Nason Street in the City's Bicycle Master Plan. No bicycle facility has been recommended along Perris Boulevard.

4.4 EXISTING TRANSIT FACILITIES

Riverside Transit Agency (RTA) is the Consolidated Transportation Service Agency for western Riverside County and is responsible for coordinating transit services throughout the approximately 2,500-square mile service area. RTA provides both local and regional services throughout the region with 33 fixed routes, five CommuterLink express routes, and Dial-A-Ride services using 334 vehicles. RTA Local bus routes 18, 19, 19A, 20, and 31 operate within the study area, connecting Moreno Valley to neighboring communities including Perris, Riverside, Banning, Beaumont, and San Jacinto.

4.5 EXISTING PEDESTRIAN FACILITIES

The City of Moreno Valley considers other methods and modes of transportation to improve mobility around the region while creating environmental benefits, health benefits, and economic benefits for the City. Figure 4-6 illustrates the Master Plan of Trails within the City and surrounding region. These trails include bikeways and multiuse trails readily available and planned for both pedestrian and cyclist usage.

4.6 EXISTING TRAFFIC VOLUMES

For all intersections and roadway segments, existing traffic volumes are based on counts collected by Counts Unlimited in May 2019. Daily tube counts were collected for roadway segments while a.m. and p.m. peak hour turning-movement counts were collected at study intersections. Detailed count sheets are included in Appendix B.

Counts were not collected at the intersection of Sunset Lane/Atwood Avenue. At present, there are four single-family houses located on Sunset Lane. A separate trip generation was developed for these houses using the rates obtained from the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (10th Edition). As shown in Table 4-B, these residential units generate 3 trips in both the a.m. and p.m. peak hours and 38 daily trips. At the intersection of Sunset Lane/Atwood Avenue, all the inbound trips from these residential units were assumed to make an eastbound right, while all the outbound trips were assumed to make a northbound left. The eastbound and westbound through volumes at the intersection were obtained based on balance of flow using volumes from the adjacent intersection of Perris Boulevard/Atwood Avenue. This volume development methodology has been approved by City staff. Additionally, eastbound and westbound through volumes at the future intersection of Project Driveway 2/Dracaea Avenue were obtained

based on balance of flow using volumes from the adjacent intersection of Perris Boulevard/Dracaea Avenue.

Vehicle classification counts were conducted at the following intersections: Perris Boulevard/Eucalyptus Avenue, Perris Boulevard/Dracaea Avenue, and Perris Boulevard/Cottonwood Avenue. At these locations, counts were converted to Passenger Car Equivalent (PCE) volumes. The concept of PCEs accounts for the larger impact of trucks on traffic operations. It does so by assigning each type of truck a PCE factor that represents the number of passenger vehicles that could travel through an intersection in the same time that a particular type of truck could. Pursuant to the City's TIA guidelines, PCE volumes at study intersections were computed using a factor of 1.5 for 2-axle trucks, 2.0 for 3-axle trucks, and 3.0 for trucks with four or more axles.

The percentage of trucks at the remaining study intersections without classification counts was determined based on truck percentages derived from adjacent intersections with classification counts. At these locations, truck PCE volumes were computed using a PCE factor of 2.0 for all trucks, consistent with the HCM 6 methodologies.

Figure 4-7 illustrates existing peak hour traffic volumes at study intersections. Table 4-C summarizes the existing roadway segment daily traffic volumes.

4.7 EXISTING LEVELS OF SERVICE

4.7.1 Study Intersections

Figure 4-1 illustrates existing study intersection geometrics and traffic control. An intersection LOS analysis was conducted for existing conditions using the methodologies previously discussed. Table 4-D summarizes the results of this analysis and shows that the following intersection is currently operating at an unsatisfactory LOS:

- Perris Boulevard/Atwood Avenue (both a.m. and p.m. peak hours).

All other study intersections are forecast to operate at a satisfactory LOS.

4.7.2 Roadway Segments

A roadway segment LOS analysis was conducted for existing conditions using the methodologies previously discussed. Table 4-E summarizes the results of this analysis and shows that all roadway segments are currently operating at a satisfactory LOS.

4.8 LIST OF CHAPTER 4.0 FIGURES AND TABLES

- Figure 4-1: Existing Study Intersection Geometrics and Traffic Control
- Figure 4-2: City of Moreno Valley General Plan Street Classifications
- Figure 4-3: Conceptual Striping Plan for Proposed Improvements
- Figure 4-4: Study Intersection Geometrics and Traffic Control under with Project Conditions
- Figure 4-5: City of Moreno Valley Bicycle Lane Network Plan

- Figure 4-6: City of Moreno Valley Master Plan of Trails
- Figure 4-7: Existing Peak Hour Traffic Volumes
- Table 4-A: Existing Roadway Segment Classification
- Table 4-B: Residential Uses Trip Generation
- Table 4-C: Existing Roadway Segment Daily Traffic Volumes
- Table 4-D: Existing Intersection Levels of Service
- Table 4-E: Existing Roadway Segment Levels of Service

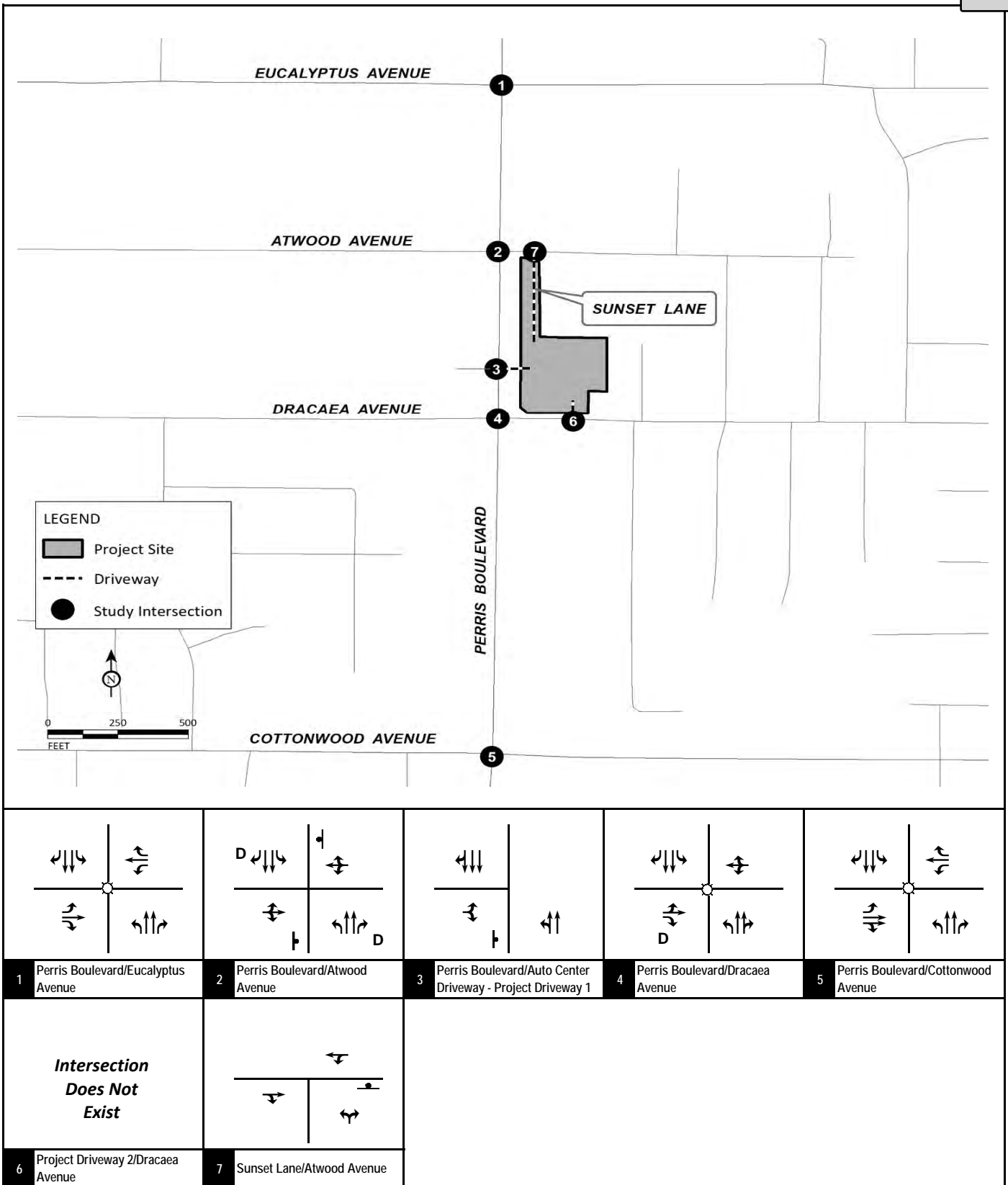


FIGURE 4-1

LSA

Legend

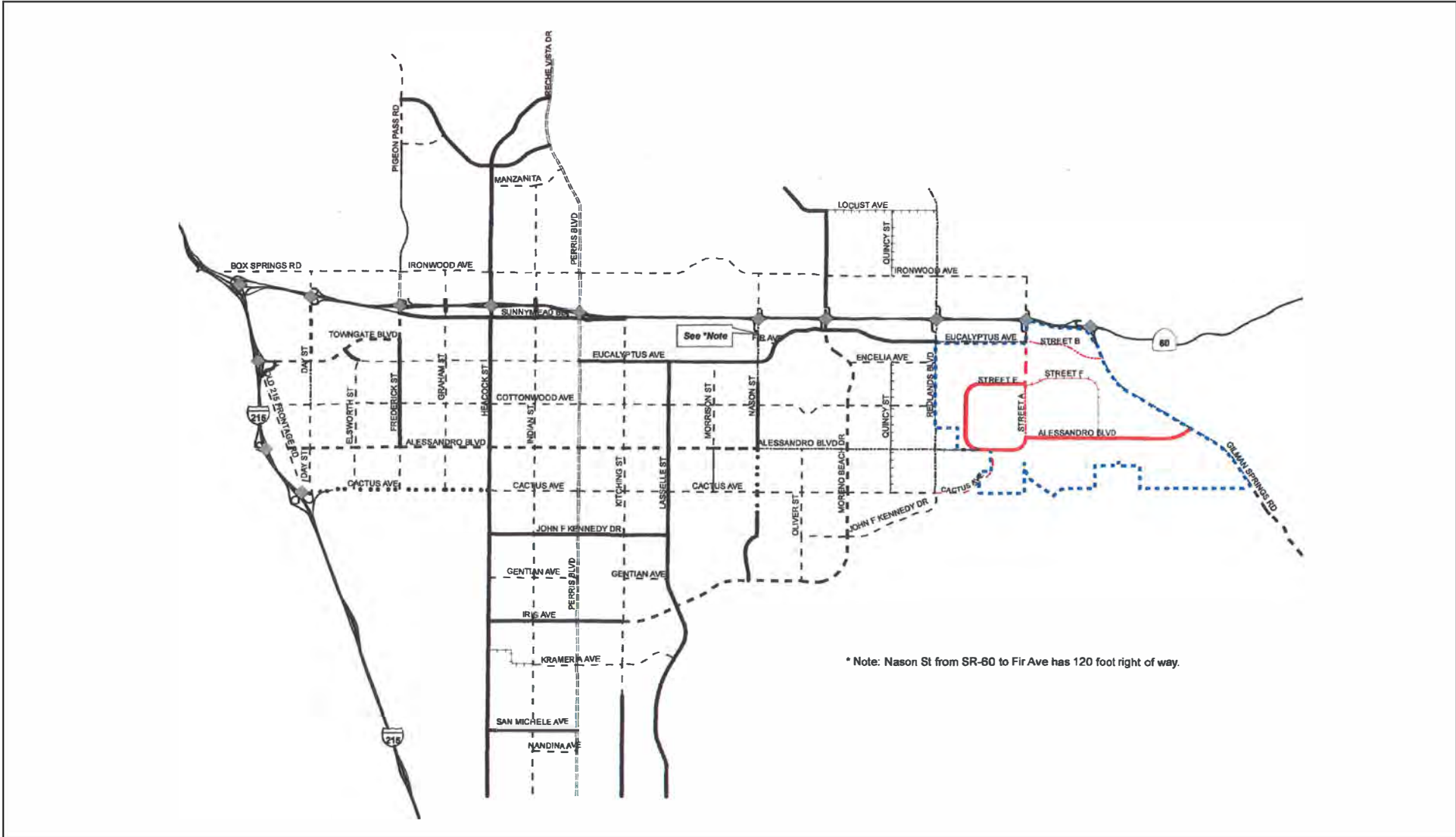
□ Signal

→ Stop Sign

D De Facto Right Turn

*Perris/Dracaea Commercial Project
Transportation Impact Analysis*

Existing Study Intersection Geometrics and Traffic Control



LSA FIGURE 4-2



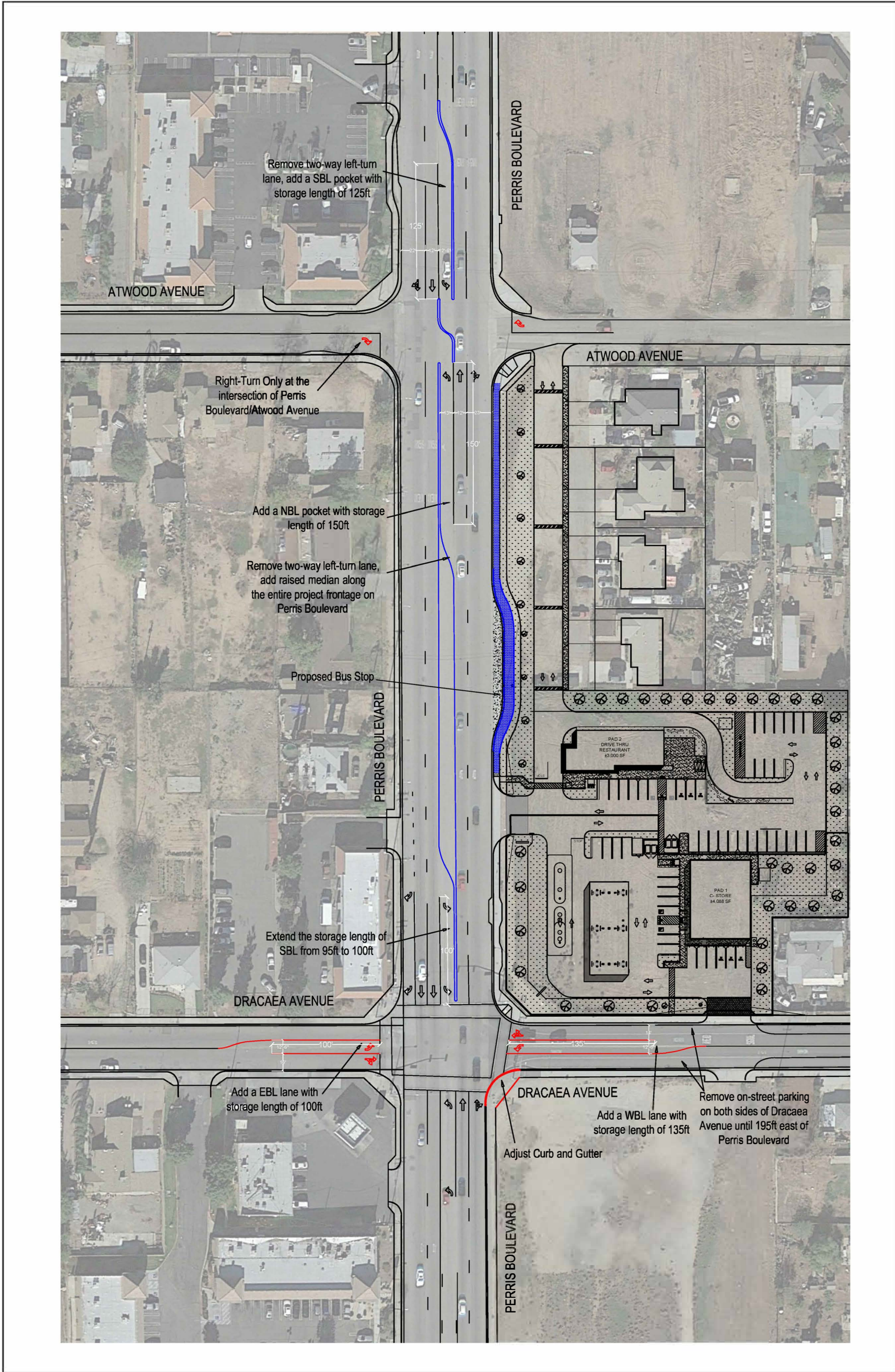
- Legend**
- Freeway
 - Divided Major Arterial
 - Divided Major Arterial - Reduced Cross Section
 - Divided Arterial - 6 lane
 - Divided Arterial - 4 lane
 - Arterial
 - Minor Arterial
 - Minor Arterial - Pigeon Pass Cross Section
 - Collector
 - Freeway Overpass
 - Freeway Interchange

*Perris/Dracaea Commercial Project
Transportation Impact Analysis*

City of Moreno Valley General Plan Street Classifications

SOURCE: City of Moreno Valley Staff
I:\CAQ1901\Reports\Traffic\fig4-2_MorenoValley_StreetClassification.ai (09/02/2020)

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a



Attachment: Exhibit A to Resolution No. 2020- Initial Study MND (4210 : Conditional Use Permits for a Drive Thru Restaurant, Fueling Station and Convenience Store)

LSA

LEGEND

- Existing
- Project Design Features
- Proposed Improvements

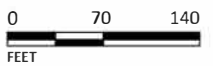


FIGURE 4-3

Perris/Dracaea Commercial Project
Transportation Impact Analysis

Conceptual Striping Plan for the Proposed Improvements

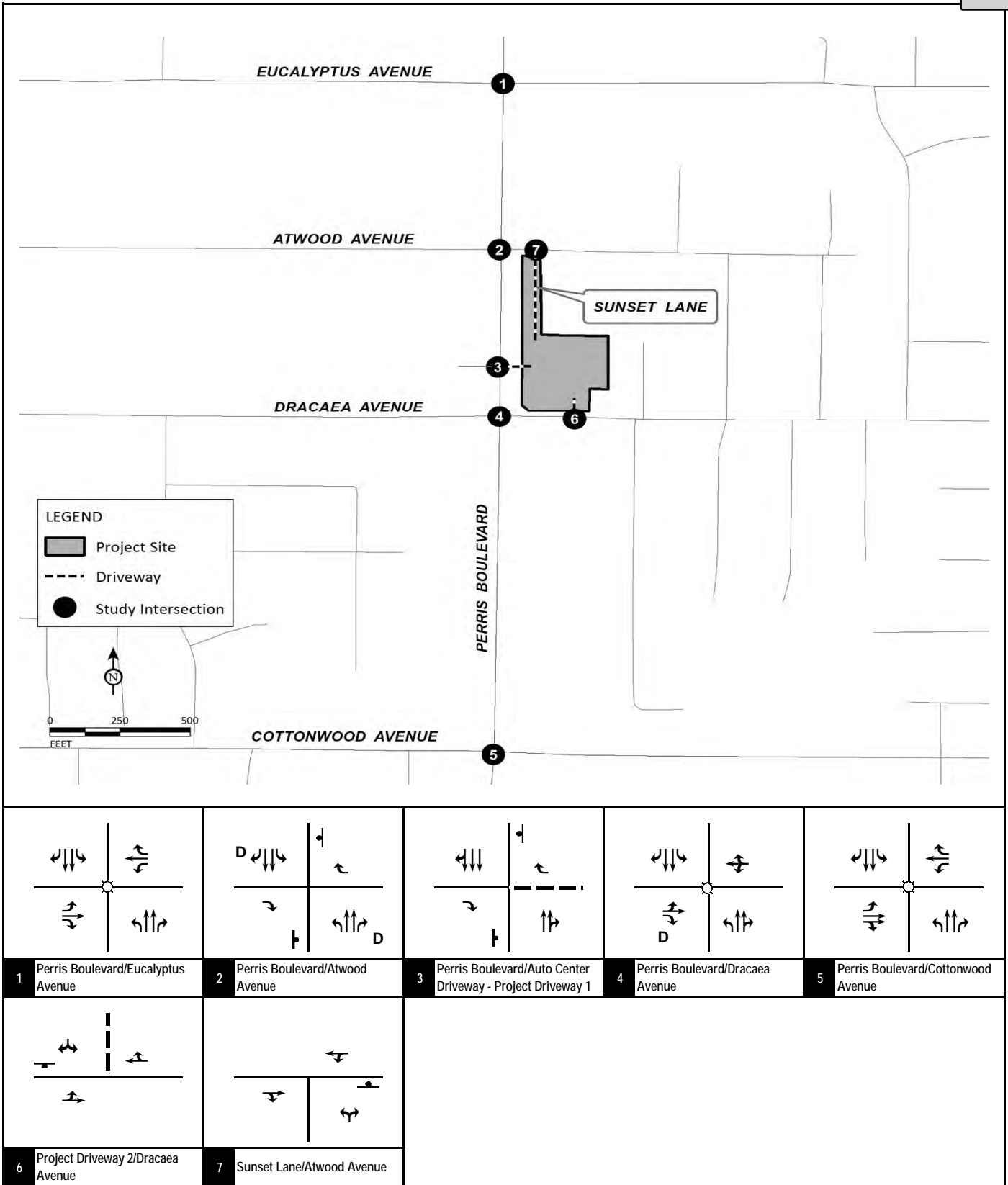


FIGURE 4-4

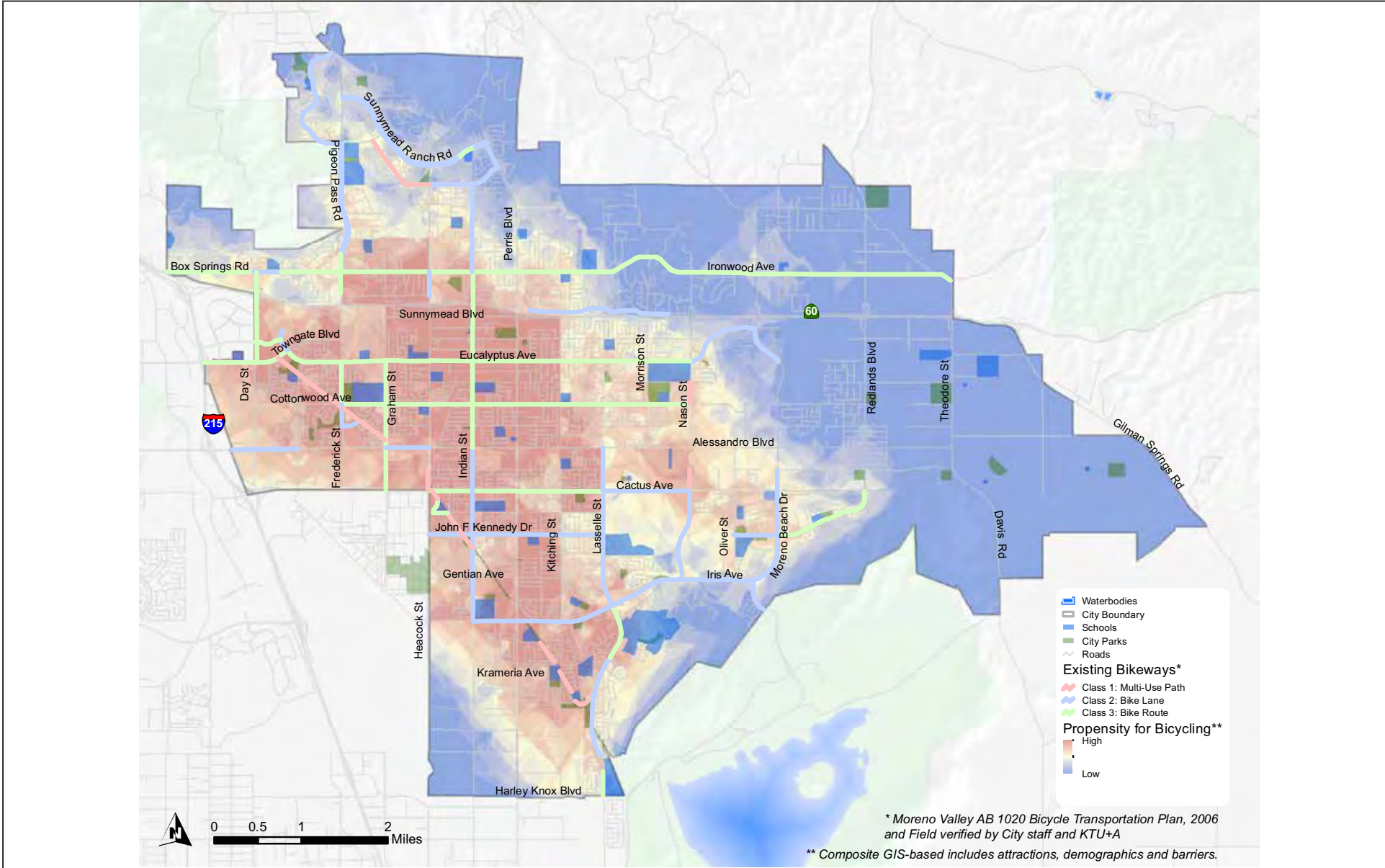
LSA

Legend

- Signal
- ⊣ Stop Sign
- Project Driveway
- D De Facto Right Turn

*Perris/Dracaea Commercial Project
Transportation Impact Analysis*

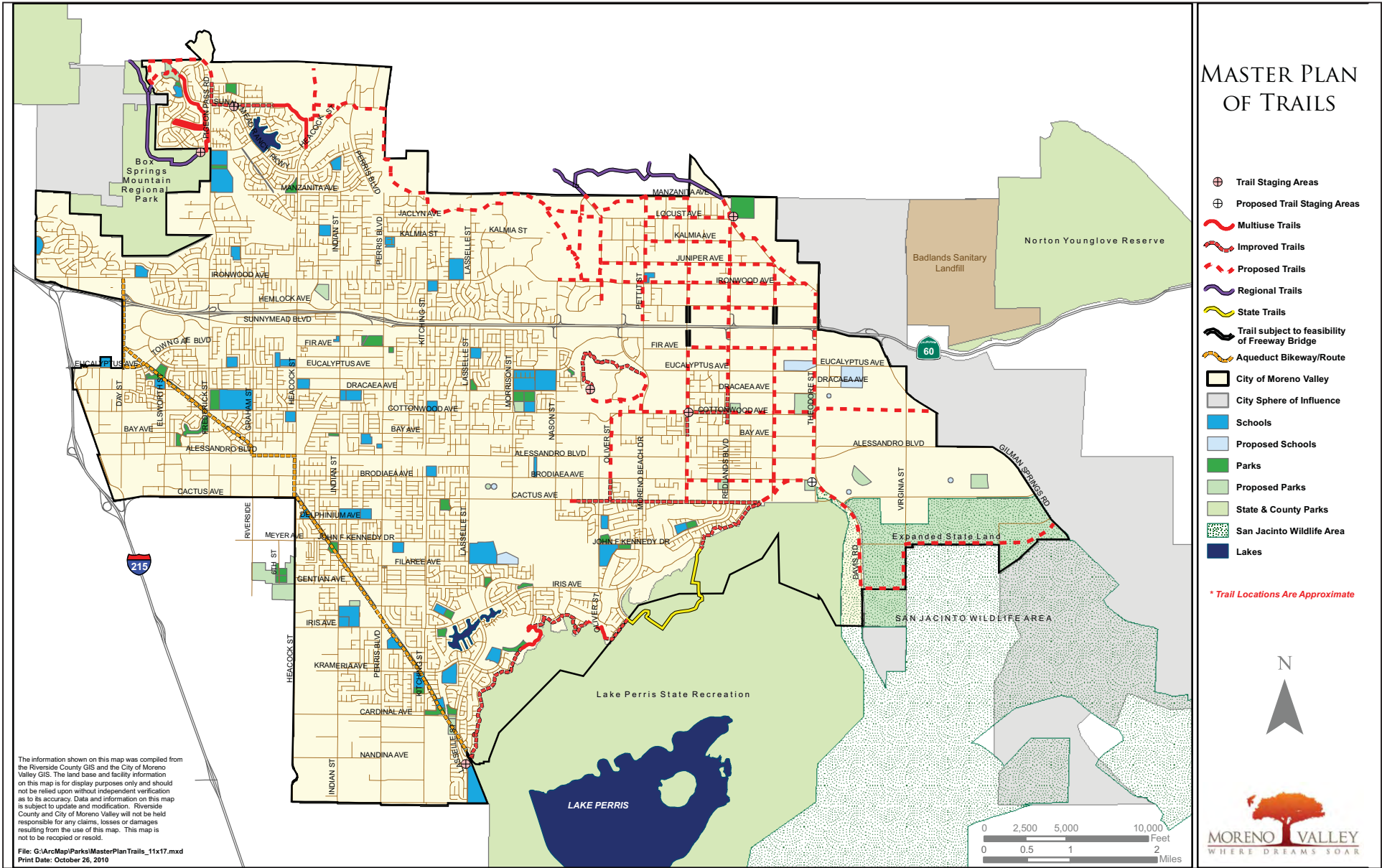
Study Intersections Geometrics and Traffic Control under with Project Conditions



LSA FIGURE 4-5

Perris/Dracaea Commercial Project
Transportation Impact Analysis

City of Moreno Valley Bicycle Lane Network Plan



The information shown on this map was compiled from the Riverside County GIS and the City of Moreno Valley GIS. The land base and facility information on this map is for display purposes only and should not be relied upon without independent verification as to its accuracy. Data and information on this map is subject to update and modification. Riverside County and City of Moreno Valley will not be held responsible for any claims, losses or damages resulting from the use of this map. This map is not to be recycled or resold.

File: G:\ArcMap\Parks\MasterPlanTrails_11x17.mxd
Print Date: October 26, 2010

LSA

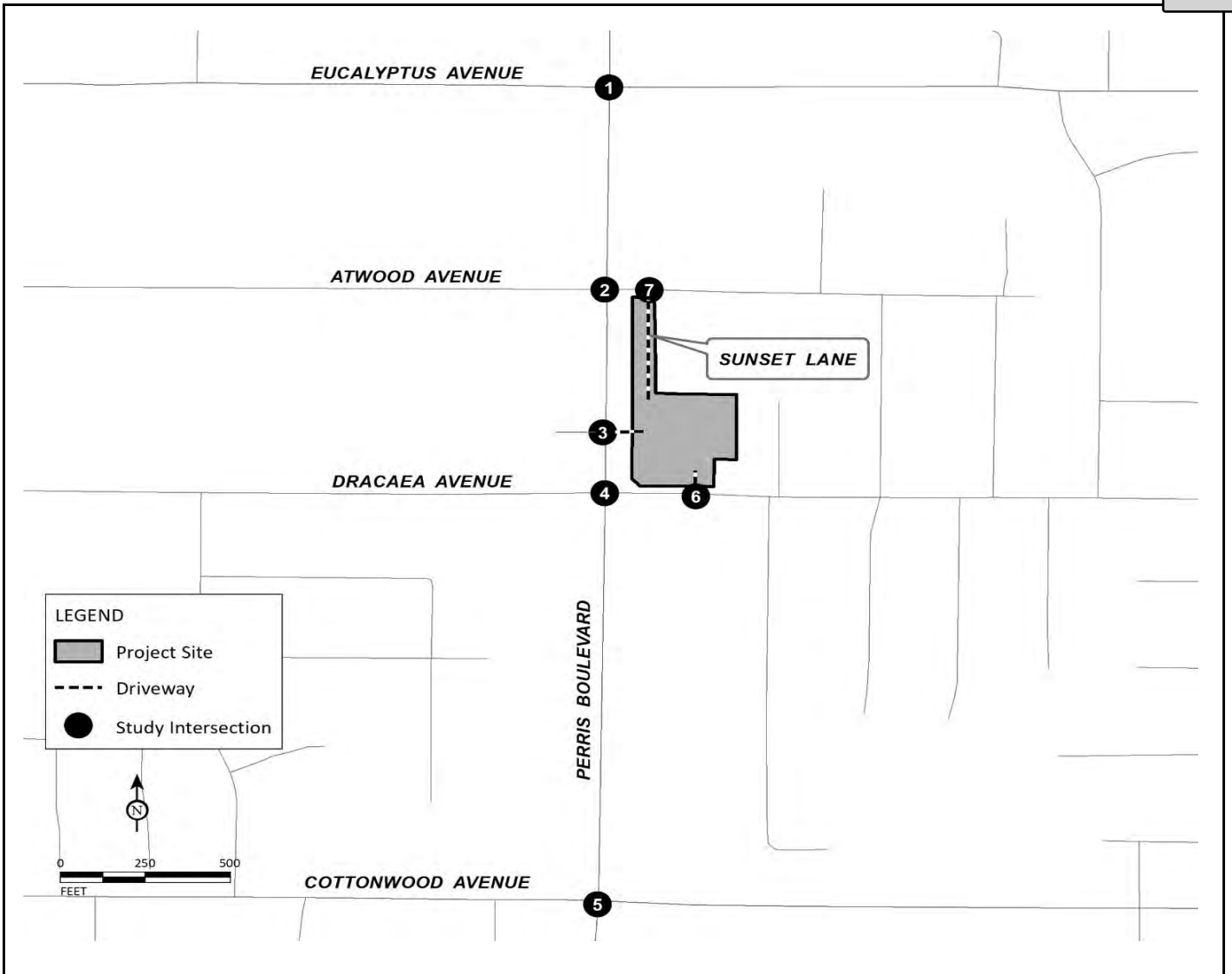
FIGURE 4-6

Perris/Dracaean Commercial Project
Transportation Impact Analysis

City of Moreno Valley Master Plan of Trails

SOURCE: Moreno Valley Master Plan Trails Map, 2010

I:\CAQ1901\Reports\Traffic\fig3-5_TrailsMasterPlan.cdr (09/01/2020)



<pre> 32 / 45 1006 / 1063 72 / 97 194 / 108 217 / 127 83 / 52 22 / 51 73 / 189 56 / 79 50 / 51 1002 / 978 58 / 59 1002 / 978 58 / 59 </pre>	<pre> 16 / 15 1135 / 1120 16 / 42 9 / 13 2 / 1 4 / 3 13 / 27 1 / 1 37 / 36 52 / 43 1067 / 1059 3 / 1 </pre>	<pre> 7 / 2 1157 / 1151 1 / 0 1137 / 1105 </pre>	<pre> 86 / 68 1039 / 1036 33 / 47 67 / 47 96 / 73 70 / 32 64 / 69 71 / 102 52 / 22 50 / 29 1006 / 988 20 / 36 </pre>	<pre> 109 / 83 930 / 964 131 / 98 159 / 87 276 / 134 61 / 23 87 / 92 224 / 197 54 / 75 59 / 64 870 / 931 49 / 31 </pre>
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>Intersection Does Not Exist</p>		<pre> 13 / 16 19 / 42 1 / 2 2 / 1 </pre>		
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 4-7



XXXX / YYYY
AM / PM Peak Hour PCE Volumes

Perris/Dracaea Commercial Project
Transportation Impact Analysis
Existing Peak Hour Traffic Volumes

Table 4-A - Existing Roadway Segment Classification

Roadway	#	Segment	Existing Condition Number of Lanes	Jurisdiction	General Plan Classification ¹
Perris Boulevard	1	between Eucalyptus Avenue and Atwood Avenue	4	Moreno Valley	Divided Arterial - 6 Lane
	2	between Atwood Avenue and Dracaea Avenue	4	Moreno Valley	Divided Arterial - 6 Lane
	3	between Dracaea Avenue and Cottonwood Avenue	4	Moreno Valley	Divided Arterial - 6 Lane
Dracaea Avenue	4	between Perris Boulevard and Birchwood Drive	2	Moreno Valley	Not classified in the General Plan

Notes:

¹ Classifications for all segments obtained from the City of Moreno Valley *Circulation Plan* obtained from City Staff.



Table 4-B - Residential Uses Trip Generation

Land Use	Units	A.M. Peak Hour			P.M. Peak Hour			Daily
		In	Out	Total	In	Out	Total	
Residential Projects	4 DU							
Trips/Unit ¹		0.19	0.55	0.74	0.62	0.37	0.99	9.44
Trip Generation		1	2	3	2	1	3	38

Note:

DU = Dwelling Units

¹ Rates from the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (10th Edition), Land Use 210 - "Single-Family Detached Housing", Setting/Location - "General Urban/Suburban."

Table 4-C - Existing Roadway Segment Daily Traffic Volumes

Roadway	#	Segment	Existing ADT	Net Project Trips	Pass-by Trips	Existing With Project ADT
Perris Boulevard	1	between Eucalyptus Avenue and Atwood Avenue	31,829	870	0	32,699
	2	between Atwood Avenue and Dracaea Avenue	30,997	608	(323)	31,282
	3	between Dracaea Avenue and Cottonwood Avenue	29,577	1,044	0	30,621
Dracaea Avenue	4	between Perris Boulevard and Birchwood Drive	4,912	1,996	343	7,251



Table 4-D - Existing Intersection Levels of Service

Intersection	Jurisdiction	Control	LOS Standard	Without Project			
				A.M. Peak Hour		P.M. Peak Hour	
				Delay (sec.)	LOS	Delay (sec.)	LOS
1 . Perris Boulevard/Eucalyptus Avenue	Moreno Valley	Signal	D	17.9	B	28.1	C
2 . Perris Boulevard/Atwood Avenue	Moreno Valley	TWSC	D	>100	F *	>100	F *
3 . Perris Boulevard/Auto Center Driveway - Project Driveway 1 ¹	Moreno Valley	OWSC	D	10.6	B	0.0	A
4 . Perris Boulevard/Dracaea Avenue	Moreno Valley	Signal	D	27.0	C	7.6	A
5 . Perris Boulevard/Cottonwood Avenue	Moreno Valley	Signal	D	26.2	C	22.2	C
6 . Project Driveway 2/Dracaea Avenue	Moreno Valley	OWSC	D	Does not Exist		Does not Exist	
7 . Sunset Lane/Atwood Avenue	Moreno Valley	OWSC	D	8.7	A	8.8	A

Notes:

OWSC = One-Way Stop Control; TWSC = Two-Way Stop Control

Delay = Average control delay in seconds (For OWSC and TWSC intersections, reported delay is for worst-case movement).

* Exceeds LOS Standard

¹ This intersection operates as an OWSC intersection under without project conditions. Under without project conditions, Synchro did not report a delay at this intersection during the p.m. peak hour as there are no conflicting movements.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Table 4-E - Existing Roadway Segment Levels of Service

Roadway Segment	Classification ¹	Roadway Capacity ²	Without Project		
			Daily Volume	V/C Ratio	LOS
Segments on Perris Boulevard					
1 . between Eucalyptus Avenue and Atwood Avenue	Four Lane Divided Arterial	37,500	31,900	0.85	D
2 . between Atwood Avenue and Dracaea Avenue	Four Lane Divided Arterial	37,500	31,000	0.83	D
3 . between Dracaea Avenue and Cottonwood Avenue	Four Lane Divided Arterial	37,500	29,600	0.79	C
Segments on Dracaea Avenue					
4 . between Perris Boulevard and Birchwood Drive	Two Lane Undivided Arterial	12,500	5,000	0.40	A

Notes:

LOS = Level of Service

¹ Classifications for all segments have been obtained from the City of Moreno Valley Transportation Engineering Division *Transportation Impact Analysis Preparation Guide*, dated June 2020.

² Roadway capacities for all segments have been obtained from the City of Moreno Valley Transportation Engineering Division *Transportation Impact Analysis Preparation Guide*, dated June 2020.

5.0 PROJECT TRAFFIC

5.1 PROJECT TRIP GENERATION

The trip generation for the proposed project was developed using rates from the ITE *Trip Generation Manual*, 10th Edition, for Land Uses 945 – “Gasoline/Service Station with Convenience Market” and 937 – “Coffee/Donut Shop with Drive-Through Window.” Table 5-A summarizes the project trip generation.

Gas stations and drive-through restaurants typically draw some of their trips from the adjacent street traffic. Therefore, some of the external project trips will come from the adjacent street traffic. These trips are not actually “new” trips added to the surrounding circulation system. These trips are referred to as “pass-by” trips and are made as intermediate stops en route to a destination without diverting from the main route. For the proposed project, pass-by trips would occur on Perris Boulevard and Dracaea Avenue en route to a final destination.

The percentage of pass-by trips for the gas station has been obtained from the ITE *Trip Generation Handbook* (3rd Edition). However, pass-by trip rates for Land Use 937 – “Coffee/Donut Shop with Drive-Through Window” are not provided in the ITE Manual. Hence, as instructed by City staff, pass-by trips for this land use were not considered. Project pass-by trip assignment has been developed based on traffic counts collected on the adjacent street system. Figure 5-1 illustrates the pass-by trip assignment that has been developed based on existing traffic volumes.

After deducting the pass-by trips from the overall external trips, the net project trip generation has been obtained. As illustrated in Table 5-A, the project is anticipated to generate 324 net trips in the a.m. peak hour, 204 net trips in the p.m. peak hour, and 3,471 net daily trips.

5.2 PROJECT TRIP DISTRIBUTION AND ASSIGNMENT

The distribution of project trips was developed based on the regional roadway network and the locations of residential, employment, and commercial centers in relation to the proposed project. Figure 5-2 illustrates the trip distribution for the proposed project at the study intersections. Trip assignment for project trips is the product of the project trip generation and the trip distribution percentages. Figure 5-3 illustrates the net project trip assignment.

5.3 LIST OF CHAPTER 5.0 FIGURES AND TABLES

- Figure 5-1: Pass-by Trip Assignment
- Figure 5-2: Project Trip Distribution
- Figure 5-3: Net Project Trip Assignment
- Table 5-A: Project Trip Generation

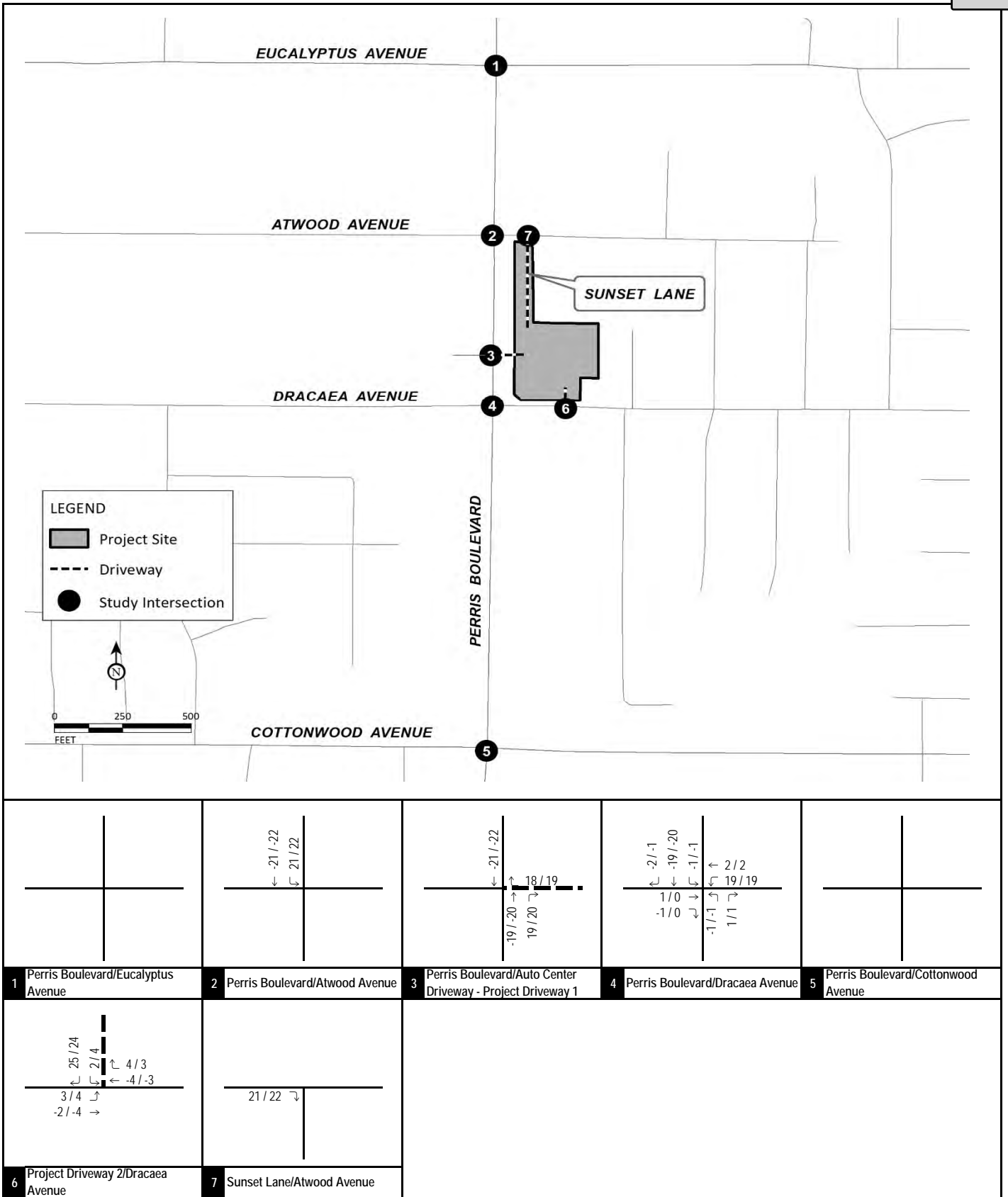


FIGURE 5-1

LSA

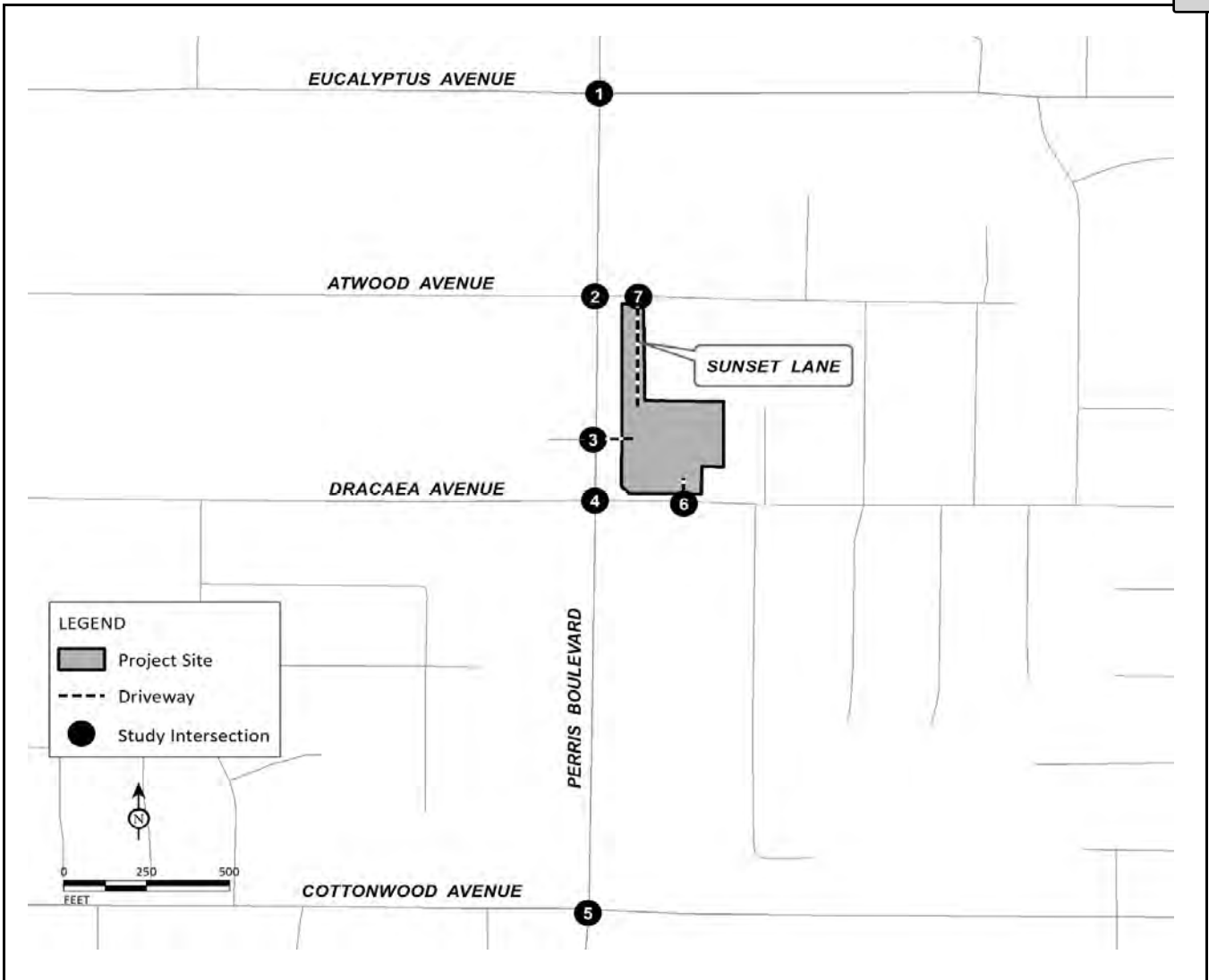
XX / YY

AM / PM Peak Hour Trips

----- Project Driveway

Perris/Dracaea Commercial Project
Transportation Impact Analysis

Pass-by Trip Assignment



<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 5-2

LSA

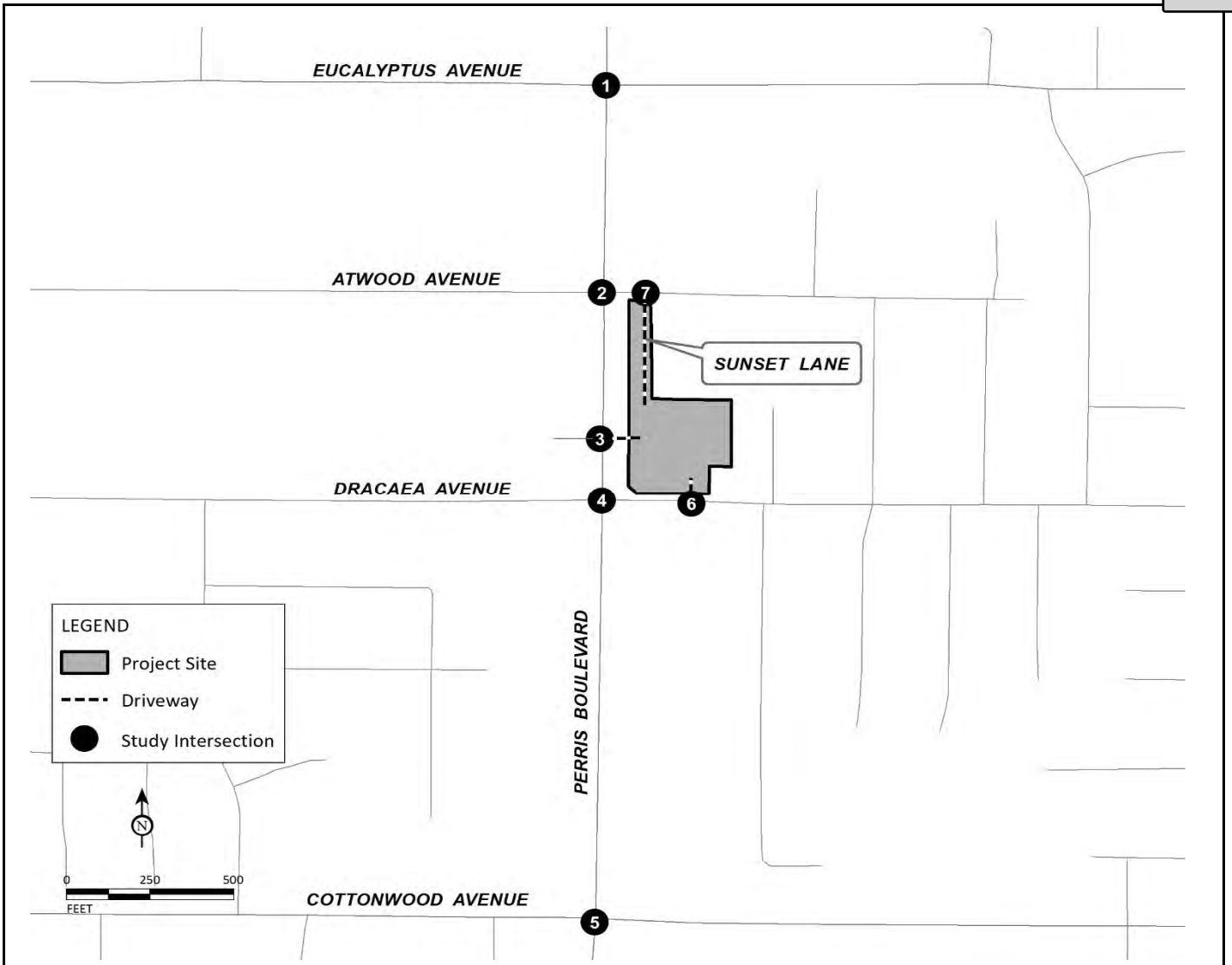
XX% (YY%)

Inbound (Outbound) Trip Distribution

---- Project Driveway

*Perris/Dracaea Commercial Project
Transportation Impact Analysis*

Project Trip Distribution



<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 5-3

LSA

XX / YY
 AM / PM Peak Hour Trips
 - - - - Project Driveway

Perris/Dracaea Commercial Project
 Transportation Impact Analysis
 Net Project Trip Assignment



Table 5-A - Project Trip Generation

Land Use	Units	A.M. Peak Hour			P.M. Peak Hour			Daily
		In	Out	Total	In	Out	Total	
Gas Station with Convenience Store	12 VFP							
Trips/Unit ¹		6.36	6.11	12.47	7.13	6.86	13.99	205.36
Trip Generation		76	73	149	86	82	168	2,464
Pass-by Trips ²		(47)	(45)	(92)	(48)	(46)	(94)	(1,454)
Total Net Trips		29	28	57	38	36	74	1,010
Coffee Shop with Drive Through Window	3.00 TSF							
Trips/Unit ³		45.38	43.61	88.99	21.69	21.69	43.38	820.38
Trip Generation		136	131	267	65	65	130	2,461
	Total Trip Generation	212	204	416	151	147	298	4,925
	Pass-By Trips	(47)	(45)	(92)	(48)	(46)	(94)	(1,454)
	Net Trip Generation	165	159	324	103	101	204	3,471

Note:

VFP = Vehicle Fueling Positions; TSF = Thousand Square Feet

¹ Rates from the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (10th Edition), Land Use 945 - "Gasoline/Service Station with Convenience Market", Setting/Location - "General Urban/Suburban."

² Pass-by rates from the ITE *Trip Generation Handbook* (3rd Edition) for Land Use 945 - "Gasoline/Service Station with Convenience Market." A pass-by rate of 62% was used for the a.m. peak hour and a pass-by rate of 56% was used for the p.m. peak hour. Since there is no data available for daily pass-by trips, the average of a.m. and p.m. pass-by rates was used as the daily pass-by rate.

³ Rates from the ITE *Trip Generation Manual* (10th Edition), Land Use 937 - "Coffee/Donut Shop with Drive-Through Window", Setting/Location - "General Urban/Suburban."

6.0 PROJECT COMPLETION ANALYSIS

6.1 PROJECT COMPLETION (2022) WITHOUT PROJECT TRAFFIC VOLUMES

As approved during the City's scoping agreement process (Appendix A), traffic volumes for project completion without project conditions were developed by applying a 2.0 percent annual growth rate to the existing traffic volumes for all the study intersections and adding trips from cumulative projects in the area.

Information concerning cumulative projects in the vicinity of the proposed project was obtained from the City of Moreno Valley Economic Development website. Figure 6-1 illustrates the cumulative project locations.

The trip generation for cumulative projects was developed using rates from the *ITE Trip Generation Manual* (10th Edition), and from traffic studies of cumulative projects. Table 6-A lists the cumulative projects included in this analysis and shows the cumulative projects are expected to generate 1,184 a.m. peak hour trips, 1,604 p.m. peak hour trips, and 20,032 daily trips.

Project trips for these cumulative projects were assigned to the roadway network based on their locations in relation to surrounding land uses and regional arterials. Figure 6-2 illustrates the total peak hour cumulative project trip assignment at study area intersections. Figure 6-3 illustrates the peak hour traffic volumes at study intersections under project completion without project conditions. Table 6-B summarizes the project completion roadway segment daily traffic volumes.

6.2 PROJECT COMPLETION (2022) WITH PROJECT TRAFFIC VOLUMES

As noted in Section 4.2, the project will add a raised median along Perris Boulevard extending north of the intersection of Perris Boulevard/Atwood Avenue. The raised median will prohibit eastbound and westbound left-turn and through movements at the intersection. These trips have been rerouted accordingly under the project completion with project scenario.

Project completion with project traffic volumes were developed by adding project traffic to the readjusted traffic volumes under the project completion without project scenario.

Figure 6-4 illustrates "with project" peak hour traffic volumes at study intersections under project completion conditions. Table 6-B summarizes the "with project" roadway segment daily traffic volumes under project completion conditions.

Detailed volume development worksheets are included in Appendix C.

6.3 PROJECT COMPLETION (2022) WITHOUT PROJECT LEVELS OF SERVICE

6.3.1 Study Intersections

An intersection LOS analysis was conducted for project completion without project conditions using the methodologies previously discussed. Table 6-C summarizes the results of this analysis and shows that the following intersection is forecast to operate at an unsatisfactory LOS:

- Perris Boulevard/Atwood Avenue (both a.m. and p.m. peak hours).

All other study intersections are forecast to operate at a satisfactory LOS.

6.3.2 Roadway Segments

A roadway segment LOS analysis was conducted for project completion without project conditions using the methodologies previously discussed. Table 6-D summarizes the results of this analysis and shows that the following roadway segments are forecast to operate at an unsatisfactory LOS:

- Perris Boulevard between Eucalyptus Avenue and Atwood Avenue;
- Perris Boulevard between Atwood Avenue and Dracaea Avenue; and
- Perris Boulevard between Dracaea Avenue and Cottonwood Avenue.

Only the segment of Dracaea Avenue between Perris Boulevard and Birchwood Drive is forecast to operate at a satisfactory LOS.

6.4 PROJECT COMPLETION (2022) WITH PROJECT LEVELS OF SERVICE

6.4.1 Study Intersections

An intersection LOS analysis was conducted for project completion with project conditions using the methodologies previously discussed. Table 6-C summarizes the results of this analysis and shows that all study intersections are forecast to operate at a satisfactory LOS.

6.4.2 Roadway Segments

A roadway segment LOS analysis was conducted for project completion with project conditions using the methodologies previously discussed. Table 6-D summarizes the results of this analysis and shows that the following roadway segments are forecast to operate at an unsatisfactory LOS:

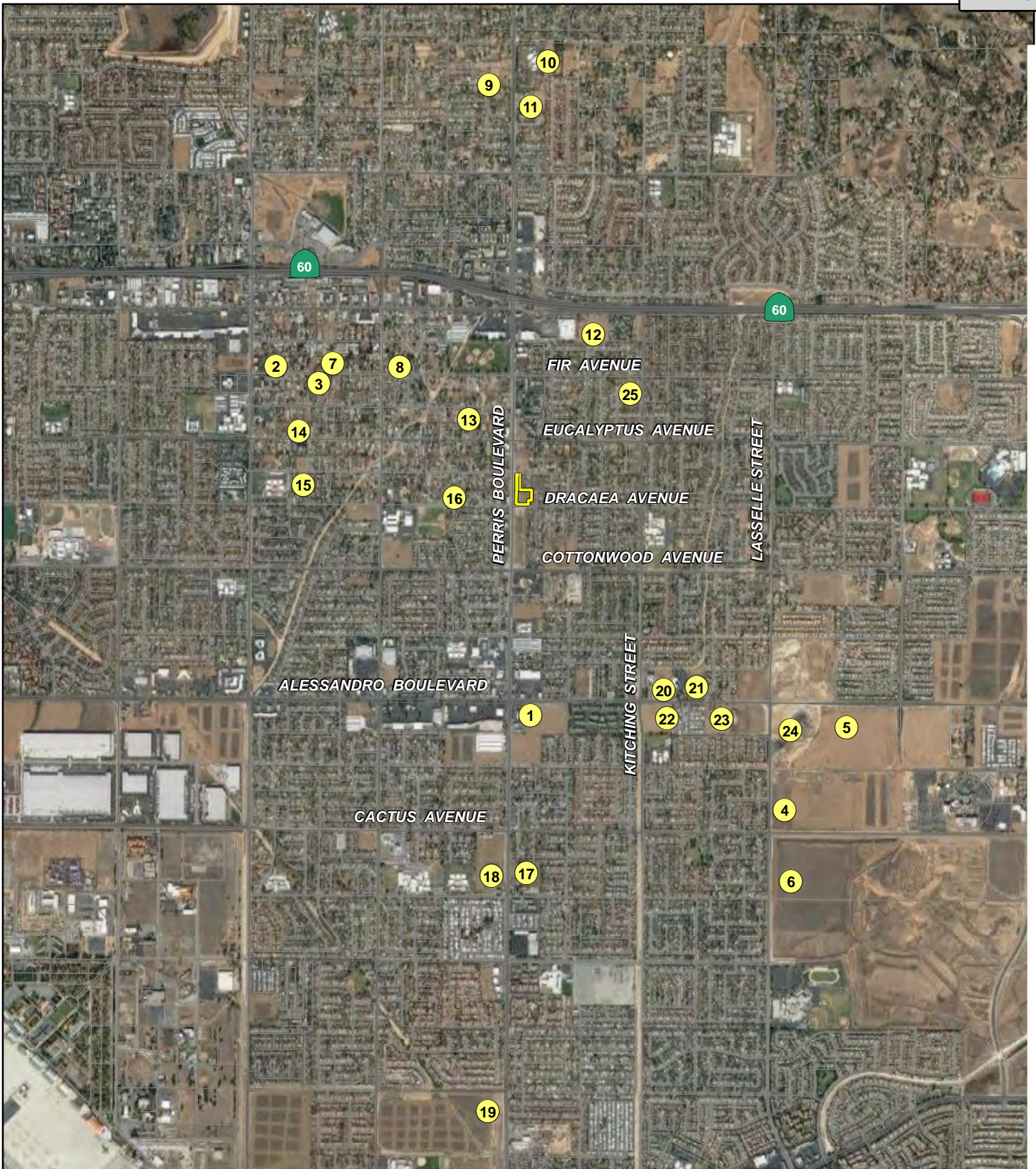
- Perris Boulevard between Eucalyptus Avenue and Atwood Avenue;
- Perris Boulevard between Atwood Avenue and Dracaea Avenue; and
- Perris Boulevard between Dracaea Avenue and Cottonwood Avenue.

These segments are forecast to operate at an unsatisfactory LOS even under project completion without project conditions. However, as shown in Table 6-D, the project increases the V/C ratio by less than 0.05 at these segments. Therefore, pursuant to the City's TIA guidelines, improvements are not required for any of these segments. The segment of Dracaea Avenue between Perris Boulevard and Birchwood Drive is forecast to operate at a satisfactory LOS.

6.5 LIST OF CHAPTER 6.0 FIGURES AND TABLES

- Figure 6-1: Cumulative Project Locations
- Figure 6-2: Cumulative Projects Trip Assignment
- Figure 6-3: Project Completion (2022) without Project Peak Hour Traffic Volumes

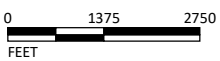
- Figure 6-4: Project Completion (2022) with Project Peak Hour Traffic Volumes
- Table 6-A: Cumulative Project Trip Generation
- Table 6-B: Project Completion (2022) Roadway Segment Daily Traffic Volumes
- Table 6-C: Project Completion (2022) Intersection Levels of Service
- Table 6-D: Project Completion (2022) Roadway Segment Levels of Service



LSA

LEGEND

- Project Site
- Cumulative Project

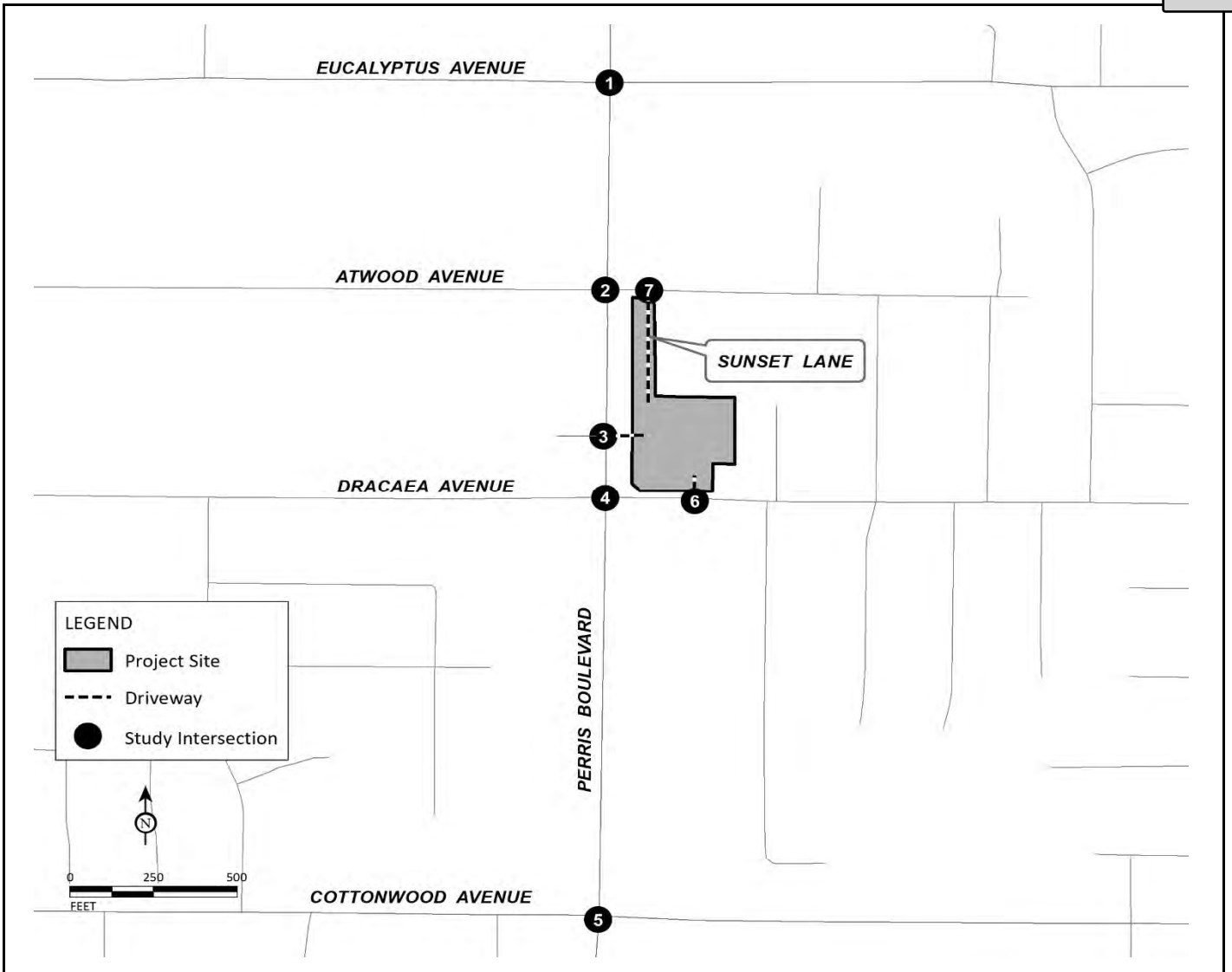


SOURCE: ESRI Streetmap, 2013; Google Earth, 2018.

I:\CAQ1901\Reports\Traffic\fig6-1_CumulProj.mxd (09/01/2020)

FIGURE 6-:

Perris/Dracaea Commercial Project
 Transportation Impact Analysis
 Cumulative Project Locations



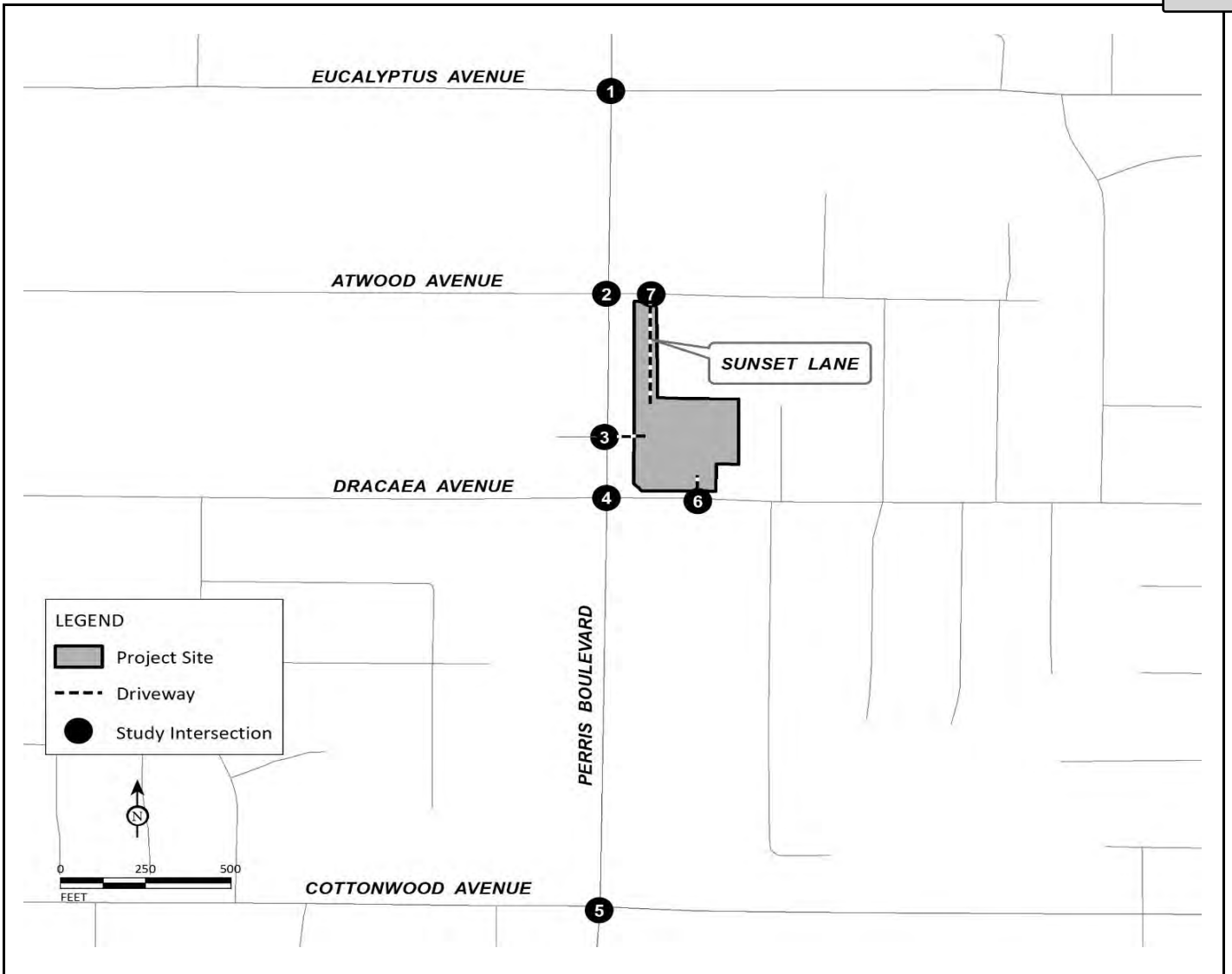
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>Intersection Does Not Exist</p>				
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 6-2



XXX / YYY
AM / PM Peak Hour Traffic Volumes

Perris/Dracaea Commercial Project
Transportation Impact Analysis
Cumulative Projects Trip Assignment



<p>34 / 49 1114 / 1250 76 / 103 206 / 114 230 / 135 88 / 55 24 / 54 77 / 200 60 / 84 53 / 55 1177 / 1123 61 / 63 1245 / 1210</p>	<p>17 / 17 1252 / 1309 17 / 45 10 / 14 2 / 1 4 / 3 15 / 29 1 / 1 40 / 38 55 / 47 1245 / 1210 3 / 1</p>	<p>7 / 2 1276 / 1342 1 / 0 1319 / 1259</p>	<p>91 / 73 1151 / 1219 35 / 50 71 / 50 102 / 77 74 / 34 69 / 74 75 / 108 55 / 23 53 / 31 1179 / 1134 21 / 38</p>	<p>116 / 88 1036 / 1143 139 / 104 169 / 92 293 / 142 65 / 24 92 / 98 237 / 209 57 / 80 63 / 68 1035 / 1074 52 / 33</p>
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>Intersection Does Not Exist</p>	<p>14 / 17 20 / 45 1 / 2 2 / 1</p>			
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

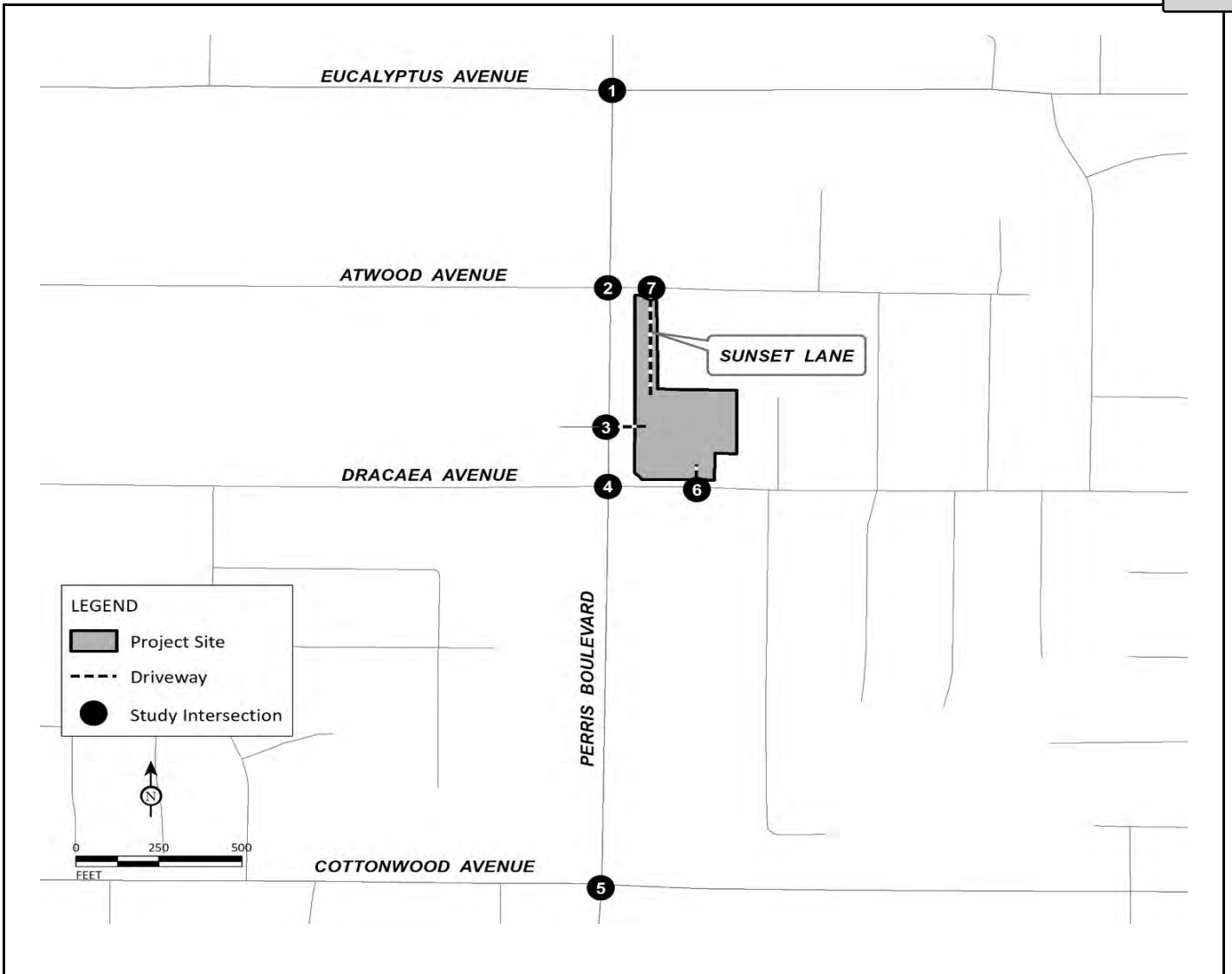
FIGURE 6-3



XXXX / YYYY
AM / PM Peak Hour PCE Volumes

Perris/Dracaea Commercial Project
Transportation Impact Analysis

Project Completion (2022) without Project Peak Hour Traffic Volumes



<p>34 / 49 1122 / 1255 76 / 103 206 / 114 230 / 135 105 / 65 27 / 60 77 / 200 77 / 94 6 / 4 69 / 65 1182 / 1122 77 / 73</p>	<p>19 / 18 1252 / 1300 63 / 82 32 / 28 61 / 67 63 / 52 1281 / 1248 4 / 2</p>	<p>7 / 2 1293 / 1359 50 / 39 1 / 0 1313 / 1263 19 / 20</p>	<p>89 / 72 1132 / 1199 59 / 64 13 / 24 71 / 50 136 / 99 141 / 83 69 / 74 109 / 129 54 / 23 52 / 30 1179 / 1134 72 / 70</p>	<p>132 / 98 1052 / 1153 155 / 114 186 / 102 293 / 142 65 / 24 109 / 108 237 / 209 57 / 80 63 / 68 1052 / 1084 52 / 33</p>
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>105 / 75 34 / 24 37 / 24 243 / 158 110 / 71 129 / 192</p>	<p>14 / 17 20 / 45 47 / 39 18 / 11</p>			
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 6-4



XXXX / YYYY

AM / PM Peak Hour PCE Volumes

---- Project Driveway

Perris/Dracaea Commercial Project
Transportation Impact Analysis

Project Completion (2022) with Project Peak Hour Traffic Volumes

Table 6-A - Cumulative Projects Trip Generation

Project	Units	A.M. Peak Hour			P.M. Peak Hour			Daily
		In	Out	Total	In	Out	Total	
1. PEN16-0039 - LactoSC Inc. Trips/Unit ¹ Trip Generation	272 DU	0.11 30	0.35 95	0.46 125	0.35 95	0.21 57	0.56 152	7.32 1,991
2. Tract 33771 - Jian Qiang Liu Trips/Unit ¹ Trip Generation	12 DU	0.11 1	0.35 4	0.46 5	0.35 4	0.21 3	0.56 7	7.32 88
3. PEN 16-0066 - Cal Choice Inv. Invc Trips/Unit ¹ Trip Generation	20 DU	0.11 2	0.35 7	0.46 9	0.35 7	0.21 4	0.56 11	7.32 146
4. PA 15-0046 - LA Jolla Development/Rocas Grandes Trips/Unit ¹ Trip Generation	426 DU	0.11 47	0.35 149	0.46 196	0.35 149	0.21 89	0.56 238	7.32 3,118
5. PA 13-0006 - Rancho Belago Developers, Inc. Trips/Unit ¹ Trip Generation	141 DU	0.11 16	0.35 49	0.46 65	0.35 49	0.21 30	0.56 79	7.32 1,032
6. PEN 16 - MV Bella Vista GP, LLC Trips/Unit ¹ Trip Generation	220 DU	0.11 24	0.35 77	0.46 101	0.35 77	0.21 46	0.56 123	7.32 1,610
7. Tract 35663 - Jimmy Lee Trips/Unit ¹ Trip Generation	12 DU	0.11 1	0.35 4	0.46 5	0.35 4	0.21 3	0.56 7	7.32 88
8. PEN 16-0060 - Frederick Homes Trips/Unit ¹ Trip Generation	24 DU	0.11 3	0.35 8	0.46 11	0.35 8	0.21 5	0.56 13	7.32 176
9. Tract 31621 - Victoria Homes "Skyline" Trips/Unit ¹ Trip Generation	12 DU	0.19 2	0.55 7	0.74 9	0.62 7	0.37 4	0.99 11	9.44 113
10. Tract 35606 - Metric Homes Trips/Unit ¹ Trip Generation	16 DU	0.19 3	0.55 9	0.74 12	0.62 10	0.37 6	0.99 16	9.44 151
11. Tract 31297 - Randy McFarland Trips/Unit ¹ Trip Generation	7 DU	0.19 1	0.55 4	0.74 5	0.62 4	0.37 3	0.99 7	9.44 66
12. Tract 31814 - Jesse Huizar Trips/Unit ¹ Trip Generation	60 DU	0.11 7	0.35 21	0.46 28	0.35 21	0.21 13	0.56 34	7.32 439
13. Tract 35369 - Tason Myer Property Trips/Unit ¹ Trip Generation	12 DU	0.11 1	0.35 4	0.46 5	0.35 4	0.21 3	0.56 7	7.32 88
14. Tract 35769 - Michael Chen Trips/Unit ¹ Trip Generation	16 DU	0.11 2	0.35 6	0.46 8	0.35 6	0.21 3	0.56 9	7.32 117
15. PA 09-0006 - Jim Nydam Trips/Unit ¹ Trip Generation	15 DU	0.11 2	0.35 5	0.46 7	0.35 5	0.21 3	0.56 8	7.32 110

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table 6-A - Cumulative Projects Trip Generation

Project	Units	A.M. Peak Hour			P.M. Peak Hour			Daily
		In	Out	Total	In	Out	Total	
16. Tract 35304 - Jimmy Lee Trips/Unit ¹ Trip Generation	12 DU	0.11 1	0.35 4	0.46 5	0.35 4	0.21 3	0.56 7	7.32 88
17. Mo Ghiassi TL Group Trips/Unit ¹ Trip Generation	52 DU	0.11 6	0.35 18	0.46 24	0.35 18	0.21 11	0.56 29	7.32 381
18. Tract 36708 - Nova Homes Trips/Unit ¹ Trip Generation	122 DU	0.11 13	0.35 43	0.46 56	0.35 43	0.21 26	0.56 69	7.32 893
19. Wal-Mart Trips/Unit ⁴ Trip Generation Pass-by Trips Net Trip Generation	193.00 TSF	1.04 201 0 201	0.81 156 0 156	1.85 357 0 357	2.12 409 (119) 290	2.21 427 (124) 303	4.33 836 (243) 593	50.70 9,785 (2,838) 6,947
20. Tract 34216 - Creative Design Assoc. Trips/Unit ¹ Trip Generation	39 DU	0.11 4	0.35 14	0.46 18	0.35 14	0.21 8	0.56 22	7.32 285
21. Tract 35429 - Creative Design Assoc. Trips/Unit ¹ Trip Generation	58 DU	0.11 6	0.35 20	0.46 26	0.35 20	0.21 12	0.56 32	7.32 425
22. Tract 35304 - Jimmy Lee Trips/Unit ¹ Trip Generation	24 DU	0.11 3	0.35 8	0.46 11	0.35 8	0.21 5	0.56 13	7.32 176
23. Tract 34681 - Perris Pacific Company Trips/Unit ¹ Trip Generation	49 DU	0.11 5	0.35 17	0.46 22	0.35 17	0.21 10	0.56 27	7.32 359
24. PEN17 - 0064 Boulder Ridge Trips/Unit ¹ Trip Generation	141 DU	0.11 16	0.35 49	0.46 65	0.35 49	0.21 30	0.56 79	7.32 1,032
25. Tract 34043 - RM3 Building and Development Trips/Unit ² Trip Generation	12 DU	0.19 2	0.55 7	0.74 9	0.62 7	0.37 4	0.99 11	9.44 113
Total Net Trip Generation		399	785	1,184	920	684	1,604	20,032

Notes:

DU=Dwelling Units; TSF= Thousand Square Feet

¹ Rates based on the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (10th Edition) for Land Use 220 – “Multifamily Housing (Low-Rise)”, Setting/Location - “General Urban/Suburban.”

² Rates based on the ITE *Trip Generation Manual* (10th Edition) for Land Use 210 – “Single-Family Detached Housing”, Setting/Location - “General Urban/Suburban.”

³ Rates based on the ITE *Trip Generation Manual* (10th Edition) for Land Use 813 – “Free-Standing Discount Superstore”, Setting/Location - “General Urban/Suburban.”

⁴ Pass-by rates obtained from the ITE *Trip Generation Handbook* (3rd Edition). A.m. peak hour and daily pass-by rates are not provided in the ITE Handbook. Hence, no reduction has been considered for the a.m. peak hour and the p.m. peak hour rate has been used as the daily rate.



Table 6-B - Project Completion (2022) Roadway Segment Daily Traffic Volumes

Roadway	#	Segment	Existing (2019) ADT	2019 - 2022 Growth	Cumulative Projects Trips	Proj Comp (2022) Without Project ADT	Project Trips	Pass-By Trips	Proj Comp (2022) With Project ADT
Perris Boulevard	1	between Eucalyptus Avenue and Atwood Avenue	31,829	1,910	2,708	36,447	870	0	37,317
	2	between Atwood Avenue and Dracaea Avenue	30,997	1,860	2,702	35,559	608	(323)	35,844
	3	between Dracaea Avenue and Cottonwood Avenue	29,577	1,775	2,684	34,036	1,044	0	35,080
Dracaea Avenue	4	between Perris Boulevard and Birchwood Drive	4,912	295	0	5,207	1,996	343	7,546

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

Table 6-C - Project Completion (2022) Intersection Levels of Service

Intersection	Jurisdiction	Control	LOS Standard	Without Project				With Project				A.M. Peak Hour	P.M. Peak Hour	Improvements Required ³
				A.M. Peak Hour		P.M. Peak Hour		A.M. Peak Hour		P.M. Peak Hour		Increase in Delay	Increase in Delay	
				Delay (sec.)	LOS	Delay (sec.)	LOS	Delay (sec.)	LOS	Delay (sec.)	LOS	(sec.)	(sec.)	
1 . Perris Boulevard/Eucalyptus Avenue	Moreno Valley	Signal	D	22.6	C	29.5	C	23.3	C	30.0	C	0.7	0.5	No
2 . Perris Boulevard/Atwood Avenue ¹	Moreno Valley	TWSC	D	>100	F *	>100	F *	18.0	C	15.9	C	> -100	> -100	No
3 . Perris Boulevard/Auto Center Driveway - Project Driveway 1 ²	Moreno Valley	OWSC/TWSC	D	10.6	B	0.0	A	12.7	B	12.1	B	2.1	12.1	No
4 . Perris Boulevard/Dracaea Avenue	Moreno Valley	Signal	D	35.1	D	7.6	A	36.5	D	8.9	A	1.4	1.3	No
5 . Perris Boulevard/Cottonwood Avenue	Moreno Valley	Signal	D	35.1	D	23.7	C	36.8	D	23.8	C	1.7	0.1	No
6 . Project Driveway 2/Dracaea Avenue	Moreno Valley	OWSC	D	Does not Exist		Does not Exist		12.1	B	10.6	B	-	-	No
7 . Sunset Lane/Atwood Avenue	Moreno Valley	OWSC	D	8.7	A	8.8	A	8.9	A	8.9	A	0.2	0.1	No

Notes:

OWSC = One-Way Stop Control; TWSC = Two-Way Stop Control

Delay = Average control delay in seconds (For OWSC and TWSC intersections, reported delay is for worst-case movement).

* Exceeds LOS Standard

¹ The intersection of Perris Boulevard/Atwood Avenue has high through volumes that block the turn movements throughout the peak hour. As such, Synchro did not report a delay at this intersection for the blocked turn movements. Therefore, the worst-case movements at this intersection operate at LOS F.

² This intersection operates as an OWSC intersection under without project conditions. However, under with project conditions, it operates as a TWSC intersection. Under without project conditions, Synchro did not report a delay at this intersection during the p.m. peak hour as there are no conflicting movements.

³ Improvements required determined based on the criteria included in the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis - Preparation Guide*, dated June 2020.

Table 6-D - Project Completion (2022) Roadway Segment Levels of Service

Roadway Segment	Classification ¹	Roadway Capacity ²	Without Project			With Project			V/C Ratio Difference	Improvements Required ³
			Daily Volume	V/C Ratio	LOS	Daily Volume	V/C Ratio	LOS		
Segments on Perris Boulevard										
1 . between Eucalyptus Avenue and Atwood Avenue	Four Lane Divided Arterial	37,500	36,500	0.97	E *	37,400	1.00	E *	0.03	No
2 . between Atwood Avenue and Dracaea Avenue	Four Lane Divided Arterial	37,500	35,600	0.95	E *	35,900	0.96	E *	0.01	No
3 . between Dracaea Avenue and Cottonwood Avenue	Four Lane Divided Arterial	37,500	34,100	0.91	E *	35,100	0.94	E *	0.03	No
Segments on Dracaea Avenue										
4 . between Perris Boulevard and Birchwood Drive	Two Lane Undivided Arterial	12,500	5,300	0.42	A	7,600	0.61	B	0.19	No

Notes:
 LOS = Level of Service
 * Exceeds LOS Standard
¹ Classifications for all segments have been obtained from the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis Preparation Guide*, dated June 2020.
² Roadway capacities for all segments have been obtained from the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis Preparation Guide*, dated June 2020.
³ Improvements required determined based on the criteria included in the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis Preparation Guide*, dated June 2020.

7.0 GENERAL PLAN BUILD-OUT ANALYSIS

7.1 GENERAL PLAN BUILD-OUT (2040) WITHOUT PROJECT TRAFFIC VOLUMES

General Plan build-out conditions traffic volumes were developed using forecast volumes obtained from the Moreno Valley Traffic Model (MVTM) and by applying the National Cooperative Highway Research Program (NCHRP) post-processing methodologies. Figure 7-1 illustrates the peak hour traffic volumes at study intersections under General Plan build-out conditions. Table 7-A summarizes the General Plan build-out roadway segment daily traffic volumes.

Detailed volume development worksheets are included in Appendix C.

7.2 GENERAL PLAN BUILD-OUT (2040) WITH PROJECT TRAFFIC VOLUMES

As noted in Section 4.2, the project will add a raised median along Perris Boulevard extending north of the intersection of Perris Boulevard/Atwood Avenue. The raised median will prohibit eastbound and westbound left-turn and through movements at the intersection. These trips have been rerouted accordingly under the General Plan build-out with project scenario.

Figure 7-2 illustrates “with project” peak hour traffic volumes at study intersections under project completion and General Plan build-out conditions, respectively. Previously referenced Table 7-A summarizes the “with project” roadway segment daily traffic volumes under General Plan build-out conditions.

Detailed volume development worksheets are included in Appendix C.

7.3 GENERAL PLAN BUILD-OUT (2040) WITHOUT PROJECT LEVELS OF SERVICE

7.3.1 Study Intersections

An intersection LOS analysis was conducted for General Plan build-out without project conditions using the methodologies previously discussed. Table 7-B summarizes the results of this analysis and shows that the following intersections are forecast to operate at an unsatisfactory LOS:

- Perris Boulevard/Atwood Avenue (both a.m. and p.m. peak hours); and
- Perris Boulevard/Dracaea Avenue (p.m. peak hour only).

All other study intersections are forecast to operate at a satisfactory LOS.

7.3.2 Roadway Segments

A roadway segment LOS analysis was conducted for General Plan build-out without project conditions using the methodologies previously discussed. Table 7-C summarizes the results of this analysis and shows that the following roadway segments are forecast to operate at an unsatisfactory LOS:

- Perris Boulevard between Eucalyptus Avenue and Atwood Avenue;

- Perris Boulevard between Atwood Avenue and Dracaea Avenue; and
- Perris Boulevard between Dracaea Avenue and Cottonwood Avenue.

Only the segment of Dracaea Avenue between Perris Boulevard and Birchwood Drive is forecast to operate at a satisfactory LOS.

7.4 GENERAL PLAN BUILD-OUT (2040) WITH PROJECT LEVELS OF SERVICE

7.4.1 Study Intersections

An intersection LOS analysis was conducted for General Plan build-out with project conditions using the methodologies previously discussed. Table 7-B summarizes the results of this analysis and shows that the following intersection is forecast to operate at an unsatisfactory LOS:

- Perris Boulevard/Dracaea Avenue (p.m. peak hour only).

This intersection is forecast to operate at an unsatisfactory LOS even under General Plan build-out without project conditions. Additionally, as shown in Table 7-B, the project increases the delay at this intersection by more than 5.0 seconds. Therefore, pursuant to the City's TIA guidelines, the project needs to identify improvements to offset the delay at this intersection.

All other study intersections are forecast to operate at a satisfactory LOS.

7.4.2 Roadway Segments

A roadway segment LOS analysis was conducted for General Plan build-out with project conditions using the methodologies previously discussed. Table 7-C summarizes the results of this analysis and shows that the following roadway segments are forecast to operate at an unsatisfactory LOS:

- Perris Boulevard between Eucalyptus Avenue and Atwood Avenue;
- Perris Boulevard between Atwood Avenue and Dracaea Avenue; and
- Perris Boulevard between Dracaea Avenue and Cottonwood Avenue.

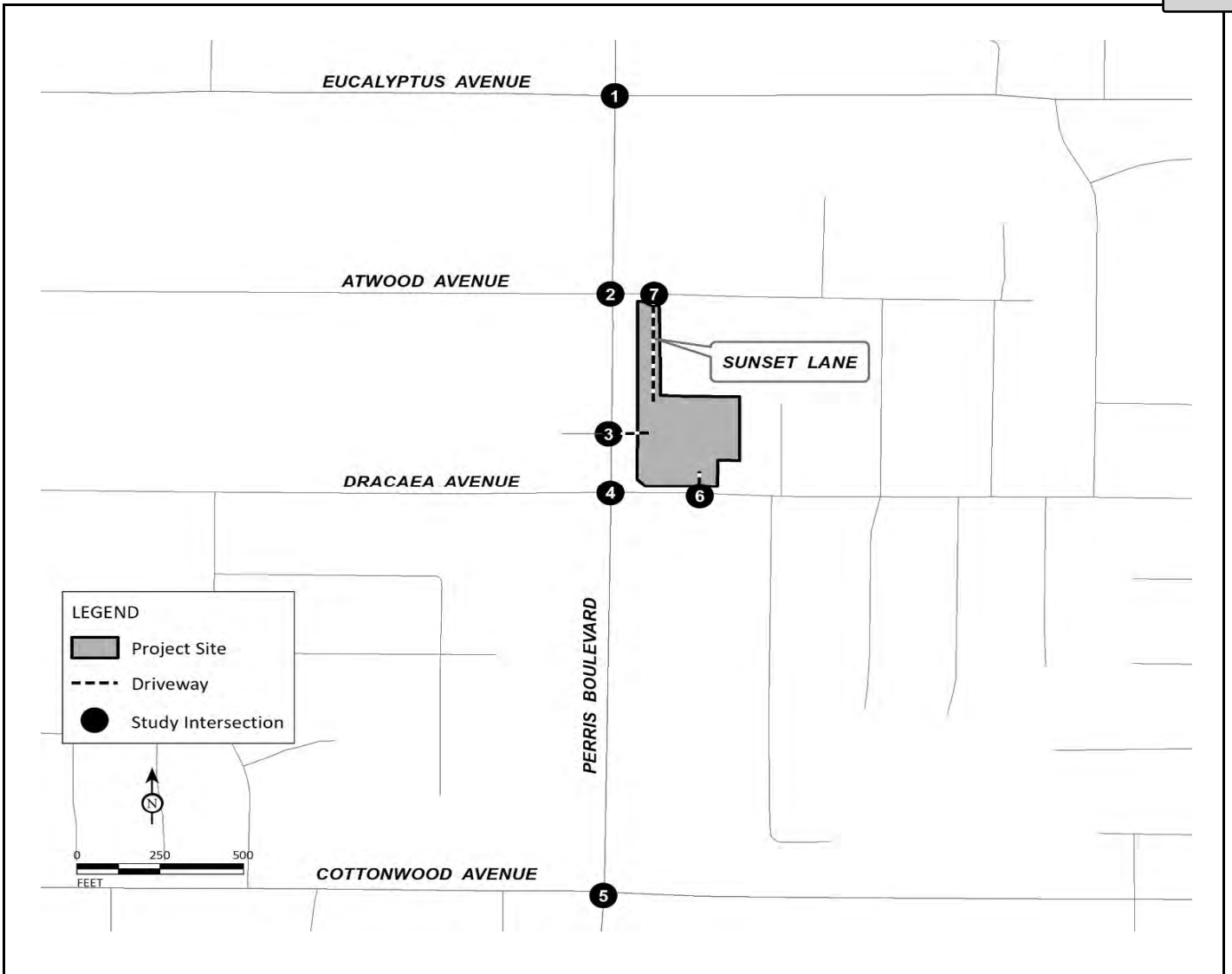
These segments are forecast to operate at an unsatisfactory LOS even under General Plan build-out without project conditions. However, as shown in Table 7-C, the project increases the V/C ratio by less than 0.05 at these segments. Therefore, pursuant to the City's TIA guidelines, improvements are not required for any of these segments. The segment of Dracaea Avenue between Perris Boulevard and Birchwood Drive is forecast to operate at a satisfactory LOS.

Detailed Level of Service Worksheets are included in Appendix D.

7.5 LIST OF CHAPTER 7.0 FIGURES AND TABLES

- Figure 7-1: General Plan Build-out (2040) without Project Peak Hour Traffic Volumes
- Figure 7-2: General Plan Build-out (2040) with Project Peak Hour Traffic Volumes
- Table 7-A: General Plan Build-out (2040) Roadway Segment Daily Traffic Volumes

- Table 7-B: General Plan Build-out (2040) Intersection Levels of Service
- Table 7-C: General Plan Build-out (2040) Roadway Segment Levels of Service



<p>92 / 100 1438 / 1490 216 / 140 401 / 301 92 / 78 58 / 149 170 / 516 150 / 265 129 / 147 1274 / 1551 65 / 88</p>	<p>18 / 18 1654 / 1776 59 / 58 11 / 4 50 / 40 16 / 30 3 / 3 43 / 51 61 / 74 1445 / 1855 19 / 12</p>	<p>7 / 2 1747 / 1867 1 / 0 1525 / 1941</p>	<p>177 / 118 1527 / 1679 43 / 70 91 / 72 147 / 118 77 / 48 92 / 258 79 / 344 60 / 81 75 / 50 1342 / 1611 22 / 53</p>	<p>139 / 126 1383 / 1567 162 / 165 172 / 173 349 / 221 90 / 41 97 / 190 239 / 374 69 / 137 96 / 80 1213 / 1408 78 / 43</p>
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>Intersection Does Not Exist</p>		<p>119 / 101 76 / 165 1 / 2 2 / 1</p>		
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

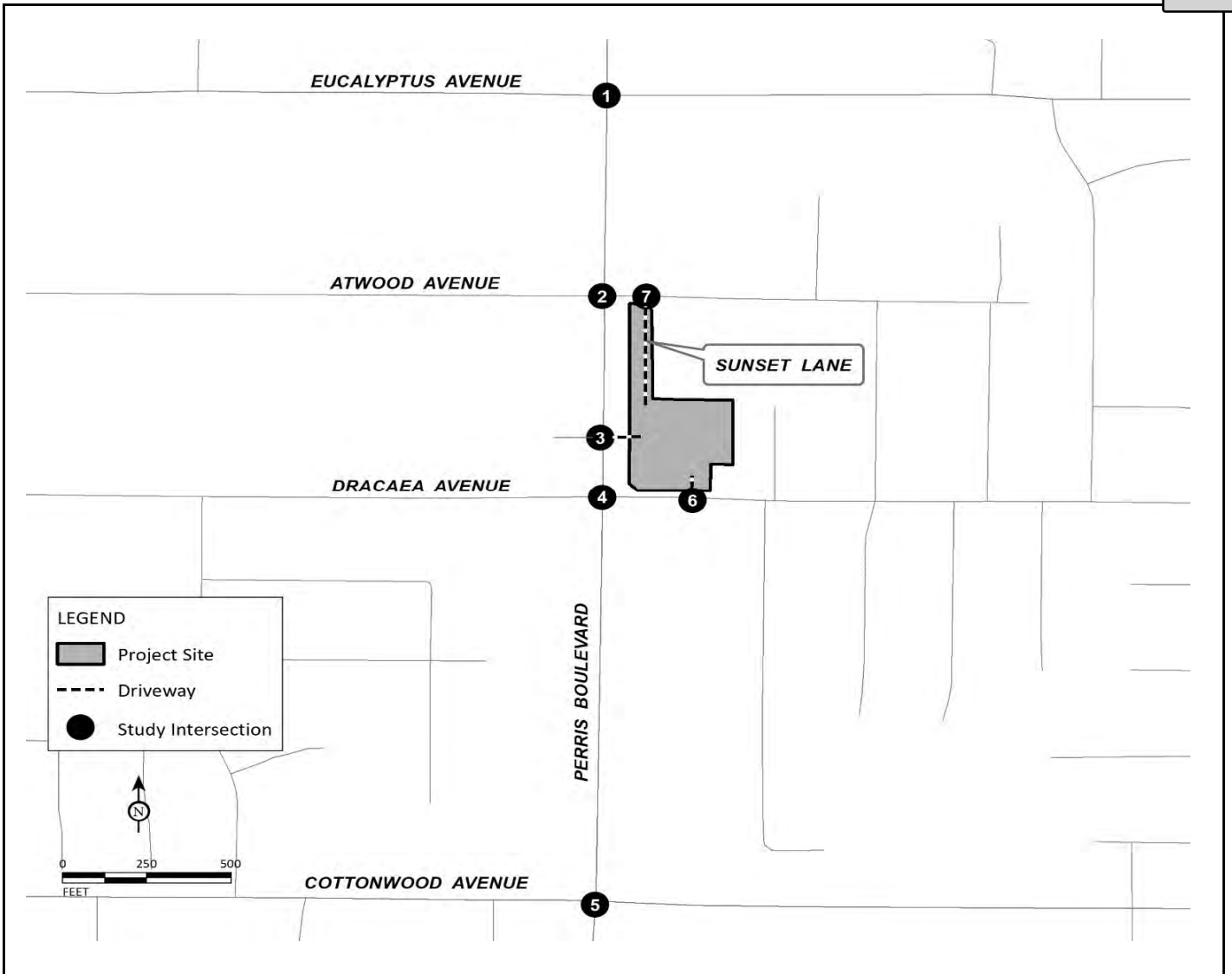
FIGURE 7-1



XXXX / YYYY
AM / PM Peak Hour PCE Volumes

Perris/Dracaea Commercial Project
Transportation Impact Analysis

General Plan Build-out (2040) without Project Peak Hour Traffic Volumes



<p>92 / 100 1446 / 1495 216 / 140 401 / 301 109 / 88 61 / 155 170 / 516 167 / 275 61 / 44 145 / 157 1279 / 1550 81 / 98</p>	<p>29 / 22 1700 / 1804 102 / 190 136 / 112 67 / 83 69 / 79 1482 / 1894 22 / 15</p>	<p>7 / 2 1767 / 1887 50 / 39 1 / 0 1522 / 1948 19 / 20</p>	<p>175 / 117 1508 / 1659 67 / 84 16 / 27 91 / 72 181 / 140 144 / 97 92 / 258 113 / 365 59 / 81 74 / 49 1342 / 1611 73 / 85</p>	<p>155 / 136 1399 / 1577 178 / 175 189 / 183 349 / 221 90 / 41 114 / 200 239 / 374 69 / 137 96 / 80 1230 / 1418 78 / 43</p>
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>105 / 75 34 / 24 37 / 24 311 / 235 110 / 71 142 / 463</p>	<p>119 / 101 76 / 165 47 / 39 18 / 11</p>			
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 7-2



XXXX / YYYY
AM / PM Peak Hour PCE Volumes
---- Project Driveway

Perris/Dracaea Commercial Project
Transportation Impact Analysis

General Plan Build-out (2040) with Project Peak Hour Traffic Volumes

Table 7-A - General Plan Build-out (2040) Roadway Segment Daily Traffic Volumes

Roadway	#	Segment	General Plan Build-out (2040) Without Project ADT	Net Project Trips	Pass-by Trips	General Plan Build-out (2040) With Project ADT
Perris Boulevard	1	between Eucalyptus Avenue and Atwood Avenue	45,861	870	0	46,731
	2	between Atwood Avenue and Dracaea Avenue	46,776	608	(323)	47,061
	3	between Dracaea Avenue and Cottonwood Avenue	42,959	1,044	0	44,003
Dracaea Avenue	4	between Perris Boulevard and Birchwood Drive	6,735	1,996	343	9,074



Table 7-B - General Plan Build-out (2040) Intersection Levels of Service

Intersection	Jurisdiction	Control	LOS Standard	Without Project				With Project				A.M. Peak Hour Increase in Delay (sec.)	P.M. Peak Hour Increase in Delay (sec.)	Improvements Required ³
				A.M. Peak Hour		P.M. Peak Hour		A.M. Peak Hour		P.M. Peak Hour				
				Delay (sec.)	LOS	Delay (sec.)	LOS	Delay (sec.)	LOS	Delay (sec.)	LOS			
1 . Perris Boulevard/Eucalyptus Avenue	Moreno Valley	Signal	D	30.4	C	53.4	D	31.8	C	54.6	D	1.4	1.2	No
2 . Perris Boulevard/Atwood Avenue ¹	Moreno Valley	TWSC	D	>100	F *	-	F *	20.9	C	32.4	D	> -100	> -100	No
3 . Perris Boulevard/Auto Center Driveway - Project Driveway 1 ²	Moreno Valley	OWSC/TWSC	D	19.7	C	0.0	A	14.0	B	21.1	C	-5.7	21.1	No
4 . Perris Boulevard/Dracaea Avenue	Moreno Valley	Signal	D	35.2	D	94.7	F *	46.1	D	>100	F *	10.9	18.4	Yes
5 . Perris Boulevard/Cottonwood Avenue	Moreno Valley	Signal	D	38.2	D	48.1	D	39.4	D	49.4	D	1.2	1.3	No
6 . Project Driveway 2/Dracaea Avenue	Moreno Valley	OWSC	D	Does not Exist		Does not Exist		13.1	B	12.5	B	-	-	No
7 . Sunset Lane/Atwood Avenue	Moreno Valley	OWSC	D	9.5	A	10.0	A	9.8	A	10.2	B	0.3	0.2	No

Notes:

OWSC = One-Way Stop Control; TWSC = Two-Way Stop Control

Delay = Average control delay in seconds (For OWSC and TWSC intersections, reported delay is for worst-case movement).

* Exceeds LOS Standard

¹ The intersection of Perris Boulevard/Atwood Avenue has high through volumes that block the turn movements throughout the peak hour. As such, Synchro did not report a delay at this intersection for the blocked turn movements. Therefore, the worst-case movements at this intersection operate at LOS F.

² This intersection operates as an OWSC intersection under without project conditions. However, under with project conditions, it operates as a TWSC intersection. Under without project conditions, Synchro did not report a delay at this intersection during the p.m. peak hour as there are no conflicting movements.

³ Improvements required determined based on the criteria included in the City of Moreno Valley Transportation Engineering Division *Transportation Impact Analysis* Preparation Guide, dated June 2020.

Table 7-C - General Plan Build-Out (2040) Roadway Segment Levels of Service

Roadway Segment	Classification ¹	Roadway Capacity ²	Without Project			With Project			V/C Ratio Difference	Improvements Required ³
			Daily Volume	V/C Ratio	LOS	Daily Volume	V/C Ratio	LOS		
Segments on Perris Boulevard										
1 . between Eucalyptus Avenue and Atwood Avenue	Four Lane Divided Arterial	37,500	45,900	1.22	F *	46,800	1.25	F *	0.03	No
2 . between Atwood Avenue and Dracaea Avenue	Four Lane Divided Arterial	37,500	46,800	1.25	F *	47,100	1.26	F *	0.01	No
3 . between Dracaea Avenue and Cottonwood Avenue	Four Lane Divided Arterial	37,500	43,000	1.15	F *	44,100	1.18	F *	0.03	No
Segments on Dracaea Avenue										
4 . between Perris Boulevard and Birchwood Drive	Two Lane Undivided Arterial	12,500	6,800	0.54	A	9,100	0.73	C	0.19	No

Notes:

LOS = Level of Service

* Exceeds LOS Standard

¹ Classifications for all segments have been obtained from the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis Preparation Guide*, dated June 2020.

² Roadway capacities for all segments have been obtained from the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis Preparation Guide*, dated June 2020.

³ Improvements required determined based on the criteria included in the City of Moreno Valley Transportation Engineering Division's *Transportation Impact Analysis Preparation Guide*, dated June 2020.

8.0 SITE ACCESS ANALYSIS

The City has requested evaluation of circulation issues for the commercial center with respect to fuel tanker-trucks. Truck turning templates were obtained from the Caltrans Design Manual Turn Templates. As a conservative estimate, large semitrailer templates were used to evaluate availability of adequate truck turning radii.

Fuel tanker-trucks will not interfere with the drive-through operations for any of the facilities or the overall site circulation.

Since discharging fuel for the gas station will occur during the off-peak hours for the gas station, there will be no circulation issues for fuel tanker-trucks. Ingress and egress movements as well as internal circulation for fuel tanker-trucks are illustrated in Figure 8-1. For the truck turning figures, the large Interstate Semitrailer templates have been used as a conservative approach. As illustrated in the Figure 8-1, the trucks will have adequate turning radii to ingress using the project driveway on Perris Boulevard and egress using the project driveway on Atwood Avenue. Additionally, adequate space is available on site for fuel tanker-trucks to park during fuel discharge operations. Previously referenced Figure 4-3 illustrates the project design features.

8.1 LIST OF CHAPTER 8.0 FIGURES

- Figure 8-1: Truck Turning Template

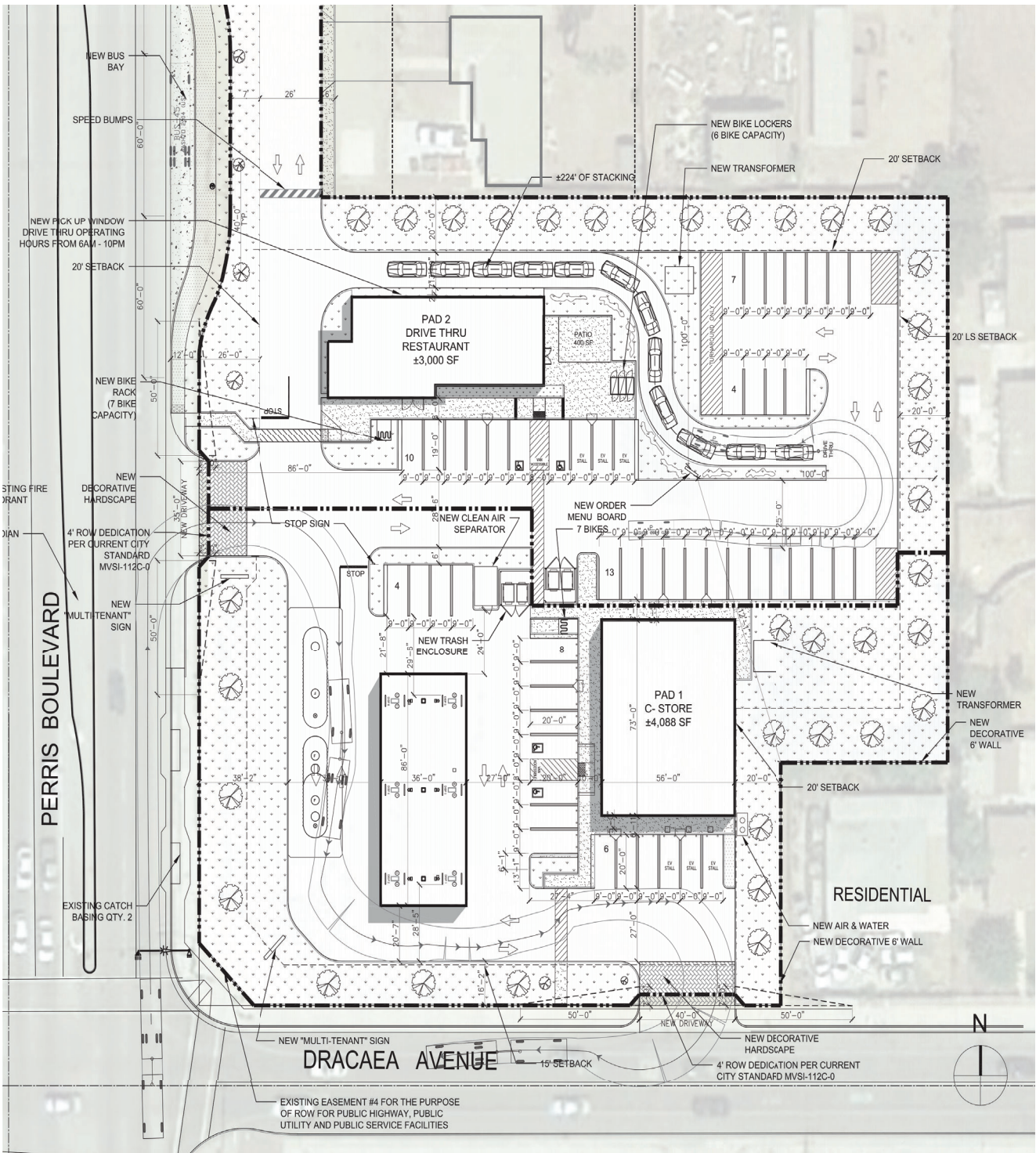
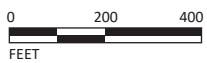


FIGURE 8-1



SOURCE: Cadence Capital Investments, LLC.

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Perris/Dracaea Commercial Project
Transportation Impact Analysis

(WB-50) Truck Turning Template

9.0 QUEUING ANALYSIS

Tables 9-A, 9-B, and 9-C list the available turn-pocket storage lengths and summarize the 95th percentile back-of-queue lengths at the study intersections under existing, project completion, and General Plan build-out conditions. Pursuant to discussions with City staff, queues have only been reported for those turn movements where the project adds to the traffic volumes. The queues for the signalized intersections have been reported from Synchro, while for unsignalized intersections, the SimTraffic queues have been reported since Synchro does not appropriately report queues at unsignalized intersections. Recommended improvements based on queuing analysis results have been included in Section 11.1.

Detailed queuing worksheets are included in Appendix E.

9.1 LIST OF CHAPTER 9.0 TABLES

- Table 9-A: Existing Queuing Analysis
- Table 9-B: Project Completion (2022) Queuing Analysis
- Table 9-C: General Plan Build-out (2040) Queuing Analysis



Table 9-A - Existing Queuing Analysis

Intersection	Movement	Storage Length ² (ft/ln)	Without Project ³	
			AM	PM
1 . Perris Boulevard/Eucalyptus Avenue Signal	NBL	95	45	60
	NBR	95	5	25
	EBR	50	0	10
	WBL	100	85	75
2 . Perris Boulevard/Atwood Avenue TWSC	NBL	50	50	55
	SBL	50	35	50
3 . Perris Boulevard/Auto Center Driveway - Project Driveway 1 OWSC ¹	WBR	150	0	0
4 . Perris Boulevard/Dracaea Avenue Signal	SBL	95	40	65
5 . Perris Boulevard/Cottonwood Avenue Signal	SBL	95	190	145
	SBR	200	30	35
	EBL	115	110	105
	WBR	355	40	0
6 . Project Driveway 2/Dracaea Avenue Signal	SBLR	130	-	-
7 . Sunset Lane/Atwood Avenue OWSC	NBLR	300	20	15

Notes:

ft/ln = feet per lane

OWSC = One-Way Stop Control; TWSC = Two-Way Stop Control

EB = Eastbound; WB = Westbound; NB = Northbound; SB = Southbound

L = Left; T = Through; R = Right

Bold = Queue exceeds available storage.

¹ This intersection operates as a OWSC intersection under without project conditions. However, under with project conditions, it operates as a TWSC intersection.

² Storage length for all other movements have been obtained from Google Earth measurements.

³ All queues reported are 95th percentile queues. Queues for signalized intersections have been reported from Synchro, while queues for unsignalized intersections have been reported from SimTraffic.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



Table 9-B - Project Completion (2022) Queuing Analysis

Intersection	Movement	Storage Length ² (ft/ln)	Without Project ³		With Project ³	
			AM	PM	AM	PM
1 . Perris Boulevard/Eucalyptus Avenue Signal	NBL	95	40	60	105	165
	NBR	95	5	25	5	25
	EBR	50	0	15	0	20
	WBL	100	90	80	105	95
2 . Perris Boulevard/Atwood Avenue TWSC	NBL ⁴	50/150	50	50	55	50
	SBL ⁵	50/125	25	55	50	70
3 . Perris Boulevard/Auto Center Driveway - Project Driveway 1 OWSC/TWSC ¹	WBR	150	0	0	45	50
4 . Perris Boulevard/Dracaea Avenue Signal	SBL ⁶	95/100	40	60	60	85
5 . Perris Boulevard/Cottonwood Avenue Signal	SBL	95	165	160	155	175
	SBR	200	0	30	20	30
	EBL	115	95	110	145	120
	WBR	355	40	0	45	0
6 . Project Driveway 2/Dracaea Avenue Signal	SBLR	130	-	-	75	55
7 . Sunset Lane/Atwood Avenue OWSC	NBLR	300	20	20	40	40

Notes:

ft/ln = feet per lane

OWSC = One-Way Stop Control; TWSC = Two-Way Stop Control

EB = Eastbound; WB = Westbound; NB = Northbound; SB = Southbound

L = Left; T = Through; R = Right

Bold = Queue exceeds available storage.

¹ This intersection operates as a OWSC intersection under without project conditions. However, under with project conditions, it operates as a TWSC intersection.

² Storage length for all other movements have been obtained from Google Earth measurements.

³ All queues reported are 95th percentile queues. Queues for signalized intersections have been reported from Synchro, while queues for unsignalized intersections have been reported from SimTraffic.

⁴ Under without project condition, the left turn pocket has a storage length of 50 feet, with the implementation of the project, the storage length is increased to 150 feet.

⁵ Under without project condition, the left turn pocket has a storage length of 50 feet, with the implementation of the project, the storage length is increased to 125 feet.

⁶ Under without project condition, the left turn pocket has a storage length of 95 feet, with the implementation of the project, the storage length is increased to 100 feet.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



Table 9-C - General Plan Build-out (2040) Queuing Analysis

Intersection	Movement	Storage Length ² (ft/ln)	Without Project ³		With Project ³	
			AM	PM	AM	PM
1 . Perris Boulevard/Eucalyptus Avenue Signal	NBL	95	120	100	265	280
	NBR	95	5	15	5	15
	EBR	50	35	145	45	155
	WBL	100	95	115	125	135
2 . Perris Boulevard/Atwood Avenue TWSC	NBL ⁴	50/150	80	100	150	145
	SBL ⁵	50/125	85	115	85	125
3 . Perris Boulevard/Auto Center Driveway - Project Driveway 1 OWSC/TWSC ¹	WBR	150	0	0	60	65
4 . Perris Boulevard/Dracaea Avenue Signal	SBL ⁶	95/100	30	65	50	100
5 . Perris Boulevard/Cottonwood Avenue Signal	SBL	95	145	155	135	165
	SBR	200	25	20	25	20
	EBL	115	115	180	140	185
	WBR	355	45	50	45	60
6 . Project Driveway 2/Dracaea Avenue Signal	SBLR	130	-	-	100	115
7 . Sunset Lane/Atwood Avenue OWSC	NBLR	300	45	0	80	45

Notes:

ft/ln = feet per lane

OWSC = One-Way Stop Control; TWSC = Two-Way Stop Control

EB = Eastbound; WB = Westbound; NB = Northbound; SB = Southbound

L = Left; T = Through; R = Right

Bold = Queue exceeds available storage.

¹ This intersection operates as a OWSC intersection under without project conditions. However, under with project conditions, it operates as a TWSC intersection.

² Storage length for all other movements have been obtained from Google Earth measurements.

³ All queues reported are 95th percentile queues. Queues for signalized intersections have been reported from Synchro, while queues for unsignalized intersections have been reported from SimTraffic.

⁴ Under without project condition, the left turn pocket has a storage length of 50 feet, with the implementation of the project, the storage length is increased to 150 feet.

⁵ Under without project condition, the left turn pocket has a storage length of 50 feet, with the implementation of the project, the storage length is increased to 125 feet.

⁶ Under without project condition, the left turn pocket has a storage length of 95 feet, with the implementation of the project, the storage length is increased to 100 feet.

10.0 ACTIVE TRANSPORTATION AND PUBLIC TRANSIT ANALYSIS

According to the City's TIA guidelines, a significant impact occurs when a project conflicts with adopted plans, policies, or programs regarding bicycle, public transit, or pedestrian facilities, or otherwise decreases the performance or safety of such facilities.

At present, bike lanes do not exist along Perris Boulevard or Dracaea Avenue within the study area. However, a bicycle boulevard has been proposed along Dracaea Avenue between Elsworth Street and Nason Street in the City's Bicycle Master Plan. The project will not decrease the performance or the safety of nearby bicycle facilities.

According to the City of Moreno Valley Master Plan of Trails, there are no existing or proposed trail facilities in the vicinity of the project. The project will not decrease the performance of nearby pedestrian facilities.

The RTA Route 19 is the only bus route through the study area. The project is installing a bus stop for this route along the project frontage. There is no other proposed transit route in the study area. Therefore, the project will not decrease the service level of public transit near the project.

The project does not conflict with existing and proposed bicycle, pedestrian, and transit facilities. Therefore, it can be considered as conforming to all adopted plan, policies, and programs concerning these facilities and will not have a significant impact.

11.0 IMPROVEMENTS AND RECOMMENDATIONS

11.1 RECOMMENDED IMPROVEMENTS

Based on the results of the LOS and queuing analysis, improvements have been recommended at study area intersections. Figure 11-1 illustrates the General Plan build-out with project with improvements study intersection geometrics and traffic control. Previously referenced Figure 4-3 illustrates the striping plan for the proposed improvements along the project frontage. Table 11-A illustrates the post-improvement intersection levels of service under General Plan build-out conditions. As shown in Table 11-A, with the implementation of the proposed improvements, all study intersections are forecast to operate at a satisfactory LOS under all scenarios.

As shown in previously referenced Table 7-B, the intersection of Perris Boulevard/Dracaea Avenue is forecast to operate at an unsatisfactory LOS under build-out without project condition. An eastbound left-turn lane with a storage length of 100 ft and a westbound left-turn lane with a storage length of 135 ft will be required at this intersection as part of intersection LOS and queuing improvements. The project will be required to pay its fair share for the improvement.

As shown in previously referenced Tables 9-B, and 9-C, queues for some of movements affected by project traffic are projected to exceed the existing available turn-pocket storage lengths under project completion and General Plan build-out without and with project conditions. Table 11-B summarizes recommended improvements at these locations and the project fair share corresponding to these improvements. With the implementation of the improvements summarized in Table 11-B, the queuing issue can be resolved for all scenarios.

11.2 FUNDING SOURCES AND MECHANISMS

Where there is a funding mechanism (fee program) for the recommended improvements, payment into the fee program would be considered sufficient. At study locations where the addition of project traffic is solely responsible for the operational deficiency and there is no funding mechanism in place, the project will be responsible for the implementation of the improvements. For all other improvements, the project is responsible for its fair-share payment. Funding mechanisms for the proposed improvements at study intersections have been listed in Table 11-B.

11.2.1 Transportation Uniform Mitigation Fee (TUMF) Program

The underlying purpose of the TUMF program is “the need to establish a comprehensive funding source to mitigate the cumulative regional transportation impacts of new development on regional arterial highways.” As new development occurs in western Riverside County, the cumulative transportation impacts of this new development are reflected in increased demand for transportation infrastructure leading to decreased levels of service, increased delay and increased congestion on regional transportation facilities, and an overall decline in regional mobility. Therefore, the need to invest in additional transportation infrastructure to meet the increased travel demand and to sustain pre-development traffic conditions to “keep traffic flowing” represents the fundamental premise of the TUMF program.

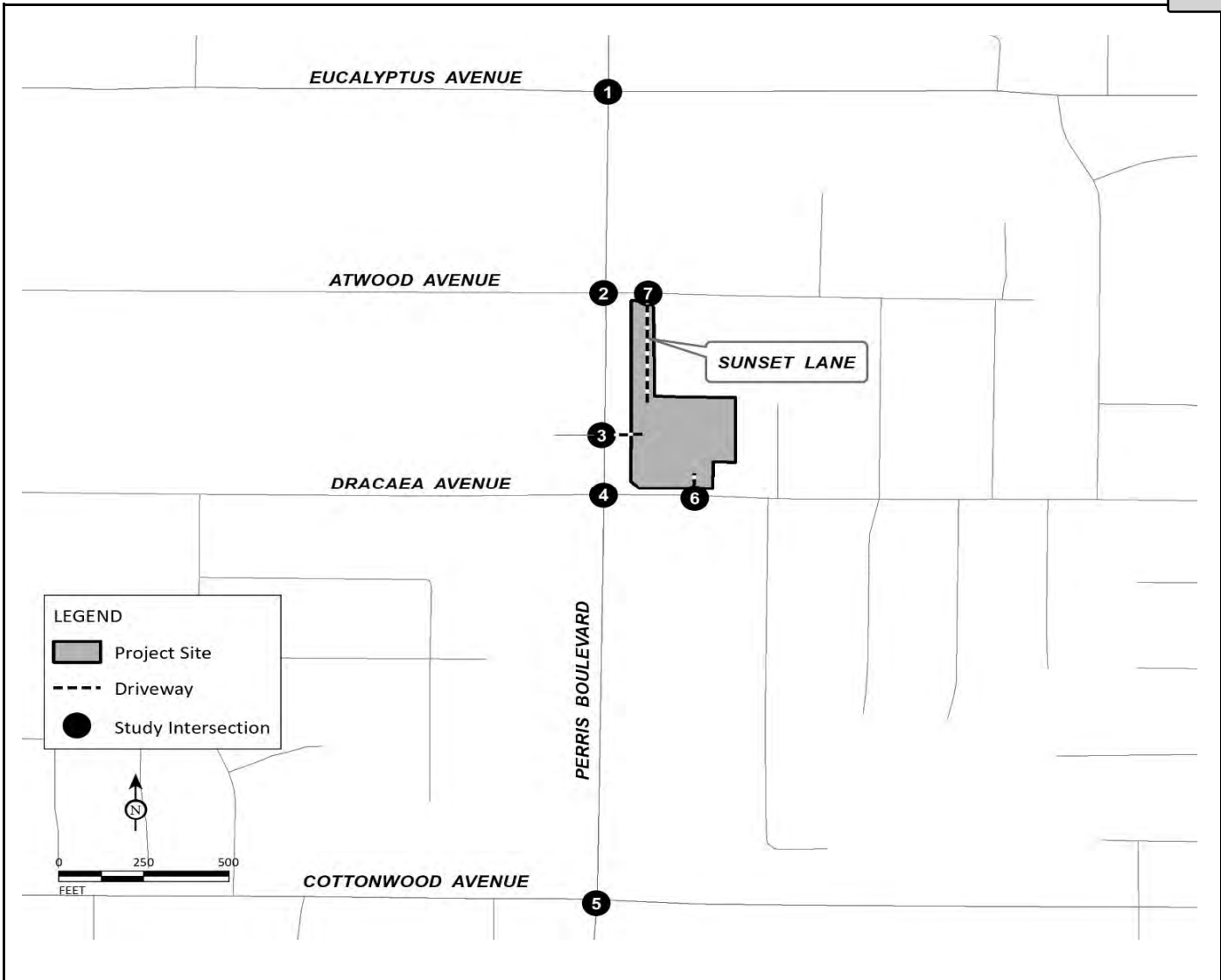
11.2.2 Project Fair Share

In the absence of a fee program, where an improvement has been recommended on the roadway network, the project shall pay its fair share of the cost required for the improvement. The project's fair share has been calculated based on project traffic as a percentage of total growth from existing to General Plan build-out conditions.

Table 11-B summarizes the project fair share corresponding to the improvements recommended and the funding programs in place that covers recommended improvements for study intersections.

11.3 LIST OF CHAPTER 11.0 FIGURES AND TABLES

- Figure 11-1: General Plan Build-out with Project with Improvements Study Intersection Geometrics and Traffic Control
- Table 11-A: General Plan Build-out (2040) with Project with Improvements Intersection Levels of Service
- Table 11-B: Recommended Improvements for Intersections and Funding Mechanism



<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 11-1

LSA

Legend

- ◻ Signal
- ◻ Stop Sign
- Project Driveway
- D De Facto Right Turn
- ↗ Recommended Improvement

*Perris/Dracaea Commercial Project
Transportation Impact Analysis*

General Plan Build-out with Project with Improvements Study Intersection Geometrics and Traffic Control

R:\CAQ1901\Traffic\August 2020\g30_Geo_BO_MIT_P.xlsx\Figure (9/2/2020)

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table 11-A - General Plan Build-out (2040) with Project with Improvements Intersection Levels of Service

Intersection	Jurisdiction	LOS Standard	Control	With Project Without Improvements				With Project With Improvements				
				A.M. Peak Hour		P.M. Peak Hour		A.M. Peak Hour		P.M. Peak Hour		
				Delay (sec.)	LOS	Delay (sec.)	LOS	Delay (sec.)	LOS	Delay (sec.)	LOS	
4 . Perris Boulevard/Dracaea Avenue	Moreno Valley	Signal	D	46.1	D	>100	F *	Signal	26.4	C	32.1	C

Notes:

- TWSC = Two-Way Stop Control
- Delay = Average control delay in seconds
- LOS = Level of Service
- * Exceeds LOS Standard

Table 11-B - Recommended Improvements for Intersections and Funding Mechanism

Intersection	Project Completion (2022) with Project Improvements	General Plan Build-Out (2040) with Project Improvements	Funding Mechanism	Improvements Covered by TUMF	Improvements Covered by Fair Share	Fair Share Percentage
1 . Perris Boulevard/Eucalyptus Avenue	Extend storage length for NBL turn lane from 95 feet to 280 feet, Extend storage length for WBL turn lane from 100 feet to 135 feet.	Project Completion Improvements + Extend EBR turn lane from 50 feet to 155 feet.	Fair Share		Extend storage length for NBL turn lane from 95 feet to 280 feet, Extend EBR turn lane from 50 feet to 155 feet, Extend storage length for WBL turn lane from 100 feet to 135 feet.	5.89%
4 . Perris Boulevard/Dracaea Avenue	-	Add a EBL turn lane with storage length of 100 feet, Add a WBL turn lane with storage length of 135 feet.	Fair Share		Add a EBL turn lane with storage length of 100 feet, Add a WBL turn lane with storage length of 135 feet.	14.86%
5 . Perris Boulevard/Cottonwood Avenue	Extend storage length for SBL turn lane from 95 feet to 190 feet, Extend storage length for EBL turn lane from 115 feet to 185 feet.	Project Completion Improvements	Fair Share		Extend storage length for SBL turn lane from 95 feet to 190 feet, Extend storage length for EBL turn lane from 115 feet to 185 feet.	8.41%

Notes:

EB = Eastbound, WB = Westbound

L = Left

TUMF refers to the Transportation Uniform Mitigation Fee program.

Project Fair Share Percentage is the highest fair share value of the AM and PM peak hour when both peak hours are impacted by the project, or only in the peak hour where the project has an impact.

12.0 VEHICLE MILES TRAVELED ANALYSIS

On December 28, 2018, the California Office of Administrative Law cleared the revised *CEQA Guidelines* for use. Among the changes to the guidelines was removal of vehicle delay and level of service from consideration under CEQA. With the adopted guidelines, transportation impacts are to be evaluated based on a project's effect on VMT. Lead agencies are allowed to opt-in to the revised transportation guidelines, but the new guidelines must be used starting July 1, 2020.

The City recently adopted its VMT analysis guidelines. Pursuant to the City's VMT analysis guidelines, retail less than 50,000 sf, which serve the local community and have the potential to reduce VMT, are exempted from a VMT assessment. It is anticipated that this type of small retail facility will serve all the residential units in the vicinity and provide them with the convenience of availing certain amenities close to home. Residents will no longer have to travel long distances to avail such amenities. Therefore, trip lengths will be shortened and vehicle travel will be reduced. This project has a much lower square footage (7,088 sf for all uses combined) compared to the 50,000-square foot threshold. Therefore, the project can be considered as a local serving retail and will not have a significant VMT impact.

APPENDIX A:

SCOPING AGREEMENT

EXHIBIT A

Project Scoping Form

This scoping form shall be submitted to the Lead Agency to assist in identifying infrastructure improvements that may be required to support traffic from the proposed project.

Project Identification:

Approved
[Signature] 8/26/20

Case Number:	PPA18-0018
Related Cases:	
SP No.	
EIR No.	
GPA No.	
CZ No.	
Project Name:	Perris/Draacea Commercial Project
Project Address:	Northeast Corner of Perris Boulevard and Draacea Avenue, Moreno Valley, CA 92555
Project Opening Year:	2024
Project Description:	Gasoline Station with Convenience Store and Coffee Shop with Drive-Through Window

	Consultant:	Developer:
Name:	LSA Associates, Inc.	Cadence Acquisition LLC
Address:	1500 Iowa Avenue, Suite 200 Riverside, CA 92507	6400 S. Fiddlers Green Circle, St. 1820, Greenwood Village, CO 80111
Telephone:	951-781-9310	720-493-5100
Email:		

Trip Generation Information:

Trip Generation Data Source: Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition)

Current General Plan Land Use:

Residential/Office

Proposed General Plan Land Use:

Commercial

Current Zoning:

Office

Proposed Zoning:

Commercial

	Existing Trip Generation			Proposed Trip Generation		
	In	Out	Total	In	Out	Total
AM Trips				165	159	324
PM Trips				103	101	204

Trip Internalization: Yes No (_____% Trip Discount)

Pass-By Allowance: Yes No (62(AM)% Trip Discount
56(PM))

Potential Screening Checks

Is your project screened from specific analyses (see Page 3 of the guidelines related to LOS assessment and Pages 22-23 for VMT screening criteria).

Is the project screened from LOS assessment? Yes No

LOS screening justification (see Page 3 of the guidelines): _____

Is the project screened from VMT assessment? Yes No

VMT screening justification (see Pages 22-23 of the guidelines): Local serving retail project less than 50,000 square feet.

Level of Service Scoping

- Proposed Trip Distribution (Attach Graphic for Detailed Distribution):

North	South	East	West
30%	30%	20%	20%

Link level of service and data collection:

X will be required
 ___ will not be required

- Attach list of study intersections (and roadway segments if applicable)
- Attach site plan
- Other specific items to be addressed:
 - Site access
 - On-site circulation
 - Parking
 - Consistency with Plans supporting Bikes/Peds/Transit
 - Other Queuing Analysis
- Date of Traffic Counts 05/23/2019
- Attach proposed analysis scenarios (years plus proposed forecasting approach)
- Attach proposed phasing approach (if the project is phased)

VMT Scoping

For projects that are not screened, identify the following:

- Travel Demand Forecasting Model Used _____
- Attach WRCOG Screening VMT Assessment output or describe why it is not appropriate for use
- Attach proposed Model Land Use Inputs and Assumed Conversion Factors (attach)

Analysis Scenarios

The TIA will be prepared to satisfy the requirements established by the City's TIA Guidelines, as well as the requirements for the disclosure of potential impacts and mitigation measures pursuant to the California Environmental Quality Act (CEQA). Since the proposed project will require a GPA, based on the TIA Guidelines, an analysis of General Plan Build-out conditions is required for the project. Therefore, the TIA will address existing traffic conditions, future traffic forecasts, project-related impacts, and mitigations under the following scenarios:

1. Existing (2019) Conditions;
2. Project Completion (2022) without Project Conditions*;
3. Project Completion (2022) with Project Conditions;
4. General Plan Build-out (2040) without Project Conditions; and
5. General Plan Build-out (2040) with Project Conditions.

*Project completion year should have a minimum two (2) year horizon as per the City's TIA guidelines.

Study Area Intersections

Based on the City's TIA Guidelines, the TIA is required to analyze all intersections of Collector or higher classification streets where the project will contribute 50 or more peak hour trips, not exceeding a 5-mile radius from the project site. As such, an operational analysis of the following intersections is proposed during the a.m. and p.m. peak hours.

1. Perris Boulevard/Eucalyptus Avenue;
2. Perris Boulevard/Atwood Avenue;
3. Perris Boulevard/Auto Center Driveway - Project Driveway 1;
4. Perris Boulevard/Dracaea Avenue;
5. Perris Boulevard/Cottonwood Avenue;
6. Project Driveway 2/Dracaea Avenue; and
7. Sunset Lane/Atwood Avenue.

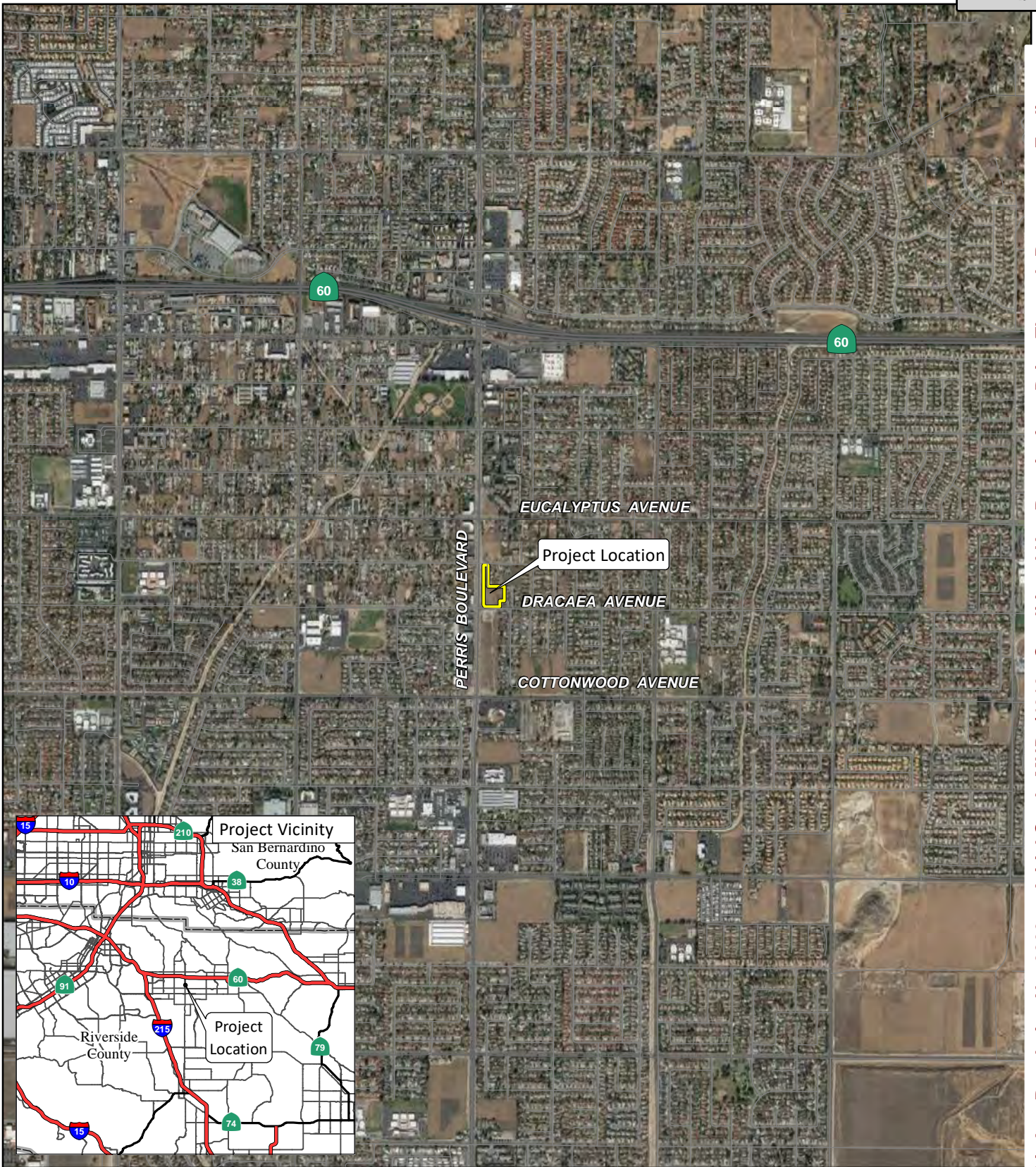
All study intersections will be analyzed during the a.m. and p.m. peak hours. For all intersections, the *Highway Capacity Manual 6* (HCM 6) analysis methodologies will be used to determine intersection levels of service. Intersection LOS will be calculated using Synchro 10 software, which uses the HCM 6 methodologies.

Roadway Segments

All roadway segments adjacent to intersection analysis locations will be analyzed.

LSA proposes to include the following roadway segments:

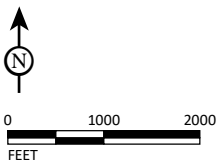
1. Perris Boulevard, between Eucalyptus Avenue and Atwood Avenue;
2. Perris Boulevard, between Atwood Avenue and Dracaea Avenue;
3. Perris Boulevard, between Dracaea Avenue and Cottonwood Avenue; and
4. Dracaea Avenue, between Perris Boulevard and Birchwood Drive.



Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

FIGURE :

LSA



SOURCE: ESRI Streetmap, 2013; Google Earthl, 2018.

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Perris/Dracaea Commercial Projec
Traffic Impact Analy
Regional and Project Location

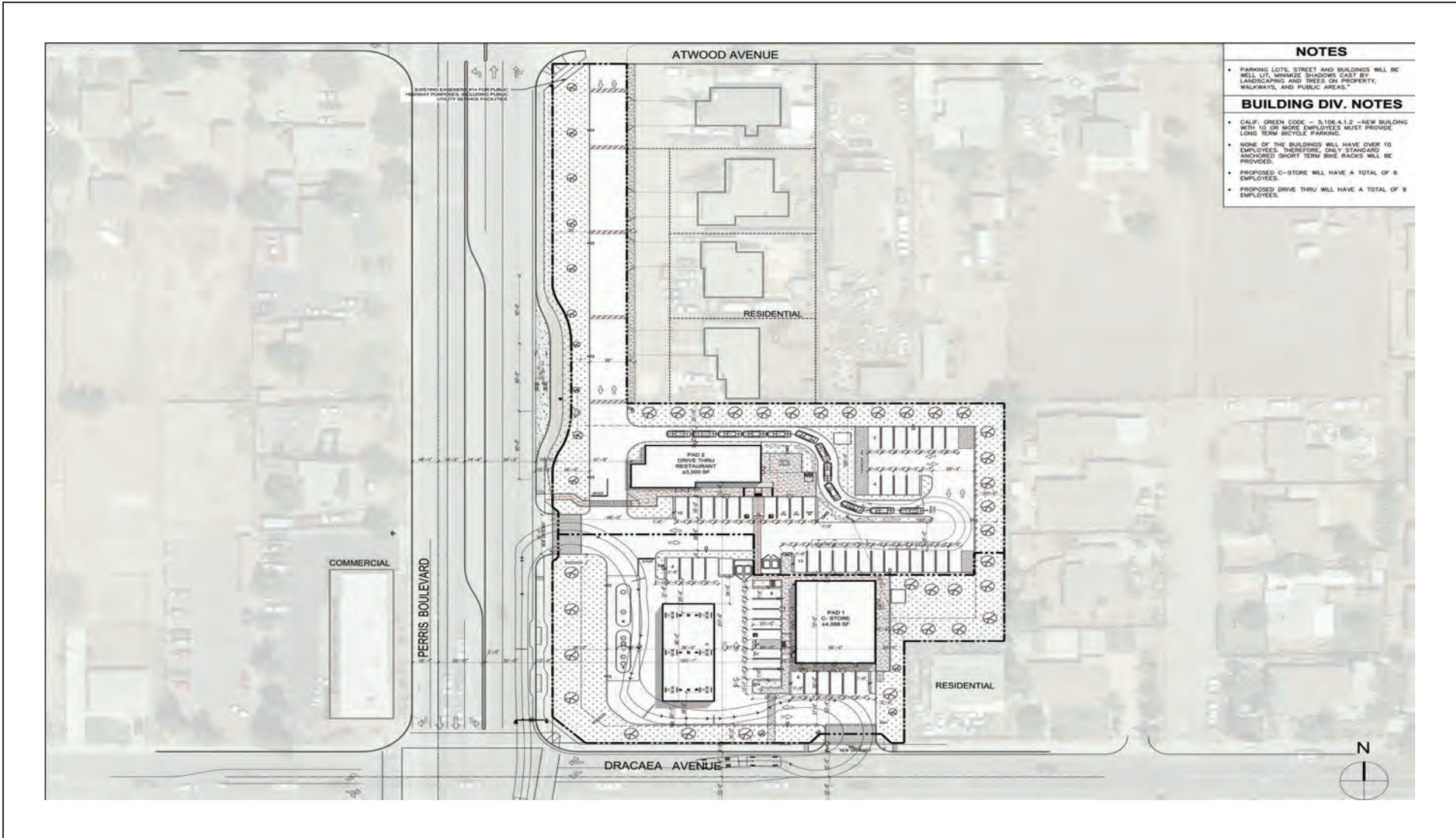
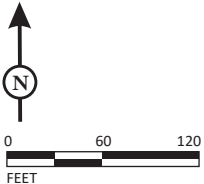


FIGURE 2

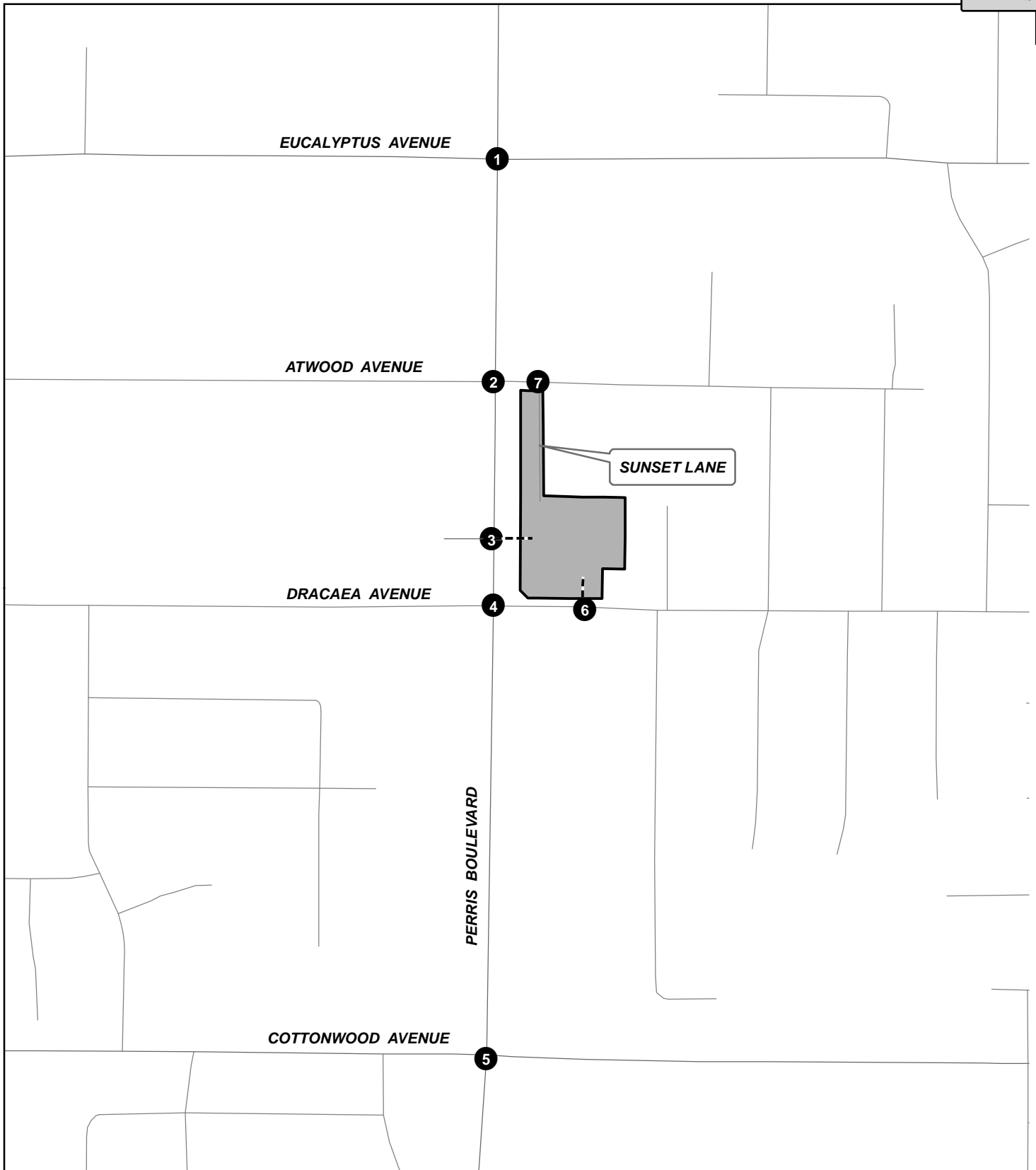
LSA



Perris/Dracaea Commercial Project
Transportation Impact Analysis

Conceptual Site Plan

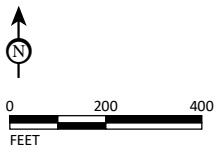
SOURCE: Cadence Capital Investments LLC; September, 2020
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Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



- LEGEND
- Project Site
 - Driveway
 - Study Intersection



SOURCE: ESRI Streetmap, 2013

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FIGURE :

Perris/Dracaea Commercial Project
 Traffic Impact Analysis
 Study Area Intersections

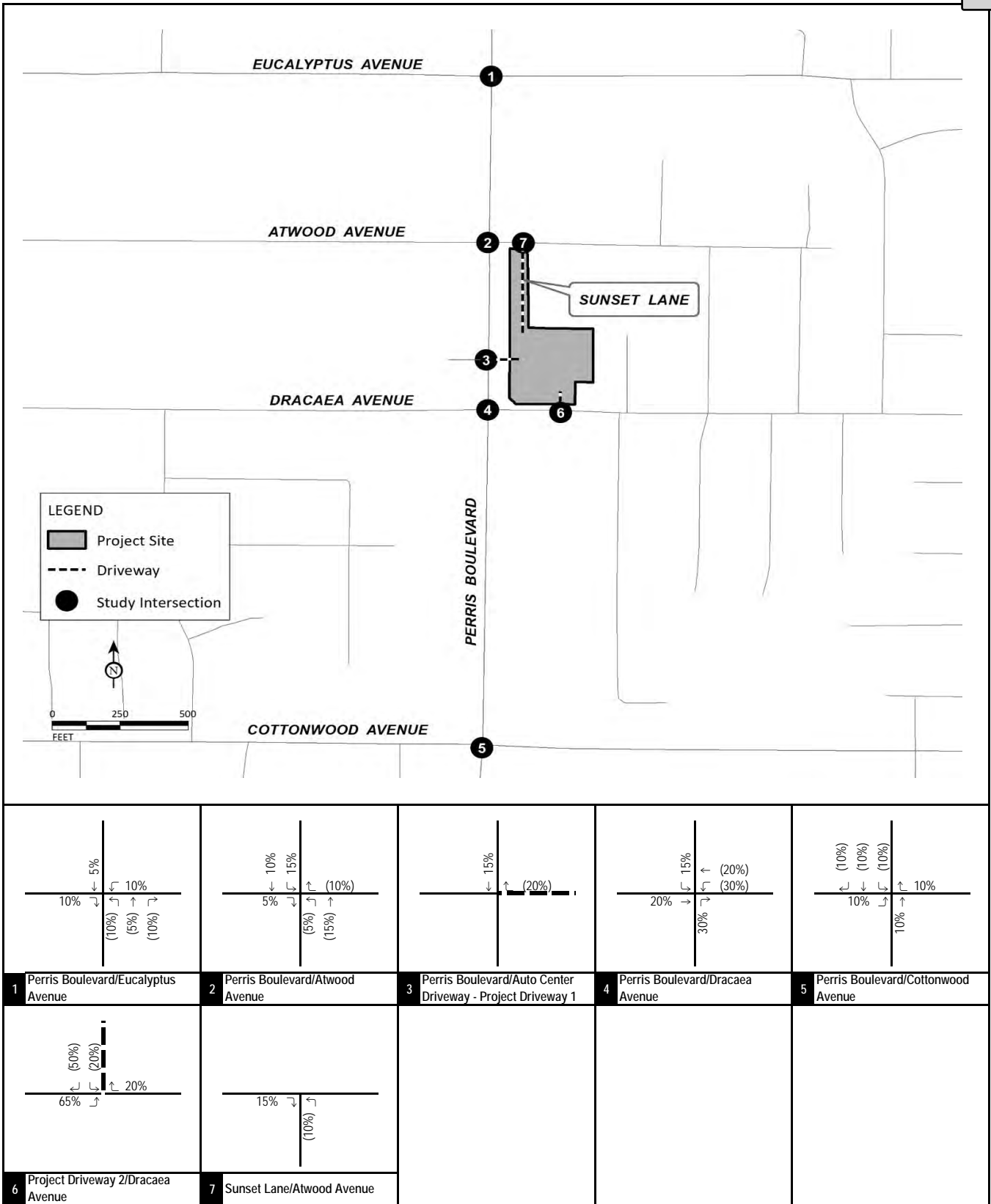


FIGURE 4

LSA

XX% (YY%)

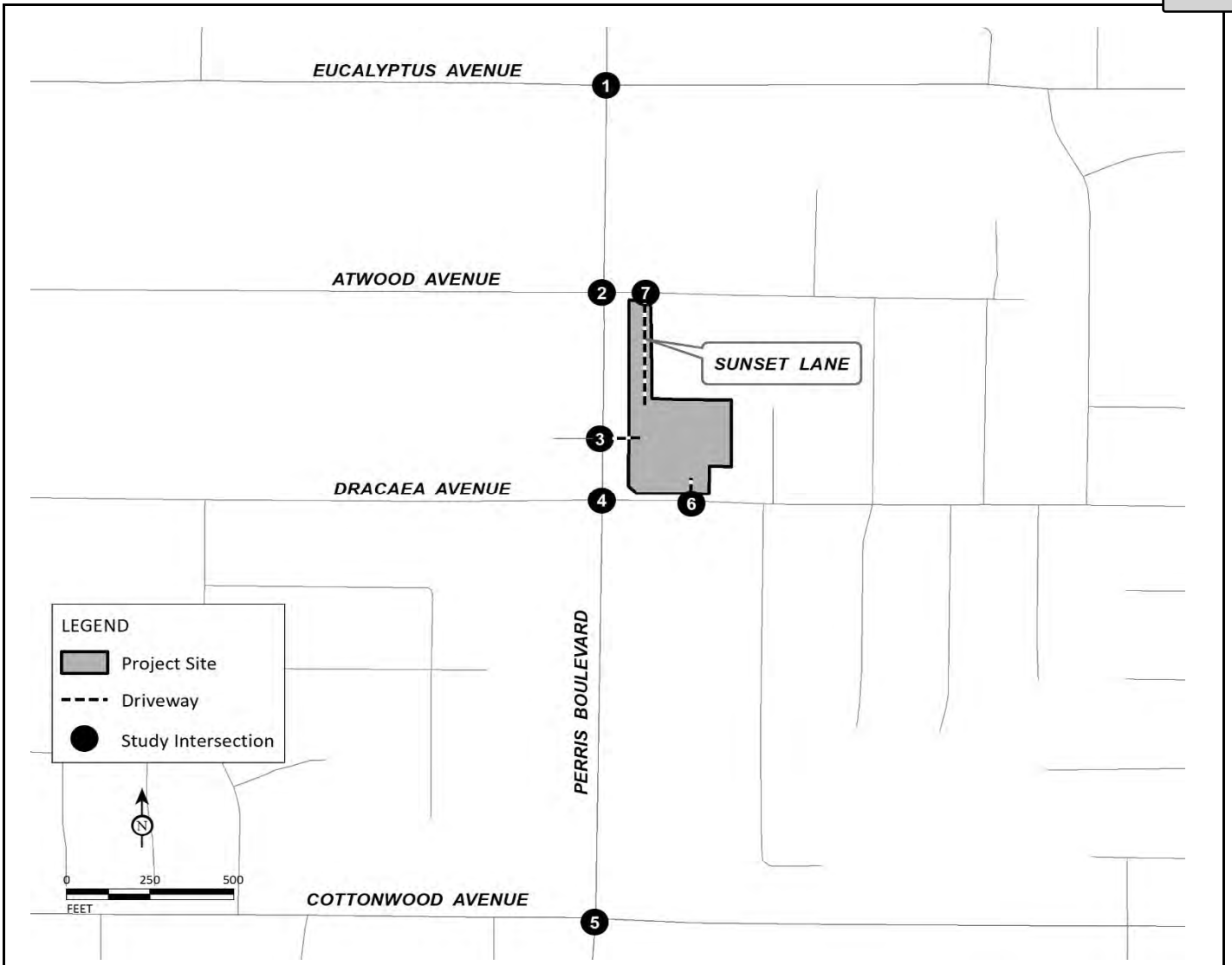
Inbound (Outbound) Trip Distribution

---- Project Driveway

Perris/Dracaea Commercial Project

Traffic Impact Analysis

Project Trip Distribution



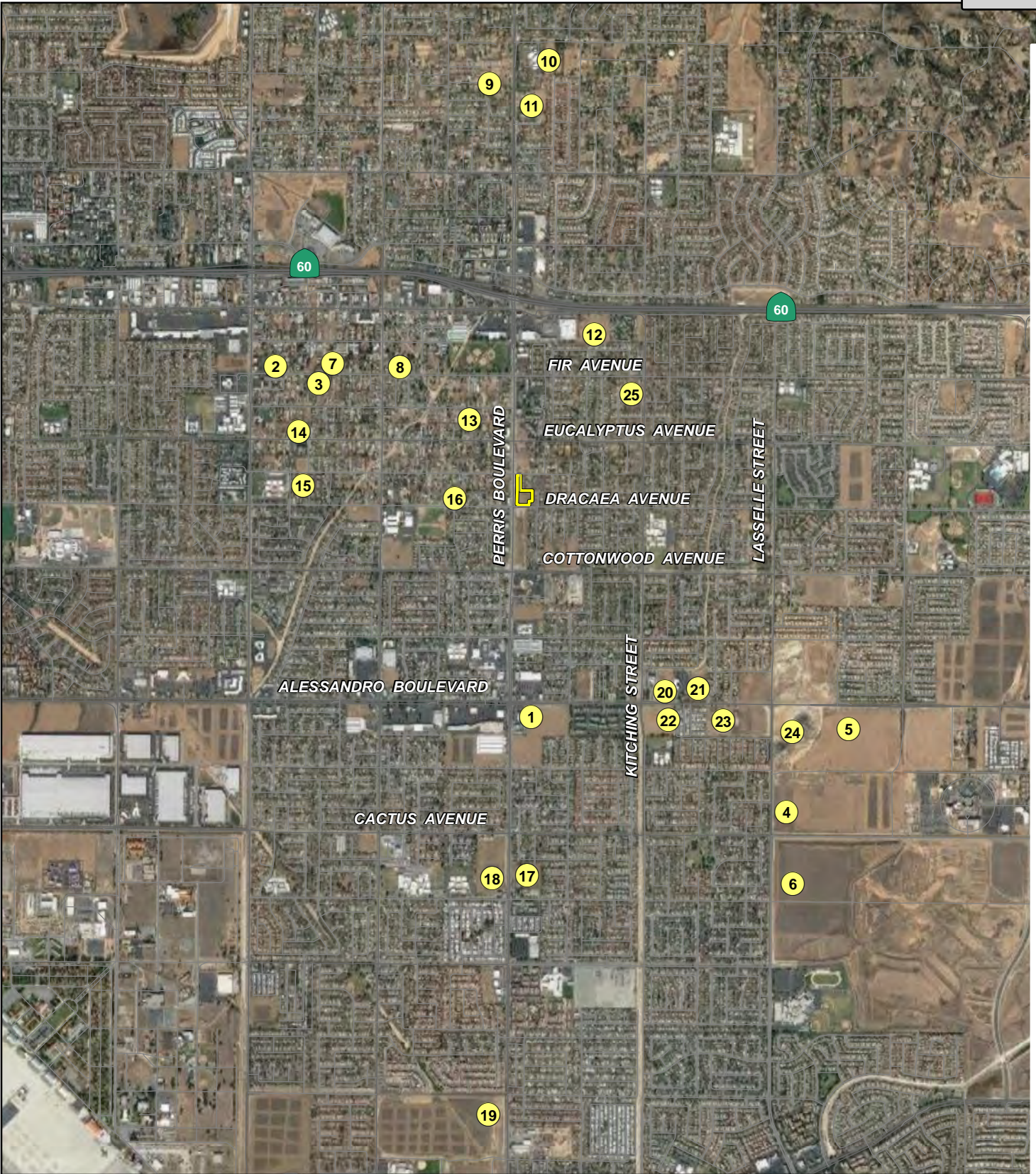
<p>1 Perris Boulevard/Eucalyptus Avenue</p>	<p>2 Perris Boulevard/Atwood Avenue</p>	<p>3 Perris Boulevard/Auto Center Driveway - Project Driveway 1</p>	<p>4 Perris Boulevard/Dracaea Avenue</p>	<p>5 Perris Boulevard/Cottonwood Avenue</p>
<p>6 Project Driveway 2/Dracaea Avenue</p>	<p>7 Sunset Lane/Atwood Avenue</p>			

FIGURE 5



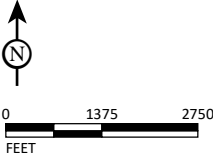
XX / YY
 AM / PM Peak Hour Trips
 - - - - Project Driveway

Perris/Dracaea Commercial Project
 Traffic Impact Analysis
 Net Project Trip Assignment



LSA

- LEGEND
- Project Site
 - Cumulative Project



SOURCE: ESRI Streetmap, 2013; Google Earth, 2018.
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FIGURE (

Perris/Dracaea Commercial Projec
 Traffic Impact Analy
 Cumulative Project Locations

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling



Table A - Project Trip Generation

Land Use	Units	A.M. Peak Hour			P.M. Peak Hour			Daily
		In	Out	Total	In	Out	Total	
Gas Station with Convenience Store	12 VFP							
Trips/Unit ¹		6.36	6.11	12.47	7.13	6.86	13.99	205.36
Trip Generation		76	73	149	86	82	168	2,464
Pass-by Trips ²		(47)	(45)	(92)	(48)	(46)	(94)	(1,454)
Total Net Trips		29	28	57	38	36	74	1,010
Coffee Shop with Drive Through Window	3.00 TSF							
Trips/Unit		45.38	43.61	88.99	21.69	21.69	43.38	820.38
Trip Generation ³		136	131	267	65	65	130	2,461
	Total Trip Generation	212	204	416	151	147	298	4,925
	Pass-By Trips	(47)	(45)	(92)	(48)	(46)	(94)	(1,454)
	Net Trip Generation	165	159	324	103	101	204	3,471

Note:

VFP = Vehicle Fueling Positions; TSF = Thousand Square Feet

¹ Rates from the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (10th Edition), Land Use 945 - "Gasoline/Service Station with Convenience Market", Setting/Location - "General Urban/Suburban."

² Pass-by rates from the ITE *Trip Generation Handbook* (3rd Edition) for Land Use 945 - "Gasoline/Service Station with Convenience Market." A pass-by rate of 62% was used for the a.m. peak hour and a pass-by rate of 56% was used for the p.m. peak hour. Since there is no data available for daily pass-by trips, the average of a.m. and p.m. pass-by rates was used as the daily pass-by rate.

³ Rates from the ITE *Trip Generation Manual* (10th Edition), Land Use 937 - "Coffee/Donut Shop with Drive-Through Window", Setting/Location - "General Urban/Suburban."

Table B - Cumulative Projects

Project	Land Use	Units
1 . PEN16-0039 - LactoSC Inc.	Multi-Family Housing	272 DU
2 . Tract 33771 - Jian Qiang Liu	Multi-Family Housing	12 DU
3 . PEN 16-0066 - Cal Choice Inv. Invc	Multi-Family Housing	20 DU
4 . PA 15-0046 - LA Jolla Development/Rocas Grandes	Multi-Family Housing	426 DU
5 . PA 13-0006 - Rancho Belago Developers, Inc.	Multi-Family Housing	141 DU
6 . PEN 16 - MV Bella Vista GP, LLC	Multi-Family Housing	220 DU
7 . Tract 35663 - Jimmy Lee	Multi-Family Housing	12 DU
8 . PEN 16-0060 - Frederick Homes	Multi-Family Housing	24 DU
9 . Tract 31621 - Victoria Homes "Skyline"	Single-Family Housing	12 DU
10 . Tract 35606 - Metric Homes	Single-Family Housing	16 DU
11 . Tract 31297 - Randy McFarland	Single-Family Housing	7 DU
12 . Tract 31814 - Jesse Huizar	Multi-Family Housing	60 DU
13 . Tract 35369 - Tason Myer Property	Multi-Family Housing	12 DU
14 . Tract 35769 - Michael Chen	Multi-Family Housing	16 DU
15 . PA 09-0006 - Jim Nydam	Multi-Family Housing	15 DU
16 . Tract 35304 - Jimmy Lee	Multi-Family Housing	12 DU
17 . Mo Ghiassi TL Group	Multi-Family Housing	52 DU
18 . Tract 36708 - Nova Homes	Multi-Family Housing	122 DU
19 . Wal-Mart	Commercial	193.00 TSF
20 . Tract 34216 - Creative Design Assoc.	Multi-Family Housing	39 DU
21 . Tract 35429 - Creative Design Assoc.	Multi-Family Housing	58 DU
22 . Tract 35304 - Jimmy Lee	Multi-Family Housing	24 DU
23 . Tract 34681 - Perris Pacific Company	Multi-Family Housing	49 DU
24 . PEN17 - 0064 Boulder Ridge	Multi-Family Housing	141 DU
25 . Tract 34043 - RM3 Building and Development	Single-Family Housing	12 DU

Notes:

DU = Dwelling Units; TSF = Thousand Square Feet

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

APPENDIX B:

TRAFFIC COUNT SHEETS

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	8	151	4	163	6	32	46	84	2	232	3	237	4	11	7	22	506
07:15 AM	7	229	12	248	12	40	53	105	7	213	5	225	4	13	9	26	604
07:30 AM	17	299	7	323	22	56	42	120	9	229	13	251	7	20	15	42	736
07:45 AM	26	276	4	306	28	64	43	135	15	267	21	303	7	30	21	58	802
Total	58	955	27	1040	68	192	184	444	33	941	42	1016	22	74	52	148	2648
08:00 AM	21	175	9	205	20	55	54	129	18	262	16	296	3	10	10	23	653
08:15 AM	13	152	4	169	10	42	27	79	18	206	5	229	7	9	5	21	498
08:30 AM	15	112	8	135	7	28	18	53	7	194	8	209	10	13	4	27	424
08:45 AM	10	142	10	162	7	29	35	71	11	200	4	215	13	14	6	33	481
Total	59	581	31	671	44	154	134	332	54	862	33	949	33	46	25	104	2056
Grand Total	117	1536	58	1711	112	346	318	776	87	1803	75	1965	55	120	77	252	4704
Apprch %	6.8	89.8	3.4		14.4	44.6	41		4.4	91.8	3.8		21.8	47.6	30.6		
Total %	2.5	32.7	1.2	36.4	2.4	7.4	6.8	16.5	1.8	38.3	1.6	41.8	1.2	2.6	1.6	5.4	
Passenger Vehicles	115	1462	58	1635	110	340	312	762	84	1730	70	1884	53	119	73	245	4526
% Passenger Vehicles	98.3	95.2	100	95.6	98.2	98.3	98.1	98.2	96.6	96	93.3	95.9	96.4	99.2	94.8	97.2	96.2
Large 2 Axle Vehicles	2	58	0	60	2	6	6	14	3	44	2	49	2	1	4	7	130
% Large 2 Axle Vehicles	1.7	3.8	0	3.5	1.8	1.7	1.9	1.8	3.4	2.4	2.7	2.5	3.6	0.8	5.2	2.8	2.8
3 Axle Vehicles	0	2	0	2	0	0	0	0	0	2	2	4	0	0	0	0	6
% 3 Axle Vehicles	0	0.1	0	0.1	0	0	0	0	0	0.1	2.7	0.2	0	0	0	0	0.1
4+ Axle Trucks	0	14	0	14	0	0	0	0	0	27	1	28	0	0	0	0	42
% 4+ Axle Trucks	0	0.9	0	0.8	0	0	0	0	0	1.5	1.3	1.4	0	0	0	0	0.9

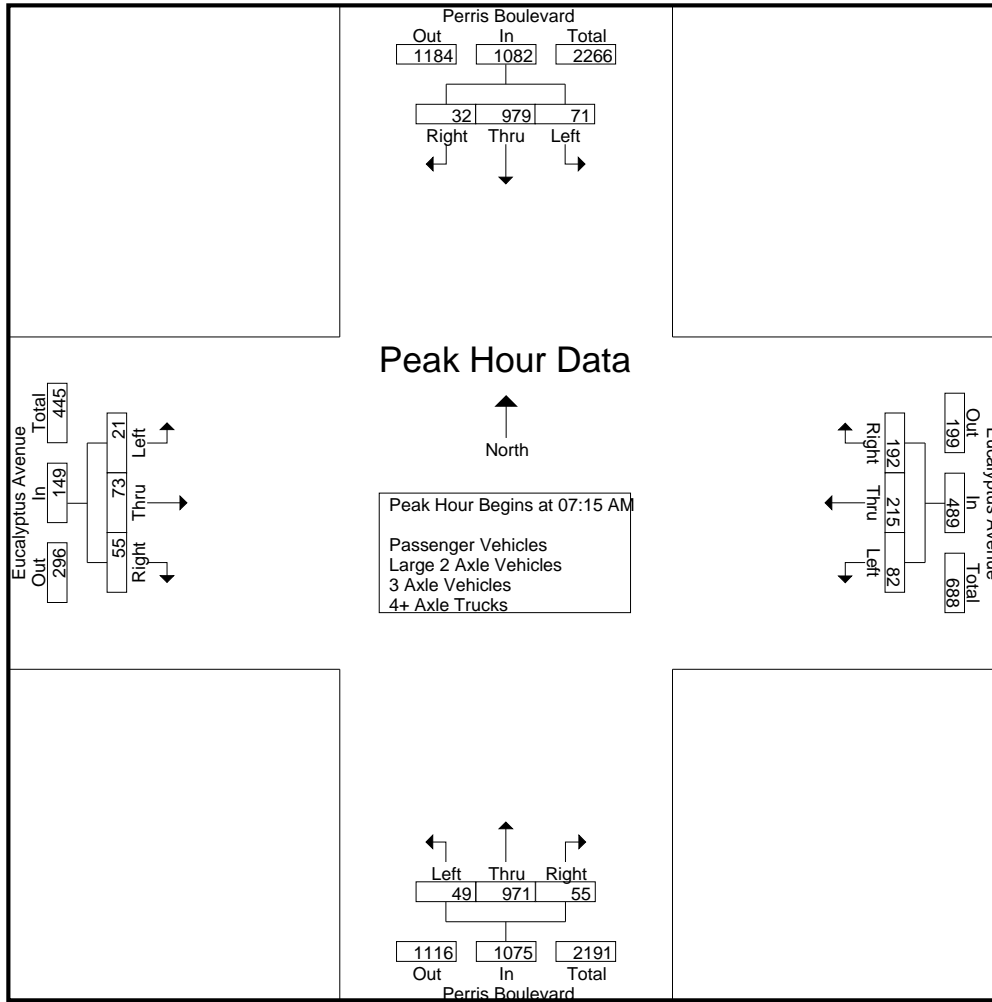
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	7	229	12	248	12	40	53	105	7	213	5	225	4	13	9	26	604
07:30 AM	17	299	7	323	22	56	42	120	9	229	13	251	7	20	15	42	736
07:45 AM	26	276	4	306	28	64	43	135	15	267	21	303	7	30	21	58	802
08:00 AM	21	175	9	205	20	55	54	129	18	262	16	296	3	10	10	23	653
Total Volume	71	979	32	1082	82	215	192	489	49	971	55	1075	21	73	55	149	2795
% App. Total	6.6	90.5	3		16.8	44	39.3		4.6	90.3	5.1		14.1	49	36.9		
PHF	.683	.819	.667	.837	.732	.840	.889	.906	.681	.909	.655	.887	.750	.608	.655	.642	.871

Attachment: Exhibit A to Resolution No. 2020- Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:30 AM				07:15 AM			
+0 mins.	7	229	12	248	12	40	53	105	9	229	13	251	4	13	9	26
+15 mins.	17	299	7	323	22	56	42	120	15	267	21	303	7	20	15	42
+30 mins.	26	276	4	306	28	64	43	135	18	262	16	296	7	30	21	58
+45 mins.	21	175	9	205	20	55	54	129	18	206	5	229	3	10	10	23
Total Volume	71	979	32	1082	82	215	192	489	60	964	55	1079	21	73	55	149
% App. Total	6.6	90.5	3		16.8	44	39.3		5.6	89.3	5.1		14.1	49	36.9	
PHF	.683	.819	.667	.837	.732	.840	.889	.906	.833	.903	.655	.890	.750	.608	.655	.642

Counts Unlimited
 PO Box 1178
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	8	143	4	155	6	31	45	82	2	220	3	225	3	10	7	20	482
07:15 AM	7	223	12	242	11	40	53	104	7	206	4	217	3	13	8	24	587
07:30 AM	16	293	7	316	22	56	42	120	8	220	11	239	7	20	14	41	716
07:45 AM	26	263	4	293	28	62	42	132	15	258	21	294	7	30	21	58	777
Total	57	922	27	1006	67	189	182	438	32	904	39	975	20	73	50	143	2562
08:00 AM	20	168	9	197	20	53	52	125	18	254	15	287	3	10	10	23	632
08:15 AM	13	138	4	155	10	42	26	78	17	196	4	217	7	9	3	19	469
08:30 AM	15	101	8	124	6	28	18	52	7	187	8	202	10	13	4	27	405
08:45 AM	10	133	10	153	7	28	34	69	10	189	4	203	13	14	6	33	458
Total	58	540	31	629	43	151	130	324	52	826	31	909	33	46	23	102	1964
Grand Total	115	1462	58	1635	110	340	312	762	84	1730	70	1884	53	119	73	245	4526
Apprch %	7	89.4	3.5		14.4	44.6	40.9		4.5	91.8	3.7		21.6	48.6	29.8		
Total %	2.5	32.3	1.3	36.1	2.4	7.5	6.9	16.8	1.9	38.2	1.5	41.6	1.2	2.6	1.6	5.4	

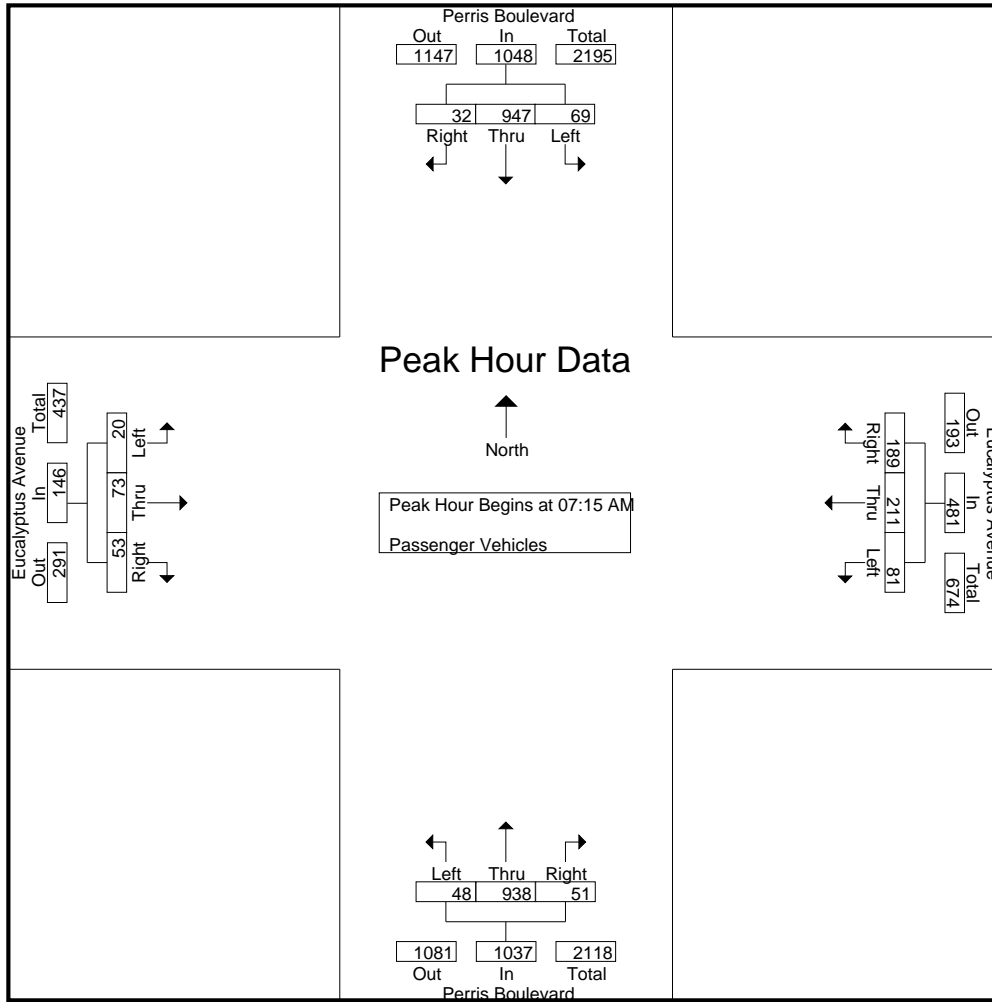
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	7	223	12	242	11	40	53	104	7	206	4	217	3	13	8	24	587
07:30 AM	16	293	7	316	22	56	42	120	8	220	11	239	7	20	14	41	716
07:45 AM	26	263	4	293	28	62	42	132	15	258	21	294	7	30	21	58	777
08:00 AM	20	168	9	197	20	53	52	125	18	254	15	287	3	10	10	23	632
Total Volume	69	947	32	1048	81	211	189	481	48	938	51	1037	20	73	53	146	2712
% App. Total	6.6	90.4	3.1		16.8	43.9	39.3		4.6	90.5	4.9		13.7	50	36.3		
PHF	.663	.808	.667	.829	.723	.851	.892	.911	.667	.909	.607	.882	.714	.608	.631	.629	.873

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	7	223	12	242	11	40	53	104	7	206	4	217	3	13	8	24
+15 mins.	16	293	7	316	22	56	42	120	8	220	11	239	7	20	14	41
+30 mins.	26	263	4	293	28	62	42	132	15	258	21	294	7	30	21	58
+45 mins.	20	168	9	197	20	53	52	125	18	254	15	287	3	10	10	23
Total Volume	69	947	32	1048	81	211	189	481	48	938	51	1037	20	73	53	146
% App. Total	6.6	90.4	3.1		16.8	43.9	39.3		4.6	90.5	4.9		13.7	50	36.3	
PHF	.663	.808	.667	.829	.723	.851	.892	.911	.667	.909	.607	.882	.714	.608	.631	.629

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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Large 2 Axle Vehicles

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	7	0	7	0	1	1	2	0	7	0	7	1	1	0	2	18
07:15 AM	0	6	0	6	1	0	0	1	0	4	0	4	1	0	1	2	13
07:30 AM	1	4	0	5	0	0	0	0	1	7	1	9	0	0	1	1	15
07:45 AM	0	11	0	11	0	2	1	3	0	8	0	8	0	0	0	0	22
Total	1	28	0	29	1	3	2	6	1	26	1	28	2	1	2	5	68
08:00 AM	1	3	0	4	0	2	2	4	0	4	1	5	0	0	0	0	13
08:15 AM	0	10	0	10	0	0	1	1	1	4	0	5	0	0	2	2	18
08:30 AM	0	11	0	11	1	0	0	1	0	4	0	4	0	0	0	0	16
08:45 AM	0	6	0	6	0	1	1	2	1	6	0	7	0	0	0	0	15
Total	1	30	0	31	1	3	4	8	2	18	1	21	0	0	2	2	62
Grand Total	2	58	0	60	2	6	6	14	3	44	2	49	2	1	4	7	130
Apprch %	3.3	96.7	0		14.3	42.9	42.9		6.1	89.8	4.1		28.6	14.3	57.1		
Total %	1.5	44.6	0	46.2	1.5	4.6	4.6	10.8	2.3	33.8	1.5	37.7	1.5	0.8	3.1	5.4	

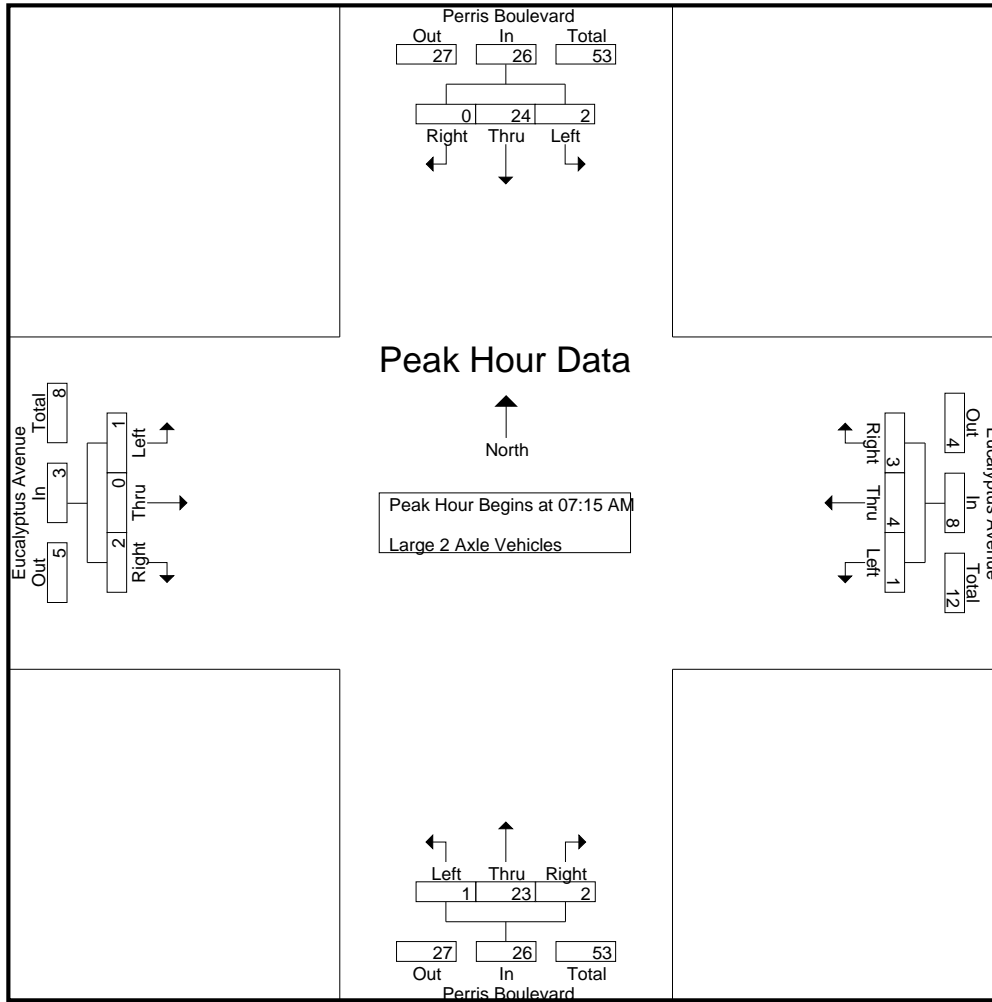
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	6	0	6	1	0	0	1	0	4	0	4	1	0	1	2	13
07:30 AM	1	4	0	5	0	0	0	0	1	7	1	9	0	0	1	1	15
07:45 AM	0	11	0	11	0	2	1	3	0	8	0	8	0	0	0	0	22
08:00 AM	1	3	0	4	0	2	2	4	0	4	1	5	0	0	0	0	13
Total Volume	2	24	0	26	1	4	3	8	1	23	2	26	1	0	2	3	63
% App. Total	7.7	92.3	0		12.5	50	37.5		3.8	88.5	7.7		33.3	0	66.7		
PHF	.500	.545	.000	.591	.250	.500	.375	.500	.250	.719	.500	.722	.250	.000	.500	.375	.716

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
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 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	6	0	6	1	0	0	1	0	4	0	4	1	0	1	2
+15 mins.	1	4	0	5	0	0	0	0	1	7	1	9	0	0	1	1
+30 mins.	0	11	0	11	0	2	1	3	0	8	0	8	0	0	0	0
+45 mins.	1	3	0	4	0	2	2	4	0	4	1	5	0	0	0	0
Total Volume	2	24	0	26	1	4	3	8	1	23	2	26	1	0	2	3
% App. Total	7.7	92.3	0		12.5	50	37.5		3.8	88.5	7.7		33.3	0	66.7	
PHF	.500	.545	.000	.591	.250	.500	.375	.500	.250	.719	.500	.722	.250	.000	.500	.375

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 3 Axle Vehicles

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
07:15 AM	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	2
07:30 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	1
07:45 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	1	0	1	0	0	0	0	0	2	2	4	0	0	0	0	5
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Grand Total	0	2	0	2	0	0	0	0	0	2	2	4	0	0	0	0	6
Apprch %	0	100	0		0	0	0		0	50	50		0	0	0		
Total %	0	33.3	0	33.3	0	0	0	0	0	33.3	33.3	66.7	0	0	0	0	

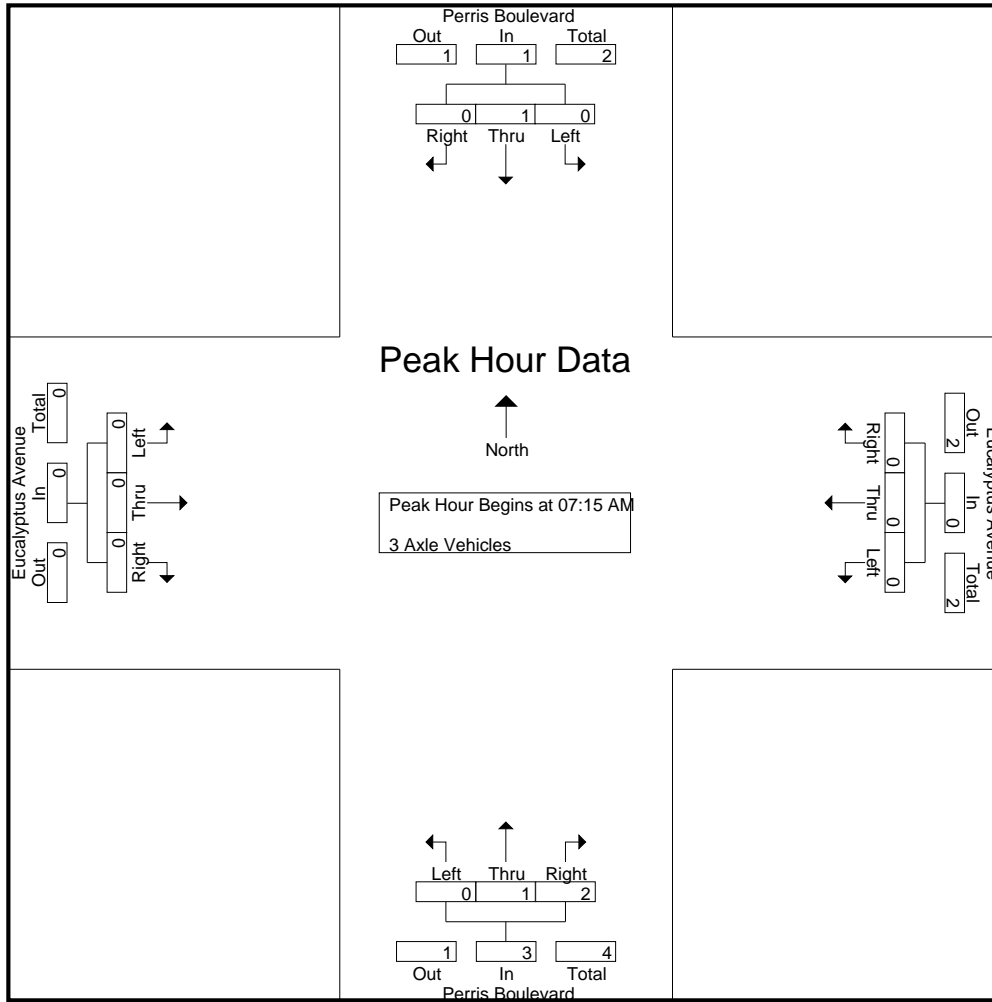
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	2
07:30 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	1
07:45 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	1	0	0	0	0	0	1	2	3	0	0	0	0	4
% App. Total	0	100	0		0	0	0		0	33.3	66.7		0	0	0		
PHF	.000	.250	.000	.250	.000	.000	.000	.000	.000	.250	.500	.375	.000	.000	.000	.000	.500

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0
+15 mins.	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+45 mins.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	1	0	0	0	0	0	1	2	3	0	0	0	0
% App. Total	0	100	0	0	0	0	0	0	0	33.3	66.7		0	0	0	0
PHF	.000	.250	.000	.250	.000	.000	.000	.000	.000	.250	.500	.375	.000	.000	.000	.000

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Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	1	0	1	0	0	0	0	0	4	0	4	0	0	0	0	5
07:15 AM	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2
07:30 AM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
07:45 AM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
Total	0	4	0	4	0	0	0	0	0	9	0	9	0	0	0	0	13
08:00 AM	0	4	0	4	0	0	0	0	0	4	0	4	0	0	0	0	8
08:15 AM	0	3	0	3	0	0	0	0	0	6	1	7	0	0	0	0	10
08:30 AM	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	3
08:45 AM	0	3	0	3	0	0	0	0	0	5	0	5	0	0	0	0	8
Total	0	10	0	10	0	0	0	0	0	18	1	19	0	0	0	0	29
Grand Total	0	14	0	14	0	0	0	0	0	27	1	28	0	0	0	0	42
Apprch %	0	100	0		0	0	0		0	96.4	3.6		0	0	0		
Total %	0	33.3	0	33.3	0	0	0	0	0	64.3	2.4	66.7	0	0	0	0	

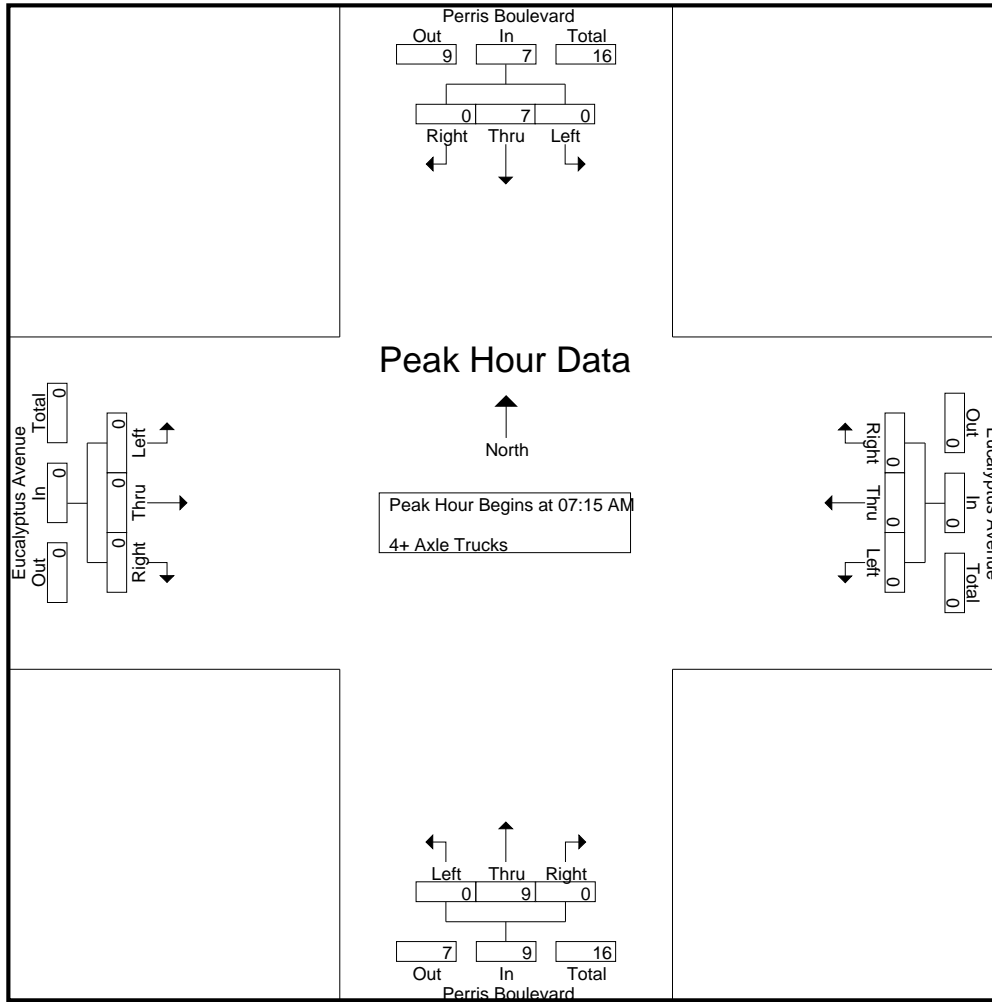
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2
07:30 AM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
07:45 AM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
08:00 AM	0	4	0	4	0	0	0	0	0	4	0	4	0	0	0	0	8
Total Volume	0	7	0	7	0	0	0	0	0	9	0	9	0	0	0	0	16
% App. Total	0	100	0		0	0	0		0	100	0		0	0	0		
PHF	.000	.438	.000	.438	.000	.000	.000	.000	.000	.563	.000	.563	.000	.000	.000	.000	.500

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0
+15 mins.	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0
+45 mins.	0	4	0	4	0	0	0	0	0	4	0	4	0	0	0	0
Total Volume	0	7	0	7	0	0	0	0	0	9	0	9	0	0	0	0
% App. Total	0	100	0	0	0	0	0	0	0	100	0	0	0	0	0	0
PHF	.000	.438	.000	.438	.000	.000	.000	.000	.000	.563	.000	.563	.000	.000	.000	.000

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 PO Box 1178
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	24	259	6	289	7	29	36	72	10	218	17	245	15	50	20	85	691
04:15 PM	20	253	8	281	12	24	35	71	11	260	9	280	12	42	18	72	704
04:30 PM	25	276	7	308	16	28	24	68	10	252	15	277	14	29	20	63	716
04:45 PM	17	255	14	286	10	30	21	61	11	241	17	269	7	34	14	55	671
Total	86	1043	35	1164	45	111	116	272	42	971	58	1071	48	155	72	275	2782
05:00 PM	27	268	15	310	12	30	35	77	16	236	10	262	11	64	25	100	749
05:15 PM	27	241	9	277	14	39	28	81	13	234	16	263	19	59	19	97	718
05:30 PM	26	271	7	304	7	35	27	69	11	213	11	235	9	56	13	78	686
05:45 PM	17	253	17	287	13	33	29	75	9	240	13	262	10	38	17	65	689
Total	97	1033	48	1178	46	137	119	302	49	923	50	1022	49	217	74	340	2842
Grand Total	183	2076	83	2342	91	248	235	574	91	1894	108	2093	97	372	146	615	5624
Apprch %	7.8	88.6	3.5		15.9	43.2	40.9		4.3	90.5	5.2		15.8	60.5	23.7		
Total %	3.3	36.9	1.5	41.6	1.6	4.4	4.2	10.2	1.6	33.7	1.9	37.2	1.7	6.6	2.6	10.9	
Passenger Vehicles	181	2024	83	2288	91	248	234	573	88	1856	107	2051	97	368	145	610	5522
% Passenger Vehicles	98.9	97.5	100	97.7	100	100	99.6	99.8	96.7	98	99.1	98	100	98.9	99.3	99.2	98.2
Large 2 Axle Vehicles	2	32	0	34	0	0	1	1	3	27	0	30	0	3	0	3	68
% Large 2 Axle Vehicles	1.1	1.5	0	1.5	0	0	0.4	0.2	3.3	1.4	0	1.4	0	0.8	0	0.5	1.2
3 Axle Vehicles	0	5	0	5	0	0	0	0	0	2	1	3	0	0	1	1	9
% 3 Axle Vehicles	0	0.2	0	0.2	0	0	0	0	0	0.1	0.9	0.1	0	0	0.7	0.2	0.2
4+ Axle Trucks	0	15	0	15	0	0	0	0	0	9	0	9	0	1	0	1	25
% 4+ Axle Trucks	0	0.7	0	0.6	0	0	0	0	0	0.5	0	0.4	0	0.3	0	0.2	0.4

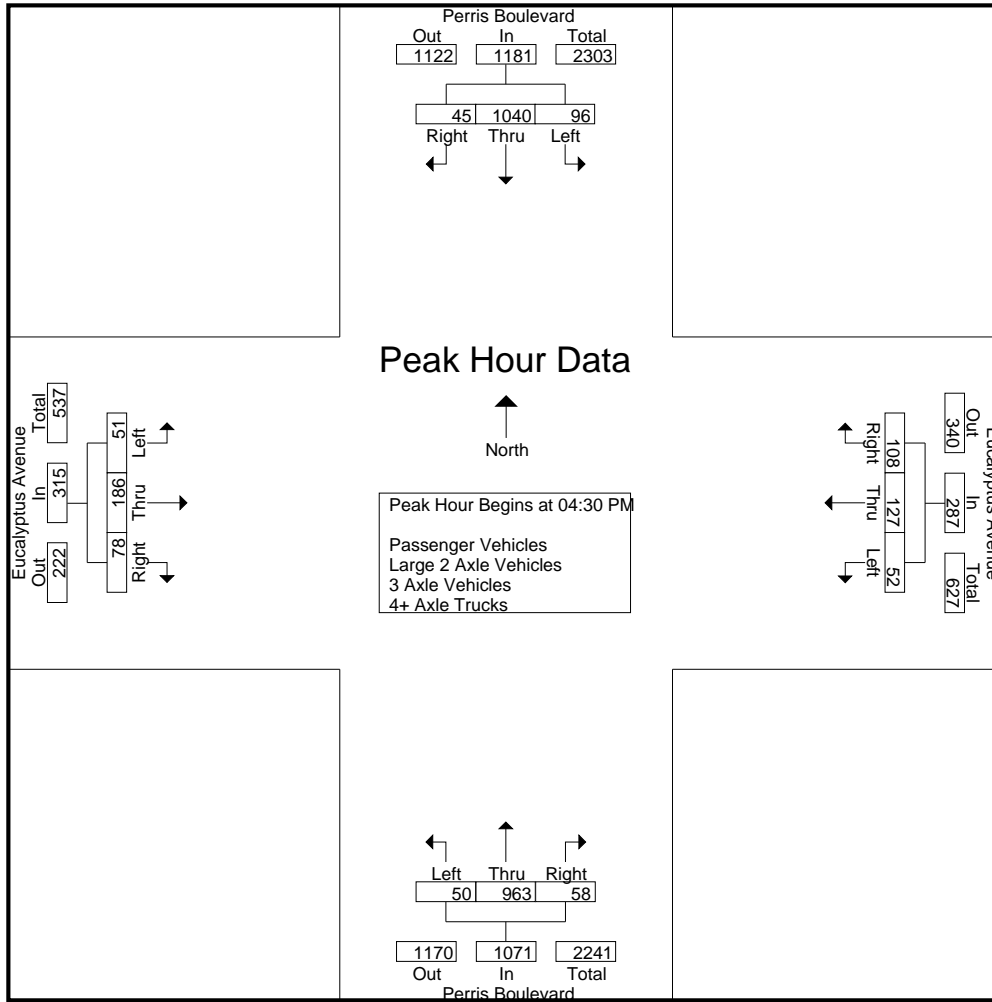
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	25	276	7	308	16	28	24	68	10	252	15	277	14	29	20	63	716
04:45 PM	17	255	14	286	10	30	21	61	11	241	17	269	7	34	14	55	671
05:00 PM	27	268	15	310	12	30	35	77	16	236	10	262	11	64	25	100	749
05:15 PM	27	241	9	277	14	39	28	81	13	234	16	263	19	59	19	97	718
Total Volume	96	1040	45	1181	52	127	108	287	50	963	58	1071	51	186	78	315	2854
% App. Total	8.1	88.1	3.8		18.1	44.3	37.6		4.7	89.9	5.4		16.2	59	24.8		
PHF	.889	.942	.750	.952	.813	.814	.771	.886	.781	.955	.853	.967	.671	.727	.780	.788	.953

Attachment: Exhibit A to Resolution No. 2020- Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				05:00 PM				04:15 PM				05:00 PM			
+0 mins.	20	253	8	281	12	30	35	77	11	260	9	280	11	64	25	100
+15 mins.	25	276	7	308	14	39	28	81	10	252	15	277	19	59	19	97
+30 mins.	17	255	14	286	7	35	27	69	11	241	17	269	9	56	13	78
+45 mins.	27	268	15	310	13	33	29	75	16	236	10	262	10	38	17	65
Total Volume	89	1052	44	1185	46	137	119	302	48	989	51	1088	49	217	74	340
% App. Total	7.5	88.8	3.7		15.2	45.4	39.4		4.4	90.9	4.7		14.4	63.8	21.8	
PHF	.824	.953	.733	.956	.821	.878	.850	.932	.750	.951	.750	.971	.645	.848	.740	.850

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	24	251	6	281	7	29	35	71	8	210	17	235	15	49	20	84	671
04:15 PM	20	240	8	268	12	24	35	71	11	257	9	277	12	42	18	72	688
04:30 PM	24	269	7	300	16	28	24	68	10	248	15	273	14	28	19	61	702
04:45 PM	17	248	14	279	10	30	21	61	10	238	17	265	7	34	14	55	660
Total	85	1008	35	1128	45	111	115	271	39	953	58	1050	48	153	71	272	2721
05:00 PM	27	264	15	306	12	30	35	77	16	231	9	256	11	64	25	100	739
05:15 PM	26	235	9	270	14	39	28	81	13	229	16	258	19	58	19	96	705
05:30 PM	26	269	7	302	7	35	27	69	11	212	11	234	9	56	13	78	683
05:45 PM	17	248	17	282	13	33	29	75	9	231	13	253	10	37	17	64	674
Total	96	1016	48	1160	46	137	119	302	49	903	49	1001	49	215	74	338	2801
Grand Total	181	2024	83	2288	91	248	234	573	88	1856	107	2051	97	368	145	610	5522
Apprch %	7.9	88.5	3.6		15.9	43.3	40.8		4.3	90.5	5.2		15.9	60.3	23.8		
Total %	3.3	36.7	1.5	41.4	1.6	4.5	4.2	10.4	1.6	33.6	1.9	37.1	1.8	6.7	2.6	11	

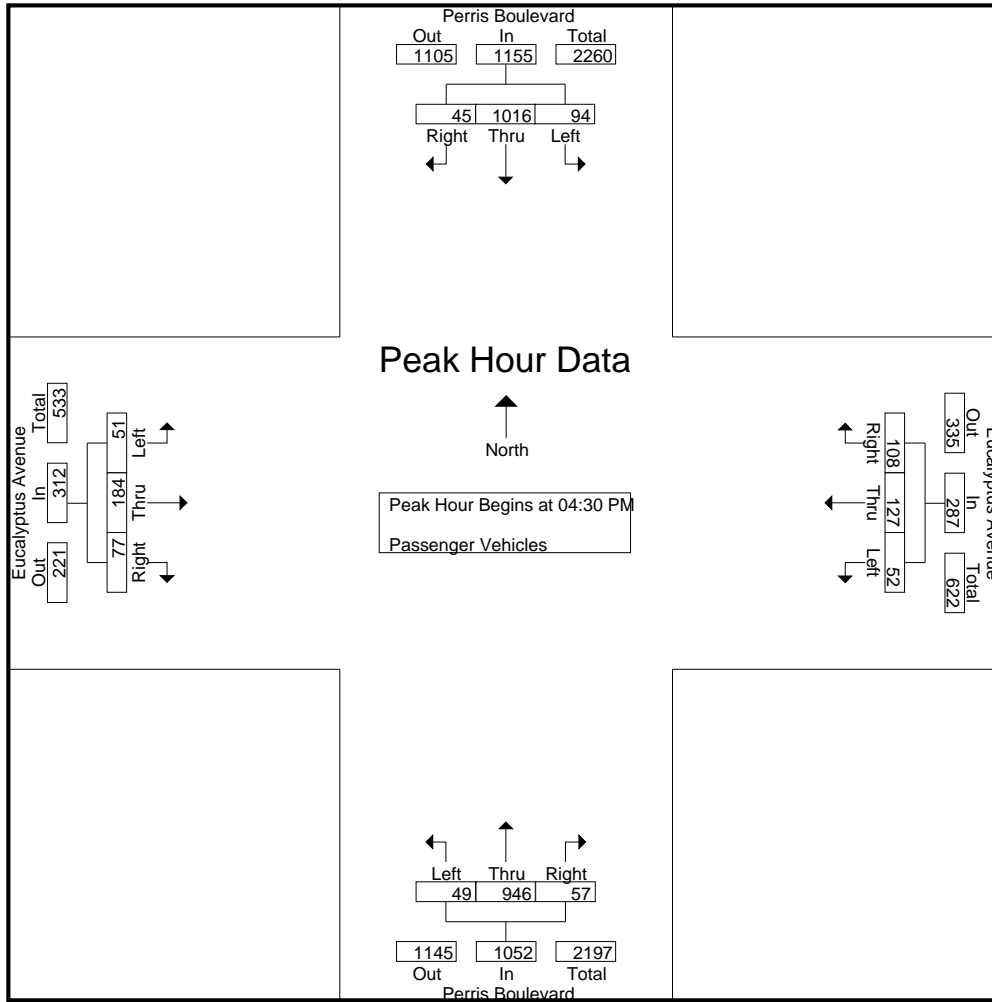
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	24	269	7	300	16	28	24	68	10	248	15	273	14	28	19	61	702
04:45 PM	17	248	14	279	10	30	21	61	10	238	17	265	7	34	14	55	660
05:00 PM	27	264	15	306	12	30	35	77	16	231	9	256	11	64	25	100	739
05:15 PM	26	235	9	270	14	39	28	81	13	229	16	258	19	58	19	96	705
Total Volume	94	1016	45	1155	52	127	108	287	49	946	57	1052	51	184	77	312	2806
% App. Total	8.1	88	3.9		18.1	44.3	37.6		4.7	89.9	5.4		16.3	59	24.7		
PHF	.870	.944	.750	.944	.813	.814	.771	.886	.766	.954	.838	.963	.671	.719	.770	.780	.949

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	24	269	7	300	16	28	24	68	10	248	15	273	14	28	19	61
+15 mins.	17	248	14	279	10	30	21	61	10	238	17	265	7	34	14	55
+30 mins.	27	264	15	306	12	30	35	77	16	231	9	256	11	64	25	100
+45 mins.	26	235	9	270	14	39	28	81	13	229	16	258	19	58	19	96
Total Volume	94	1016	45	1155	52	127	108	287	49	946	57	1052	51	184	77	312
% App. Total	8.1	88	3.9		18.1	44.3	37.6		4.7	89.9	5.4		16.3	59	24.7	
PHF	.870	.944	.750	.944	.813	.814	.771	.886	.766	.954	.838	.963	.671	.719	.770	.780

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Large 2 Axle Vehicles

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	5	0	5	0	0	1	1	2	6	0	8	0	1	0	1	15
04:15 PM	0	6	0	6	0	0	0	0	0	3	0	3	0	0	0	0	9
04:30 PM	1	4	0	5	0	0	0	0	0	2	0	2	0	1	0	1	8
04:45 PM	0	4	0	4	0	0	0	0	1	3	0	4	0	0	0	0	8
Total	1	19	0	20	0	0	1	1	3	14	0	17	0	2	0	2	40
05:00 PM	0	3	0	3	0	0	0	0	0	4	0	4	0	0	0	0	7
05:15 PM	1	4	0	5	0	0	0	0	0	3	0	3	0	0	0	0	8
05:30 PM	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:45 PM	0	4	0	4	0	0	0	0	0	6	0	6	0	1	0	1	11
Total	1	13	0	14	0	0	0	0	0	13	0	13	0	1	0	1	28
Grand Total	2	32	0	34	0	0	1	1	3	27	0	30	0	3	0	3	68
Apprch %	5.9	94.1	0		0	0	100		10	90	0		0	100	0		
Total %	2.9	47.1	0	50	0	0	1.5	1.5	4.4	39.7	0	44.1	0	4.4	0	4.4	

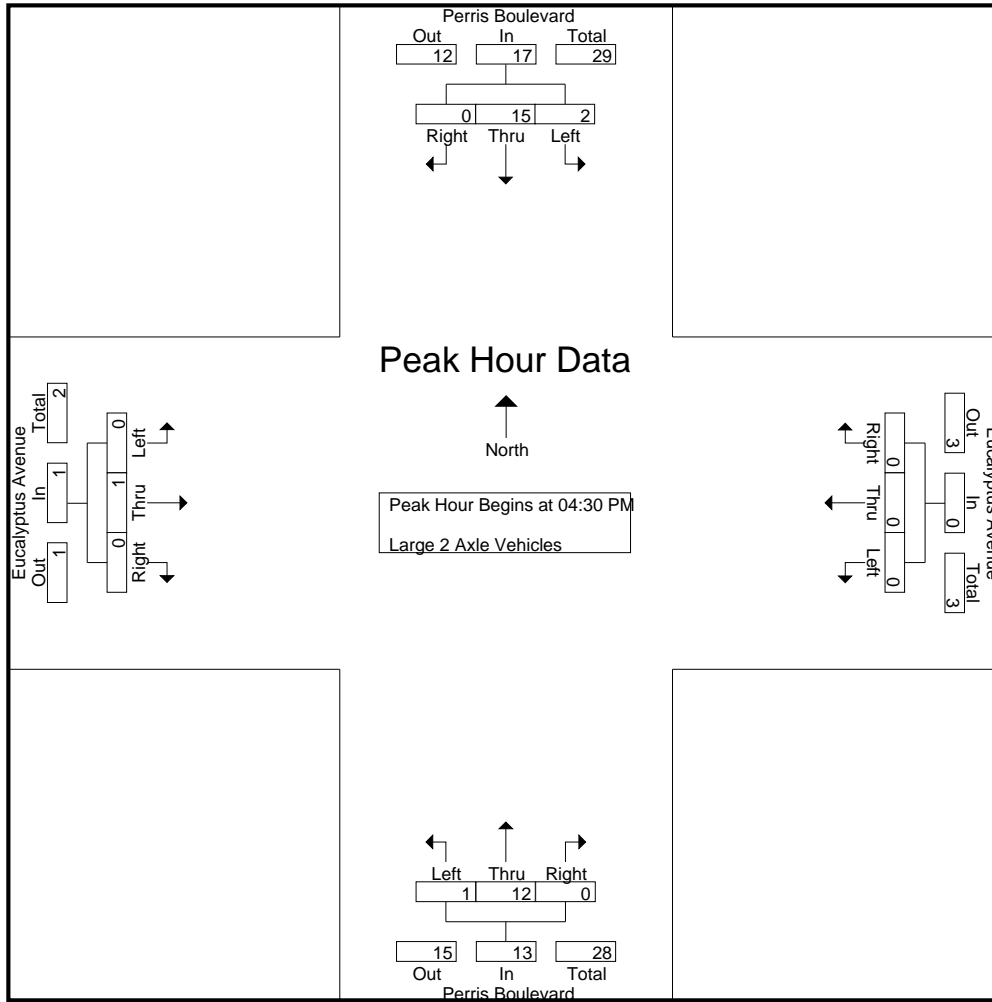
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	1	4	0	5	0	0	0	0	0	2	0	2	0	1	0	1	8
04:45 PM	0	4	0	4	0	0	0	0	1	3	0	4	0	0	0	0	8
05:00 PM	0	3	0	3	0	0	0	0	0	4	0	4	0	0	0	0	7
05:15 PM	1	4	0	5	0	0	0	0	0	3	0	3	0	0	0	0	8
Total Volume	2	15	0	17	0	0	0	0	1	12	0	13	0	1	0	1	31
% App. Total	11.8	88.2	0		0	0	0		7.7	92.3	0		0	100	0		
PHF	.500	.938	.000	.850	.000	.000	.000	.000	.250	.750	.000	.813	.000	.250	.000	.250	.969

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 PO Box 1178
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	1	4	0	5	0	0	0	0	0	2	0	2	0	1	0	1
+15 mins.	0	4	0	4	0	0	0	0	1	3	0	4	0	0	0	0
+30 mins.	0	3	0	3	0	0	0	0	0	4	0	4	0	0	0	0
+45 mins.	1	4	0	5	0	0	0	0	0	3	0	3	0	0	0	0
Total Volume	2	15	0	17	0	0	0	0	1	12	0	13	0	1	0	1
% App. Total	11.8	88.2	0		0	0	0		7.7	92.3	0		0	100	0	
PHF	.500	.938	.000	.850	.000	.000	.000	.000	.250	.750	.000	.813	.000	.250	.000	.250

Counts Unlimited
 PO Box 1178
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City of Moreno Valley
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 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 3 Axle Vehicles

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
04:15 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:30 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	1	2
04:45 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	4	0	4	0	0	0	0	0	1	0	1	0	0	1	1	6
05:00 PM	0	1	0	1	0	0	0	0	0	0	1	1	0	0	0	0	2
05:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	0	0	0	0	0	1	1	2	0	0	0	0	3
Grand Total	0	5	0	5	0	0	0	0	0	2	1	3	0	0	1	1	9
Apprch %	0	100	0		0	0	0		0	66.7	33.3		0	0	100		
Total %	0	55.6	0	55.6	0	0	0	0	0	22.2	11.1	33.3	0	0	11.1	11.1	

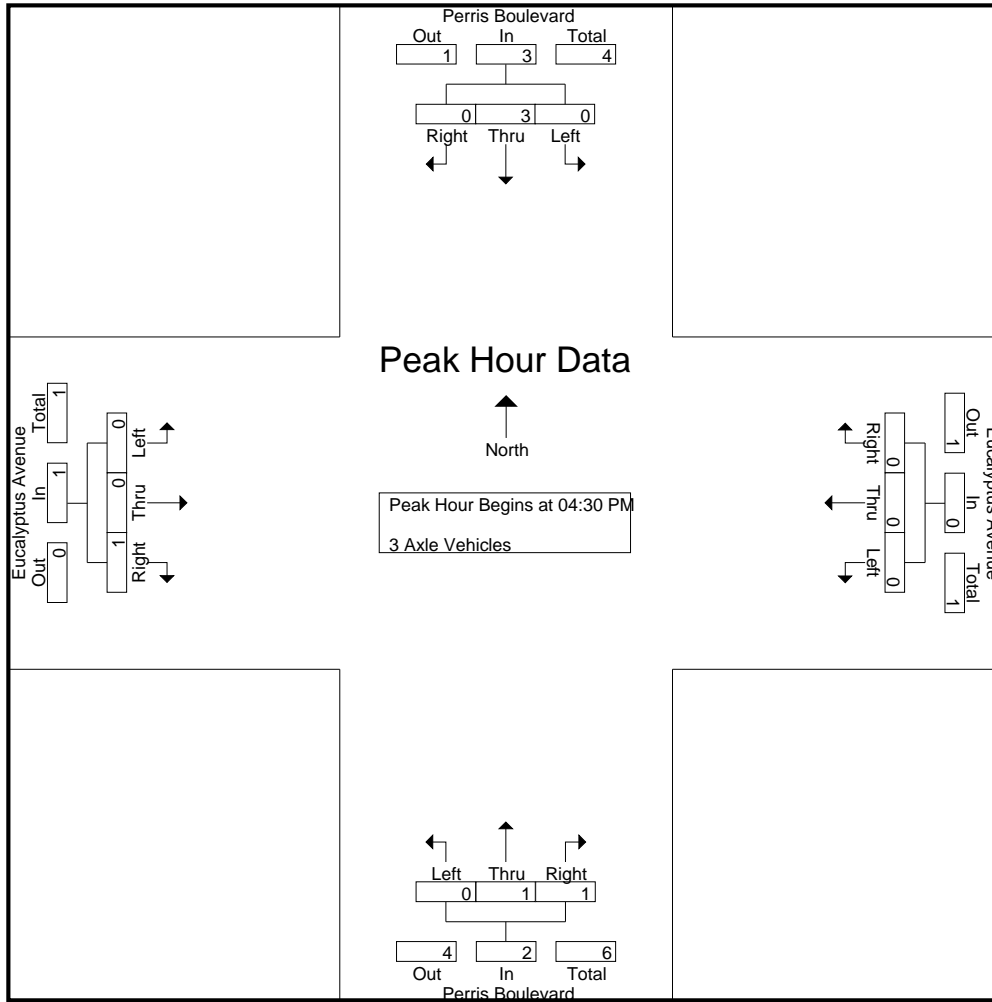
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	1	2
04:45 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	1	0	1	0	0	0	0	0	0	1	1	0	0	0	0	2
05:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
Total Volume	0	3	0	3	0	0	0	0	0	1	1	2	0	0	1	1	6
% App. Total	0	100	0		0	0	0		0	50	50		0	0	100		
PHF	.000	.750	.000	.750	.000	.000	.000	.000	.000	.250	.250	.500	.000	.000	.250	.250	.750

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	1
+15 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	0	1	1	0	0	0	0
+45 mins.	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0
Total Volume	0	3	0	3	0	0	0	0	0	1	1	2	0	0	1	1
% App. Total	0	100	0		0	0	0		0	50	50		0	0	100	
PHF	.000	.750	.000	.750	.000	.000	.000	.000	.000	.250	.250	.500	.000	.000	.250	.250

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:15 PM	0	6	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
04:30 PM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
04:45 PM	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	0	12	0	12	0	0	0	0	0	3	0	3	0	0	0	0	15
05:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:15 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	1	0	1	4
05:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:45 PM	0	1	0	1	0	0	0	0	0	3	0	3	0	0	0	0	4
Total	0	3	0	3	0	0	0	0	0	6	0	6	0	1	0	1	10
Grand Total	0	15	0	15	0	0	0	0	0	9	0	9	0	1	0	1	25
Apprch %	0	100	0		0	0	0		0	100	0		0	100	0		
Total %	0	60	0	60	0	0	0	0	0	36	0	36	0	4	0	4	

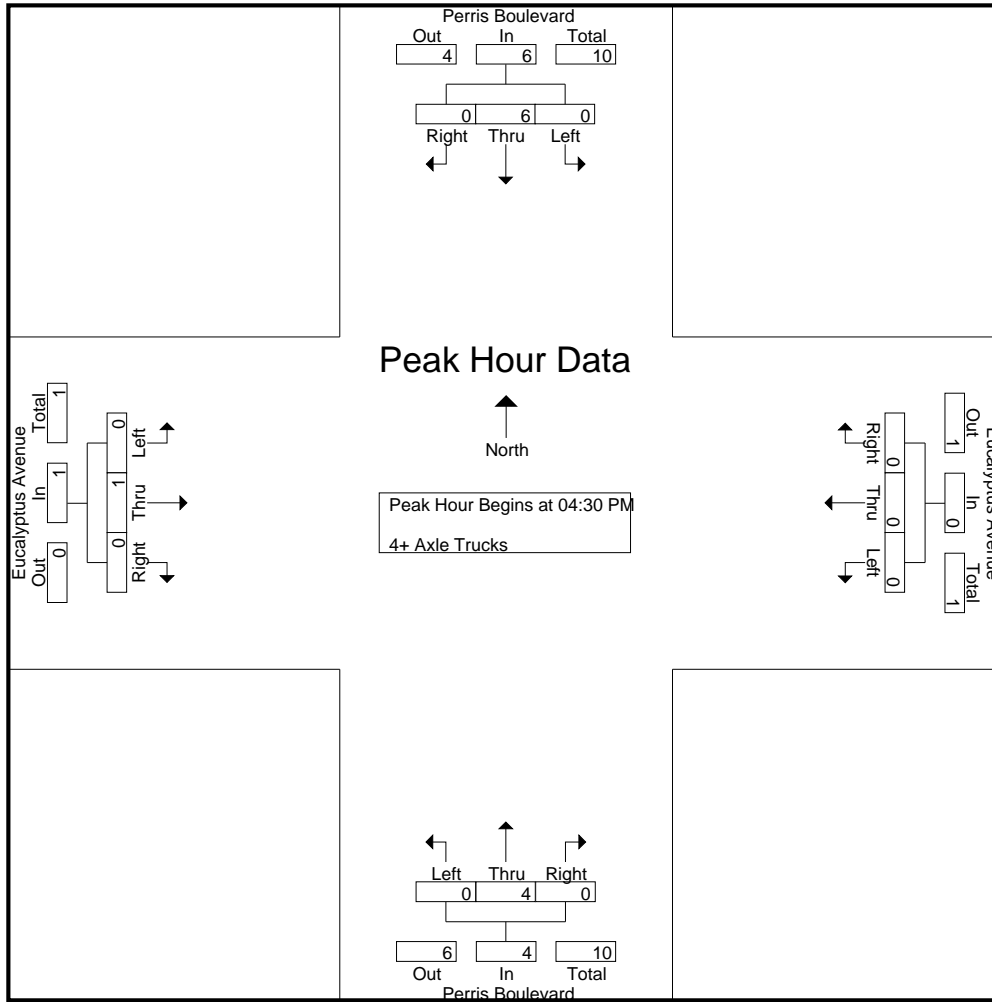
Start Time	Perris Boulevard Southbound				Eucalyptus Avenue Westbound				Perris Boulevard Northbound				Eucalyptus Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
04:45 PM	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:15 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	1	0	1	4
Total Volume	0	6	0	6	0	0	0	0	0	4	0	4	0	1	0	1	11
% App. Total	0	100	0		0	0	0		0	100	0		0	100	0		
PHF	.000	.750	.000	.750	.000	.000	.000	.000	.000	.500	.000	.500	.000	.250	.000	.250	.688

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue
 Weather: Clear

File Name : 01_MRV_Perris_Eucalyptus PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0
+15 mins.	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0
+30 mins.	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0
+45 mins.	0	2	0	2	0	0	0	0	0	1	0	1	0	1	0	1
Total Volume	0	6	0	6	0	0	0	0	0	4	0	4	0	1	0	1
% App. Total	0	100	0		0	0	0		0	100	0		0	100	0	
PHF	.000	.750	.000	.750	.000	.000	.000	.000	.000	.500	.000	.500	.000	.250	.000	.250

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue



Date: 5/23/2019
 Day: Thursday

PEDESTRIANS

	North Leg Perris Boulevard Pedestrians	East Leg Eucalyptus Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Eucalyptus Avenue Pedestrians	
7:00 AM	0	0	0	0	0
7:15 AM	0	1	0	1	2
7:30 AM	1	0	0	0	1
7:45 AM	0	1	0	1	2
8:00 AM	0	2	2	0	4
8:15 AM	0	2	1	2	5
8:30 AM	0	0	0	1	1
8:45 AM	2	2	0	1	5
TOTAL VOLUMES:	3	8	3	6	20

	North Leg Perris Boulevard Pedestrians	East Leg Eucalyptus Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Eucalyptus Avenue Pedestrians	
4:00 PM	0	0	0	0	0
4:15 PM	1	0	1	1	3
4:30 PM	0	1	0	2	3
4:45 PM	0	0	0	0	0
5:00 PM	0	0	1	1	2
5:15 PM	0	1	1	3	5
5:30 PM	1	0	0	1	2
5:45 PM	0	0	0	0	0
TOTAL VOLUMES:	2	2	3	8	15

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Eucalyptus Avenue



Date: 5/23/2019
 Day: Thursday

BICYCLES

	Southbound Perris Boulevard			Westbound Eucalyptus Avenue			Northbound Perris Boulevard			Eastbound Eucalyptus Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	1	0	0	0	0	0	0	1	0	2
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	1	0	1	0	0	0	0	0	0	1	0	3

	Southbound Perris Boulevard			Westbound Eucalyptus Avenue			Northbound Perris Boulevard			Eastbound Eucalyptus Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	1	0	0	0	0	0	1	0	0	0	0	2
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	2	0	0	0	0	2
TOTAL VOLUMES:	0	3	0	0	0	0	0	3	0	0	0	0	6

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Atwood Avenue
 Weather: Clear

File Name : 02_MRV_Perris_Atwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Total Volume

Start Time	Perris Boulevard Southbound				Atwood Avenue Westbound				Perris Boulevard Northbound				Atwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	4	148	2	154	1	0	3	4	4	228	1	233	4	0	7	11	402
07:15 AM	1	250	2	253	1	0	5	6	8	210	0	218	4	0	3	7	484
07:30 AM	8	321	4	333	0	0	1	1	11	246	1	258	2	0	16	18	610
07:45 AM	5	332	5	342	1	0	2	3	18	306	0	324	3	1	13	17	686
Total	18	1051	13	1082	3	0	11	14	41	990	2	1033	13	1	39	53	2182
08:00 AM	2	195	5	202	2	2	1	5	15	271	2	288	4	0	5	9	504
08:15 AM	2	153	2	157	0	0	2	2	13	219	1	233	1	0	3	4	396
08:30 AM	2	116	4	122	1	1	2	4	6	209	0	215	1	1	0	2	343
08:45 AM	4	146	1	151	0	0	1	1	4	208	0	212	2	0	5	7	371
Total	10	610	12	632	3	3	6	12	38	907	3	948	8	1	13	22	1614
Grand Total	28	1661	25	1714	6	3	17	26	79	1897	5	1981	21	2	52	75	3796
Apprch %	1.6	96.9	1.5		23.1	11.5	65.4		4	95.8	0.3		28	2.7	69.3		
Total %	0.7	43.8	0.7	45.2	0.2	0.1	0.4	0.7	2.1	50	0.1	52.2	0.6	0.1	1.4	2	

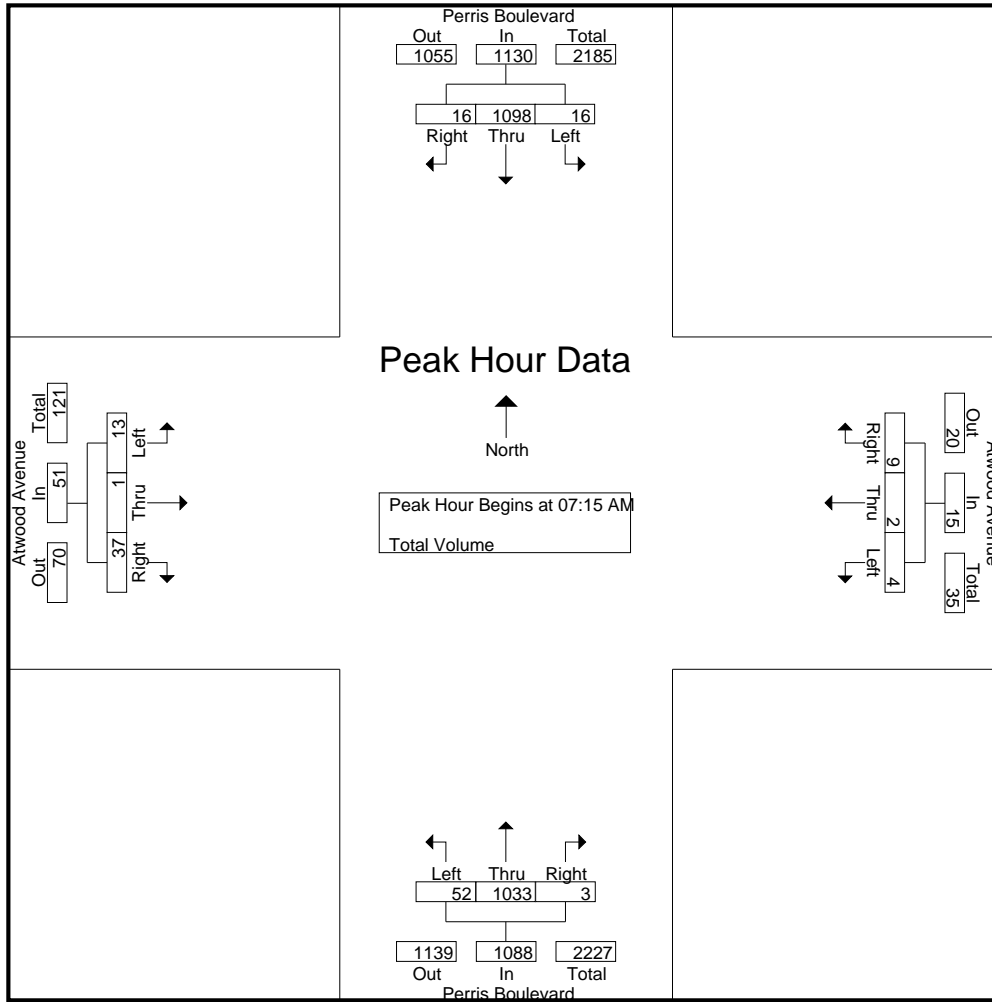
Start Time	Perris Boulevard Southbound				Atwood Avenue Westbound				Perris Boulevard Northbound				Atwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	1	250	2	253	1	0	5	6	8	210	0	218	4	0	3	7	484
07:30 AM	8	321	4	333	0	0	1	1	11	246	1	258	2	0	16	18	610
07:45 AM	5	332	5	342	1	0	2	3	18	306	0	324	3	1	13	17	686
08:00 AM	2	195	5	202	2	2	1	5	15	271	2	288	4	0	5	9	504
Total Volume	16	1098	16	1130	4	2	9	15	52	1033	3	1088	13	1	37	51	2284
% App. Total	1.4	97.2	1.4		26.7	13.3	60		4.8	94.9	0.3		25.5	2	72.5		
PHF	.500	.827	.800	.826	.500	.250	.450	.625	.722	.844	.375	.840	.813	.250	.578	.708	.832

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Atwood Avenue
 Weather: Clear

File Name : 02_MRV_Perris_Atwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:30 AM				07:00 AM			
+0 mins.	1	250	2	253	1	0	5	6	11	246	1	258	4	0	7	11
+15 mins.	8	321	4	333	0	0	1	1	18	306	0	324	4	0	3	7
+30 mins.	5	332	5	342	1	0	2	3	15	271	2	288	2	0	16	18
+45 mins.	2	195	5	202	2	2	1	5	13	219	1	233	3	1	13	17
Total Volume	16	1098	16	1130	4	2	9	15	57	1042	4	1103	13	1	39	53
% App. Total	1.4	97.2	1.4		26.7	13.3	60		5.2	94.5	0.4		24.5	1.9	73.6	
PHF	.500	.827	.800	.826	.500	.250	.450	.625	.792	.851	.500	.851	.813	.250	.609	.736

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Atwood Avenue
 Weather: Clear

File Name : 02_MRV_Perris_Atwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Total Volume

Start Time	Perris Boulevard Southbound				Atwood Avenue Westbound				Perris Boulevard Northbound				Atwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	13	270	3	286	1	0	7	8	10	247	1	258	9	0	9	18	570
04:15 PM	9	275	4	288	1	1	3	5	7	270	0	277	5	1	11	17	587
04:30 PM	8	288	5	301	0	0	0	0	12	266	0	278	7	0	10	17	596
04:45 PM	12	260	3	275	1	0	3	4	14	250	0	264	6	0	6	12	555
Total	42	1093	15	1150	3	1	13	17	43	1033	1	1077	27	1	36	64	2308
05:00 PM	12	281	5	298	0	0	2	2	4	243	1	248	4	0	6	10	558
05:15 PM	7	254	7	268	0	0	3	3	10	255	1	266	0	2	10	12	549
05:30 PM	10	274	8	292	1	1	3	5	11	221	1	233	7	1	14	22	552
05:45 PM	13	266	7	286	1	0	5	6	12	255	0	267	5	2	4	11	570
Total	42	1075	27	1144	2	1	13	16	37	974	3	1014	16	5	34	55	2229
Grand Total	84	2168	42	2294	5	2	26	33	80	2007	4	2091	43	6	70	119	4537
Apprch %	3.7	94.5	1.8		15.2	6.1	78.8		3.8	96	0.2		36.1	5	58.8		
Total %	1.9	47.8	0.9	50.6	0.1	0	0.6	0.7	1.8	44.2	0.1	46.1	0.9	0.1	1.5	2.6	

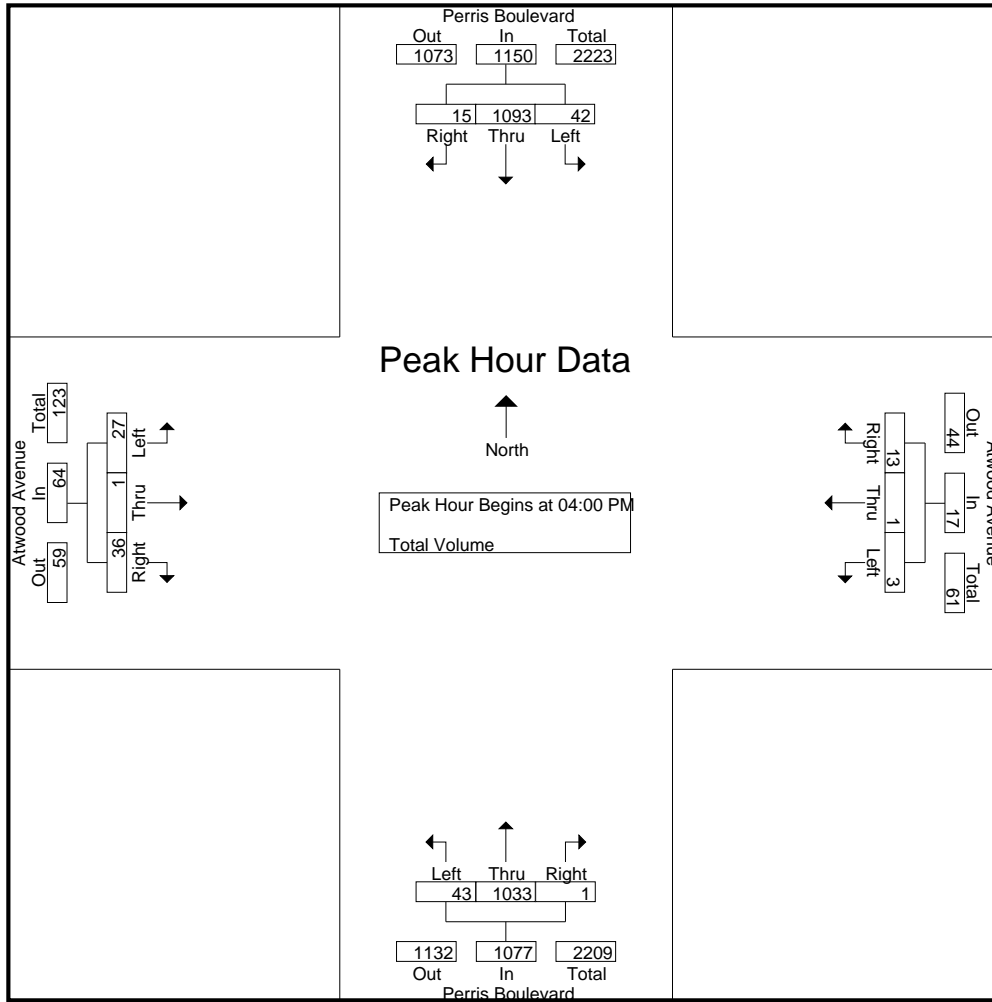
Start Time	Perris Boulevard Southbound				Atwood Avenue Westbound				Perris Boulevard Northbound				Atwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:00 PM																	
04:00 PM	13	270	3	286	1	0	7	8	10	247	1	258	9	0	9	18	570
04:15 PM	9	275	4	288	1	1	3	5	7	270	0	277	5	1	11	17	587
04:30 PM	8	288	5	301	0	0	0	0	12	266	0	278	7	0	10	17	596
04:45 PM	12	260	3	275	1	0	3	4	14	250	0	264	6	0	6	12	555
Total Volume	42	1093	15	1150	3	1	13	17	43	1033	1	1077	27	1	36	64	2308
% App. Total	3.7	95	1.3		17.6	5.9	76.5		4	95.9	0.1		42.2	1.6	56.2		
PHF	.808	.949	.750	.955	.750	.250	.464	.531	.768	.956	.250	.969	.750	.250	.818	.889	.968

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Atwood Avenue
 Weather: Clear

File Name : 02_MRV_Perris_Atwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				04:00 PM				04:00 PM				04:00 PM			
+0 mins.	9	275	4	288	1	0	7	8	10	247	1	258	9	0	9	18
+15 mins.	8	288	5	301	1	1	3	5	7	270	0	277	5	1	11	17
+30 mins.	12	260	3	275	0	0	0	0	12	266	0	278	7	0	10	17
+45 mins.	12	281	5	298	1	0	3	4	14	250	0	264	6	0	6	12
Total Volume	41	1104	17	1162	3	1	13	17	43	1033	1	1077	27	1	36	64
% App. Total	3.5	95	1.5		17.6	5.9	76.5		4	95.9	0.1		42.2	1.6	56.2	
PHF	.854	.958	.850	.965	.750	.250	.464	.531	.768	.956	.250	.969	.750	.250	.818	.889

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Atwood Avenue



Date: 5/23/2019
 Day: Thursday

PEDESTRIANS

	North Leg Perris Boulevard Pedestrians	East Leg Atwood Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Atwood Avenue Pedestrians	
7:00 AM	0	0	0	1	1
7:15 AM	0	0	0	2	2
7:30 AM	0	0	0	1	1
7:45 AM	1	0	0	2	3
8:00 AM	1	0	0	2	3
8:15 AM	0	1	0	5	6
8:30 AM	0	0	0	0	0
8:45 AM	0	1	0	1	2
TOTAL VOLUMES:	2	2	0	14	18

	North Leg Perris Boulevard Pedestrians	East Leg Atwood Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Atwood Avenue Pedestrians	
4:00 PM	0	0	0	0	0
4:15 PM	0	0	0	1	1
4:30 PM	0	0	0	2	2
4:45 PM	0	0	0	1	1
5:00 PM	0	0	0	3	3
5:15 PM	0	0	0	0	0
5:30 PM	0	0	0	1	1
5:45 PM	0	0	0	1	1
TOTAL VOLUMES:	0	0	0	9	9

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Atwood Avenue



Date: 5/23/2019
 Day: Thursday

BICYCLES

	Southbound Perris Boulevard			Westbound Atwood Avenue			Northbound Perris Boulevard			Eastbound Atwood Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
7:00 AM	0	0	0	0	0	0	0	0	0	1	0	2	3
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	1	0	0	0	0	0	1	0	0	0	0	2
8:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	2
8:15 AM	0	3	0	0	0	0	0	0	0	0	0	0	3
8:30 AM	0	2	0	0	0	0	0	0	0	0	0	0	2
8:45 AM	0	0	0	0	0	0	0	2	0	0	0	0	2
TOTAL VOLUMES:	0	8	0	0	0	0	0	4	0	1	0	2	15

	Southbound Perris Boulevard			Westbound Atwood Avenue			Northbound Perris Boulevard			Eastbound Atwood Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	1	0	0	0	0	1
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	1	1
TOTAL VOLUMES:	0	0	0	0	0	0	0	1	0	0	0	1	2

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: J's Family Auto Center Driveway
 Weather: Clear

File Name : 03_MRV_Perris_Js Fam Auto DW AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Total Volume

Start Time	Perris Boulevard Southbound			Perris Boulevard Northbound			J's Family Auto Center Driveway Eastbound			Int. Total
	Thru	Right	App. Total	Left	Thru	App. Total	Left	Right	App. Total	
07:00 AM	155	0	155	1	232	233	0	0	0	388
07:15 AM	259	0	259	0	220	220	0	0	0	479
07:30 AM	328	2	330	0	255	255	0	1	1	586
07:45 AM	336	5	341	0	325	325	0	0	0	666
Total	1078	7	1085	1	1032	1033	0	1	1	2119
08:00 AM	201	0	201	0	289	289	0	0	0	490
08:15 AM	160	1	161	0	240	240	0	1	1	402
08:30 AM	117	0	117	0	213	213	1	0	1	331
08:45 AM	155	2	157	0	212	212	0	1	1	370
Total	633	3	636	0	954	954	1	2	3	1593
Grand Total	1711	10	1721	1	1986	1987	1	3	4	3712
Apprch %	99.4	0.6		0.1	99.9		25	75		
Total %	46.1	0.3	46.4	0	53.5	53.5	0	0.1	0.1	

Start Time	Perris Boulevard Southbound			Perris Boulevard Northbound			J's Family Auto Center Driveway Eastbound			Int. Total
	Thru	Right	App. Total	Left	Thru	App. Total	Left	Right	App. Total	
07:15 AM	259	0	259	0	220	220	0	0	0	479
07:30 AM	328	2	330	0	255	255	0	1	1	586
07:45 AM	336	5	341	0	325	325	0	0	0	666
08:00 AM	201	0	201	0	289	289	0	0	0	490
Total Volume	1124	7	1131	0	1089	1089	0	1	1	2221
% App. Total	99.4	0.6		0	100		0	100		
PHF	.836	.350	.829	.000	.838	.838	.000	.250	.250	.834

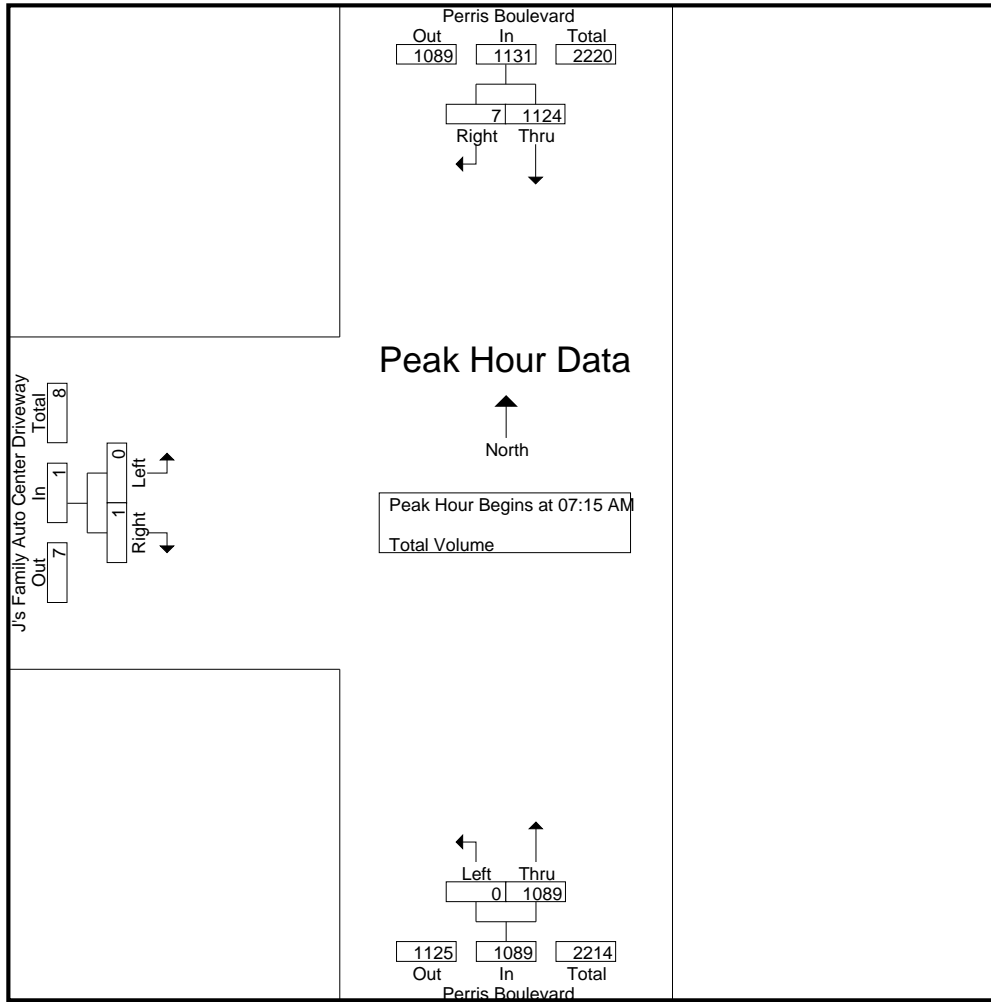
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Entire Intersection Begins at 07:15 AM

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: J's Family Auto Center Driveway
 Weather: Clear

File Name : 03_MRV_Perris_Js Fam Auto DW AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM			07:30 AM			08:00 AM		
+0 mins.	259	0	259	0	255	255	0	0	0
+15 mins.	328	2	330	0	325	325	0	1	1
+30 mins.	336	5	341	0	289	289	1	0	1
+45 mins.	201	0	201	0	240	240	0	1	1
Total Volume	1124	7	1131	0	1109	1109	1	2	3
% App. Total	99.4	0.6		0	100		33.3	66.7	
PHF	.836	.350	.829	.000	.853	.853	.250	.500	.750

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: J's Family Auto Center Driveway
 Weather: Clear

File Name : 03_MRV_Perris_Js Fam Auto DW PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Total Volume

Start Time	Perris Boulevard Southbound			Perris Boulevard Northbound			J's Family Auto Center Driveway Eastbound			Int. Total
	Thru	Right	App. Total	Left	Thru	App. Total	Left	Right	App. Total	
04:00 PM	282	0	282	0	258	258	0	0	0	540
04:15 PM	278	0	278	0	278	278	0	0	0	556
04:30 PM	298	0	298	0	278	278	0	0	0	576
04:45 PM	265	2	267	0	264	264	0	0	0	531
Total	1123	2	1125	0	1078	1078	0	0	0	2203
05:00 PM	282	3	285	0	252	252	0	2	2	539
05:15 PM	265	0	265	0	267	267	0	0	0	532
05:30 PM	288	1	289	0	232	232	0	0	0	521
05:45 PM	273	0	273	0	269	269	0	0	0	542
Total	1108	4	1112	0	1020	1020	0	2	2	2134
Grand Total	2231	6	2237	0	2098	2098	0	2	2	4337
Apprch %	99.7	0.3		0	100		0	100		
Total %	51.4	0.1	51.6	0	48.4	48.4	0	0	0	

Start Time	Perris Boulevard Southbound			Perris Boulevard Northbound			J's Family Auto Center Driveway Eastbound			Int. Total
	Thru	Right	App. Total	Left	Thru	App. Total	Left	Right	App. Total	
04:00 PM	282	0	282	0	258	258	0	0	0	540
04:15 PM	278	0	278	0	278	278	0	0	0	556
04:30 PM	298	0	298	0	278	278	0	0	0	576
04:45 PM	265	2	267	0	264	264	0	0	0	531
Total Volume	1123	2	1125	0	1078	1078	0	0	0	2203
% App. Total	99.8	0.2		0	100		0	0		
PHF	.942	.250	.944	.000	.969	.969	.000	.000	.000	.956

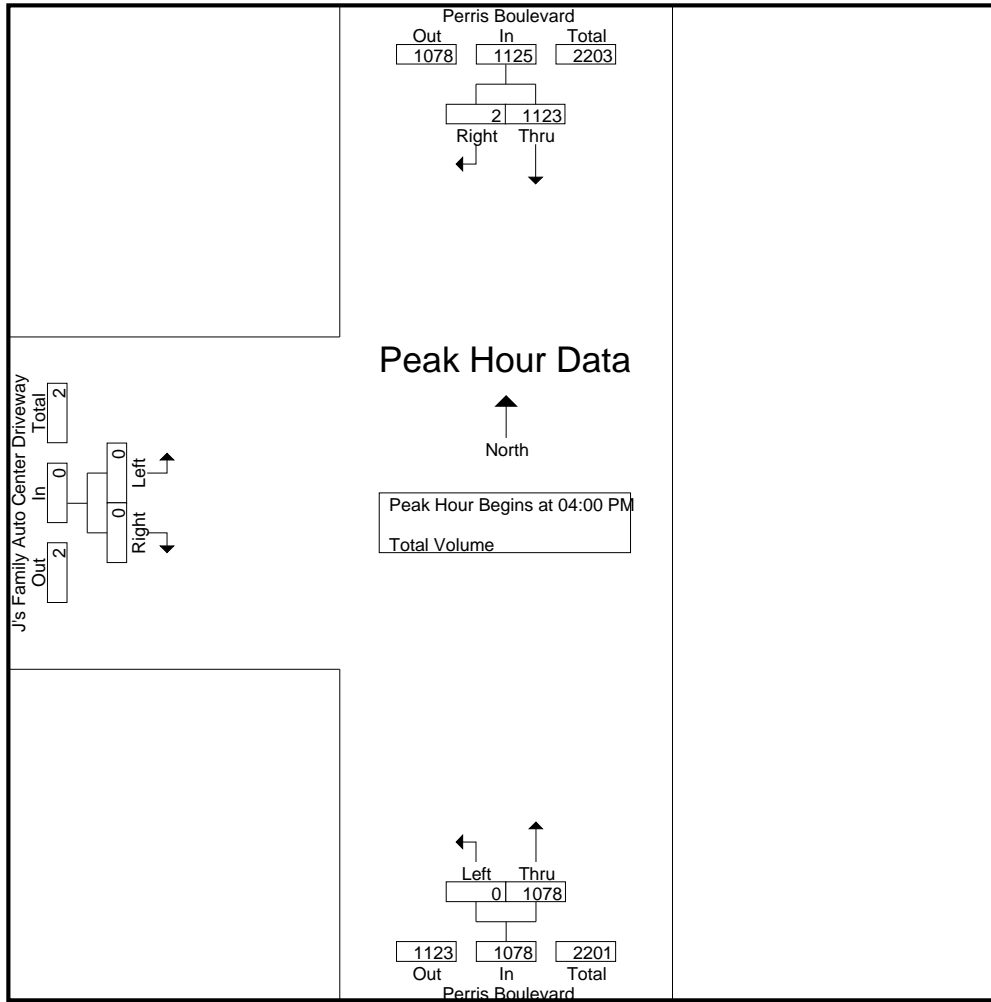
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Entire Intersection Begins at 04:00 PM

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: J's Family Auto Center Driveway
 Weather: Clear

File Name : 03_MRV_Perris_Js Fam Auto DW PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM			04:00 PM			04:15 PM		
+0 mins.	278	0	278	0	258	258	0	0	0
+15 mins.	298	0	298	0	278	278	0	0	0
+30 mins.	265	2	267	0	278	278	0	0	0
+45 mins.	282	3	285	0	264	264	0	2	2
Total Volume	1123	5	1128	0	1078	1078	0	2	2
% App. Total	99.6	0.4		0	100		0	100	
PHF	.942	.417	.946	.000	.969	.969	.000	.250	.250

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: J's Family Auto Center Driveway



Date: 5/23/2019
 Day: Thursday

PEDESTRIANS

	North Leg Perris Boulevard Pedestrians	East Leg Dead End Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg J's Fam Auto Ctr Driveway Pedestrians	
7:00 AM	0	0	0	2	2
7:15 AM	0	0	0	4	4
7:30 AM	0	0	0	1	1
7:45 AM	0	0	0	5	5
8:00 AM	0	0	0	1	1
8:15 AM	0	0	0	1	1
8:30 AM	0	0	0	0	0
8:45 AM	0	0	0	1	1
TOTAL VOLUMES:	0	0	0	15	15

	North Leg Perris Boulevard Pedestrians	East Leg Dead End Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg J's Fam Auto Ctr Driveway Pedestrians	
4:00 PM	0	0	0	1	1
4:15 PM	0	0	0	1	1
4:30 PM	0	0	0	0	0
4:45 PM	0	0	0	1	1
5:00 PM	0	0	0	4	4
5:15 PM	0	0	0	0	0
5:30 PM	0	0	0	1	1
5:45 PM	0	0	0	1	1
TOTAL VOLUMES:	0	0	0	9	9

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: J's Family Auto Center Driveway



Date: 5/23/2019
 Day: Thursday

BICYCLES

	Southbound Perris Boulevard			Westbound Dead End			Northbound Perris Boulevard			Eastbound J's Fam Auto Ctr Driveway			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	1	0	0	0	0	0	1	0	0	0	0	2
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
TOTAL VOLUMES:	0	2	0	0	0	0	0	1	0	0	0	0	3

	Southbound Perris Boulevard			Westbound Dead End			Northbound Perris Boulevard			Eastbound J's Fam Auto Ctr Driveway			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
4:00 PM	0	2	0	0	0	0	0	1	0	0	0	0	3
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	0	1	0	0	0	0	0	1	0	0	0	0	2
5:00 PM	0	1	0	0	0	0	0	2	0	0	0	0	3
5:15 PM	0	2	1	0	0	0	0	0	0	1	0	0	4
5:30 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	2	0	0	0	0	2
TOTAL VOLUMES:	0	9	1	0	0	0	0	6	0	1	0	0	17

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	4	143	8	155	8	15	12	35	3	210	3	216	9	5	4	18	424
07:15 AM	3	229	13	245	12	17	14	43	5	196	4	205	9	19	5	33	526
07:30 AM	13	303	26	342	14	25	22	61	10	230	5	245	17	14	9	40	688
07:45 AM	7	310	26	343	18	24	14	56	20	291	3	314	20	18	20	58	771
Total	27	985	73	1085	52	81	62	195	38	927	15	980	55	56	38	149	2409
08:00 AM	10	169	20	199	25	30	14	69	13	255	7	275	17	18	17	52	595
08:15 AM	6	142	10	158	10	24	14	48	9	207	2	218	20	16	6	42	466
08:30 AM	9	90	9	108	4	11	11	26	8	192	5	205	9	12	6	27	366
08:45 AM	4	144	10	158	7	14	12	33	3	190	4	197	10	8	3	21	409
Total	29	545	49	623	46	79	51	176	33	844	18	895	56	54	32	142	1836
Grand Total	56	1530	122	1708	98	160	113	371	71	1771	33	1875	111	110	70	291	4245
Apprch %	3.3	89.6	7.1		26.4	43.1	30.5		3.8	94.5	1.8		38.1	37.8	24.1		
Total %	1.3	36	2.9	40.2	2.3	3.8	2.7	8.7	1.7	41.7	0.8	44.2	2.6	2.6	1.6	6.9	
Passenger Vehicles	54	1449	121	1624	93	159	110	362	67	1694	32	1793	106	106	68	280	4059
% Passenger Vehicles	96.4	94.7	99.2	95.1	94.9	99.4	97.3	97.6	94.4	95.7	97	95.6	95.5	96.4	97.1	96.2	95.6
Large 2 Axle Vehicles	2	65	1	68	5	1	1	7	3	46	1	50	3	4	2	9	134
% Large 2 Axle Vehicles	3.6	4.2	0.8	4	5.1	0.6	0.9	1.9	4.2	2.6	3	2.7	2.7	3.6	2.9	3.1	3.2
3 Axle Vehicles	0	2	0	2	0	0	2	2	1	2	0	3	0	0	0	0	7
% 3 Axle Vehicles	0	0.1	0	0.1	0	0	1.8	0.5	1.4	0.1	0	0.2	0	0	0	0	0.2
4+ Axle Trucks	0	14	0	14	0	0	0	0	0	29	0	29	2	0	0	2	45
% 4+ Axle Trucks	0	0.9	0	0.8	0	0	0	0	0	1.6	0	1.5	1.8	0	0	0.7	1.1

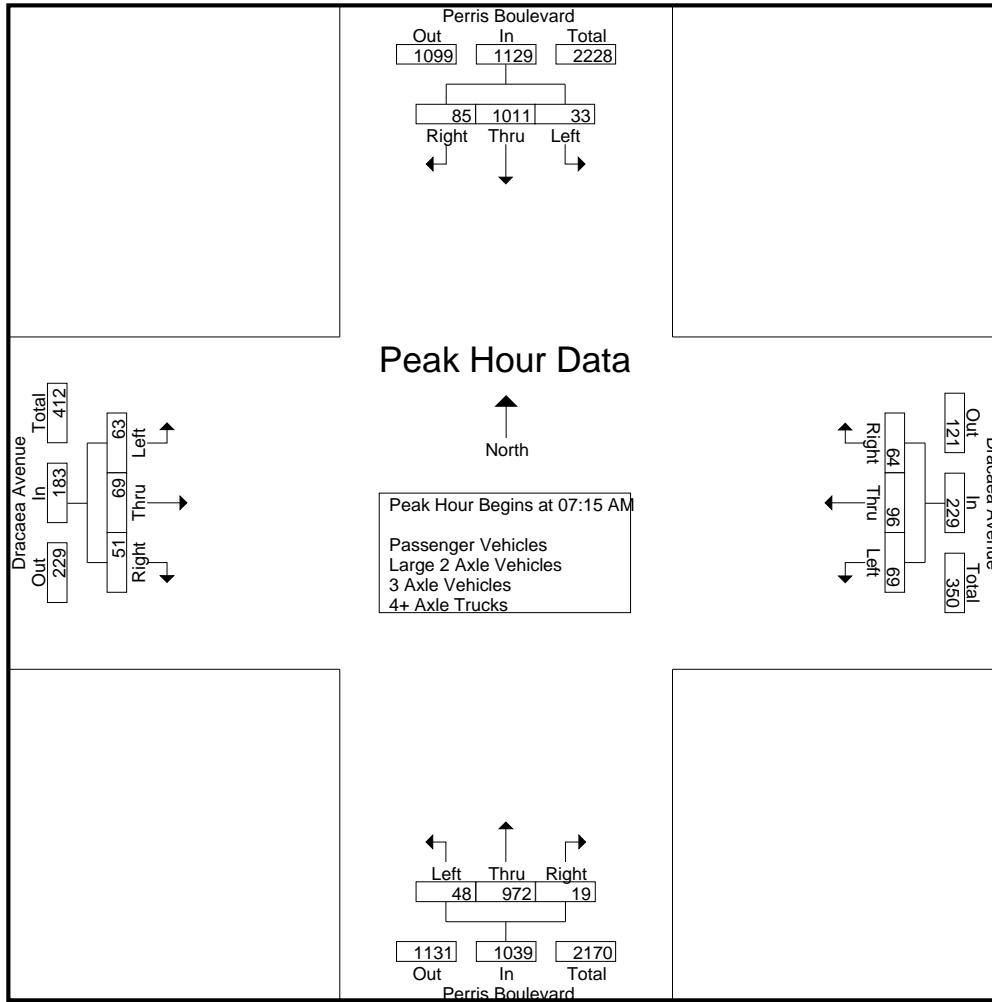
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	3	229	13	245	12	17	14	43	5	196	4	205	9	19	5	33	526
07:30 AM	13	303	26	342	14	25	22	61	10	230	5	245	17	14	9	40	688
07:45 AM	7	310	26	343	18	24	14	56	20	291	3	314	20	18	20	58	771
08:00 AM	10	169	20	199	25	30	14	69	13	255	7	275	17	18	17	52	595
Total Volume	33	1011	85	1129	69	96	64	229	48	972	19	1039	63	69	51	183	2580
% App. Total	2.9	89.5	7.5		30.1	41.9	27.9		4.6	93.6	1.8		34.4	37.7	27.9		
PHF	.635	.815	.817	.823	.690	.800	.727	.830	.600	.835	.679	.827	.788	.908	.638	.789	.837

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:30 AM				07:30 AM							
+0 mins.	3	229	13	245	14	25	22	61	10	230	5	245	17	14	9	40
+15 mins.	13	303	26	342	18	24	14	56	20	291	3	314	20	18	20	58
+30 mins.	7	310	26	343	25	30	14	69	13	255	7	275	17	18	17	52
+45 mins.	10	169	20	199	10	24	14	48	9	207	2	218	20	16	6	42
Total Volume	33	1011	85	1129	67	103	64	234	52	983	17	1052	74	66	52	192
% App. Total	2.9	89.5	7.5		28.6	44	27.4		4.9	93.4	1.6		38.5	34.4	27.1	
PHF	.635	.815	.817	.823	.670	.858	.727	.848	.650	.845	.607	.838	.925	.917	.650	.828

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 PO Box 1178
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	4	136	8	148	7	14	12	33	2	198	3	203	9	5	4	18	402
07:15 AM	3	222	13	238	12	17	13	42	4	192	3	199	8	16	4	28	507
07:30 AM	13	295	26	334	14	25	20	59	10	218	5	233	17	14	8	39	665
07:45 AM	7	300	25	332	18	24	14	56	19	280	3	302	20	18	20	58	748
Total	27	953	72	1052	51	80	59	190	35	888	14	937	54	53	36	143	2322
08:00 AM	10	161	20	191	23	30	14	67	13	248	7	268	17	18	17	52	578
08:15 AM	5	123	10	138	10	24	14	48	8	194	2	204	19	15	6	40	430
08:30 AM	8	80	9	97	4	11	11	26	8	185	5	198	8	12	6	26	347
08:45 AM	4	132	10	146	5	14	12	31	3	179	4	186	8	8	3	19	382
Total	27	496	49	572	42	79	51	172	32	806	18	856	52	53	32	137	1737
Grand Total	54	1449	121	1624	93	159	110	362	67	1694	32	1793	106	106	68	280	4059
Apprch %	3.3	89.2	7.5		25.7	43.9	30.4		3.7	94.5	1.8		37.9	37.9	24.3		
Total %	1.3	35.7	3	40	2.3	3.9	2.7	8.9	1.7	41.7	0.8	44.2	2.6	2.6	1.7	6.9	

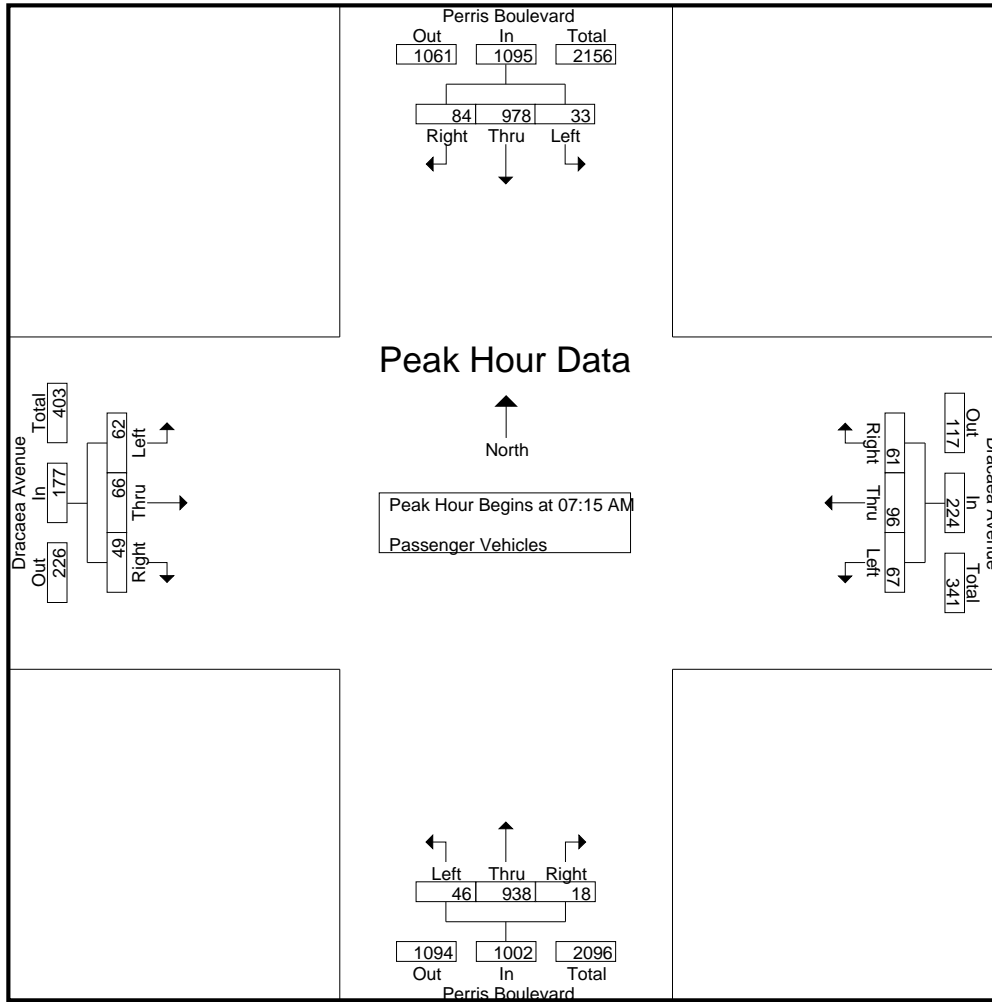
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	3	222	13	238	12	17	13	42	4	192	3	199	8	16	4	28	507
07:30 AM	13	295	26	334	14	25	20	59	10	218	5	233	17	14	8	39	665
07:45 AM	7	300	25	332	18	24	14	56	19	280	3	302	20	18	20	58	748
08:00 AM	10	161	20	191	23	30	14	67	13	248	7	268	17	18	17	52	578
Total Volume	33	978	84	1095	67	96	61	224	46	938	18	1002	62	66	49	177	2498
% App. Total	3	89.3	7.7		29.9	42.9	27.2		4.6	93.6	1.8		35	37.3	27.7		
PHF	.635	.815	.808	.820	.728	.800	.763	.836	.605	.838	.643	.829	.775	.917	.613	.763	.835

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	3	222	13	238	12	17	13	42	4	192	3	199	8	16	4	28
+15 mins.	13	295	26	334	14	25	20	59	10	218	5	233	17	14	8	39
+30 mins.	7	300	25	332	18	24	14	56	19	280	3	302	20	18	20	58
+45 mins.	10	161	20	191	23	30	14	67	13	248	7	268	17	18	17	52
Total Volume	33	978	84	1095	67	96	61	224	46	938	18	1002	62	66	49	177
% App. Total	3	89.3	7.7		29.9	42.9	27.2		4.6	93.6	1.8		35	37.3	27.7	
PHF	.635	.815	.808	.820	.728	.800	.763	.836	.605	.838	.643	.829	.775	.917	.613	.763

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Large 2 Axle Vehicles

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	6	0	6	1	1	0	2	1	7	0	8	0	0	0	0	16
07:15 AM	0	7	0	7	0	0	0	0	0	1	1	2	1	3	1	5	14
07:30 AM	0	6	0	6	0	0	1	1	0	9	0	9	0	0	1	1	17
07:45 AM	0	8	1	9	0	0	0	0	1	8	0	9	0	0	0	0	18
Total	0	27	1	28	1	1	1	3	2	25	1	28	1	3	2	6	65
08:00 AM	0	4	0	4	2	0	0	2	0	4	0	4	0	0	0	0	10
08:15 AM	1	15	0	16	0	0	0	0	1	6	0	7	0	1	0	1	24
08:30 AM	1	10	0	11	0	0	0	0	0	4	0	4	1	0	0	1	16
08:45 AM	0	9	0	9	2	0	0	2	0	7	0	7	1	0	0	1	19
Total	2	38	0	40	4	0	0	4	1	21	0	22	2	1	0	3	69
Grand Total	2	65	1	68	5	1	1	7	3	46	1	50	3	4	2	9	134
Apprch %	2.9	95.6	1.5		71.4	14.3	14.3		6	92	2		33.3	44.4	22.2		
Total %	1.5	48.5	0.7	50.7	3.7	0.7	0.7	5.2	2.2	34.3	0.7	37.3	2.2	3	1.5	6.7	

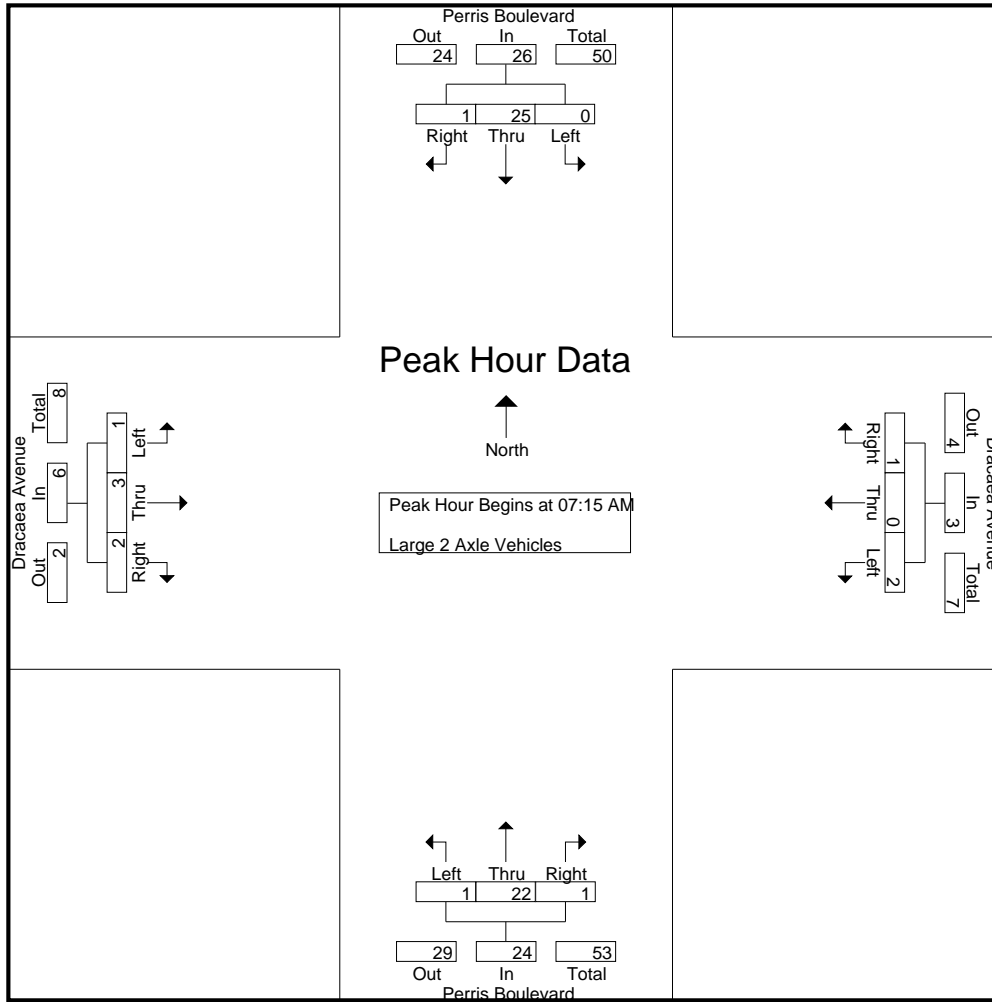
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	7	0	7	0	0	0	0	0	1	1	2	1	3	1	5	14
07:30 AM	0	6	0	6	0	0	1	1	0	9	0	9	0	0	1	1	17
07:45 AM	0	8	1	9	0	0	0	0	1	8	0	9	0	0	0	0	18
08:00 AM	0	4	0	4	2	0	0	2	0	4	0	4	0	0	0	0	10
Total Volume	0	25	1	26	2	0	1	3	1	22	1	24	1	3	2	6	59
% App. Total	0	96.2	3.8		66.7	0	33.3		4.2	91.7	4.2		16.7	50	33.3		
PHF	.000	.781	.250	.722	.250	.000	.250	.375	.250	.611	.250	.667	.250	.250	.500	.300	.819

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	7	0	7	0	0	0	0	0	1	1	2	1	3	1	5
+15 mins.	0	6	0	6	0	0	1	1	0	9	0	9	0	0	1	1
+30 mins.	0	8	1	9	0	0	0	0	1	8	0	9	0	0	0	0
+45 mins.	0	4	0	4	2	0	0	2	0	4	0	4	0	0	0	0
Total Volume	0	25	1	26	2	0	1	3	1	22	1	24	1	3	2	6
% App. Total	0	96.2	3.8		66.7	0	33.3		4.2	91.7	4.2		16.7	50	33.3	
PHF	.000	.781	.250	.722	.250	.000	.250	.375	.250	.611	.250	.667	.250	.250	.500	.300

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MR.V_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 3 Axle Vehicles

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
07:15 AM	0	0	0	0	0	0	1	1	1	1	0	2	0	0	0	0	3
07:30 AM	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
07:45 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	1	0	1	0	0	2	2	1	2	0	3	0	0	0	0	6
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Grand Total	0	2	0	2	0	0	2	2	1	2	0	3	0	0	0	0	7
Apprch %	0	100	0		0	0	100		33.3	66.7	0		0	0	0		
Total %	0	28.6	0	28.6	0	0	28.6	28.6	14.3	28.6	0	42.9	0	0	0	0	

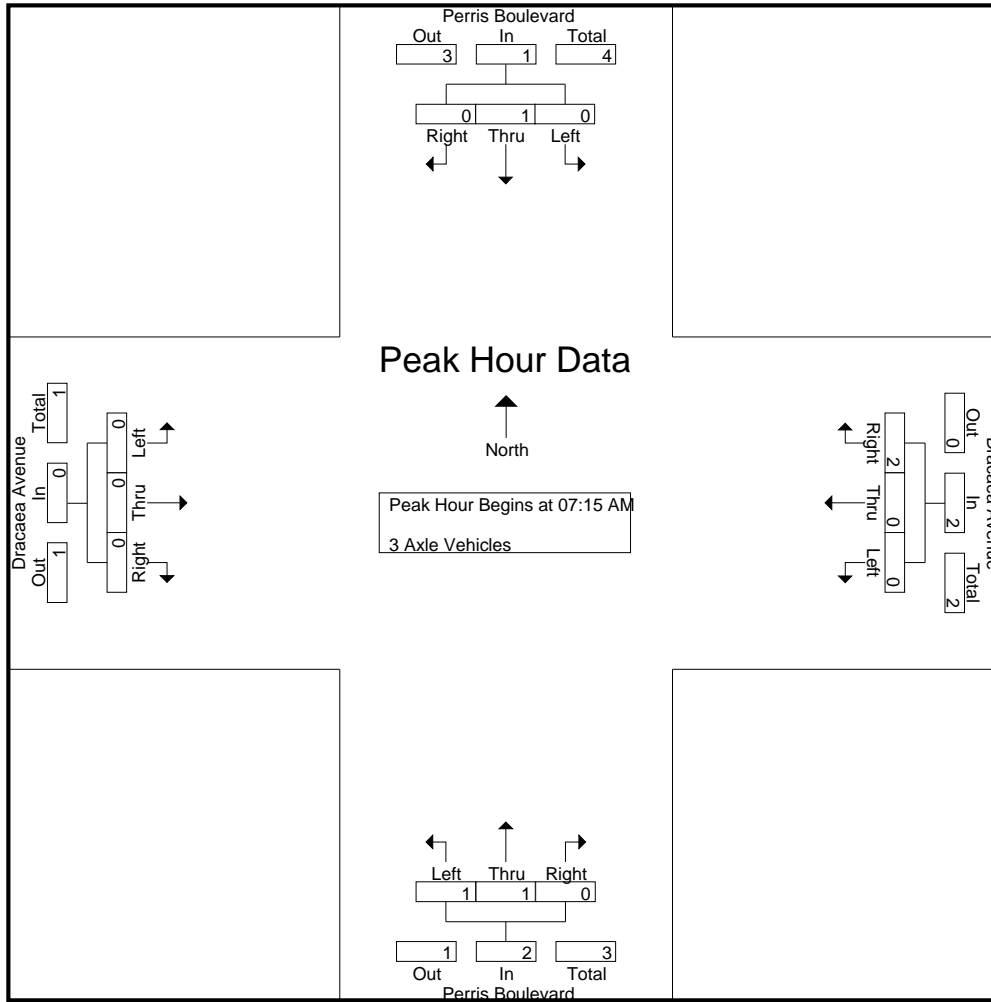
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	0	0	0	0	0	1	1	1	1	0	2	0	0	0	0	3
07:30 AM	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	1
07:45 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	1	0	0	2	2	1	1	0	2	0	0	0	0	5
% App. Total	0	100	0		0	0	100		50	50	0		0	0	0		
PHF	.000	.250	.000	.250	.000	.000	.500	.500	.250	.250	.000	.250	.000	.000	.000	.000	.417

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	0	0	0	0	0	1	1	1	1	0	2	0	0	0	0
+15 mins.	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+45 mins.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	1	0	0	2	2	1	1	0	2	0	0	0	0
% App. Total	0	100	0	0	0	0	100	100	50	50	0	0	0	0	0	0
PHF	.000	.250	.000	.250	.000	.000	.500	.500	.250	.250	.000	.250	.000	.000	.000	.000

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MR_V_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	1	0	1	0	0	0	0	0	4	0	4	0	0	0	0	5
07:15 AM	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2
07:30 AM	0	2	0	2	0	0	0	0	0	3	0	3	0	0	0	0	5
07:45 AM	0	1	0	1	0	0	0	0	0	3	0	3	0	0	0	0	4
Total	0	4	0	4	0	0	0	0	0	12	0	12	0	0	0	0	16
08:00 AM	0	4	0	4	0	0	0	0	0	3	0	3	0	0	0	0	7
08:15 AM	0	3	0	3	0	0	0	0	0	7	0	7	1	0	0	1	11
08:30 AM	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	3
08:45 AM	0	3	0	3	0	0	0	0	0	4	0	4	1	0	0	1	8
Total	0	10	0	10	0	0	0	0	0	17	0	17	2	0	0	2	29
Grand Total	0	14	0	14	0	0	0	0	0	29	0	29	2	0	0	2	45
Apprch %	0	100	0		0	0	0		0	100	0		100	0	0		
Total %	0	31.1	0	31.1	0	0	0	0	0	64.4	0	64.4	4.4	0	0	4.4	

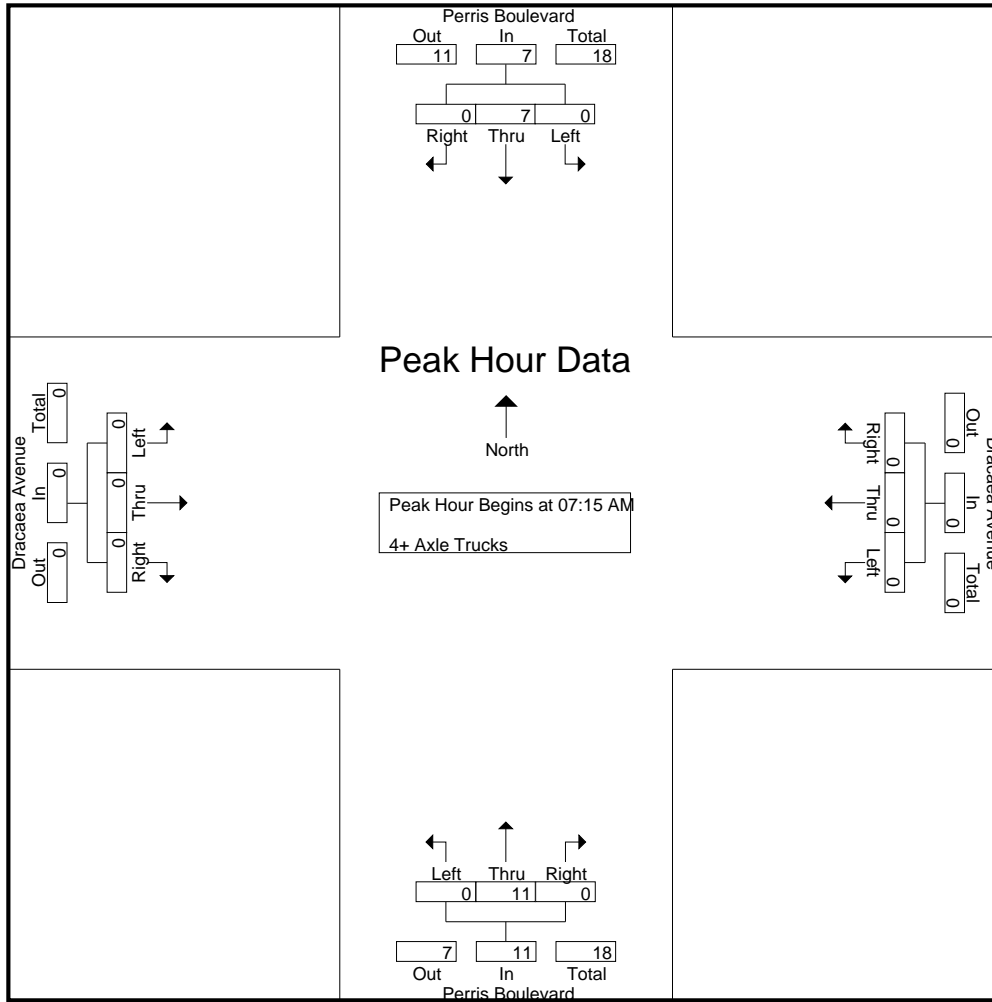
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2
07:30 AM	0	2	0	2	0	0	0	0	0	3	0	3	0	0	0	0	5
07:45 AM	0	1	0	1	0	0	0	0	0	3	0	3	0	0	0	0	4
08:00 AM	0	4	0	4	0	0	0	0	0	3	0	3	0	0	0	0	7
Total Volume	0	7	0	7	0	0	0	0	0	11	0	11	0	0	0	0	18
% App. Total	0	100	0		0	0	0		0	100	0		0	0	0		
PHF	.000	.438	.000	.438	.000	.000	.000	.000	.000	.917	.000	.917	.000	.000	.000	.000	.643

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0
+15 mins.	0	2	0	2	0	0	0	0	0	3	0	3	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	3	0	3	0	0	0	0
+45 mins.	0	4	0	4	0	0	0	0	0	3	0	3	0	0	0	0
Total Volume	0	7	0	7	0	0	0	0	0	11	0	11	0	0	0	0
% App. Total	0	100	0	0	0	0	0	0	0	100	0	0	0	0	0	0
PHF	.000	.438	.000	.438	.000	.000	.000	.000	.000	.917	.000	.917	.000	.000	.000	.000

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	11	254	19	284	8	14	14	36	3	225	11	239	17	23	10	50	609
04:15 PM	9	254	14	277	7	22	9	38	7	262	9	278	10	24	3	37	630
04:30 PM	12	264	19	295	6	19	10	35	8	245	8	261	21	27	8	56	647
04:45 PM	16	227	21	264	9	17	15	41	9	232	7	248	20	20	7	47	600
Total	48	999	73	1120	30	72	48	150	27	964	35	1026	68	94	28	190	2486
05:00 PM	9	262	14	285	9	15	13	37	5	226	11	242	18	30	4	52	616
05:15 PM	14	244	17	275	10	14	12	36	5	239	16	260	16	18	7	41	612
05:30 PM	14	255	23	292	5	21	13	39	10	205	12	227	13	26	8	47	605
05:45 PM	15	243	16	274	3	21	12	36	10	242	4	256	21	19	12	52	618
Total	52	1004	70	1126	27	71	50	148	30	912	43	985	68	93	31	192	2451
Grand Total	100	2003	143	2246	57	143	98	298	57	1876	78	2011	136	187	59	382	4937
Apprch %	4.5	89.2	6.4		19.1	48	32.9		2.8	93.3	3.9		35.6	49	15.4		
Total %	2	40.6	2.9	45.5	1.2	2.9	2	6	1.2	38	1.6	40.7	2.8	3.8	1.2	7.7	
Passenger Vehicles	99	1944	143	2186	55	143	97	295	57	1831	77	1965	135	186	59	380	4826
% Passenger Vehicles	99	97.1	100	97.3	96.5	100	99	99	100	97.6	98.7	97.7	99.3	99.5	100	99.5	97.8
Large 2 Axle Vehicles	1	38	0	39	1	0	0	1	0	32	1	33	1	1	0	2	75
% Large 2 Axle Vehicles	1	1.9	0	1.7	1.8	0	0	0.3	0	1.7	1.3	1.6	0.7	0.5	0	0.5	1.5
3 Axle Vehicles	0	5	0	5	1	0	1	2	0	2	0	2	0	0	0	0	9
% 3 Axle Vehicles	0	0.2	0	0.2	1.8	0	1	0.7	0	0.1	0	0.1	0	0	0	0	0.2
4+ Axle Trucks	0	16	0	16	0	0	0	0	0	11	0	11	0	0	0	0	27
% 4+ Axle Trucks	0	0.8	0	0.7	0	0	0	0	0	0.6	0	0.5	0	0	0	0	0.5

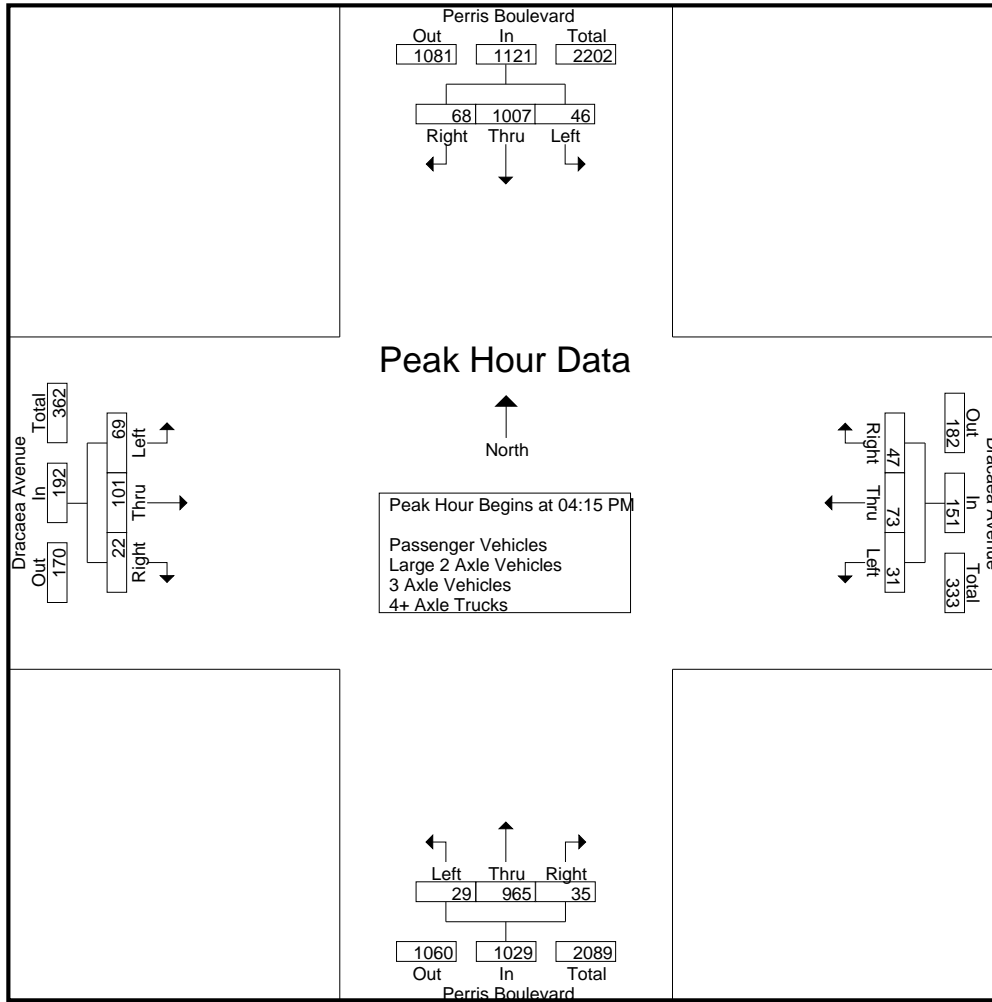
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:15 PM																	
04:15 PM	9	254	14	277	7	22	9	38	7	262	9	278	10	24	3	37	630
04:30 PM	12	264	19	295	6	19	10	35	8	245	8	261	21	27	8	56	647
04:45 PM	16	227	21	264	9	17	15	41	9	232	7	248	20	20	7	47	600
05:00 PM	9	262	14	285	9	15	13	37	5	226	11	242	18	30	4	52	616
Total Volume	46	1007	68	1121	31	73	47	151	29	965	35	1029	69	101	22	192	2493
% App. Total	4.1	89.8	6.1		20.5	48.3	31.1		2.8	93.8	3.4		35.9	52.6	11.5		
PHF	.719	.954	.810	.950	.861	.830	.783	.921	.806	.921	.795	.925	.821	.842	.688	.857	.963

Attachment: Exhibit A to Resolution No. 2020- Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	05:00 PM				04:45 PM				04:15 PM				04:30 PM			
+0 mins.	9	262	14	285	9	17	15	41	7	262	9	278	21	27	8	56
+15 mins.	14	244	17	275	9	15	13	37	8	245	8	261	20	20	7	47
+30 mins.	14	255	23	292	10	14	12	36	9	232	7	248	18	30	4	52
+45 mins.	15	243	16	274	5	21	13	39	5	226	11	242	16	18	7	41
Total Volume	52	1004	70	1126	33	67	53	153	29	965	35	1029	75	95	26	196
% App. Total	4.6	89.2	6.2		21.6	43.8	34.6		2.8	93.8	3.4		38.3	48.5	13.3	
PHF	.867	.958	.761	.964	.825	.798	.883	.933	.806	.921	.795	.925	.893	.792	.813	.875

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Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	11	243	19	273	8	14	14	36	3	213	11	227	17	23	10	50	586
04:15 PM	9	240	14	263	7	22	9	38	7	257	8	272	10	24	3	37	610
04:30 PM	11	256	19	286	5	19	10	34	8	242	8	258	21	27	8	56	634
04:45 PM	16	221	21	258	9	17	15	41	9	227	7	243	20	20	7	47	589
Total	47	960	73	1080	29	72	48	149	27	939	34	1000	68	94	28	190	2419
05:00 PM	9	256	14	279	9	15	13	37	5	219	11	235	18	29	4	51	602
05:15 PM	14	238	17	269	9	14	11	34	5	236	16	257	16	18	7	41	601
05:30 PM	14	251	23	288	5	21	13	39	10	203	12	225	13	26	8	47	599
05:45 PM	15	239	16	270	3	21	12	36	10	234	4	248	20	19	12	51	605
Total	52	984	70	1106	26	71	49	146	30	892	43	965	67	92	31	190	2407
Grand Total	99	1944	143	2186	55	143	97	295	57	1831	77	1965	135	186	59	380	4826
Apprch %	4.5	88.9	6.5		18.6	48.5	32.9		2.9	93.2	3.9		35.5	48.9	15.5		
Total %	2.1	40.3	3	45.3	1.1	3	2	6.1	1.2	37.9	1.6	40.7	2.8	3.9	1.2	7.9	

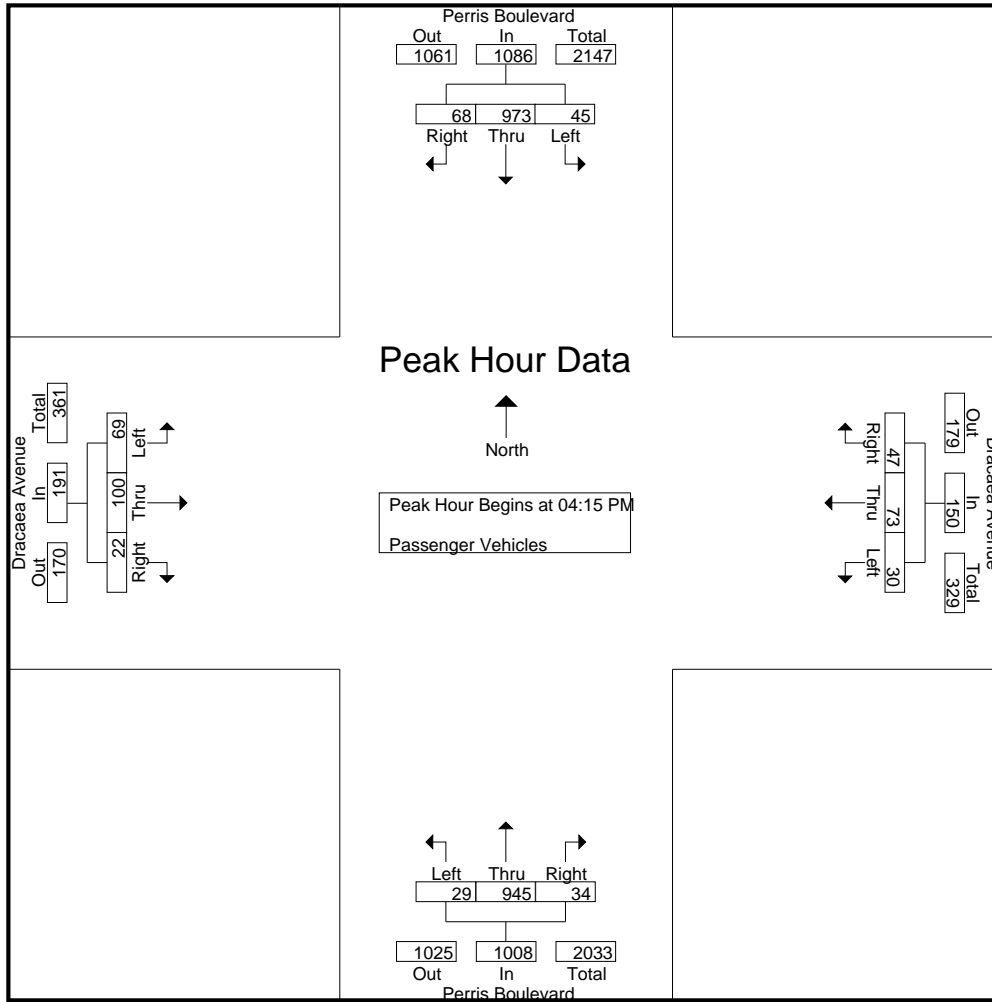
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:15 PM																	
04:15 PM	9	240	14	263	7	22	9	38	7	257	8	272	10	24	3	37	610
04:30 PM	11	256	19	286	5	19	10	34	8	242	8	258	21	27	8	56	634
04:45 PM	16	221	21	258	9	17	15	41	9	227	7	243	20	20	7	47	589
05:00 PM	9	256	14	279	9	15	13	37	5	219	11	235	18	29	4	51	602
Total Volume	45	973	68	1086	30	73	47	150	29	945	34	1008	69	100	22	191	2435
% App. Total	4.1	89.6	6.3		20	48.7	31.3		2.9	93.8	3.4		36.1	52.4	11.5		
PHF	.703	.950	.810	.949	.833	.830	.783	.915	.806	.919	.773	.926	.821	.862	.688	.853	.960

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				04:15 PM				04:15 PM				04:15 PM			
+0 mins.	9	240	14	263	7	22	9	38	7	257	8	272	10	24	3	37
+15 mins.	11	256	19	286	5	19	10	34	8	242	8	258	21	27	8	56
+30 mins.	16	221	21	258	9	17	15	41	9	227	7	243	20	20	7	47
+45 mins.	9	256	14	279	9	15	13	37	5	219	11	235	18	29	4	51
Total Volume	45	973	68	1086	30	73	47	150	29	945	34	1008	69	100	22	191
% App. Total	4.1	89.6	6.3		20	48.7	31.3		2.9	93.8	3.4		36.1	52.4	11.5	
PHF	.703	.950	.810	.949	.833	.830	.783	.915	.806	.919	.773	.926	.821	.862	.688	.853

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MR_V_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Large 2 Axle Vehicles

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	8	0	8	0	0	0	0	0	9	0	9	0	0	0	0	17
04:15 PM	0	7	0	7	0	0	0	0	0	3	1	4	0	0	0	0	11
04:30 PM	1	5	0	6	1	0	0	1	0	2	0	2	0	0	0	0	9
04:45 PM	0	3	0	3	0	0	0	0	0	5	0	5	0	0	0	0	8
Total	1	23	0	24	1	0	0	1	0	19	1	20	0	0	0	0	45
05:00 PM	0	5	0	5	0	0	0	0	0	5	0	5	0	1	0	1	11
05:15 PM	0	3	0	3	0	0	0	0	0	2	0	2	0	0	0	0	5
05:30 PM	0	4	0	4	0	0	0	0	0	1	0	1	0	0	0	0	5
05:45 PM	0	3	0	3	0	0	0	0	0	5	0	5	1	0	0	1	9
Total	0	15	0	15	0	0	0	0	0	13	0	13	1	1	0	2	30
Grand Total	1	38	0	39	1	0	0	1	0	32	1	33	1	1	0	2	75
Apprch %	2.6	97.4	0		100	0	0		0	97	3		50	50	0		
Total %	1.3	50.7	0	52	1.3	0	0	1.3	0	42.7	1.3	44	1.3	1.3	0	2.7	

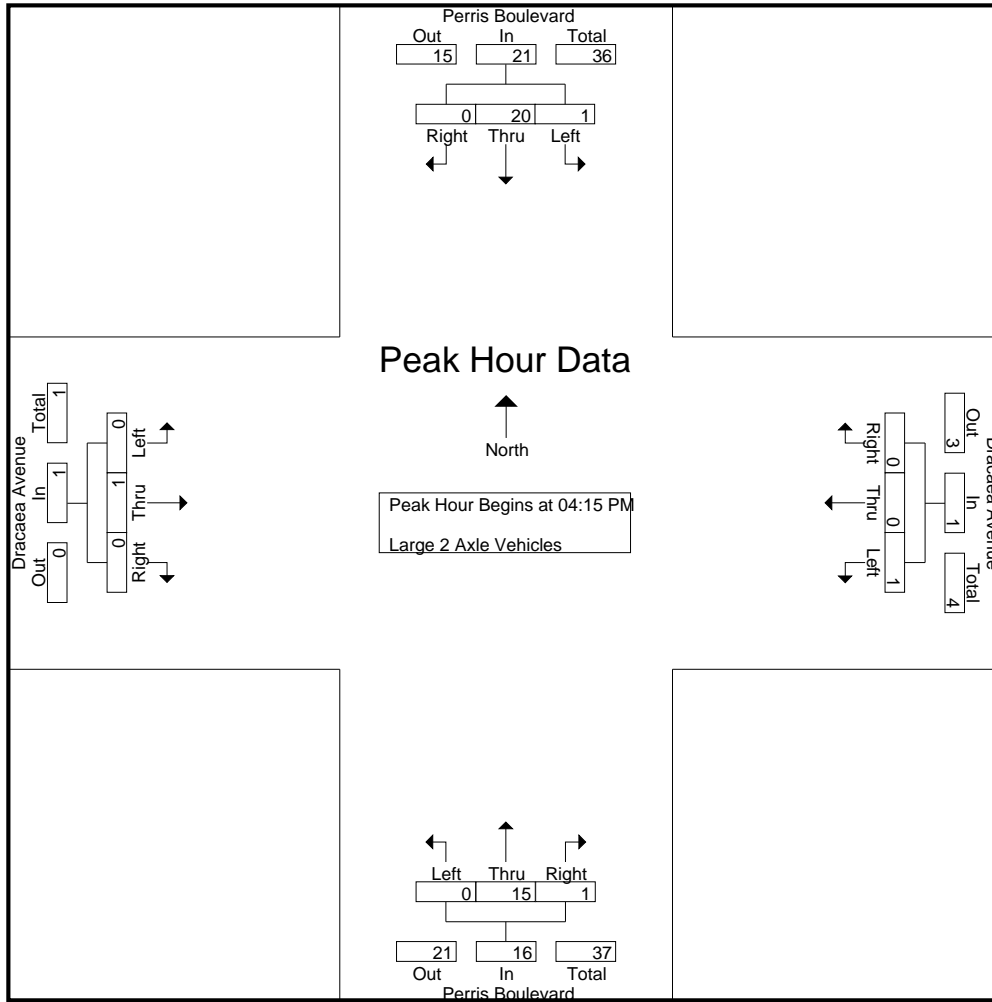
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:15 PM																	
04:15 PM	0	7	0	7	0	0	0	0	0	3	1	4	0	0	0	0	11
04:30 PM	1	5	0	6	1	0	0	1	0	2	0	2	0	0	0	0	9
04:45 PM	0	3	0	3	0	0	0	0	0	5	0	5	0	0	0	0	8
05:00 PM	0	5	0	5	0	0	0	0	0	5	0	5	0	1	0	1	11
Total Volume	1	20	0	21	1	0	0	1	0	15	1	16	0	1	0	1	39
% App. Total	4.8	95.2	0		100	0	0		0	93.8	6.2		0	100	0		
PHF	.250	.714	.000	.750	.250	.000	.000	.250	.000	.750	.250	.800	.000	.250	.000	.250	.886

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				04:15 PM				04:15 PM				04:15 PM			
+0 mins.	0	7	0	7	0	0	0	0	0	3	1	4	0	0	0	0
+15 mins.	1	5	0	6	1	0	0	1	0	2	0	2	0	0	0	0
+30 mins.	0	3	0	3	0	0	0	0	0	5	0	5	0	0	0	0
+45 mins.	0	5	0	5	0	0	0	0	0	5	0	5	0	1	0	1
Total Volume	1	20	0	21	1	0	0	1	0	15	1	16	0	1	0	1
% App. Total	4.8	95.2	0		100	0	0		0	93.8	6.2		0	100	0	
PHF	.250	.714	.000	.750	.250	.000	.000	.250	.000	.750	.250	.800	.000	.250	.000	.250

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MR_V_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 3 Axle Vehicles

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
04:15 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:30 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	4	0	4	0	0	0	0	0	1	0	1	0	0	0	0	5
05:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
05:15 PM	0	0	0	0	1	0	1	2	0	0	0	0	0	0	0	0	2
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	1	0	1	2	0	1	0	1	0	0	0	0	4
Grand Total	0	5	0	5	1	0	1	2	0	2	0	2	0	0	0	0	9
Apprch %	0	100	0		50	0	50		0	100	0		0	0	0		
Total %	0	55.6	0	55.6	11.1	0	11.1	22.2	0	22.2	0	22.2	0	0	0	0	

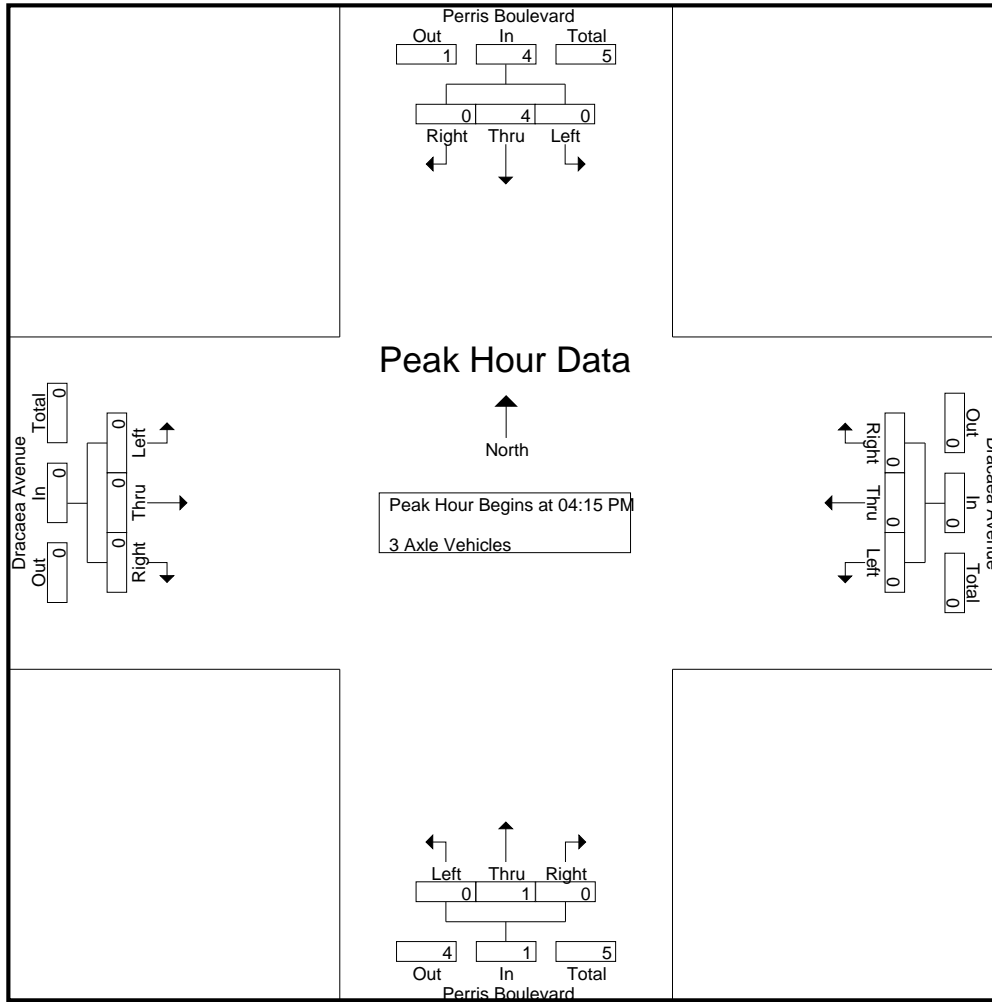
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:15 PM																	
04:15 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:30 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
Total Volume	0	4	0	4	0	0	0	0	0	1	0	1	0	0	0	0	5
% App. Total	0	100	0		0	0	0		0	100	0		0	0	0		
PHF	.000	1.00	.000	1.00	.000	.000	.000	.000	.000	.250	.000	.250	.000	.000	.000	.000	.625

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				04:15 PM				04:15 PM				04:15 PM			
+0 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+15 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+45 mins.	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0
Total Volume	0	4	0	4	0	0	0	0	0	1	0	1	0	0	0	0
% App. Total	0	100	0		0	0	0		0	100	0		0	0	0	
PHF	.000	1.000	.000	1.000	.000	.000	.000	.000	.000	.250	.000	.250	.000	.000	.000	.000

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRJV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
04:15 PM	0	6	0	6	0	0	0	0	0	2	0	2	0	0	0	0	8
04:30 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:45 PM	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	0	12	0	12	0	0	0	0	0	5	0	5	0	0	0	0	17
05:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:15 PM	0	3	0	3	0	0	0	0	0	1	0	1	0	0	0	0	4
05:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:45 PM	0	1	0	1	0	0	0	0	0	3	0	3	0	0	0	0	4
Total	0	4	0	4	0	0	0	0	0	6	0	6	0	0	0	0	10
Grand Total	0	16	0	16	0	0	0	0	0	11	0	11	0	0	0	0	27
Apprch %	0	100	0		0	0	0		0	100	0		0	0	0		
Total %	0	59.3	0	59.3	0	0	0	0	0	40.7	0	40.7	0	0	0	0	

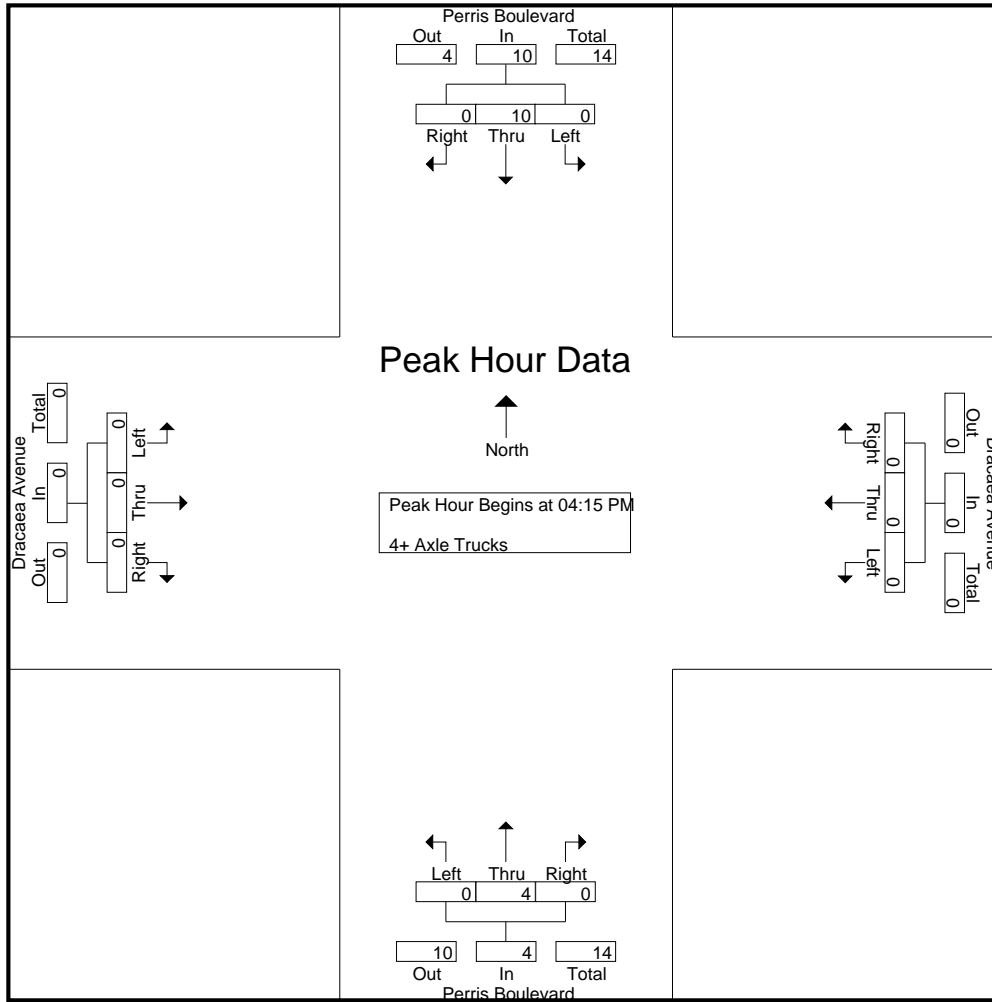
Start Time	Perris Boulevard Southbound				Dracaea Avenue Westbound				Perris Boulevard Northbound				Dracaea Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:15 PM																	
04:15 PM	0	6	0	6	0	0	0	0	0	2	0	2	0	0	0	0	8
04:30 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:45 PM	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
Total Volume	0	10	0	10	0	0	0	0	0	4	0	4	0	0	0	0	14
% App. Total	0	100	0		0	0	0		0	100	0		0	0	0		
PHF	.000	.417	.000	.417	.000	.000	.000	.000	.000	.500	.000	.500	.000	.000	.000	.000	.438

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue
 Weather: Clear

File Name : 04_MRV_Perris_Dracaea PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:15 PM to 05:00 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				04:15 PM				04:15 PM				04:15 PM							
+0 mins.	0	6	0	6	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0
+15 mins.	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0
+30 mins.	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
+45 mins.	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0
Total Volume	0	10	0	10	0	0	0	0	0	4	0	4	0	0	0	0	0	0	0	0
% App. Total	0	100	0	100	0	0	0	0	0	100	0	100	0	0	0	0	0	0	0	0
PHF	.000	.417	.000	.417	.000	.000	.000	.000	.000	.500	.000	.500	.000	.000	.000	.000	.000	.000	.000	.000

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue



Date: 5/23/2019
 Day: Thursday

PEDESTRIANS

	North Leg Perris Boulevard Pedestrians	East Leg Dracaea Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Dracaea Avenue Pedestrians	
7:00 AM	0	0	0	3	3
7:15 AM	1	1	1	4	7
7:30 AM	0	0	1	1	2
7:45 AM	0	0	0	4	4
8:00 AM	0	0	2	1	3
8:15 AM	0	1	1	1	3
8:30 AM	0	0	0	0	0
8:45 AM	3	0	0	0	3
TOTAL VOLUMES:	4	2	5	14	25

	North Leg Perris Boulevard Pedestrians	East Leg Dracaea Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Dracaea Avenue Pedestrians	
4:00 PM	0	1	1	0	2
4:15 PM	0	0	0	2	2
4:30 PM	0	0	0	0	0
4:45 PM	1	0	0	1	2
5:00 PM	0	0	1	1	2
5:15 PM	0	0	0	0	0
5:30 PM	0	0	5	0	5
5:45 PM	0	0	2	0	2
TOTAL VOLUMES:	1	1	9	4	15

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Dracaea Avenue



Date: 5/23/2019
 Day: Thursday

BICYCLES

	Southbound Perris Boulevard			Westbound Dracaea Avenue			Northbound Perris Boulevard			Eastbound Dracaea Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	1	0	0	0	0	0	0	0	1
7:45 AM	0	1	0	0	0	0	0	0	0	1	0	0	2
8:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	1
8:15 AM	0	0	0	0	0	0	0	0	1	0	0	0	1
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
TOTAL VOLUMES:	0	2	0	0	2	0	0	0	1	1	0	0	6

	Southbound Perris Boulevard			Westbound Dracaea Avenue			Northbound Perris Boulevard			Eastbound Dracaea Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
4:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	3
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
4:45 PM	0	1	0	0	0	0	0	1	0	0	0	0	2
5:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	2
5:15 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
5:30 PM	0	2	0	2	0	0	0	0	1	0	1	0	6
5:45 PM	0	0	0	0	0	1	0	0	0	1	0	0	2
TOTAL VOLUMES:	0	7	3	2	0	1	0	2	1	1	1	0	18

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	9	132	11	152	9	35	25	69	7	184	2	193	8	32	4	44	458
07:15 AM	19	225	15	259	13	42	27	82	7	178	8	193	13	37	9	59	593
07:30 AM	39	241	25	305	9	60	29	98	13	219	11	243	17	65	15	97	743
07:45 AM	40	252	48	340	16	80	56	152	21	232	14	267	29	79	14	122	881
Total	107	850	99	1056	47	217	137	401	48	813	35	896	67	213	42	322	2675
08:00 AM	31	187	21	239	21	90	44	155	17	211	15	243	27	41	15	83	720
08:15 AM	11	140	10	161	4	34	16	54	14	175	4	193	13	28	8	49	457
08:30 AM	7	95	4	106	8	36	28	72	7	172	8	187	16	15	7	38	403
08:45 AM	12	121	12	145	14	38	27	79	12	163	9	184	13	21	8	42	450
Total	61	543	47	651	47	198	115	360	50	721	36	807	69	105	38	212	2030
Grand Total	168	1393	146	1707	94	415	252	761	98	1534	71	1703	136	318	80	534	4705
Apprch %	9.8	81.6	8.6		12.4	54.5	33.1		5.8	90.1	4.2		25.5	59.6	15		
Total %	3.6	29.6	3.1	36.3	2	8.8	5.4	16.2	2.1	32.6	1.5	36.2	2.9	6.8	1.7	11.3	
Passenger Vehicles	164	1316	145	1625	90	401	246	737	95	1462	68	1625	133	310	77	520	4507
% Passenger Vehicles	97.6	94.5	99.3	95.2	95.7	96.6	97.6	96.8	96.9	95.3	95.8	95.4	97.8	97.5	96.2	97.4	95.8
Large 2 Axle Vehicles	4	62	1	67	4	14	5	23	3	43	3	49	3	8	3	14	153
% Large 2 Axle Vehicles	2.4	4.5	0.7	3.9	4.3	3.4	2	3	3.1	2.8	4.2	2.9	2.2	2.5	3.8	2.6	3.3
3 Axle Vehicles	0	2	0	2	0	0	1	1	0	2	0	2	0	0	0	0	5
% 3 Axle Vehicles	0	0.1	0	0.1	0	0	0.4	0.1	0	0.1	0	0.1	0	0	0	0	0.1
4+ Axle Trucks	0	13	0	13	0	0	0	0	0	27	0	27	0	0	0	0	40
% 4+ Axle Trucks	0	0.9	0	0.8	0	0	0	0	0	1.8	0	1.6	0	0	0	0	0.9

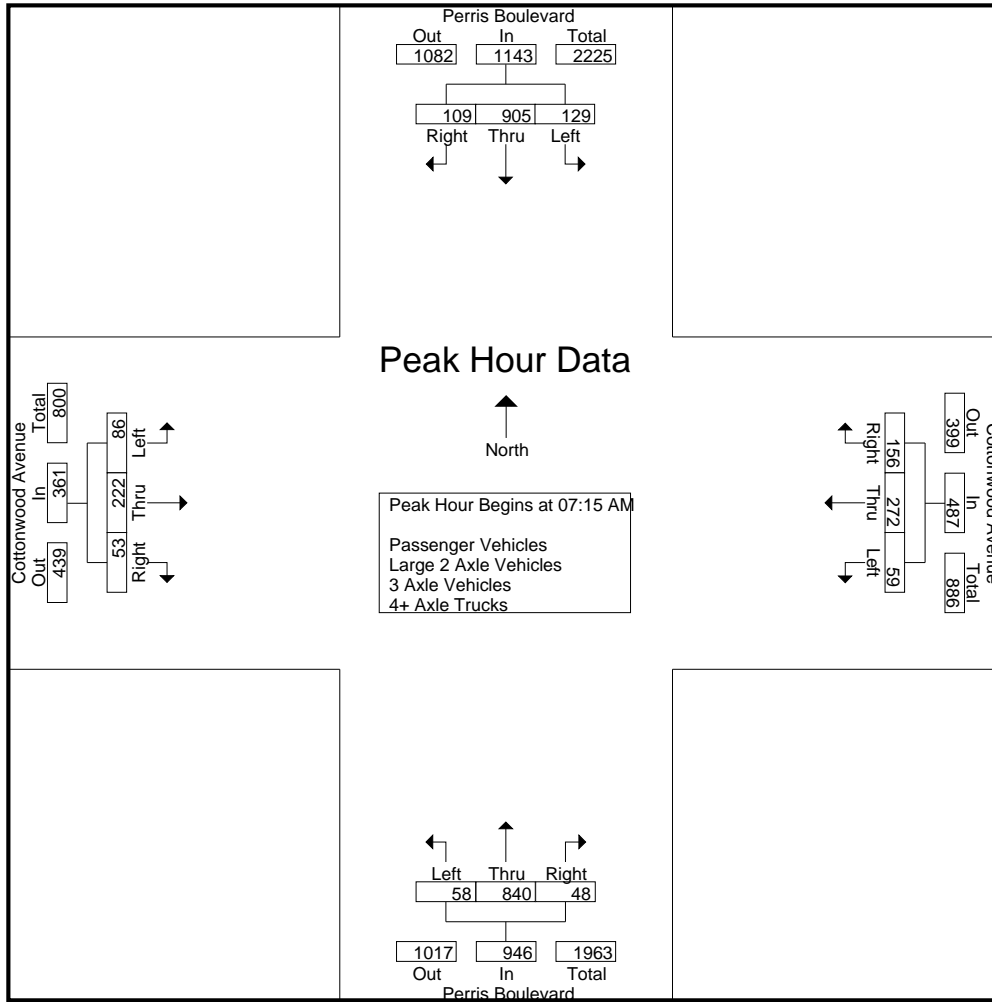
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	19	225	15	259	13	42	27	82	7	178	8	193	13	37	9	59	593
07:30 AM	39	241	25	305	9	60	29	98	13	219	11	243	17	65	15	97	743
07:45 AM	40	252	48	340	16	80	56	152	21	232	14	267	29	79	14	122	881
08:00 AM	31	187	21	239	21	90	44	155	17	211	15	243	27	41	15	83	720
Total Volume	129	905	109	1143	59	272	156	487	58	840	48	946	86	222	53	361	2937
% App. Total	11.3	79.2	9.5		12.1	55.9	32		6.1	88.8	5.1		23.8	61.5	14.7		
PHF	.806	.898	.568	.840	.702	.756	.696	.785	.690	.905	.800	.886	.741	.703	.883	.740	.833

Attachment: Exhibit A to Resolution No. 2020- Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM							
+0 mins.	19	225	15	259	13	42	27	82	7	178	8	193	13	37	9	59
+15 mins.	39	241	25	305	9	60	29	98	13	219	11	243	17	65	15	97
+30 mins.	40	252	48	340	16	80	56	152	21	232	14	267	29	79	14	122
+45 mins.	31	187	21	239	21	90	44	155	17	211	15	243	27	41	15	83
Total Volume	129	905	109	1143	59	272	156	487	58	840	48	946	86	222	53	361
% App. Total	11.3	79.2	9.5		12.1	55.9	32		6.1	88.8	5.1		23.8	61.5	14.7	
PHF	.806	.898	.568	.840	.702	.756	.696	.785	.690	.905	.800	.886	.741	.703	.883	.740

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	9	123	11	143	9	35	25	69	6	171	0	177	8	31	4	43	432
07:15 AM	18	220	15	253	13	42	26	81	7	173	7	187	13	34	8	55	576
07:30 AM	38	235	25	298	8	59	28	95	13	210	11	234	16	65	15	96	723
07:45 AM	39	243	48	330	15	75	53	143	20	222	14	256	29	79	14	122	851
Total	104	821	99	1024	45	211	132	388	46	776	32	854	66	209	41	316	2582
08:00 AM	31	177	21	229	20	89	44	153	17	203	15	235	27	40	14	81	698
08:15 AM	10	125	10	145	4	31	16	51	13	165	4	182	12	27	7	46	424
08:30 AM	7	85	4	96	8	33	28	69	7	165	8	180	16	14	7	37	382
08:45 AM	12	108	11	131	13	37	26	76	12	153	9	174	12	20	8	40	421
Total	60	495	46	601	45	190	114	349	49	686	36	771	67	101	36	204	1925
Grand Total	164	1316	145	1625	90	401	246	737	95	1462	68	1625	133	310	77	520	4507
Apprch %	10.1	81	8.9		12.2	54.4	33.4		5.8	90	4.2		25.6	59.6	14.8		
Total %	3.6	29.2	3.2	36.1	2	8.9	5.5	16.4	2.1	32.4	1.5	36.1	3	6.9	1.7	11.5	

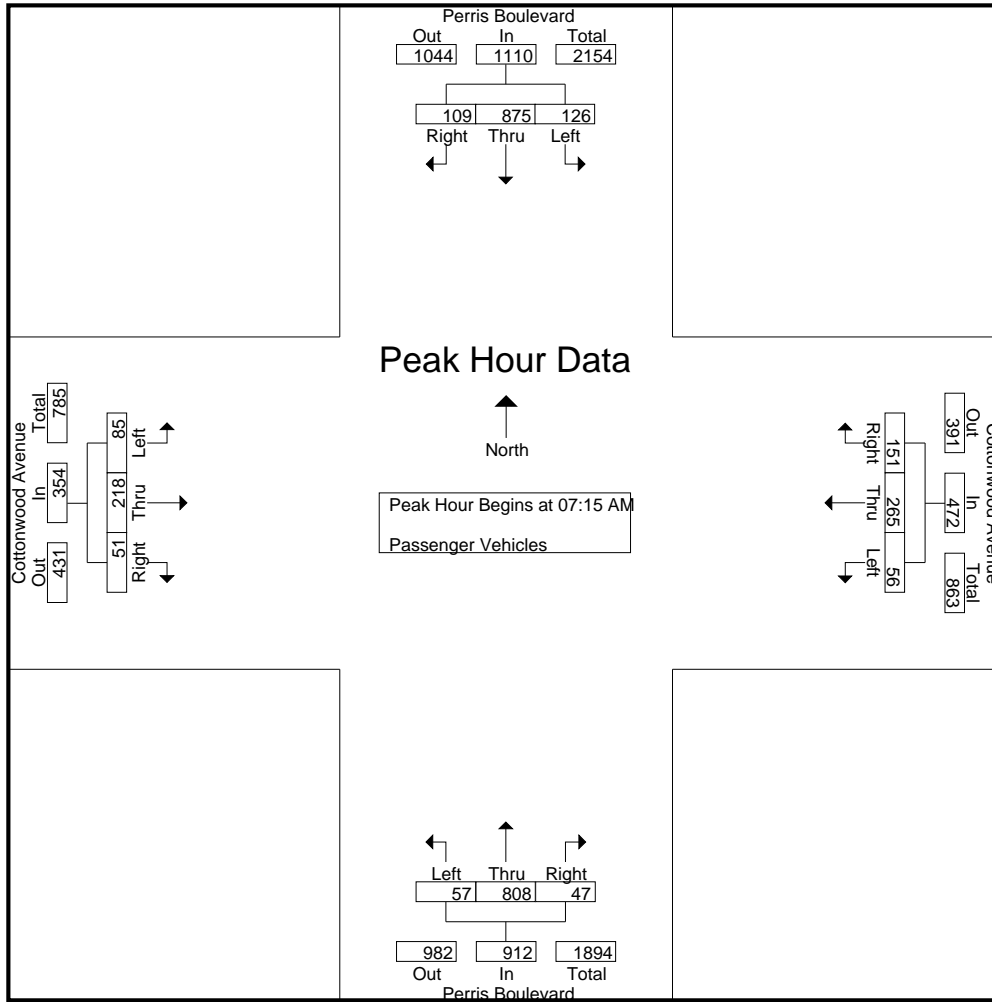
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	18	220	15	253	13	42	26	81	7	173	7	187	13	34	8	55	576
07:30 AM	38	235	25	298	8	59	28	95	13	210	11	234	16	65	15	96	723
07:45 AM	39	243	48	330	15	75	53	143	20	222	14	256	29	79	14	122	851
08:00 AM	31	177	21	229	20	89	44	153	17	203	15	235	27	40	14	81	698
Total Volume	126	875	109	1110	56	265	151	472	57	808	47	912	85	218	51	354	2848
% App. Total	11.4	78.8	9.8		11.9	56.1	32		6.2	88.6	5.2		24	61.6	14.4		
PHF	.808	.900	.568	.841	.700	.744	.712	.771	.713	.910	.783	.891	.733	.690	.850	.725	.837

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	18	220	15	253	13	42	26	81	7	173	7	187	13	34	8	55
+15 mins.	38	235	25	298	8	59	28	95	13	210	11	234	16	65	15	96
+30 mins.	39	243	48	330	15	75	53	143	20	222	14	256	29	79	14	122
+45 mins.	31	177	21	229	20	89	44	153	17	203	15	235	27	40	14	81
Total Volume	126	875	109	1110	56	265	151	472	57	808	47	912	85	218	51	354
% App. Total	11.4	78.8	9.8		11.9	56.1	32		6.2	88.6	5.2		24	61.6	14.4	
PHF	.808	.900	.568	.841	.700	.744	.712	.771	.713	.910	.783	.891	.733	.690	.850	.725

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Large 2 Axle Vehicles

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	8	0	8	0	0	0	0	1	10	2	13	0	1	0	1	22
07:15 AM	1	5	0	6	0	0	0	0	0	3	1	4	0	3	1	4	14
07:30 AM	1	4	0	5	1	1	1	3	0	7	0	7	1	0	0	1	16
07:45 AM	1	8	0	9	1	5	3	9	1	6	0	7	0	0	0	0	25
Total	3	25	0	28	2	6	4	12	2	26	3	31	1	4	1	6	77
08:00 AM	0	6	0	6	1	1	0	2	0	6	0	6	0	1	1	2	16
08:15 AM	1	11	0	12	0	3	0	3	1	3	0	4	1	1	1	3	22
08:30 AM	0	10	0	10	0	3	0	3	0	4	0	4	0	1	0	1	18
08:45 AM	0	10	1	11	1	1	1	3	0	4	0	4	1	1	0	2	20
Total	1	37	1	39	2	8	1	11	1	17	0	18	2	4	2	8	76
Grand Total	4	62	1	67	4	14	5	23	3	43	3	49	3	8	3	14	153
Apprch %	6	92.5	1.5		17.4	60.9	21.7		6.1	87.8	6.1		21.4	57.1	21.4		
Total %	2.6	40.5	0.7	43.8	2.6	9.2	3.3	15	2	28.1	2	32	2	5.2	2	9.2	

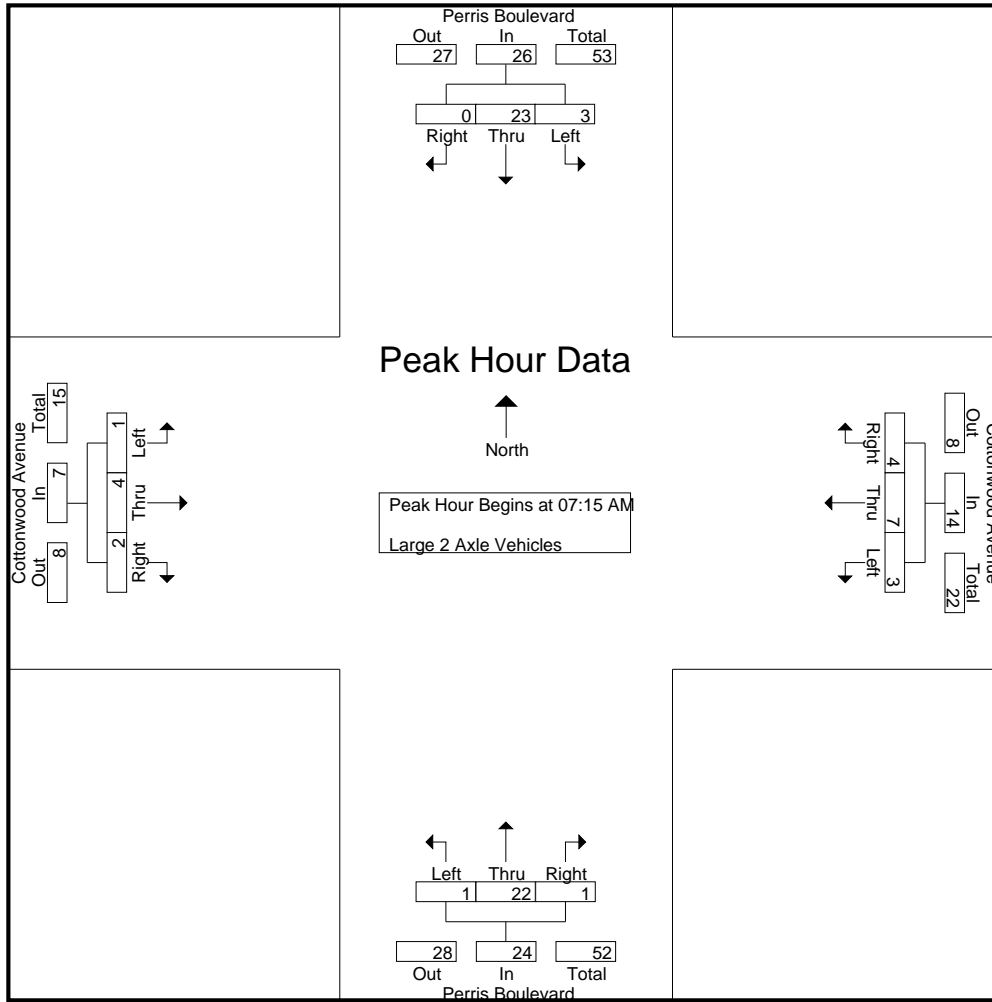
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	1	5	0	6	0	0	0	0	0	3	1	4	0	3	1	4	14
07:30 AM	1	4	0	5	1	1	1	3	0	7	0	7	1	0	0	1	16
07:45 AM	1	8	0	9	1	5	3	9	1	6	0	7	0	0	0	0	25
08:00 AM	0	6	0	6	1	1	0	2	0	6	0	6	0	1	1	2	16
Total Volume	3	23	0	26	3	7	4	14	1	22	1	24	1	4	2	7	71
% App. Total	11.5	88.5	0		21.4	50	28.6		4.2	91.7	4.2		14.3	57.1	28.6		
PHF	.750	.719	.000	.722	.750	.350	.333	.389	.250	.786	.250	.857	.250	.333	.500	.438	.710

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	1	5	0	6	0	0	0	0	0	3	1	4	0	3	1	4
+15 mins.	1	4	0	5	1	1	1	3	0	7	0	7	1	0	0	1
+30 mins.	1	8	0	9	1	5	3	9	1	6	0	7	0	0	0	0
+45 mins.	0	6	0	6	1	1	0	2	0	6	0	6	0	1	1	2
Total Volume	3	23	0	26	3	7	4	14	1	22	1	24	1	4	2	7
% App. Total	11.5	88.5	0		21.4	50	28.6		4.2	91.7	4.2		14.3	57.1	28.6	
PHF	.750	.719	.000	.722	.750	.350	.333	.389	.250	.786	.250	.857	.250	.333	.500	.438

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 3 Axle Vehicles

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0
07:15 AM	0	0	0	0	0	0	1	1	0	1	0	1	0	0	0	0	0
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	0	0	1	1	0	2	0	2	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	2	0	2	0	0	1	1	0	2	0	2	0	0	0	0	0
Apprch %	0	100	0		0	0	100		0	100	0		0	0	0		
Total %	0	40	0	40	0	0	20	20	0	40	0	40	0	0	0	0	0

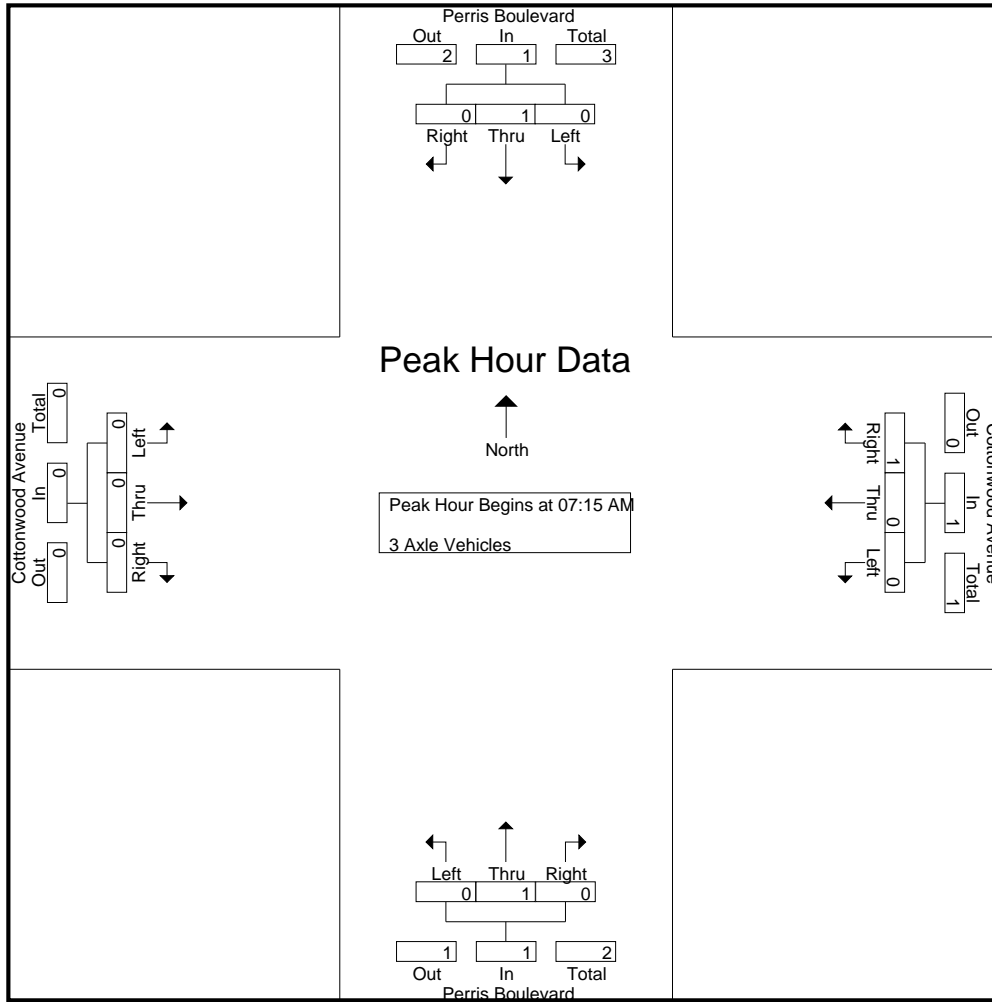
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	0	0	0	0	0	1	1	0	1	0	1	0	0	0	0	2
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	1	0	0	1	1	0	1	0	1	0	0	0	0	3
% App. Total	0	100	0		0	0	100		0	100	0		0	0	0		
PHF	.000	.250	.000	.250	.000	.000	.250	.250	.000	.250	.000	.250	.000	.000	.000	.000	.375

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	0	0	0	0	0	1	1	0	1	0	1	0	0	0	0
+15 mins.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+45 mins.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	1	0	1	0	0	1	1	0	1	0	1	0	0	0	0
% App. Total	0	100	0	0	0	0	100	0	0	100	0	0	0	0	0	0
PHF	.000	.250	.000	.250	.000	.000	.250	.250	.000	.250	.000	.250	.000	.000	.000	.000

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
07:00 AM	0	1	0	1	0	0	0	0	0	2	0	2	0	0	0	0	3
07:15 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
07:30 AM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
07:45 AM	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	4
Total	0	3	0	3	0	0	0	0	0	9	0	9	0	0	0	0	12
08:00 AM	0	4	0	4	0	0	0	0	0	2	0	2	0	0	0	0	6
08:15 AM	0	3	0	3	0	0	0	0	0	7	0	7	0	0	0	0	10
08:30 AM	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	3
08:45 AM	0	3	0	3	0	0	0	0	0	6	0	6	0	0	0	0	9
Total	0	10	0	10	0	0	0	0	0	18	0	18	0	0	0	0	28
Grand Total	0	13	0	13	0	0	0	0	0	27	0	27	0	0	0	0	40
Apprch %	0	100	0		0	0	0		0	100	0		0	0	0		
Total %	0	32.5	0	32.5	0	0	0	0	0	67.5	0	67.5	0	0	0	0	

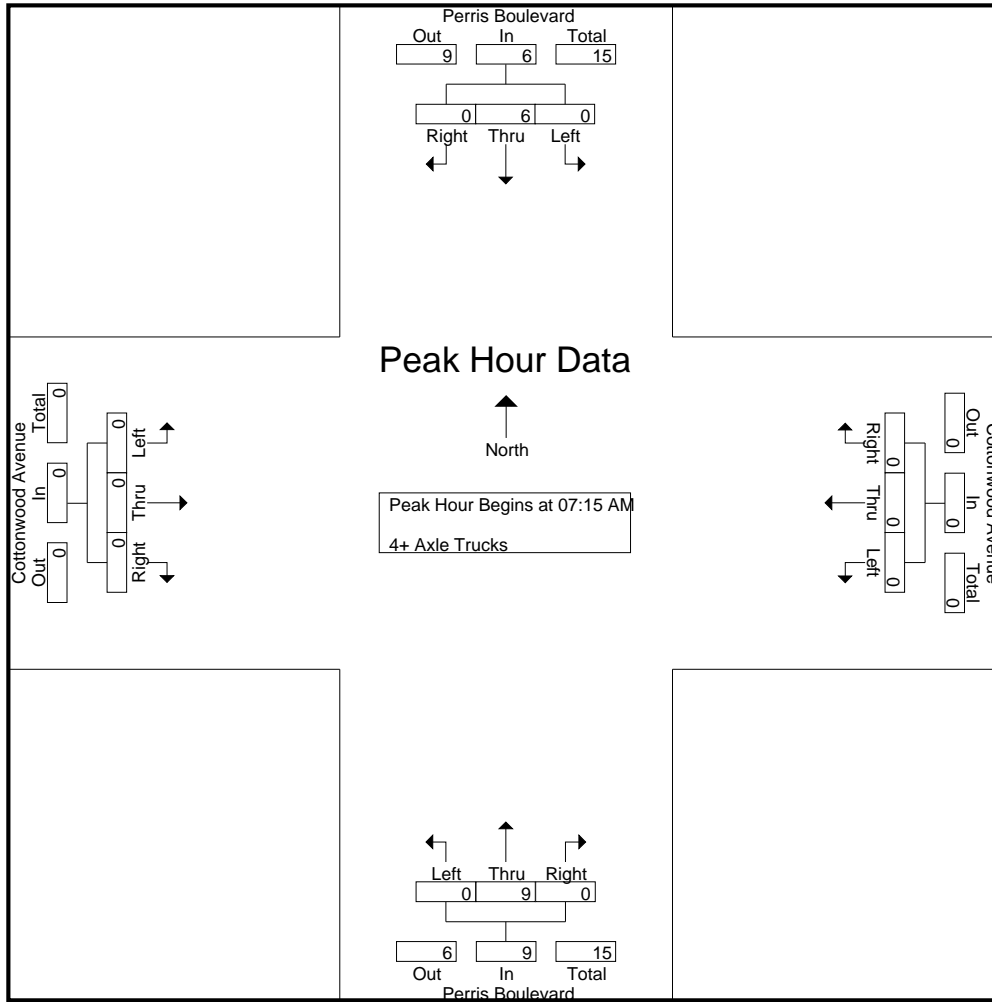
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:15 AM																	
07:15 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
07:30 AM	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0	4
07:45 AM	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	4
08:00 AM	0	4	0	4	0	0	0	0	0	2	0	2	0	0	0	0	6
Total Volume	0	6	0	6	0	0	0	0	0	9	0	9	0	0	0	0	15
% App. Total	0	100	0		0	0	0		0	100	0		0	0	0		
PHF	.000	.375	.000	.375	.000	.000	.000	.000	.000	.563	.000	.563	.000	.000	.000	.000	.625

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood AM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 07:15 AM to 08:00 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:15 AM				07:15 AM				07:15 AM			
+0 mins.	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0
+15 mins.	0	2	0	2	0	0	0	0	0	2	0	2	0	0	0	0
+30 mins.	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0
+45 mins.	0	4	0	4	0	0	0	0	0	2	0	2	0	0	0	0
Total Volume	0	6	0	6	0	0	0	0	0	9	0	9	0	0	0	0
% App. Total	0	100	0	0	0	0	0	0	0	100	0	0	0	0	0	0
PHF	.000	.375	.000	.375	.000	.000	.000	.000	.000	.563	.000	.563	.000	.000	.000	.000

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 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	20	234	24	278	12	24	22	58	12	222	5	239	26	44	24	94	669
04:15 PM	28	238	22	288	3	34	21	58	16	248	9	273	24	37	11	72	691
04:30 PM	29	239	17	285	3	38	23	64	15	225	12	252	28	46	17	91	692
04:45 PM	20	213	19	252	3	37	21	61	18	216	5	239	14	67	22	103	655
Total	97	924	82	1103	21	133	87	241	61	911	31	1003	92	194	74	360	2707
05:00 PM	30	227	27	284	5	28	25	58	16	211	10	237	15	58	12	85	664
05:15 PM	25	204	27	256	4	41	19	64	24	226	11	261	29	53	19	101	682
05:30 PM	32	225	28	285	6	29	19	54	11	210	11	232	23	62	8	93	664
05:45 PM	27	210	26	263	5	31	28	64	10	189	9	208	32	57	22	111	646
Total	114	866	108	1088	20	129	91	240	61	836	41	938	99	230	61	390	2656
Grand Total	211	1790	190	2191	41	262	178	481	122	1747	72	1941	191	424	135	750	5363
Apprch %	9.6	81.7	8.7		8.5	54.5	37		6.3	90	3.7		25.5	56.5	18		
Total %	3.9	33.4	3.5	40.9	0.8	4.9	3.3	9	2.3	32.6	1.3	36.2	3.6	7.9	2.5	14	
Passenger Vehicles	209	1734	188	2131	37	261	178	476	118	1696	72	1886	190	417	133	740	5233
% Passenger Vehicles	99.1	96.9	98.9	97.3	90.2	99.6	100	99	96.7	97.1	100	97.2	99.5	98.3	98.5	98.7	97.6
Large 2 Axle Vehicles	2	34	2	38	3	1	0	4	3	40	0	43	1	6	2	9	94
% Large 2 Axle Vehicles	0.9	1.9	1.1	1.7	7.3	0.4	0	0.8	2.5	2.3	0	2.2	0.5	1.4	1.5	1.2	1.8
3 Axle Vehicles	0	6	0	6	1	0	0	1	0	2	0	2	0	1	0	1	10
% 3 Axle Vehicles	0	0.3	0	0.3	2.4	0	0	0.2	0	0.1	0	0.1	0	0.2	0	0.1	0.2
4+ Axle Trucks	0	16	0	16	0	0	0	0	1	9	0	10	0	0	0	0	26
% 4+ Axle Trucks	0	0.9	0	0.7	0	0	0	0	0.8	0.5	0	0.5	0	0	0	0	0.5

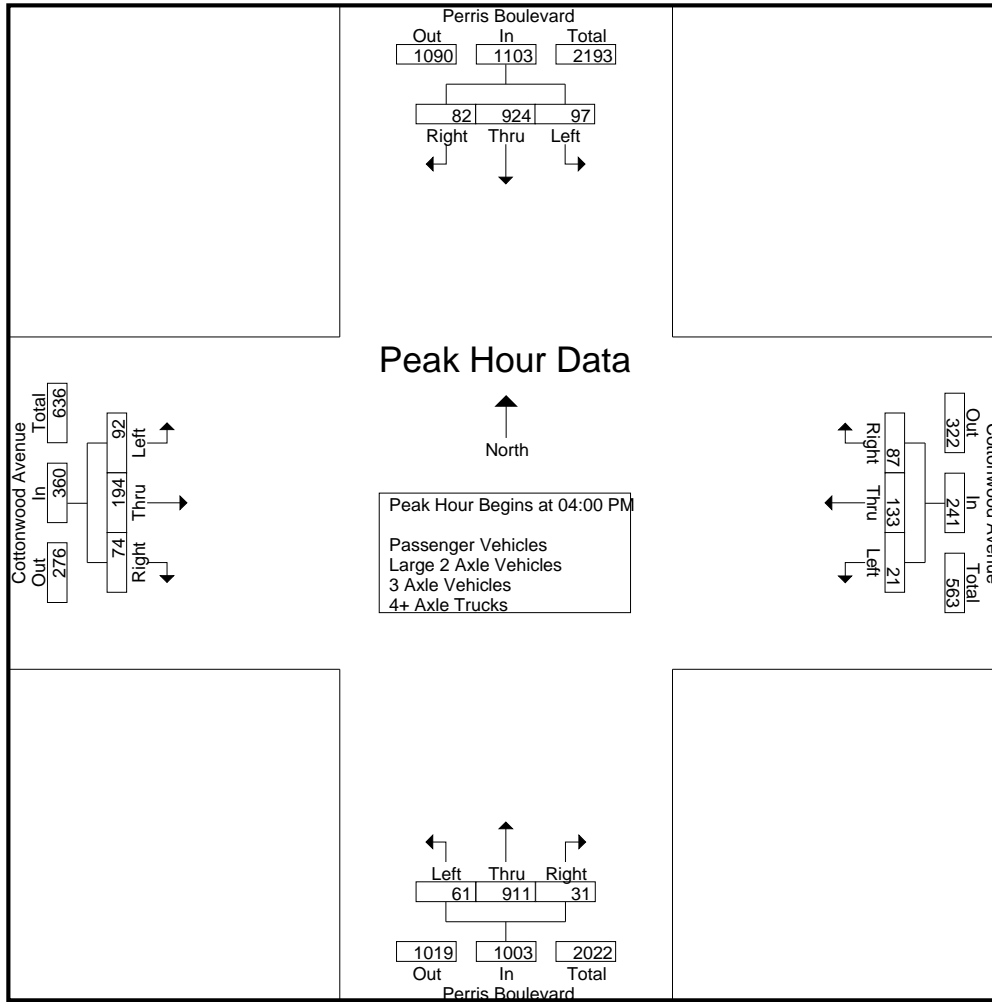
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:00 PM																	
04:00 PM	20	234	24	278	12	24	22	58	12	222	5	239	26	44	24	94	669
04:15 PM	28	238	22	288	3	34	21	58	16	248	9	273	24	37	11	72	691
04:30 PM	29	239	17	285	3	38	23	64	15	225	12	252	28	46	17	91	692
04:45 PM	20	213	19	252	3	37	21	61	18	216	5	239	14	67	22	103	655
Total Volume	97	924	82	1103	21	133	87	241	61	911	31	1003	92	194	74	360	2707
% App. Total	8.8	83.8	7.4		8.7	55.2	36.1		6.1	90.8	3.1		25.6	53.9	20.6		
PHF	.836	.967	.854	.957	.438	.875	.946	.941	.847	.918	.646	.918	.821	.724	.771	.874	.978

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:15 PM				04:30 PM				04:00 PM				05:00 PM			
+0 mins.	28	238	22	288	3	38	23	64	12	222	5	239	15	58	12	85
+15 mins.	29	239	17	285	3	37	21	61	16	248	9	273	29	53	19	101
+30 mins.	20	213	19	252	5	28	25	58	15	225	12	252	23	62	8	93
+45 mins.	30	227	27	284	4	41	19	64	18	216	5	239	32	57	22	111
Total Volume	107	917	85	1109	15	144	88	247	61	911	31	1003	99	230	61	390
% App. Total	9.6	82.7	7.7		6.1	58.3	35.6		6.1	90.8	3.1		25.4	59	15.6	
PHF	.892	.959	.787	.963	.750	.878	.880	.965	.847	.918	.646	.918	.773	.927	.693	.878

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Counts Unlimited
 PO Box 1178
 Corona, CA 92878
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Passenger Vehicles

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	20	222	23	265	11	23	22	56	12	209	5	226	26	41	23	90	637
04:15 PM	27	224	21	272	1	34	21	56	15	242	9	266	24	37	11	72	666
04:30 PM	28	232	17	277	3	38	23	64	15	222	12	249	28	44	16	88	678
04:45 PM	20	207	19	246	3	37	21	61	17	211	5	233	14	67	22	103	643
Total	95	885	80	1060	18	132	87	237	59	884	31	974	92	189	72	353	2624
05:00 PM	30	223	27	280	4	28	25	57	15	203	10	228	15	57	12	84	649
05:15 PM	25	197	27	249	4	41	19	64	24	221	11	256	29	53	19	101	670
05:30 PM	32	222	28	282	6	29	19	54	10	206	11	227	23	61	8	92	655
05:45 PM	27	207	26	260	5	31	28	64	10	182	9	201	31	57	22	110	635
Total	114	849	108	1071	19	129	91	239	59	812	41	912	98	228	61	387	2609
Grand Total	209	1734	188	2131	37	261	178	476	118	1696	72	1886	190	417	133	740	5233
Apprch %	9.8	81.4	8.8		7.8	54.8	37.4		6.3	89.9	3.8		25.7	56.4	18		
Total %	4	33.1	3.6	40.7	0.7	5	3.4	9.1	2.3	32.4	1.4	36	3.6	8	2.5	14.1	

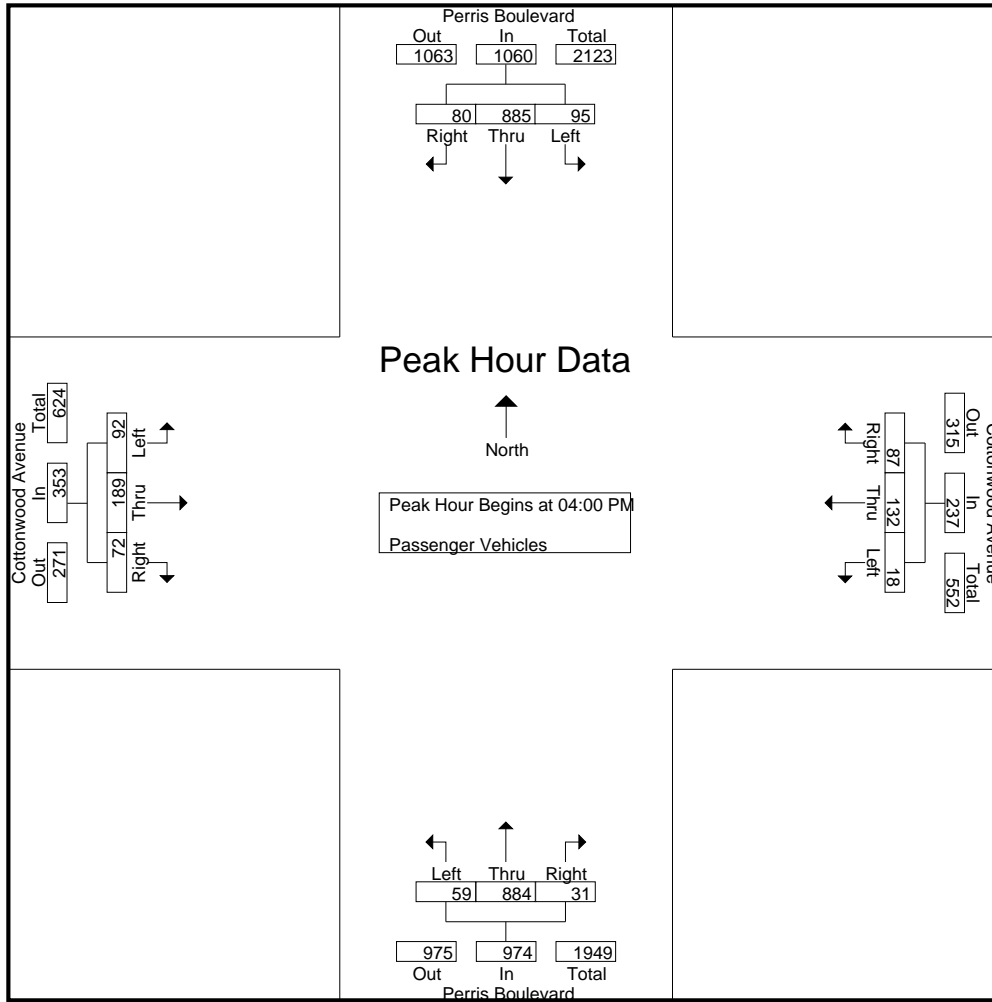
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:00 PM																	
04:00 PM	20	222	23	265	11	23	22	56	12	209	5	226	26	41	23	90	637
04:15 PM	27	224	21	272	1	34	21	56	15	242	9	266	24	37	11	72	666
04:30 PM	28	232	17	277	3	38	23	64	15	222	12	249	28	44	16	88	678
04:45 PM	20	207	19	246	3	37	21	61	17	211	5	233	14	67	22	103	643
Total Volume	95	885	80	1060	18	132	87	237	59	884	31	974	92	189	72	353	2624
% App. Total	9	83.5	7.5		7.6	55.7	36.7		6.1	90.8	3.2		26.1	53.5	20.4		
PHF	.848	.954	.870	.957	.409	.868	.946	.926	.868	.913	.646	.915	.821	.705	.783	.857	.968

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:00 PM				04:00 PM				04:00 PM							
+0 mins.	20	222	23	265	11	23	22	56	12	209	5	226	26	41	23	90
+15 mins.	27	224	21	272	1	34	21	56	15	242	9	266	24	37	11	72
+30 mins.	28	232	17	277	3	38	23	64	15	222	12	249	28	44	16	88
+45 mins.	20	207	19	246	3	37	21	61	17	211	5	233	14	67	22	103
Total Volume	95	885	80	1060	18	132	87	237	59	884	31	974	92	189	72	353
% App. Total	9	83.5	7.5		7.6	55.7	36.7		6.1	90.8	3.2		26.1	53.5	20.4	
PHF	.848	.954	.870	.957	.409	.868	.946	.926	.868	.913	.646	.915	.821	.705	.783	.857

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- Large 2 Axle Vehicles

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	9	1	10	1	1	0	2	0	11	0	11	0	3	1	4	27
04:15 PM	1	7	1	9	1	0	0	1	1	5	0	6	0	0	0	0	16
04:30 PM	1	4	0	5	0	0	0	0	0	2	0	2	0	2	1	3	10
04:45 PM	0	3	0	3	0	0	0	0	0	4	0	4	0	0	0	0	7
Total	2	23	2	27	2	1	0	3	1	22	0	23	0	5	2	7	60
05:00 PM	0	3	0	3	1	0	0	1	1	7	0	8	0	1	0	1	13
05:15 PM	0	4	0	4	0	0	0	0	0	4	0	4	0	0	0	0	8
05:30 PM	0	3	0	3	0	0	0	0	1	3	0	4	0	0	0	0	7
05:45 PM	0	1	0	1	0	0	0	0	0	4	0	4	1	0	0	1	6
Total	0	11	0	11	1	0	0	1	2	18	0	20	1	1	0	2	34
Grand Total	2	34	2	38	3	1	0	4	3	40	0	43	1	6	2	9	94
Apprch %	5.3	89.5	5.3		75	25	0		7	93	0		11.1	66.7	22.2		
Total %	2.1	36.2	2.1	40.4	3.2	1.1	0	4.3	3.2	42.6	0	45.7	1.1	6.4	2.1	9.6	

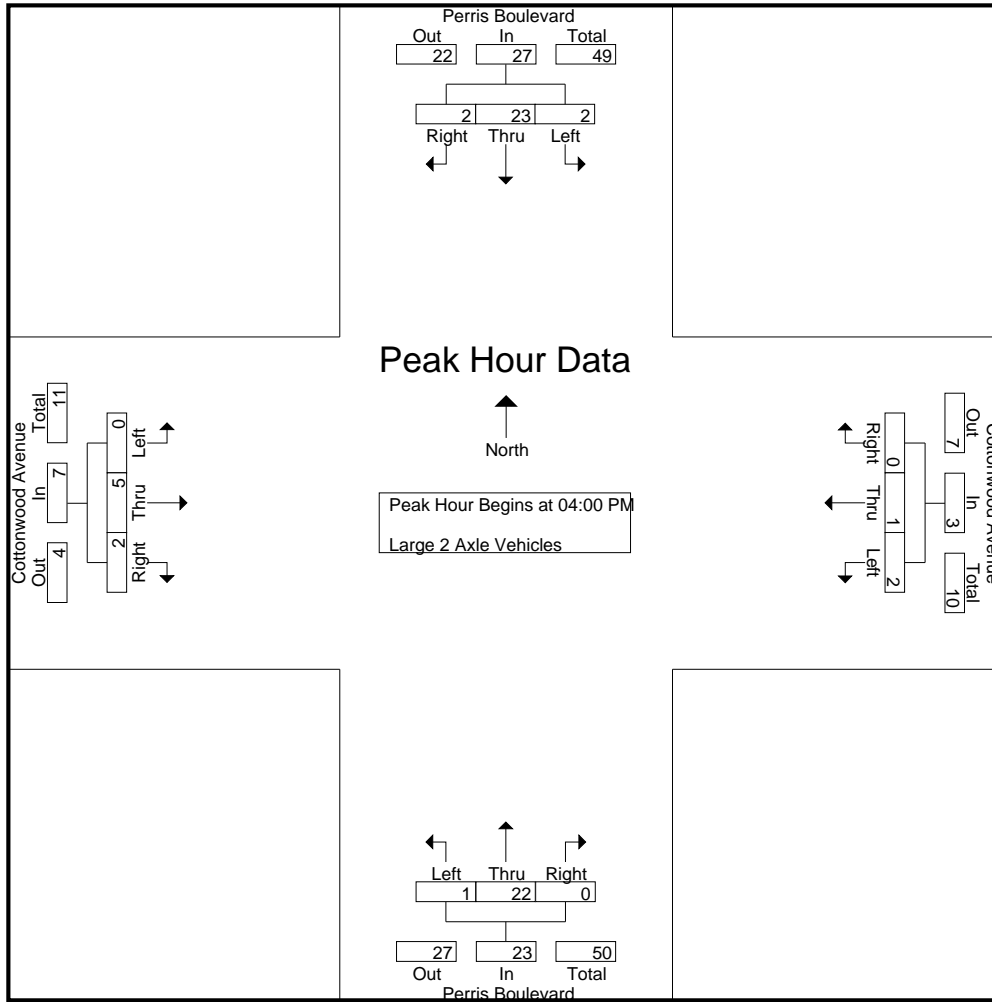
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:00 PM																	
04:00 PM	0	9	1	10	1	1	0	2	0	11	0	11	0	3	1	4	27
04:15 PM	1	7	1	9	1	0	0	1	1	5	0	6	0	0	0	0	16
04:30 PM	1	4	0	5	0	0	0	0	0	2	0	2	0	2	1	3	10
04:45 PM	0	3	0	3	0	0	0	0	0	4	0	4	0	0	0	0	7
Total Volume	2	23	2	27	2	1	0	3	1	22	0	23	0	5	2	7	60
% App. Total	7.4	85.2	7.4		66.7	33.3	0		4.3	95.7	0		0	71.4	28.6		
PHF	.500	.639	.500	.675	.500	.250	.000	.375	.250	.500	.000	.523	.000	.417	.500	.438	.556

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:00 PM				04:00 PM				04:00 PM				04:00 PM			
+0 mins.	0	9	1	10	1	1	0	2	0	11	0	11	0	3	1	4
+15 mins.	1	7	1	9	1	0	0	1	1	5	0	6	0	0	0	0
+30 mins.	1	4	0	5	0	0	0	0	0	2	0	2	0	2	1	3
+45 mins.	0	3	0	3	0	0	0	0	0	4	0	4	0	0	0	0
Total Volume	2	23	2	27	2	1	0	3	1	22	0	23	0	5	2	7
% App. Total	7.4	85.2	7.4		66.7	33.3	0		4.3	95.7	0		0	71.4	28.6	
PHF	.500	.639	.500	.675	.500	.250	.000	.375	.250	.500	.000	.523	.000	.417	.500	.438

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 3 Axle Vehicles

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
04:15 PM	0	1	0	1	1	0	0	1	0	0	0	0	0	0	0	0	2
04:30 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	4	0	4	1	0	0	1	0	1	0	1	0	0	0	0	6
05:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
05:15 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	0	2	0	2	0	0	0	0	0	1	0	1	0	1	0	1	4
Grand Total	0	6	0	6	1	0	0	1	0	2	0	2	0	1	0	1	10
Apprch %	0	100	0		100	0	0		0	100	0		0	100	0		
Total %	0	60	0	60	10	0	0	10	0	20	0	20	0	10	0	10	

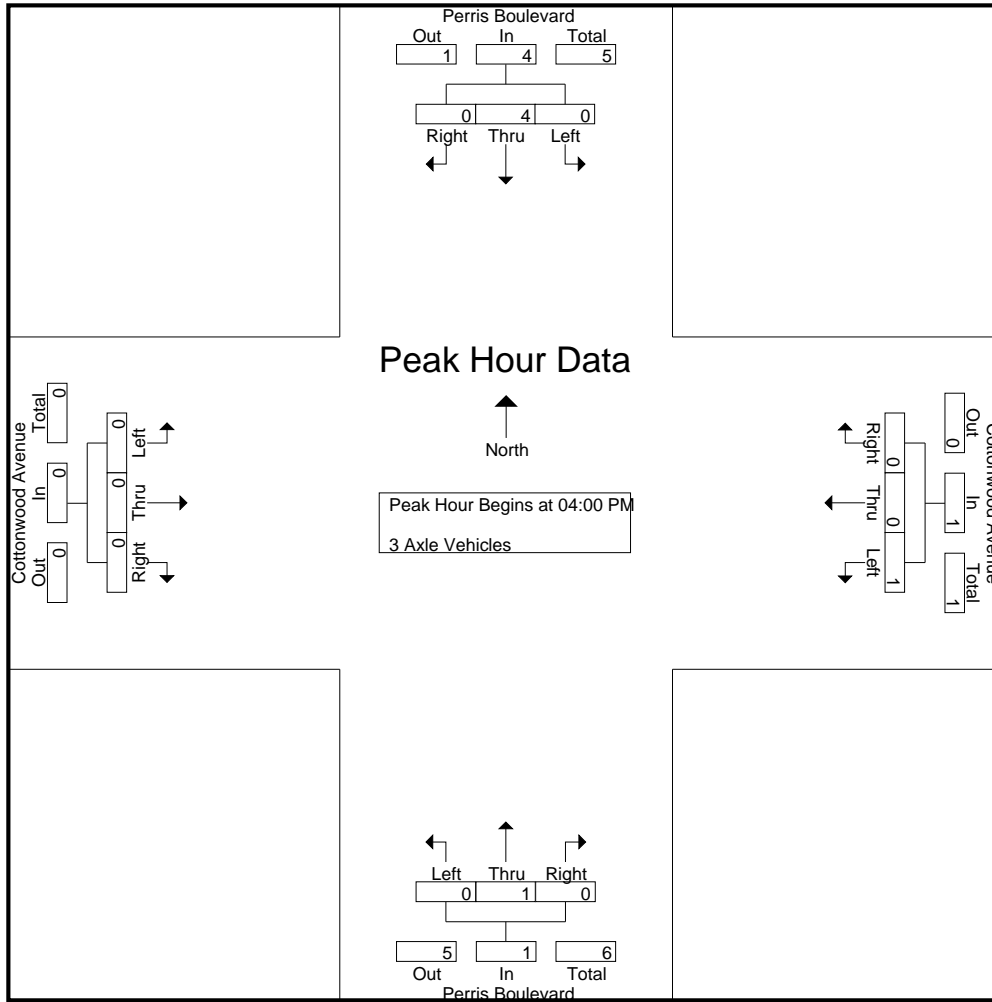
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:00 PM																	
04:00 PM	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0	2
04:15 PM	0	1	0	1	1	0	0	1	0	0	0	0	0	0	0	0	2
04:30 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Total Volume	0	4	0	4	1	0	0	1	0	1	0	1	0	0	0	0	6
% App. Total	0	100	0		100	0	0		0	100	0		0	0	0		
PHF	.000	1.00	.000	1.00	.250	.000	.000	.250	.000	.250	.000	.250	.000	.000	.000	.000	.750

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
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City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:00 PM				04:00 PM				04:00 PM				04:00 PM			
+0 mins.	0	1	0	1	0	0	0	0	0	1	0	1	0	0	0	0
+15 mins.	0	1	0	1	1	0	0	1	0	0	0	0	0	0	0	0
+30 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
+45 mins.	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	4	0	4	1	0	0	1	0	1	0	1	0	0	0	0
% App. Total	0	100	0		100	0	0		0	100	0		0	0	0	
PHF	.000	1.000	.000	1.000	.250	.000	.000	.250	.000	.250	.000	.250	.000	.000	.000	.000

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MRV_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 1

Groups Printed- 4+ Axle Trucks

Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:15 PM	0	6	0	6	0	0	0	0	0	1	0	1	0	0	0	0	7
04:30 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:45 PM	0	2	0	2	0	0	0	0	1	1	0	2	0	0	0	0	4
Total	0	12	0	12	0	0	0	0	1	4	0	5	0	0	0	0	17
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
05:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
05:45 PM	0	2	0	2	0	0	0	0	0	3	0	3	0	0	0	0	5
Total	0	4	0	4	0	0	0	0	0	5	0	5	0	0	0	0	9
Grand Total	0	16	0	16	0	0	0	0	1	9	0	10	0	0	0	0	26
Apprch %	0	100	0		0	0	0		10	90	0		0	0	0		
Total %	0	61.5	0	61.5	0	0	0	0	3.8	34.6	0	38.5	0	0	0	0	

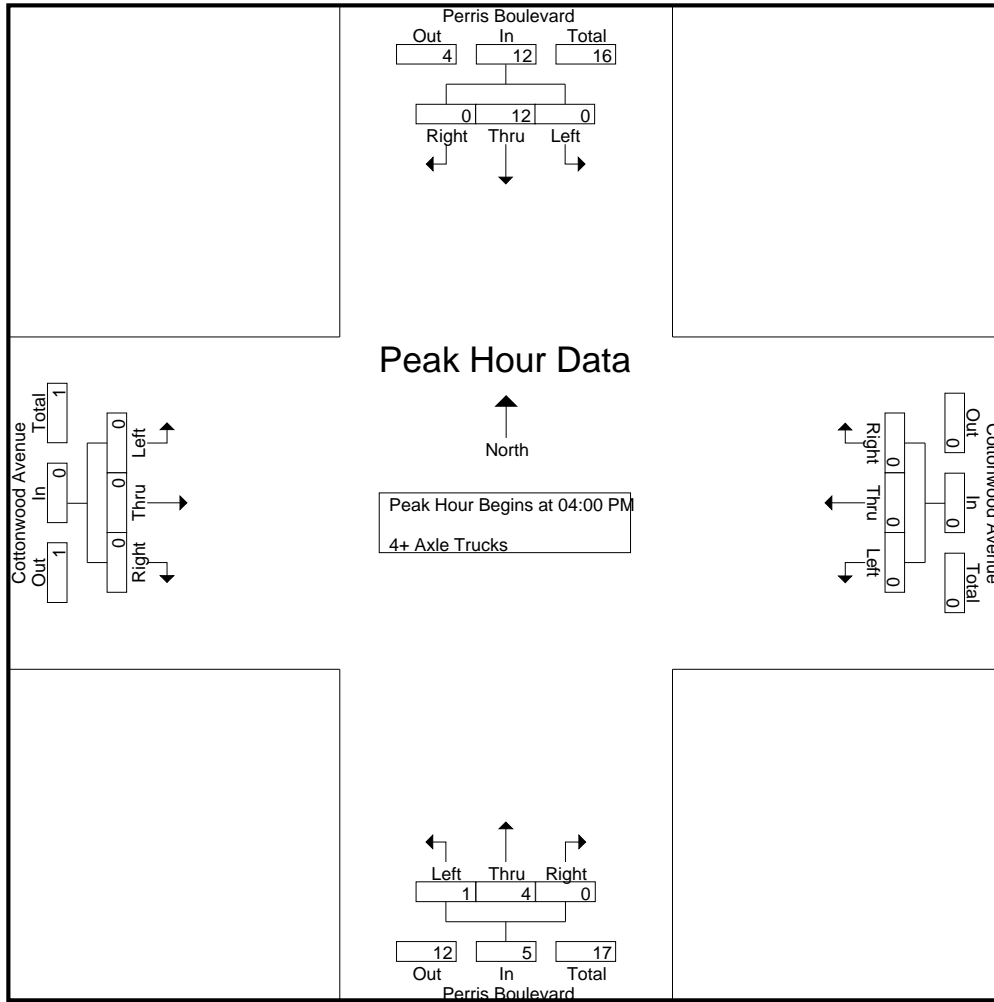
Start Time	Perris Boulevard Southbound				Cottonwood Avenue Westbound				Perris Boulevard Northbound				Cottonwood Avenue Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:00 PM																	
04:00 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:15 PM	0	6	0	6	0	0	0	0	0	1	0	1	0	0	0	0	7
04:30 PM	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0	3
04:45 PM	0	2	0	2	0	0	0	0	1	1	0	2	0	0	0	0	4
Total Volume	0	12	0	12	0	0	0	0	1	4	0	5	0	0	0	0	17
% App. Total	0	100	0		0	0	0		20	80	0		0	0	0		
PHF	.000	.500	.000	.500	.000	.000	.000	.000	.250	1.00	.000	.625	.000	.000	.000	.000	.607

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited
 PO Box 1178
 Corona, CA 92878
 (951) 268-6268

City of Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue
 Weather: Clear

File Name : 05_MR_V_Perris_Cottonwood PM
 Site Code : 00319397
 Start Date : 5/23/2019
 Page No : 2



Peak Hour Analysis From 04:00 PM to 04:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:00 PM				04:00 PM				04:00 PM				04:00 PM			
+0 mins.	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0
+15 mins.	0	6	0	6	0	0	0	0	0	1	0	1	0	0	0	0
+30 mins.	0	2	0	2	0	0	0	0	0	1	0	1	0	0	0	0
+45 mins.	0	2	0	2	0	0	0	0	1	1	0	2	0	0	0	0
Total Volume	0	12	0	12	0	0	0	0	1	4	0	5	0	0	0	0
% App. Total	0	100	0	0	0	0	0	0	20	80	0	0	0	0	0	0
PHF	.000	.500	.000	.500	.000	.000	.000	.000	.250	1.000	.000	.625	.000	.000	.000	.000

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue



Date: 5/23/2019
 Day: Thursday

PEDESTRIANS

	North Leg Perris Boulevard Pedestrians	East Leg Cottonwood Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Cottonwood Avenue Pedestrians	
7:00 AM	2	0	0	0	2
7:15 AM	2	1	0	0	3
7:30 AM	1	1	3	4	9
7:45 AM	0	0	0	2	2
8:00 AM	0	0	0	1	1
8:15 AM	0	2	0	0	2
8:30 AM	0	0	2	1	3
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	5	4	5	8	22

	North Leg Perris Boulevard Pedestrians	East Leg Cottonwood Avenue Pedestrians	South Leg Perris Boulevard Pedestrians	West Leg Cottonwood Avenue Pedestrians	
4:00 PM	1	2	0	1	4
4:15 PM	0	0	0	1	1
4:30 PM	2	0	2	1	5
4:45 PM	2	1	2	0	5
5:00 PM	0	0	0	2	2
5:15 PM	0	2	0	2	4
5:30 PM	0	2	0	1	3
5:45 PM	0	2	0	0	2
TOTAL VOLUMES:	5	9	4	8	26

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Location: Moreno Valley
 N/S: Perris Boulevard
 E/W: Cottonwood Avenue



Date: 5/23/2019
 Day: Thursday

BICYCLES

	Southbound Perris Boulevard			Westbound Cottonwood Avenue			Northbound Perris Boulevard			Eastbound Cottonwood Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
TOTAL VOLUMES:	0	1	0	0	0	0	0	0	0	0	0	0	1

	Southbound Perris Boulevard			Westbound Cottonwood Avenue			Northbound Perris Boulevard			Eastbound Cottonwood Avenue			
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
4:00 PM	0	3	0	0	1	0	0	0	0	0	0	0	4
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	1	0	0	0	0	1	1	0	0	0	0	3
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	1	0	1
5:15 PM	0	1	0	0	0	0	0	0	0	0	1	0	2
5:30 PM	0	1	1	0	0	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	6	1	0	1	0	1	1	0	0	2	0	12

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited, Inc.

City of Moreno Valley
 Perris Boulevard
 B/ Eucalyptus Avneue - Atwood Avenue
 24 Hour Directional Volume Count

PO Box 1178
 Corona, CA 92878
 Phone: (951) 268-6268
 email: counts@countsunlimited.com

MRV001
 Site Code: 003-19397

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Start Time	5/23/2019 Thu	Northbound		Hour Totals		Southbound		Hour Totals		Combined Totals	
		Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00		38	223			60	196				
12:15		39	231			40	197				
12:30		43	254			40	228				
12:45		39	227	159	935	33	204	173	825	332	1760
01:00		25	241			35	232				
01:15		22	253			32	240				
01:30		28	213			26	201				
01:45		25	276	100	983	33	211	126	884	226	1867
02:00		31	281			24	227				
02:15		24	301			26	277				
02:30		23	318			21	281				
02:45		21	296	99	1196	27	284	98	1069	197	2265
03:00		35	264			25	303				
03:15		53	256			22	280				
03:30		67	273			30	284				
03:45		78	259	233	1052	31	304	108	1171	341	2223
04:00		109	221			37	305				
04:15		122	221			59	301				
04:30		124	224			81	306				
04:45		145	253	500	919	49	268	226	1180	726	2099
05:00		171	244			63	282				
05:15		174	207			82	268				
05:30		188	191			129	336				
05:45		169	197	702	839	102	288	376	1174	1078	2013
06:00		232	215			100	241				
06:15		198	211			109	267				
06:30		220	190			107	254				
06:45		236	214	886	830	126	239	442	1001	1328	1831
07:00		217	196			133	237				
07:15		272	191			212	189				
07:30		259	202			285	182				
07:45		266	158	1014	747	258	196	888	804	1902	1551
08:00		297	176			193	194				
08:15		228	149			173	198				
08:30		235	144			151	171				
08:45		244	131	1004	600	165	180	682	743	1686	1343
09:00		213	145			146	176				
09:15		200	123			169	159				
09:30		198	105			131	165				
09:45		210	106	821	479	192	129	638	629	1459	1108
10:00		191	112			146	148				
10:15		206	96			177	101				
10:30		196	76			164	126				
10:45		221	107	814	391	185	91	672	466	1486	857
11:00		213	68			163	102				
11:15		234	58			189	87				
11:30		236	44			176	68				
11:45		202	43	885	213	207	61	735	318	1620	531
Total		7217	9184	7217	9184	5164	10264	5164	10264	12381	19448
Combined Total		16401		16401		15428		15428		31829	
AM Peak	-	07:15	-	-	-	07:15	-	-	-	-	-
Vol.	-	1094	-	-	-	948	-	-	-	-	-
P.H.F.		0.921				0.832					
PM Peak	-	-	02:00	-	-	-	03:45	-	-	-	-
Vol.	-	-	1196	-	-	-	1216	-	-	-	-
P.H.F.			0.940				0.993				
Percentage		44.0%	56.0%			33.5%	66.5%				
ADT/AADT		ADT 31,829	AADT 31,829								

Counts Unlimited, Inc.

City of Moreno Valley
 Perris Boulevard
 B/ Atwood Avenue - Auto Center Drive
 24 Hour Directional Volume Count

PO Box 1178
 Corona, CA 92878
 Phone: (951) 268-6268
 email: counts@countsunlimited.com

MRV002
 Site Code: 003-19397

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Start Time	5/23/2019 Thu	Northbound		Hour Totals		Southbound		Hour Totals		Combined Totals	
		Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00		33	223			66	204				
12:15		42	222			39	174				
12:30		38	240			36	231				
12:45		41	216	154	901	37	196	178	805	332	1706
01:00		24	222			37	228				
01:15		22	250			25	232				
01:30		27	204			32	193				
01:45		25	268	98	944	27	205	121	858	219	1802
02:00		30	274			28	230				
02:15		24	291			27	266				
02:30		22	316			19	284				
02:45		23	286	99	1167	28	270	102	1050	201	2217
03:00		33	250			26	309				
03:15		51	259			21	252				
03:30		67	245			30	284				
03:45		72	247	223	1001	32	275	109	1120	332	2121
04:00		103	225			35	304				
04:15		113	213			53	274				
04:30		113	218			74	313				
04:45		146	245	475	901	57	255	219	1146	694	2047
05:00		160	245			58	264				
05:15		163	200			83	287				
05:30		175	182			133	312				
05:45		166	199	664	826	98	306	372	1169	1036	1995
06:00		211	207			98	232				
06:15		197	208			104	263				
06:30		211	186			115	242				
06:45		228	208	847	809	123	245	440	982	1287	1791
07:00		208	183			129	230				
07:15		270	176			218	191				
07:30		260	191			283	180				
07:45		257	143	995	693	254	184	884	785	1879	1478
08:00		280	162			182	195				
08:15		218	148			182	197				
08:30		233	133			153	185				
08:45		244	124	975	567	160	170	677	747	1652	1314
09:00		201	145			145	160				
09:15		200	120			164	173				
09:30		194	106			135	150				
09:45		193	103	788	474	187	130	631	613	1419	1087
10:00		185	110			148	143				
10:15		215	96			159	111				
10:30		187	74			167	110				
10:45		210	99	797	379	178	87	652	451	1449	830
11:00		201	71			171	104				
11:15		221	57			173	84				
11:30		234	44			172	64				
11:45		206	43	862	215	207	57	723	309	1585	524
Total		6977	8877	6977	8877	5108	10035	5108	10035	12085	18912
Combined Total			15854		15854		15143		15143		30997
AM Peak	-	07:15	-	-	-	07:15	-	-	-	-	-
Vol.	-	1067	-	-	-	937	-	-	-	-	-
P.H.F.		0.953				0.828					
PM Peak	-	-	02:00	-	-	-	05:00	-	-	-	-
Vol.	-	-	1167	-	-	-	1169	-	-	-	-
P.H.F.			0.923				0.934				
Percentage		44.0%	56.0%			33.7%	66.3%				
ADT/AADT		ADT 30,997	AADT 30,997								

Counts Unlimited, Inc.

City of Moreno Valley
 Perris Boulevard
 B/ Auto Center Drive - Dracaea Avenue
 24 Hour Directional Volume Count

PO Box 1178
 Corona, CA 92878
 Phone: (951) 268-6268
 email: counts@countsunlimited.com

MRV003
 Site Code: 003-19397

Start Time	5/23/2019 Thu	Northbound		Hour Totals		Southbound		Hour Totals		Combined Totals	
		Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00		32	215			65	212				
12:15		40	223			43	181				
12:30		38	234			35	236				
12:45		39	197	149	869	36	207	179	836	328	1705
01:00		23	227			36	222				
01:15		23	232			26	233				
01:30		24	209			33	201				
01:45		19	259	89	927	27	206	122	862	211	1789
02:00		32	267			30	231				
02:15		22	284			27	270				
02:30		20	301			18	271				
02:45		19	295	93	1147	26	258	101	1030	194	2177
03:00		32	252			29	305				
03:15		54	251			22	263				
03:30		61	223			30	291				
03:45		79	247	226	973	35	277	116	1136	342	2109
04:00		103	210			33	295				
04:15		113	214			52	261				
04:30		114	218			71	315				
04:45		145	226	475	868	64	265	220	1136	695	2004
05:00		169	242			59	280				
05:15		166	199			75	272				
05:30		177	164			135	294				
05:45		167	212	679	817	101	307	370	1153	1049	1970
06:00		220	206			101	252				
06:15		189	206			102	258				
06:30		226	176			113	232				
06:45		229	215	864	803	125	244	441	986	1305	1789
07:00		216	178			132	237				
07:15		256	185			214	192				
07:30		261	182			284	173				
07:45		255	146	988	691	235	186	865	788	1853	1479
08:00		289	156			195	202				
08:15		226	147			199	185				
08:30		221	146			155	181				
08:45		236	129	972	578	152	179	701	747	1673	1325
09:00		203	138			155	174				
09:15		199	130			173	167				
09:30		196	102			141	148				
09:45		185	105	783	475	180	130	649	619	1432	1094
10:00		174	108			145	144				
10:15		207	100			152	109				
10:30		177	73			170	114				
10:45		213	96	771	377	186	93	653	460	1424	837
11:00		196	72			177	102				
11:15		211	61			183	81				
11:30		230	40			167	65				
11:45		192	45	829	218	189	63	716	311	1545	529
Total		6918	8743	6918	8743	5133	10064	5133	10064	12051	18807
Combined Total		15661		15661		15197		15197		30858	
AM Peak	-	07:15	-	-	-	07:15	-	-	-	-	-
Vol.	-	1061	-	-	-	928	-	-	-	-	-
P.H.F.	-	0.918	-	-	-	0.817	-	-	-	-	-
PM Peak	-	-	02:00	-	-	-	05:00	-	-	-	-
Vol.	-	-	1147	-	-	-	1153	-	-	-	-
P.H.F.	-	-	0.953	-	-	-	0.915	-	-	-	-
Percentage		44.2%	55.8%			33.8%	66.2%				
ADT/AADT		ADT 30,858	AADT 30,858								

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Counts Unlimited, Inc.

City of Moreno Valley
 Perris Boulevard
 B/ Dracaea Avenue - Cottonwood Avenue
 24 Hour Directional Volume Count

PO Box 1178
 Corona, CA 92878
 Phone: (951) 268-6268
 email: counts@countsunlimited.com

MRV004
 Site Code: 003-19397

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Start Time	5/23/2019 Thu	Northbound		Hour Totals		Southbound		Hour Totals		Combined Totals	
		Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00		37	225			53	201				
12:15		35	213			41	159				
12:30		40	224			38	230				
12:45		34	211	146	873	31	218	163	808	309	1681
01:00		20	218			31	202				
01:15		21	231			21	224				
01:30		26	188			33	200				
01:45		24	278	91	915	28	188	113	814	204	1729
02:00		29	270			29	219				
02:15		24	286			24	274				
02:30		18	319			17	269				
02:45		20	253	91	1128	23	251	93	1013	184	2141
03:00		35	255			27	294				
03:15		39	229			19	236				
03:30		60	250			32	257				
03:45		74	264	208	998	34	270	112	1057	320	2055
04:00		92	213			34	276				
04:15		103	207			49	249				
04:30		99	231			78	281				
04:45		133	240	427	891	64	258	225	1064	652	1955
05:00		142	218			56	266				
05:15		152	188			85	267				
05:30		166	181			141	282				
05:45		147	178	607	765	104	273	386	1088	993	1853
06:00		193	206			105	235				
06:15		173	197			106	242				
06:30		184	159			109	229				
06:45		229	192	779	754	112	222	432	928	1211	1682
07:00		201	194			129	224				
07:15		250	164			208	185				
07:30		251	182			264	170				
07:45		247	151	949	691	237	175	838	754	1787	1445
08:00		258	152			202	187				
08:15		210	132			187	173				
08:30		220	134			159	163				
08:45		216	125	904	543	143	165	691	688	1595	1231
09:00		199	125			151	157				
09:15		192	117			147	163				
09:30		165	97			135	135				
09:45		192	104	748	443	171	112	604	567	1352	1010
10:00		179	107			140	137				
10:15		188	82			140	103				
10:30		185	71			165	112				
10:45		213	93	765	353	175	84	620	436	1385	789
11:00		200	69			164	97				
11:15		208	57			177	75				
11:30		229	42			160	65				
11:45		195	42	832	210	177	57	678	294	1510	504
Total		6547	8564	6547	8564	4955	9511	4955	9511	11502	18075
Combined Total		15111		15111		14466		14466		29577	
AM Peak	-	07:15	-	-	-	07:15	-	-	-	-	-
Vol.	-	1006	-	-	-	911	-	-	-	-	-
P.H.F.	-	0.975	-	-	-	0.863	-	-	-	-	-
PM Peak	-	-	01:45	-	-	-	02:15	-	-	-	-
Vol.	-	-	1153	-	-	-	1088	-	-	-	-
P.H.F.	-	-	0.904	-	-	-	0.925	-	-	-	-
Percentage		43.3%	56.7%			34.3%	65.7%				
ADT/AADT		ADT 29,577		AADT 29,577							

Counts Unlimited, Inc.

PO Box 1178

Corona, CA 92878

Phone: (951) 268-6268

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City of Moreno Valley
 Dracaea Avenue
 B/ Peris Boulevard - Project Driveway 2
 24 Hour Directional Volume Count

MRV005
 Site Code: 003-19397

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Start Time	5/23/2019 Thu	Eastbound		Hour Totals		Westbound		Hour Totals		Combined Totals	
		Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00		19	40			9	31				
12:15		10	44			10	37				
12:30		7	39			10	53				
12:45		4	25	40	148	3	48	32	169	72	317
01:00		7	28			3	34				
01:15		1	30			5	40				
01:30		6	28			4	36				
01:45		5	42	19	128	1	32	13	142	32	270
02:00		4	49			2	26				
02:15		4	35			3	43				
02:30		5	36			2	56				
02:45		2	45	15	165	1	53	8	178	23	343
03:00		4	51			3	51				
03:15		4	46			15	53				
03:30		7	45			14	33				
03:45		5	39	20	181	9	29	41	166	61	347
04:00		7	53			15	31				
04:15		5	42			20	38				
04:30		3	49			19	33				
04:45		4	53	19	197	20	39	74	141	93	338
05:00		9	46			29	37				
05:15		4	52			20	35				
05:30		13	42			33	34				
05:45		6	55	32	195	29	48	111	154	143	349
06:00		8	51			27	33				
06:15		5	43			32	40				
06:30		7	46			32	35				
06:45		16	36	36	176	29	34	120	142	156	318
07:00		14	37			30	40				
07:15		23	38			46	35				
07:30		32	27			58	20				
07:45		23	30	92	132	59	32	193	127	285	259
08:00		23	35			61	30				
08:15		14	30			36	26				
08:30		20	36			37	27				
08:45		16	32	73	133	33	19	167	102	240	235
09:00		19	32			39	28				
09:15		29	25			36	23				
09:30		19	26			47	13				
09:45		21	22	88	105	21	13	143	77	231	182
10:00		18	39			21	19				
10:15		14	18			29	16				
10:30		15	21			29	7				
10:45		28	19	75	97	35	14	114	56	189	153
11:00		14	12			34	9				
11:15		25	13			27	6				
11:30		25	6			28	8				
11:45		28	11	92	42	20	10	109	33	201	75
Total		601	1699	601	1699	1125	1487	1125	1487	1726	3186
Combined Total			2300		2300		2612		2612		4912
AM Peak	-	07:15	-	-	-	07:15	-	-	-	-	-
Vol.	-	101	-	-	-	224	-	-	-	-	-
P.H.F.	-	0.789	-	-	-	0.918	-	-	-	-	-
PM Peak	-	-	04:30	-	-	-	02:30	-	-	-	-
Vol.	-	-	200	-	-	-	213	-	-	-	-
P.H.F.	-	-	0.943	-	-	-	0.951	-	-	-	-
Percentage		26.1%	73.9%			43.1%	56.9%				
ADT/AADT		ADT 4,912	AADT 4,912								

APPENDIX C:

VOLUME DEVELOPMENT WORKSHEETS

Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
1 Perris Boulevard/Eucalyptus Avenue								
NBL	50	16	0	66	51	10	0	61
NBT	1,002	8	0	1,010	978	5	0	983
NBR	58	16	0	74	59	10	0	69
SBL	72	0	0	72	97	0	0	97
SBT	1,006	8	0	1,014	1,063	5	0	1,068
SBR	32	0	0	32	45	0	0	45
EBL	22	0	0	22	51	0	0	51
EBT	73	0	0	73	189	0	0	189
EBR	56	17	0	73	79	10	0	89
WBL	83	17	0	100	52	10	0	62
WBT	217	0	0	217	127	0	0	127
WBR	194	0	0	194	108	0	0	108
North Leg								
Approach	1,110	8	0	1,118	1,205	5	0	1,210
Departure	1,218	8	0	1,226	1,137	5	0	1,142
Total	2,328	16	0	2,344	2,342	10	0	2,352
South Leg								
Approach	1,110	40	0	1,150	1,088	25	0	1,113
Departure	1,145	42	0	1,187	1,194	25	0	1,219
Total	2,255	82	0	2,337	2,282	50	0	2,332
East Leg								
Approach	494	17	0	511	287	10	0	297
Departure	203	16	0	219	345	10	0	355
Total	697	33	0	730	632	20	0	652
West Leg								
Approach	151	17	0	168	319	10	0	329
Departure	299	16	0	315	223	10	0	233
Total	450	33	0	483	542	20	0	562
Total Approaches								
Approach	2,865	82	0	2,947	2,899	50	0	2,949
Departure	2,865	82	0	2,947	2,899	50	0	2,949
Total	5,730	164	0	5,894	5,798	100	0	5,898

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
2 Perris Boulevard/Atwood Avenue								
NBL	52	8	0	60	43	5	0	48
NBT	1,067	24	0	1,091	1,059	15	0	1,074
NBR	3	0	0	3	1	0	0	1
SBL	16	25	21	62	42	15	22	79
SBT	1,135	17	-21	1,131	1,120	10	-22	1,108
SBR	16	0	0	16	15	0	0	15
EBL	13	0	0	13	27	0	0	27
EBT	1	0	0	1	1	0	0	1
EBR	37	8	0	45	36	5	0	41
WBL	4	0	0	4	3	0	0	3
WBT	2	0	0	2	1	0	0	1
WBR	9	16	0	25	13	10	0	23
North Leg								
Approach	1,167	42	0	1,209	1,177	25	0	1,202
Departure	1,089	40	0	1,129	1,099	25	0	1,124
Total	2,256	82	0	2,338	2,276	50	0	2,326
South Leg								
Approach	1,122	32	0	1,154	1,103	20	0	1,123
Departure	1,176	25	-21	1,180	1,159	15	-22	1,152
Total	2,298	57	-21	2,334	2,262	35	-22	2,275
East Leg								
Approach	15	16	0	31	17	10	0	27
Departure	20	25	21	66	44	15	22	81
Total	35	41	21	97	61	25	22	108
West Leg								
Approach	51	8	0	59	64	5	0	69
Departure	70	8	0	78	59	5	0	64
Total	121	16	0	137	123	10	0	133
Total Approaches								
Approach	2,355	98	0	2,453	2,361	60	0	2,421
Departure	2,355	98	0	2,453	2,361	60	0	2,421
Total	4,710	196	0	4,906	4,722	120	0	4,842

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Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
3 Perris Boulevard/Auto Center Driveway - Project Driveway 1								
NBL	0	0	0	0	0	0	0	0
NBT	1,137	0	-19	1,118	1,105	0	-20	1,085
NBR	0	0	19	19	0	0	20	20
SBL	0	0	0	0	0	0	0	0
SBT	1,157	25	-21	1,161	1,151	15	-22	1,144
SBR	7	0	0	7	2	0	0	2
EBL	0	0	0	0	0	0	0	0
EBT	0	0	0	0	0	0	0	0
EBR	1	0	0	1	0	0	0	0
WBL	0	0	0	0	0	0	0	0
WBT	0	0	0	0	0	0	0	0
WBR	0	32	18	50	0	20	19	39
North Leg								
Approach	1,164	25	-21	1,168	1,153	15	-22	1,146
Departure	1,137	32	-1	1,168	1,105	20	-1	1,124
Total	2,301	57	-22	2,336	2,258	35	-23	2,270
South Leg								
Approach	1,137	0	0	1,137	1,105	0	0	1,105
Departure	1,158	25	-21	1,162	1,151	15	-22	1,144
Total	2,295	25	-21	2,299	2,256	15	-22	2,249
East Leg								
Approach	0	32	18	50	0	20	19	39
Departure	0	0	19	19	0	0	20	20
Total	0	32	37	69	0	20	39	59
West Leg								
Approach	1	0	0	1	0	0	0	0
Departure	7	0	0	7	2	0	0	2
Total	8	0	0	8	2	0	0	2
Total Approaches								
Approach	2,302	57	-3	2,356	2,258	35	-3	2,290
Departure	2,302	57	-3	2,356	2,258	35	-3	2,290
Total	4,604	114	-6	4,712	4,516	70	-6	4,580

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
4 Perris Boulevard/Dracaea Avenue								
NBL	50	0	-1	49	29	0	-1	28
NBT	1,006	0	0	1,006	988	0	0	988
NBR	20	50	1	71	36	31	1	68
SBL	33	25	-1	57	47	15	-1	61
SBT	1,039	0	-19	1,020	1,036	0	-20	1,016
SBR	86	0	-2	84	68	0	-1	67
EBL	64	0	0	64	69	0	0	69
EBT	71	33	1	105	102	21	0	123
EBR	52	0	-1	51	22	0	0	22
WBL	70	48	19	137	32	30	19	81
WBT	96	32	2	130	73	20	2	95
WBR	67	0	0	67	47	0	0	47
North Leg								
Approach	1,158	25	-22	1,161	1,151	15	-22	1,144
Departure	1,137	0	0	1,137	1,104	0	0	1,104
Total	2,295	25	-22	2,298	2,255	15	-22	2,248
South Leg								
Approach	1,076	50	0	1,126	1,053	31	0	1,084
Departure	1,161	48	-1	1,208	1,090	30	-1	1,119
Total	2,237	98	-1	2,334	2,143	61	-1	2,203
East Leg								
Approach	233	80	21	334	152	50	21	223
Departure	124	108	1	233	185	67	0	252
Total	357	188	22	567	337	117	21	475
West Leg								
Approach	187	33	0	220	193	21	0	214
Departure	232	32	-1	263	170	20	0	190
Total	419	65	-1	483	363	41	0	404
Total Approaches								
Approach	2,654	188	-1	2,841	2,549	117	-1	2,665
Departure	2,654	188	-1	2,841	2,549	117	-1	2,665
Total	5,308	376	-2	5,682	5,098	234	-2	5,330

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
5 Perris Boulevard/Cottonwood Avenue								
NBL	59	0	0	59	64	0	0	64
NBT	870	17	0	887	931	10	0	941
NBR	49	0	0	49	31	0	0	31
SBL	131	16	0	147	98	10	0	108
SBT	930	16	0	946	964	10	0	974
SBR	109	16	0	125	83	10	0	93
EBL	87	17	0	104	92	10	0	102
EBT	224	0	0	224	197	0	0	197
EBR	54	0	0	54	75	0	0	75
WBL	61	0	0	61	23	0	0	23
WBT	276	0	0	276	134	0	0	134
WBR	159	17	0	176	87	10	0	97
North Leg								
Approach	1,170	48	0	1,218	1,145	30	0	1,175
Departure	1,116	51	0	1,167	1,110	30	0	1,140
Total	2,286	99	0	2,385	2,255	60	0	2,315
South Leg								
Approach	978	17	0	995	1,026	10	0	1,036
Departure	1,045	16	0	1,061	1,062	10	0	1,072
Total	2,023	33	0	2,056	2,088	20	0	2,108
East Leg								
Approach	496	17	0	513	244	10	0	254
Departure	404	16	0	420	326	10	0	336
Total	900	33	0	933	570	20	0	590
West Leg								
Approach	365	17	0	382	364	10	0	374
Departure	444	16	0	460	281	10	0	291
Total	809	33	0	842	645	20	0	665
Total Approaches								
Approach	3,009	99	0	3,108	2,779	60	0	2,839
Departure	3,009	99	0	3,108	2,779	60	0	2,839
Total	6,018	198	0	6,216	5,558	120	0	5,678

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
6 Project Driveway 2/Dracaea Avenue								
NBL	0	0	0	0	0	0	0	0
NBT	0	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0	0
SBL	0	32	2	34	0	20	4	24
SBT	0	0	0	0	0	0	0	0
SBR	0	80	25	105	0	51	24	75
EBL	0	107	3	110	0	67	4	71
EBT	124	0	-2	122	185	0	-4	181
EBR	0	0	0	0	0	0	0	0
WBL	0	0	0	0	0	0	0	0
WBT	233	0	-4	229	152	0	-3	149
WBR	0	33	4	37	0	21	3	24
North Leg								
Approach	0	112	27	139	0	71	28	99
Departure	0	140	7	147	0	88	7	95
Total	0	252	34	286	0	159	35	194
South Leg								
Approach	0	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0
East Leg								
Approach	233	33	0	266	152	21	0	173
Departure	124	32	0	156	185	20	0	205
Total	357	65	0	422	337	41	0	378
West Leg								
Approach	124	107	1	232	185	67	0	252
Departure	233	80	21	334	152	51	21	224
Total	357	187	22	566	337	118	21	476
Total Approaches								
Approach	357	252	28	637	337	159	28	524
Departure	357	252	28	637	337	159	28	524
Total	714	504	56	1,274	674	318	56	1,048

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-1 - Existing Peak Hour PCE Volume Summary

	AM Peak Hour			PM Peak Hour				
	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project	Existing Without Project	Net Project Trips	Pass-by Trips	Existing With Project
7 Sunset Lane/Atwood Avenue								
NBL	2	16	0	18	1	10	0	11
NBT	0	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0	0
SBL	0	0	0	0	0	0	0	0
SBT	0	0	0	0	0	0	0	0
SBR	0	0	0	0	0	0	0	0
EBL	0	0	0	0	0	0	0	0
EBT	19	0	0	19	42	0	0	42
EBR	1	25	21	47	2	15	22	39
WBL	0	0	0	0	0	0	0	0
WBT	13	0	0	13	16	0	0	16
WBR	0	0	0	0	0	0	0	0
North Leg								
Approach	0	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0
South Leg								
Approach	2	16	0	18	1	10	0	11
Departure	1	25	21	47	2	15	22	39
Total	3	41	21	65	3	25	22	50
East Leg								
Approach	13	0	0	13	16	0	0	16
Departure	19	0	0	19	42	0	0	42
Total	32	0	0	32	58	0	0	58
West Leg								
Approach	20	25	21	66	44	15	22	81
Departure	15	16	0	31	17	10	0	27
Total	35	41	21	97	61	25	22	108
Total Approaches								
Approach	35	41	21	97	61	25	22	108
Departure	35	41	21	97	61	25	22	108
Total	70	82	42	194	122	50	44	216

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing (2019) PCE	2019- 2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
1 Perris Boulevard/Eucalyptus Avenue							
NBL	50	3	0	53	16	0	69
NBT	1,002	60	115	1,177	8	0	1,185
NBR	58	3	0	61	16	0	77
SBL	72	4	0	76	0	0	76
SBT	1,006	60	48	1,114	8	0	1,122
SBR	32	2	0	34	0	0	34
EBL	22	1	1	24	0	0	24
EBT	73	4	0	77	0	0	77
EBR	56	3	1	60	17	0	77
WBL	83	5	0	88	17	0	105
WBT	217	13	0	230	0	0	230
WBR	194	12	0	206	0	0	206
North Leg							
Approach	1,110	66	48	1,224	8	0	1,232
Departure	1,218	73	116	1,407	8	0	1,415
Total	2,328	139	164	2,631	16	0	2,647
South Leg							
Approach	1,110	66	115	1,291	40	0	1,331
Departure	1,145	68	49	1,262	42	0	1,304
Total	2,255	134	164	2,553	82	0	2,635
East Leg							
Approach	494	30	0	524	17	0	541
Departure	203	11	0	214	16	0	230
Total	697	41	0	738	33	0	771
West Leg							
Approach	151	8	2	161	17	0	178
Departure	299	18	0	317	16	0	333
Total	450	26	2	478	33	0	511
Total Approaches							
Approach	2,865	170	165	3,200	82	0	3,282
Departure	2,865	170	165	3,200	82	0	3,282
Total	5,730	340	330	6,400	164	0	6,564

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
2 Perris Boulevard/Atwood Avenue							
NBL	52	3	0	55	8	0	63
NBT	1,067	64	114	1,245	24	0	1,269
NBR	3	0	0	3	0	0	3
SBL	16	1	0	17	25	21	63
SBT	1,135	68	49	1,252	17	-21	1,248
SBR	16	1	0	17	0	0	17
EBL	13	1	1	15	0	0	15
EBT	1	0	0	1	0	0	1
EBR	37	2	1	40	8	0	48
WBL	4	0	0	4	0	0	4
WBT	2	0	0	2	0	0	2
WBR	9	1	0	10	16	0	26
North Leg							
Approach	1,167	70	49	1,286	42	0	1,328
Departure	1,089	66	115	1,270	40	0	1,310
Total	2,256	136	164	2,556	82	0	2,638
South Leg							
Approach	1,122	67	114	1,303	32	0	1,335
Departure	1,176	70	50	1,296	25	-21	1,300
Total	2,298	137	164	2,599	57	-21	2,635
East Leg							
Approach	15	1	0	16	16	0	32
Departure	20	1	0	21	25	21	67
Total	35	2	0	37	41	21	99
West Leg							
Approach	51	3	2	56	8	0	64
Departure	70	4	0	74	8	0	82
Total	121	7	2	130	16	0	146
Total Approaches							
Approach	2,355	141	165	2,661	98	0	2,759
Departure	2,355	141	165	2,661	98	0	2,759
Total	4,710	282	330	5,322	196	0	5,518

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
3 Perris Boulevard/Auto Center Driveway - Project Driveway 1							
NBL	0	0	0	0	0	0	0
NBT	1,137	68	114	1,319	0	-19	1,300
NBR	0	0	0	0	0	19	19
SBL	0	0	0	0	0	0	0
SBT	1,157	69	50	1,276	25	-21	1,280
SBR	7	0	0	7	0	0	7
EBL	0	0	0	0	0	0	0
EBT	0	0	0	0	0	0	0
EBR	1	0	0	1	0	0	1
WBL	0	0	0	0	0	0	0
WBT	0	0	0	0	0	0	0
WBR	0	0	0	0	32	18	50
North Leg							
Approach	1,164	69	50	1,283	25	-21	1,287
Departure	1,137	68	114	1,319	32	-1	1,350
Total	2,301	137	164	2,602	57	-22	2,637
South Leg							
Approach	1,137	68	114	1,319	0	0	1,319
Departure	1,158	69	50	1,277	25	-21	1,281
Total	2,295	137	164	2,596	25	-21	2,600
East Leg							
Approach	0	0	0	0	32	18	50
Departure	0	0	0	0	0	19	19
Total	0	0	0	0	32	37	69
West Leg							
Approach	1	0	0	1	0	0	1
Departure	7	0	0	7	0	0	7
Total	8	0	0	8	0	0	8
Total Approaches							
Approach	2,302	137	164	2,603	57	-3	2,657
Departure	2,302	137	164	2,603	57	-3	2,657
Total	4,604	274	328	5,206	114	-6	5,314

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
4 Perris Boulevard/Dracaea Avenue							
NBL	50	3	0	53	0	-1	52
NBT	1,006	60	113	1,179	0	0	1,179
NBR	20	1	0	21	50	1	72
SBL	33	2	0	35	25	-1	59
SBT	1,039	62	50	1,151	0	-19	1,132
SBR	86	5	0	91	0	-2	89
EBL	64	4	1	69	0	0	69
EBT	71	4	0	75	33	1	109
EBR	52	3	0	55	0	-1	54
WBL	70	4	0	74	48	19	141
WBT	96	6	0	102	32	2	136
WBR	67	4	0	71	0	0	71
North Leg							
Approach	1,158	69	50	1,277	25	-22	1,280
Departure	1,137	68	114	1,319	0	0	1,319
Total	2,295	137	164	2,596	25	-22	2,599
South Leg							
Approach	1,076	64	113	1,253	50	0	1,303
Departure	1,161	69	50	1,280	48	-1	1,327
Total	2,237	133	163	2,533	98	-1	2,630
East Leg							
Approach	233	14	0	247	80	21	348
Departure	124	7	0	131	108	1	240
Total	357	21	0	378	188	22	588
West Leg							
Approach	187	11	1	199	33	0	232
Departure	232	14	0	246	32	-1	277
Total	419	25	1	445	65	-1	509
Total Approaches							
Approach	2,654	158	164	2,976	188	-1	3,163
Departure	2,654	158	164	2,976	188	-1	3,163
Total	5,308	316	328	5,952	376	-2	6,326

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
5 Perris Boulevard/Cottonwood Avenue							
NBL	59	4	0	63	0	0	63
NBT	870	52	113	1,035	17	0	1,052
NBR	49	3	0	52	0	0	52
SBL	131	8	0	139	16	0	155
SBT	930	56	50	1,036	16	0	1,052
SBR	109	7	0	116	16	0	132
EBL	87	5	0	92	17	0	109
EBT	224	13	0	237	0	0	237
EBR	54	3	0	57	0	0	57
WBL	61	4	0	65	0	0	65
WBT	276	17	0	293	0	0	293
WBR	159	10	0	169	17	0	186
North Leg							
Approach	1,170	71	50	1,291	48	0	1,339
Departure	1,116	67	113	1,296	51	0	1,347
Total	2,286	138	163	2,587	99	0	2,686
South Leg							
Approach	978	59	113	1,150	17	0	1,167
Departure	1,045	63	50	1,158	16	0	1,174
Total	2,023	122	163	2,308	33	0	2,341
East Leg							
Approach	496	31	0	527	17	0	544
Departure	404	24	0	428	16	0	444
Total	900	55	0	955	33	0	988
West Leg							
Approach	365	21	0	386	17	0	403
Departure	444	28	0	472	16	0	488
Total	809	49	0	858	33	0	891
Total Approaches							
Approach	3,009	182	163	3,354	99	0	3,453
Departure	3,009	182	163	3,354	99	0	3,453
Total	6,018	364	326	6,708	198	0	6,906

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing	2019-	Cumulative	Proj Comp	Net	Pass-by	Proj Comp
	(2019)	2022	Project	Without	Project		With
PCE	Growth	Trips	Project	Project	Trips	Trips	Project
6 Project Driveway 2/Dracaea Avenue							
NBL	0	0	0	0	0	0	0
NBT	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0
SBL	0	0	0	0	32	2	34
SBT	0	0	0	0	0	0	0
SBR	0	0	0	0	80	25	105
EBL	0	0	0	0	107	3	110
EBT	124	7	0	131	0	-2	129
EBR	0	0	0	0	0	0	0
WBL	0	0	0	0	0	0	0
WBT	233	14	0	247	0	-4	243
WBR	0	0	0	0	33	4	37
North Leg							
Approach	0	0	0	0	112	27	139
Departure	0	0	0	0	140	7	147
Total	0	0	0	0	252	34	286
South Leg							
Approach	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0
East Leg							
Approach	233	14	0	247	33	0	280
Departure	124	7	0	131	32	0	163
Total	357	21	0	378	65	0	443
West Leg							
Approach	124	7	0	131	107	1	239
Departure	233	14	0	247	80	21	348
Total	357	21	0	378	187	22	587
Total Approaches							
Approach	357	21	0	378	252	28	658
Departure	357	21	0	378	252	28	658
Total	714	42	0	756	504	56	1,316

Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	AM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
7 Sunset Lane/Atwood Avenue							
NBL	2	0	0	2	16	0	18
NBT	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0
SBL	0	0	0	0	0	0	0
SBT	0	0	0	0	0	0	0
SBR	0	0	0	0	0	0	0
EBL	0	0	0	0	0	0	0
EBT	19	1	0	20	0	0	20
EBR	1	0	0	1	25	21	47
WBL	0	0	0	0	0	0	0
WBT	13	1	0	14	0	0	14
WBR	0	0	0	0	0	0	0
North Leg							
Approach	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0
South Leg							
Approach	2	0	0	2	16	0	18
Departure	1	0	0	1	25	21	47
Total	3	0	0	3	41	21	65
East Leg							
Approach	13	1	0	14	0	0	14
Departure	19	1	0	20	0	0	20
Total	32	2	0	34	0	0	34
West Leg							
Approach	20	1	0	21	25	21	67
Departure	15	1	0	16	16	0	32
Total	35	2	0	37	41	21	99
Total Approaches							
Approach	35	2	0	37	41	21	99
Departure	35	2	0	37	41	21	99
Total	70	4	0	74	82	42	198

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019- 2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
1 Perris Boulevard/Eucalyptus Avenue							
NBL	51	3	1	55	10	0	65
NBT	978	59	86	1,123	5	0	1,128
NBR	59	4	0	63	10	0	73
SBL	97	6	0	103	0	0	103
SBT	1,063	64	123	1,250	5	0	1,255
SBR	45	3	1	49	0	0	49
EBL	51	3	0	54	0	0	54
EBT	189	11	0	200	0	0	200
EBR	79	5	0	84	10	0	94
WBL	52	3	0	55	10	0	65
WBT	127	8	0	135	0	0	135
WBR	108	6	0	114	0	0	114
North Leg							
Approach	1,205	73	124	1,402	5	0	1,407
Departure	1,137	68	86	1,291	5	0	1,296
Total	2,342	141	210	2,693	10	0	2,703
South Leg							
Approach	1,088	66	87	1,241	25	0	1,266
Departure	1,194	72	123	1,389	25	0	1,414
Total	2,282	138	210	2,630	50	0	2,680
East Leg							
Approach	287	17	0	304	10	0	314
Departure	345	21	0	366	10	0	376
Total	632	38	0	670	20	0	690
West Leg							
Approach	319	19	0	338	10	0	348
Departure	223	14	2	239	10	0	249
Total	542	33	2	577	20	0	597
Total Approaches							
Approach	2,899	175	211	3,285	50	0	3,335
Departure	2,899	175	211	3,285	50	0	3,335
Total	5,798	350	422	6,570	100	0	6,670

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
2 Perris Boulevard/Atwood Avenue							
NBL	43	3	1	47	5	0	52
NBT	1,059	64	87	1,210	15	0	1,225
NBR	1	0	0	1	0	0	1
SBL	42	3	0	45	15	22	82
SBT	1,120	67	122	1,309	10	-22	1,297
SBR	15	1	1	17	0	0	17
EBL	27	2	0	29	0	0	29
EBT	1	0	0	1	0	0	1
EBR	36	2	0	38	5	0	43
WBL	3	0	0	3	0	0	3
WBT	1	0	0	1	0	0	1
WBR	13	1	0	14	10	0	24
North Leg							
Approach	1,177	71	123	1,371	25	0	1,396
Departure	1,099	67	87	1,253	25	0	1,278
Total	2,276	138	210	2,624	50	0	2,674
South Leg							
Approach	1,103	67	88	1,258	20	0	1,278
Departure	1,159	69	122	1,350	15	-22	1,343
Total	2,262	136	210	2,608	35	-22	2,621
East Leg							
Approach	17	1	0	18	10	0	28
Departure	44	3	0	47	15	22	84
Total	61	4	0	65	25	22	112
West Leg							
Approach	64	4	0	68	5	0	73
Departure	59	4	2	65	5	0	70
Total	123	8	2	133	10	0	143
Total Approaches							
Approach	2,361	143	211	2,715	60	0	2,775
Departure	2,361	143	211	2,715	60	0	2,775
Total	4,722	286	422	5,430	120	0	5,550

Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
3 Perris Boulevard/Auto Center Driveway - Project Driveway 1							
NBL	0	0	0	0	0	0	0
NBT	1,105	66	88	1,259	0	-20	1,239
NBR	0	0	0	0	0	20	20
SBL	0	0	0	0	0	0	0
SBT	1,151	69	122	1,342	15	-22	1,335
SBR	2	0	0	2	0	0	2
EBL	0	0	0	0	0	0	0
EBT	0	0	0	0	0	0	0
EBR	0	0	0	0	0	0	0
WBL	0	0	0	0	0	0	0
WBT	0	0	0	0	0	0	0
WBR	0	0	0	0	20	19	39
North Leg							
Approach	1,153	69	122	1,344	15	-22	1,337
Departure	1,105	66	88	1,259	20	-1	1,278
Total	2,258	135	210	2,603	35	-23	2,615
South Leg							
Approach	1,105	66	88	1,259	0	0	1,259
Departure	1,151	69	122	1,342	15	-22	1,335
Total	2,256	135	210	2,601	15	-22	2,594
East Leg							
Approach	0	0	0	0	20	19	39
Departure	0	0	0	0	0	20	20
Total	0	0	0	0	20	39	59
West Leg							
Approach	0	0	0	0	0	0	0
Departure	2	0	0	2	0	0	2
Total	2	0	0	2	0	0	2
Total Approaches							
Approach	2,258	135	210	2,603	35	-3	2,635
Departure	2,258	135	210	2,603	35	-3	2,635
Total	4,516	270	420	5,206	70	-6	5,270

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
4 Perris Boulevard/Dracaea Avenue							
NBL	29	2	0	31	0	-1	30
NBT	988	59	87	1,134	0	0	1,134
NBR	36	2	0	38	31	1	70
SBL	47	3	0	50	15	-1	64
SBT	1,036	62	121	1,219	0	-20	1,199
SBR	68	4	1	73	0	-1	72
EBL	69	4	1	74	0	0	74
EBT	102	6	0	108	21	0	129
EBR	22	1	0	23	0	0	23
WBL	32	2	0	34	30	19	83
WBT	73	4	0	77	20	2	99
WBR	47	3	0	50	0	0	50
North Leg							
Approach	1,151	69	122	1,342	15	-22	1,335
Departure	1,104	66	88	1,258	0	0	1,258
Total	2,255	135	210	2,600	15	-22	2,593
South Leg							
Approach	1,053	63	87	1,203	31	0	1,234
Departure	1,090	65	121	1,276	30	-1	1,305
Total	2,143	128	208	2,479	61	-1	2,539
East Leg							
Approach	152	9	0	161	50	21	232
Departure	185	11	0	196	67	0	263
Total	337	20	0	357	117	21	495
West Leg							
Approach	193	11	1	205	21	0	226
Departure	170	10	1	181	20	0	201
Total	363	21	2	386	41	0	427
Total Approaches							
Approach	2,549	152	210	2,911	117	-1	3,027
Departure	2,549	152	210	2,911	117	-1	3,027
Total	5,098	304	420	5,822	234	-2	6,054

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
5 Perris Boulevard/Cottonwood Avenue							
NBL	64	4	0	68	0	0	68
NBT	931	56	87	1,074	10	0	1,084
NBR	31	2	0	33	0	0	33
SBL	98	6	0	104	10	0	114
SBT	964	58	121	1,143	10	0	1,153
SBR	83	5	0	88	10	0	98
EBL	92	6	0	98	10	0	108
EBT	197	12	0	209	0	0	209
EBR	75	5	0	80	0	0	80
WBL	23	1	0	24	0	0	24
WBT	134	8	0	142	0	0	142
WBR	87	5	0	92	10	0	102
North Leg							
Approach	1,145	69	121	1,335	30	0	1,365
Departure	1,110	67	87	1,264	30	0	1,294
Total	2,255	136	208	2,599	60	0	2,659
South Leg							
Approach	1,026	62	87	1,175	10	0	1,185
Departure	1,062	64	121	1,247	10	0	1,257
Total	2,088	126	208	2,422	20	0	2,442
East Leg							
Approach	244	14	0	258	10	0	268
Departure	326	20	0	346	10	0	356
Total	570	34	0	604	20	0	624
West Leg							
Approach	364	23	0	387	10	0	397
Departure	281	17	0	298	10	0	308
Total	645	40	0	685	20	0	705
Total Approaches							
Approach	2,779	168	208	3,155	60	0	3,215
Departure	2,779	168	208	3,155	60	0	3,215
Total	5,558	336	416	6,310	120	0	6,430

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Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019- 2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
6 Project Driveway 2/Dracaea Avenue							
NBL	0	0	0	0	0	0	0
NBT	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0
SBL	0	0	0	0	20	4	24
SBT	0	0	0	0	0	0	0
SBR	0	0	0	0	51	24	75
EBL	0	0	0	0	67	4	71
EBT	185	11	0	196	0	-4	192
EBR	0	0	0	0	0	0	0
WBL	0	0	0	0	0	0	0
WBT	152	9	0	161	0	-3	158
WBR	0	0	0	0	21	3	24
North Leg							
Approach	0	0	0	0	71	28	99
Departure	0	0	0	0	88	7	95
Total	0	0	0	0	159	35	194
South Leg							
Approach	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0
East Leg							
Approach	152	9	0	161	21	0	182
Departure	185	11	0	196	20	0	216
Total	337	20	0	357	41	0	398
West Leg							
Approach	185	11	0	196	67	0	263
Departure	152	9	0	161	51	21	233
Total	337	20	0	357	118	21	496
Total Approaches							
Approach	337	20	0	357	159	28	544
Departure	337	20	0	357	159	28	544
Total	674	40	0	714	318	56	1,088

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-2 - Project Completion (2022) Peak Hour PCE Volume Summary

	PM Peak Hour						
	Existing (2019) PCE	2019-2022 Growth	Cumulative Project Trips	Proj Comp Without Project	Net Project Trips	Pass-by Trips	Proj Comp With Project
7 Sunset Lane/Atwood Avenue							
NBL	1	0	0	1	10	0	11
NBT	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0
SBL	0	0	0	0	0	0	0
SBT	0	0	0	0	0	0	0
SBR	0	0	0	0	0	0	0
EBL	0	0	0	0	0	0	0
EBT	42	3	0	45	0	0	45
EBR	2	0	0	2	15	22	39
WBL	0	0	0	0	0	0	0
WBT	16	1	0	17	0	0	17
WBR	0	0	0	0	0	0	0
North Leg							
Approach	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0
South Leg							
Approach	1	0	0	1	10	0	11
Departure	2	0	0	2	15	22	39
Total	3	0	0	3	25	22	50
East Leg							
Approach	16	1	0	17	0	0	17
Departure	42	3	0	45	0	0	45
Total	58	4	0	62	0	0	62
West Leg							
Approach	44	3	0	47	15	22	84
Departure	17	1	0	18	10	0	28
Total	61	4	0	65	25	22	112
Total Approaches							
Approach	61	4	0	65	25	22	112
Departure	61	4	0	65	25	22	112
Total	122	8	0	130	50	44	224

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by Trips	GPBO (2040)	GPBO (2040)	Net	Pass-by Trips	GPBO (2040)
	Without Project	Project Trips		With Project	Without Project	Project Trips		With Project
1 Perris Boulevard/Eucalyptus Avenue								
NBL	129	16	0	145	147	10	0	157
NBT	1,274	8	0	1,282	1,551	5	0	1,556
NBR	65	16	0	81	88	10	0	98
SBL	90	0	0	90	111	0	0	111
SBT	1,438	8	0	1,446	1,490	5	0	1,495
SBR	92	0	0	92	100	0	0	100
EBL	58	0	0	58	149	0	0	149
EBT	170	0	0	170	516	0	0	516
EBR	150	17	0	167	265	10	0	275
WBL	92	17	0	109	78	10	0	88
WBT	401	0	0	401	301	0	0	301
WBR	216	0	0	216	140	0	0	140
North Leg								
Approach	1,620	8	0	1,628	1,701	5	0	1,706
Departure	1,548	8	0	1,556	1,840	5	0	1,845
Total	3,168	16	0	3,184	3,541	10	0	3,551
South Leg								
Approach	1,468	40	0	1,508	1,786	25	0	1,811
Departure	1,680	42	0	1,722	1,833	25	0	1,858
Total	3,148	82	0	3,230	3,619	50	0	3,669
East Leg								
Approach	709	17	0	726	519	10	0	529
Departure	325	16	0	341	715	10	0	725
Total	1,034	33	0	1,067	1,234	20	0	1,254
West Leg								
Approach	378	17	0	395	930	10	0	940
Departure	622	16	0	638	548	10	0	558
Total	1,000	33	0	1,033	1,478	20	0	1,498
Total Approaches								
Approach	4,175	82	0	4,257	4,936	50	0	4,986
Departure	4,175	82	0	4,257	4,936	50	0	4,986
Total	8,350	164	0	8,514	9,872	100	0	9,972

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by Trips	GPBO (2040)	GPBO (2040)	Net	Pass-by Trips	GPBO (2040)
	Without Project	Project Trips		With Project	Without Project	Project Trips		With Project
2 Perris Boulevard/Atwood Avenue								
NBL	61	8	0	69	74	5	0	79
NBT	1,445	24	0	1,469	1,855	15	0	1,870
NBR	19	0	0	19	12	0	0	12
SBL	56	25	21	102	153	15	22	190
SBT	1,654	17	-21	1,650	1,776	10	-22	1,764
SBR	18	0	0	18	18	0	0	18
EBL	16	0	0	16	30	0	0	30
EBT	3	0	0	3	3	0	0	3
EBR	43	8	0	51	51	5	0	56
WBL	50	0	0	50	40	0	0	40
WBT	11	0	0	11	4	0	0	4
WBR	59	16	0	75	58	10	0	68
North Leg								
Approach	1,728	42	0	1,770	1,947	25	0	1,972
Departure	1,520	40	0	1,560	1,943	25	0	1,968
Total	3,248	82	0	3,330	3,890	50	0	3,940
South Leg								
Approach	1,525	32	0	1,557	1,941	20	0	1,961
Departure	1,747	25	-21	1,751	1,867	15	-22	1,860
Total	3,272	57	-21	3,308	3,808	35	-22	3,821
East Leg								
Approach	120	16	0	136	102	10	0	112
Departure	78	25	21	124	168	15	22	205
Total	198	41	21	260	270	25	22	317
West Leg								
Approach	62	8	0	70	84	5	0	89
Departure	90	8	0	98	96	5	0	101
Total	152	16	0	168	180	10	0	190
Total Approaches								
Approach	3,435	98	0	3,533	4,074	60	0	4,134
Departure	3,435	98	0	3,533	4,074	60	0	4,134
Total	6,870	196	0	7,066	8,148	120	0	8,268

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by	GPBO (2040)	GPBO (2040)	Net	Pass-by	GPBO (2040)
	Without Project	Project Trips	Trips	With Project	Without Project	Project Trips	Trips	With Project
3 Perris Boulevard/Auto Center Driveway - Project Driveway 1								
NBL	0	0	0	0	0	0	0	0
NBT	1,525	0	-19	1,506	1,941	0	-20	1,921
NBR	0	0	19	19	0	0	20	20
SBL	0	0	0	0	0	0	0	0
SBT	1,747	25	-21	1,751	1,867	15	-22	1,860
SBR	7	0	0	7	2	0	0	2
EBL	0	0	0	0	0	0	0	0
EBT	0	0	0	0	0	0	0	0
EBR	1	0	0	1	0	0	0	0
WBL	0	0	0	0	0	0	0	0
WBT	0	0	0	0	0	0	0	0
WBR	0	32	18	50	0	20	19	39
North Leg								
Approach	1,754	25	-21	1,758	1,869	15	-22	1,862
Departure	1,525	32	-1	1,556	1,941	20	-1	1,960
Total	3,279	57	-22	3,314	3,810	35	-23	3,822
South Leg								
Approach	1,525	0	0	1,525	1,941	0	0	1,941
Departure	1,748	25	-21	1,752	1,867	15	-22	1,860
Total	3,273	25	-21	3,277	3,808	15	-22	3,801
East Leg								
Approach	0	32	18	50	0	20	19	39
Departure	0	0	19	19	0	0	20	20
Total	0	32	37	69	0	20	39	59
West Leg								
Approach	1	0	0	1	0	0	0	0
Departure	7	0	0	7	2	0	0	2
Total	8	0	0	8	2	0	0	2
Total Approaches								
Approach	3,280	57	-3	3,334	3,810	35	-3	3,842
Departure	3,280	57	-3	3,334	3,810	35	-3	3,842
Total	6,560	114	-6	6,668	7,620	70	-6	7,684

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Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by	GPBO (2040)	GPBO (2040)	Net	Pass-by	GPBO (2040)
	Without Project	Project Trips	Trips	With Project	Without Project	Project Trips	Trips	With Project
4 Perris Boulevard/Draacea Avenue								
NBL	75	0	-1	74	50	0	-1	49
NBT	1,342	0	0	1,342	1,611	0	0	1,611
NBR	22	50	1	73	53	31	1	85
SBL	43	25	-1	67	70	15	-1	84
SBT	1,527	0	-19	1,508	1,679	0	-20	1,659
SBR	177	0	-2	175	118	0	-1	117
EBL	92	0	0	92	258	0	0	258
EBT	79	33	1	113	344	21	0	365
EBR	60	0	-1	59	81	0	0	81
WBL	77	48	19	144	48	30	19	97
WBT	147	32	2	181	118	20	2	140
WBR	91	0	0	91	72	0	0	72
North Leg								
Approach	1,747	25	-22	1,750	1,867	15	-22	1,860
Departure	1,525	0	0	1,525	1,941	0	0	1,941
Total	3,272	25	-22	3,275	3,808	15	-22	3,801
South Leg								
Approach	1,439	50	0	1,489	1,714	31	0	1,745
Departure	1,664	48	-1	1,711	1,808	30	-1	1,837
Total	3,103	98	-1	3,200	3,522	61	-1	3,582
East Leg								
Approach	315	80	21	416	238	50	21	309
Departure	144	108	1	253	467	67	0	534
Total	459	188	22	669	705	117	21	843
West Leg								
Approach	231	33	0	264	683	21	0	704
Departure	399	32	-1	430	286	20	0	306
Total	630	65	-1	694	969	41	0	1,010
Total Approaches								
Approach	3,732	188	-1	3,919	4,502	117	-1	4,618
Departure	3,732	188	-1	3,919	4,502	117	-1	4,618
Total	7,464	376	-2	7,838	9,004	234	-2	9,236

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by	GPBO (2040)	GPBO (2040)	Net	Pass-by	GPBO (2040)
	Without Project	Project Trips	Trips	With Project	Without Project	Project Trips	Trips	With Project
5 Perris Boulevard/Cottonwood Avenue								
NBL	96	0	0	96	80	0	0	80
NBT	1,213	17	0	1,230	1,408	10	0	1,418
NBR	78	0	0	78	43	0	0	43
SBL	162	16	0	178	165	10	0	175
SBT	1,383	16	0	1,399	1,567	10	0	1,577
SBR	139	16	0	155	126	10	0	136
EBL	97	17	0	114	190	10	0	200
EBT	239	0	0	239	374	0	0	374
EBR	69	0	0	69	137	0	0	137
WBL	90	0	0	90	41	0	0	41
WBT	349	0	0	349	221	0	0	221
WBR	172	17	0	189	173	10	0	183
North Leg								
Approach	1,684	48	0	1,732	1,858	30	0	1,888
Departure	1,482	51	0	1,533	1,771	30	0	1,801
Total	3,166	99	0	3,265	3,629	60	0	3,689
South Leg								
Approach	1,387	17	0	1,404	1,531	10	0	1,541
Departure	1,542	16	0	1,558	1,745	10	0	1,755
Total	2,929	33	0	2,962	3,276	20	0	3,296
East Leg								
Approach	611	17	0	628	435	10	0	445
Departure	479	16	0	495	582	10	0	592
Total	1,090	33	0	1,123	1,017	20	0	1,037
West Leg								
Approach	405	17	0	422	701	10	0	711
Departure	584	16	0	600	427	10	0	437
Total	989	33	0	1,022	1,128	20	0	1,148
Total Approaches								
Approach	4,087	99	0	4,186	4,525	60	0	4,585
Departure	4,087	99	0	4,186	4,525	60	0	4,585
Total	8,174	198	0	8,372	9,050	120	0	9,170

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Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by Trips	GPBO (2040)	GPBO (2040)	Net	Pass-by Trips	GPBO (2040)
	Without Project	Project Trips		With Project	Without Project	Project Trips		With Project
6 Project Driveway 2/Dracaea Avenue								
NBL	0	0	0	0	0	0	0	0
NBT	0	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0	0
SBL	0	32	2	34	0	20	4	24
SBT	0	0	0	0	0	0	0	0
SBR	0	80	25	105	0	51	24	75
EBL	0	107	3	110	0	67	4	71
EBT	144	0	-2	142	467	0	-4	463
EBR	0	0	0	0	0	0	0	0
WBL	0	0	0	0	0	0	0	0
WBT	315	0	-4	311	238	0	-3	235
WBR	0	33	4	37	0	21	3	24
North Leg								
Approach	0	112	27	139	0	71	28	99
Departure	0	140	7	147	0	88	7	95
Total	0	252	34	286	0	159	35	194
South Leg								
Approach	0	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0
East Leg								
Approach	315	33	0	348	238	21	0	259
Departure	144	32	0	176	467	20	0	487
Total	459	65	0	524	705	41	0	746
West Leg								
Approach	144	107	1	252	467	67	0	534
Departure	315	80	21	416	238	51	21	310
Total	459	187	22	668	705	118	21	844
Total Approaches								
Approach	459	252	28	739	705	159	28	892
Departure	459	252	28	739	705	159	28	892
Total	918	504	56	1,478	1,410	318	56	1,784

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Table C-3 - General Plan Build-Out (2040) Peak Hour Volume Summary

	AM Peak Hour				PM Peak Hour			
	GPBO (2040)	Net	Pass-by	GPBO (2040)	GPBO (2040)	Net	Pass-by	GPBO (2040)
	Without Project	Project Trips	Trips	With Project	Without Project	Project Trips	Trips	With Project
7 Sunset Lane/Atwood Avenue								
NBL	2	16	0	18	1	10	0	11
NBT	0	0	0	0	0	0	0	0
NBR	0	0	0	0	0	0	0	0
SBL	0	0	0	0	0	0	0	0
SBT	0	0	0	0	0	0	0	0
SBR	0	0	0	0	0	0	0	0
EBL	0	0	0	0	0	0	0	0
EBT	76	0	0	76	165	0	0	165
EBR	1	25	21	47	2	15	22	39
WBL	0	0	0	0	0	0	0	0
WBT	119	0	0	119	101	0	0	101
WBR	0	0	0	0	0	0	0	0
North Leg								
Approach	0	0	0	0	0	0	0	0
Departure	0	0	0	0	0	0	0	0
Total	0	0	0	0	0	0	0	0
South Leg								
Approach	2	16	0	18	1	10	0	11
Departure	1	25	21	47	2	15	22	39
Total	3	41	21	65	3	25	22	50
East Leg								
Approach	119	0	0	119	101	0	0	101
Departure	76	0	0	76	165	0	0	165
Total	195	0	0	195	266	0	0	266
West Leg								
Approach	77	25	21	123	167	15	22	204
Departure	121	16	0	137	102	10	0	112
Total	198	41	21	260	269	25	22	316
Total Approaches								
Approach	198	41	21	260	269	25	22	316
Departure	198	41	21	260	269	25	22	316
Total	396	82	42	520	538	50	44	632

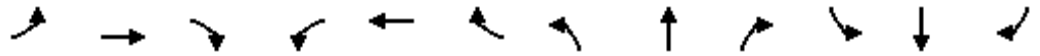
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APPENDIX D:

LEVEL OF SERVICE WORKSHEETS

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	22	73	56	83	217	194	50	1002	58	72	1006	32
Future Volume (veh/h)	22	73	56	83	217	194	50	1002	58	72	1006	32
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.99	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	25	84	64	95	249	223	57	1152	67	83	1156	37
Peak Hour Factor	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	67	175	145	205	319	270	114	1421	631	364	1920	855
Arrive On Green	0.04	0.09	0.09	0.11	0.17	0.17	0.13	0.79	0.79	0.20	0.53	0.53
Sat Flow, veh/h	1810	1900	1577	1810	1900	1607	1810	3610	1602	1810	3610	1607
Grp Volume(v), veh/h	25	84	64	95	249	223	57	1152	67	83	1156	37
Grp Sat Flow(s),veh/h/ln	1810	1900	1577	1810	1900	1607	1810	1805	1602	1810	1805	1607
Q Serve(g_s), s	1.1	3.4	2.5	3.9	10.0	6.8	2.4	15.0	0.8	3.1	17.6	0.9
Cycle Q Clear(g_c), s	1.1	3.4	2.5	3.9	10.0	6.8	2.4	15.0	0.8	3.1	17.6	0.9
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	67	175	145	205	319	270	114	1421	631	364	1920	855
V/C Ratio(X)	0.37	0.48	0.44	0.46	0.78	0.83	0.50	0.81	0.11	0.23	0.60	0.04
Avail Cap(c_a), veh/h	158	439	365	205	439	372	158	1421	631	364	1920	855
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.6	34.5	23.1	33.2	31.9	13.1	33.8	6.7	5.2	26.8	12.9	9.0
Incr Delay (d2), s/veh	3.4	2.0	2.1	1.6	6.1	10.4	3.4	5.1	0.3	0.3	1.4	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.5	1.6	1.2	1.8	5.0	4.6	1.1	3.4	0.3	1.3	6.8	0.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	40.9	36.5	25.2	34.8	37.9	23.5	37.2	11.9	5.6	27.1	14.3	9.1
LnGrp LOS	D	D	C	C	D	C	D	B	A	C	B	A
Approach Vol, veh/h		173			567			1276			1276	
Approach Delay, s/veh		33.0			31.8			12.7			15.0	
Approach LOS		C			C			B			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	20.1	35.5	13.0	11.4	9.0	46.6	7.0	17.4				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	31.5	7.0	18.5	7.0	31.5	7.0	18.5				
Max Q Clear Time (g_c+I1), s	5.1	17.0	5.9	5.4	4.4	19.6	3.1	12.0				
Green Ext Time (p_c), s	0.0	7.4	0.0	0.5	0.0	6.4	0.0	1.2				

Intersection Summary

HCM 6th Ctrl Delay	17.9
HCM 6th LOS	B

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour

Intersection												
Int Delay, s/veh	6.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑	↕	↕	↑↑	↕
Traffic Vol, veh/h	13	1	37	4	2	9	52	1067	3	16	1135	16
Future Vol, veh/h	13	1	37	4	2	9	52	1067	3	16	1135	16
Conflicting Peds, #/hr	2	0	0	0	0	2	7	0	0	0	0	7
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	50	50	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	83	83	83	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	16	1	45	5	2	11	63	1286	4	19	1367	19

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2184	2828	691	2134	2843	645	1393	0	0	1290	0	0
Stage 1	1412	1412	-	1412	1412	-	-	-	-	-	-	-
Stage 2	772	1416	-	722	1431	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	26	18	392	29	18	420	497	-	-	544	-	-
Stage 1	148	206	-	148	206	-	-	-	-	-	-	-
Stage 2	363	205	-	389	202	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	19	15	389	21	15	419	494	-	-	544	-	-
Mov Cap-2 Maneuver	19	15	-	21	15	-	-	-	-	-	-	-
Stage 1	128	197	-	129	180	-	-	-	-	-	-	-
Stage 2	304	179	-	330	194	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	236.8		139.5		0.6		0.2	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	494	-	-	60	43	544	-
HCM Lane V/C Ratio	0.127	-	-	1.024	0.42	0.035	-
HCM Control Delay (s)	13.3	-	-	236.8	139.5	11.9	-
HCM Lane LOS	B	-	-	F	F	B	-
HCM 95th %tile Q(veh)	0.4	-	-	4.9	1.5	0.1	-

HCM 6th TWSC
3: Perris Boulevard & Auto Center Driveway

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T T T		
Traffic Vol, veh/h	0	1	0	1137	1157	7
Future Vol, veh/h	0	1	0	1137	1157	7
Conflicting Peds, #/hr	0	0	11	0	0	11
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	25
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	1	0	1370	1394	8

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	2094	712	1413	0	0
Stage 1	1409	-	-	-	-
Stage 2	685	-	-	-	-
Critical Hdwy	6.25	7.1	5.3	-	-
Critical Hdwy Stg 1	6.6	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-
Follow-up Hdwy	3.65	3.9	3.1	-	-
Pot Cap-1 Maneuver	*254	*646	737	-	-
Stage 1	*613	-	-	-	-
Stage 2	*548	-	-	-	-
Platoon blocked, %	1	1	1	-	-
Mov Cap-1 Maneuver	*249	*639	729	-	-
Mov Cap-2 Maneuver	*375	-	-	-	-
Stage 1	*607	-	-	-	-
Stage 2	*543	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.6	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	729	-	639	-	-
HCM Lane V/C Ratio	-	-	0.002	-	-
HCM Control Delay (s)	0	-	10.6	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↖	↗		↔		↖	↕		↗	↕	↗
Traffic Volume (veh/h)	64	71	52	70	96	67	50	1006	20	33	1039	86
Future Volume (veh/h)	64	71	52	70	96	67	50	1006	20	33	1039	86
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		0.99	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	76	85	62	83	114	80	60	1198	24	39	1237	102
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	189	191	411	129	158	93	328	1493	30	328	1489	642
Arrive On Green	0.26	0.26	0.26	0.26	0.26	0.26	0.18	0.41	0.41	0.18	0.41	0.41
Sat Flow, veh/h	480	744	1603	277	617	363	1810	3618	72	1810	3610	1557
Grp Volume(v), veh/h	161	0	62	277	0	0	60	597	625	39	1237	102
Grp Sat Flow(s),veh/h/ln	1224	0	1603	1256	0	0	1810	1805	1886	1810	1805	1557
Q Serve(g_s), s	0.0	0.0	2.4	9.3	0.0	0.0	2.2	23.3	23.3	1.4	24.5	3.3
Cycle Q Clear(g_c), s	8.4	0.0	2.4	17.7	0.0	0.0	2.2	23.3	23.3	1.4	24.5	3.3
Prop In Lane	0.47		1.00	0.30		0.29	1.00		0.04	1.00		1.00
Lane Grp Cap(c), veh/h	380	0	411	381	0	0	328	745	778	328	1489	642
V/C Ratio(X)	0.42	0.00	0.15	0.73	0.00	0.00	0.18	0.80	0.80	0.12	0.83	0.16
Avail Cap(c_a), veh/h	475	0	513	480	0	0	328	745	778	328	1489	642
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.74	0.74	0.74	1.00	1.00	1.00
Uniform Delay (d), s/veh	24.8	0.0	23.0	29.3	0.0	0.0	27.7	20.6	20.6	27.4	21.0	14.8
Incr Delay (d2), s/veh	0.7	0.0	0.2	4.1	0.0	0.0	0.2	6.8	6.5	0.2	5.5	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.6	0.0	0.9	5.4	0.0	0.0	1.0	10.5	10.9	0.6	10.7	1.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	25.6	0.0	23.2	33.4	0.0	0.0	27.9	27.4	27.1	27.6	26.5	15.3
LnGrp LOS	C	A	C	C	A	A	C	C	C	C	C	B
Approach Vol, veh/h		223			277			1282			1378	
Approach Delay, s/veh		24.9			33.4			27.3			25.7	
Approach LOS		C			C			C			C	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	18.5	37.0		24.5	18.5	37.0		24.5				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	9.4	33.0		25.6	9.4	33.0		25.6				
Max Q Clear Time (g_c+I1), s	3.4	25.3		10.4	4.2	26.5		19.7				
Green Ext Time (p_c), s	0.0	4.6		0.9	0.0	4.4		0.8				

Intersection Summary

HCM 6th Ctrl Delay	27.0
HCM 6th LOS	C

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	87	224	54	61	276	159	59	870	49	131	930	109
Future Volume (veh/h)	87	224	54	61	276	159	59	870	49	131	930	109
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	105	270	65	73	333	192	71	1048	59	158	1120	131
Peak Hour Factor	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	143	620	147	127	390	329	126	1421	632	220	1611	713
Arrive On Green	0.08	0.21	0.21	0.07	0.21	0.21	0.07	0.39	0.39	0.12	0.45	0.45
Sat Flow, veh/h	1810	2894	684	1810	1900	1603	1810	3610	1606	1810	3610	1598
Grp Volume(v), veh/h	105	167	168	73	333	192	71	1048	59	158	1120	131
Grp Sat Flow(s),veh/h/ln	1810	1805	1773	1810	1900	1603	1810	1805	1606	1810	1805	1598
Q Serve(g_s), s	4.5	6.4	6.6	3.1	13.5	6.2	3.0	19.8	1.3	6.7	19.9	4.0
Cycle Q Clear(g_c), s	4.5	6.4	6.6	3.1	13.5	6.2	3.0	19.8	1.3	6.7	19.9	4.0
Prop In Lane	1.00		0.39	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	143	387	380	127	390	329	126	1421	632	220	1611	713
V/C Ratio(X)	0.73	0.43	0.44	0.57	0.85	0.58	0.57	0.74	0.09	0.72	0.70	0.18
Avail Cap(c_a), veh/h	158	417	410	158	439	371	192	1421	632	220	1611	713
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.80	0.80	0.80
Uniform Delay (d), s/veh	36.0	27.2	27.3	36.0	30.6	14.9	36.1	20.7	7.9	33.8	17.8	13.4
Incr Delay (d2), s/veh	14.6	0.8	0.8	4.0	13.8	1.8	3.9	3.5	0.3	8.6	2.0	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.5	2.8	2.8	1.5	7.5	3.3	1.5	8.5	0.7	3.4	8.1	1.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	50.7	28.0	28.1	40.1	44.4	16.8	40.0	24.2	8.2	42.4	19.8	13.8
LnGrp LOS	D	C	C	D	D	B	D	C	A	D	B	B
Approach Vol, veh/h		440			598			1178			1409	
Approach Delay, s/veh		33.4			35.0			24.3			21.8	
Approach LOS		C			C			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	31.7	35.5	9.6	21.1	9.6	39.7	10.3	20.4				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	31.5	7.0	18.5	8.5	30.0	7.0	18.5					
Max Q Clear Time (g_c+10), s	21.8	5.1	8.6	5.0	21.9	6.5	15.5					
Green Ext Time (p_c), s	0.0	5.1	0.0	1.3	0.0	4.9	0.0	0.8				

Intersection Summary												
HCM 6th Ctrl Delay											26.2	
HCM 6th LOS											C	

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour

Intersection						
Int Delay, s/veh	0.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	19	1	0	13	2	0
Future Vol, veh/h	19	1	0	13	2	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	23	1	0	16	2	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	24	0	40
Stage 1	-	-	-	-	24
Stage 2	-	-	-	-	16
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1604	-	977
Stage 1	-	-	-	-	1004
Stage 2	-	-	-	-	1012
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1604	-	977
Mov Cap-2 Maneuver	-	-	-	-	977
Stage 1	-	-	-	-	1004
Stage 2	-	-	-	-	1012

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	977	-	-	1604	-
HCM Lane V/C Ratio	0.002	-	-	-	-
HCM Control Delay (s)	8.7	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	51	189	79	52	127	108	51	978	59	97	1063	45
Future Volume (veh/h)	51	189	79	52	127	108	51	978	59	97	1063	45
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	54	199	83	55	134	114	54	1029	62	102	1119	47
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	98	254	214	99	255	216	98	1624	708	365	2156	937
Arrive On Green	0.05	0.13	0.13	0.05	0.13	0.13	0.02	0.15	0.15	0.20	0.60	0.60
Sat Flow, veh/h	1810	1900	1603	1810	1900	1610	1810	3610	1573	1810	3610	1569
Grp Volume(v), veh/h	54	199	83	55	134	114	54	1029	62	102	1119	47
Grp Sat Flow(s),veh/h/ln	1810	1900	1603	1810	1900	1610	1810	1805	1573	1810	1805	1569
Q Serve(g_s), s	2.9	10.1	4.7	3.0	6.6	4.5	3.0	26.8	2.6	4.8	18.1	1.2
Cycle Q Clear(g_c), s	2.9	10.1	4.7	3.0	6.6	4.5	3.0	26.8	2.6	4.8	18.1	1.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	98	254	214	99	255	216	98	1625	708	365	2156	937
V/C Ratio(X)	0.55	0.78	0.39	0.55	0.53	0.53	0.55	0.63	0.09	0.28	0.52	0.05
Avail Cap(c_a), veh/h	199	475	401	127	399	338	163	1625	708	365	2156	937
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	46.1	41.9	39.6	46.1	40.3	18.4	47.9	34.8	14.2	33.8	11.8	8.4
Incr Delay (d2), s/veh	4.7	5.3	1.1	4.8	1.7	2.0	4.7	1.9	0.2	0.4	0.9	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.4	5.1	1.9	1.5	3.2	2.6	1.5	13.3	1.3	2.1	7.0	0.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	50.8	47.2	40.7	50.8	42.0	20.4	52.6	36.7	14.4	34.2	12.7	8.5
LnGrp LOS	D	D	D	D	D	C	D	D	B	C	B	A
Approach Vol, veh/h		336			303			1145			1268	
Approach Delay, s/veh		46.2			35.5			36.3			14.2	
Approach LOS		D			D			D			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	24.2	49.0	9.5	17.4	9.4	63.7	9.4	17.4				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	45.0	7.0	25.0	9.0	43.0	11.0	21.0				
Max Q Clear Time (g_c+I1), s	6.8	28.8	5.0	12.1	5.0	20.1	4.9	8.6				
Green Ext Time (p_c), s	0.0	7.0	0.0	1.1	0.0	9.1	0.0	0.8				
Intersection Summary												
HCM 6th Ctrl Delay				28.1								
HCM 6th LOS				C								

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour

Intersection												
Int Delay, s/veh	4.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕	↕	↕	↕	↕
Traffic Vol, veh/h	27	1	36	3	1	13	43	1059	1	42	1120	15
Future Vol, veh/h	27	1	36	3	1	13	43	1059	1	42	1120	15
Conflicting Peds, #/hr	0	0	0	0	0	0	4	0	0	0	0	4
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	50	50	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	28	1	37	3	1	13	44	1092	1	43	1155	15

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1880	2426	582	1844	2440	546	1174	0	0	1093	0	0
Stage 1	1245	1245	-	1180	1180	-	-	-	-	-	-	-
Stage 2	635	1181	-	664	1260	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	45	33	461	47	32	487	602	-	-	646	-	-
Stage 1	187	248	-	205	266	-	-	-	-	-	-	-
Stage 2	438	266	-	421	244	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	38	28	459	38	28	487	600	-	-	646	-	-
Mov Cap-2 Maneuver	38	28	-	38	28	-	-	-	-	-	-	-
Stage 1	173	230	-	190	247	-	-	-	-	-	-	-
Stage 2	393	247	-	360	227	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	153.5		40.1		0.4		0.4	
HCM LOS	F		E					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	600	-	-	78	120	646	-
HCM Lane V/C Ratio	0.074	-	-	0.846	0.146	0.067	-
HCM Control Delay (s)	11.5	-	-	153.5	40.1	11	-
HCM Lane LOS	B	-	-	F	E	B	-
HCM 95th %tile Q(veh)	0.2	-	-	4.3	0.5	0.2	-

HCM 6th TWSC
3: Perris Boulevard & Auto Center Driveway

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T T T		
Traffic Vol, veh/h	0	0	0	1105	1151	2
Future Vol, veh/h	0	0	0	1105	1151	2
Conflicting Peds, #/hr	0	0	3	0	0	3
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	25
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	0	0	1151	1199	2

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1779	604	1204	0	-	0
Stage 1	1203	-	-	-	-	-
Stage 2	576	-	-	-	-	-
Critical Hdwy	6.25	7.1	5.3	-	-	-
Critical Hdwy Stg 1	6.6	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.65	3.9	3.1	-	-	-
Pot Cap-1 Maneuver	*516	*648	*815	-	-	-
Stage 1	*692	-	-	-	-	-
Stage 2	*579	-	-	-	-	-
Platoon blocked, %	1	1	1	-	-	-
Mov Cap-1 Maneuver	*513	*646	*813	-	-	-
Mov Cap-2 Maneuver	*511	-	-	-	-	-
Stage 1	*690	-	-	-	-	-
Stage 2	*577	-	-	-	-	-






















Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	* 813	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	69	102	22	32	73	47	29	988	36	47	1036	68
Future Volume (veh/h)	69	102	22	32	73	47	29	988	36	47	1036	68
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	72	106	23	33	76	49	30	1029	38	49	1079	71
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	128	160	302	61	119	61	348	2270	84	94	1805	784
Arrive On Green	0.19	0.19	0.19	0.19	0.19	0.19	0.38	1.00	1.00	0.10	1.00	1.00
Sat Flow, veh/h	413	852	1608	93	631	326	1810	3547	131	1810	3610	1568
Grp Volume(v), veh/h	178	0	23	158	0	0	30	524	543	49	1079	71
Grp Sat Flow(s),veh/h/ln	1266	0	1608	1050	0	0	1810	1805	1873	1810	1805	1568
Q Serve(g_s), s	0.0	0.0	1.2	2.4	0.0	0.0	1.1	0.0	0.0	2.6	0.0	0.0
Cycle Q Clear(g_c), s	13.7	0.0	1.2	16.1	0.0	0.0	1.1	0.0	0.0	2.6	0.0	0.0
Prop In Lane	0.40		1.00	0.21		0.31	1.00		0.07	1.00		1.00
Lane Grp Cap(c), veh/h	288	0	302	241	0	0	348	1155	1199	94	1805	784
V/C Ratio(X)	0.62	0.00	0.08	0.66	0.00	0.00	0.09	0.45	0.45	0.52	0.60	0.09
Avail Cap(c_a), veh/h	483	0	498	441	0	0	348	1155	1199	127	1805	784
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.89	0.89	0.89	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.9	0.0	33.5	37.7	0.0	0.0	25.2	0.0	0.0	43.6	0.0	0.0
Incr Delay (d2), s/veh	2.2	0.0	0.1	3.0	0.0	0.0	0.1	1.1	1.1	4.4	1.5	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.3	0.0	0.5	4.0	0.0	0.0	0.5	0.4	0.4	1.2	0.4	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	40.1	0.0	33.6	40.8	0.0	0.0	25.3	1.1	1.1	48.0	1.5	0.2
LnGrp LOS	D	A	C	D	A	A	C	A	A	D	A	A
Approach Vol, veh/h		201			158			1097			1199	
Approach Delay, s/veh		39.3			40.8			1.8			3.3	
Approach LOS		D			D			A			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.2	68.0		22.8	23.2	54.0		22.8				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	7.0	50.0		31.0	7.0	50.0		31.0				
Max Q Clear Time (g_c+I1), s	4.6	2.0		15.7	3.1	2.0		18.1				
Green Ext Time (p_c), s	0.0	9.1		0.9	0.0	10.8		0.6				

Intersection Summary												
HCM 6th Ctrl Delay			7.6									
HCM 6th LOS			A									

Notes
User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↕	↘	↙	↕	↘	↙	↕	↘	↙	↕	↘
Traffic Volume (veh/h)	92	197	75	23	134	87	64	931	31	98	964	83
Future Volume (veh/h)	92	197	75	23	134	87	64	931	31	98	964	83
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.97	1.00		0.97	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	94	201	77	23	137	89	65	950	32	100	984	85
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	122	362	134	60	202	166	106	1264	549	572	2194	954
Arrive On Green	0.07	0.14	0.14	0.03	0.11	0.11	0.06	0.35	0.35	0.63	1.00	1.00
Sat Flow, veh/h	1810	2572	952	1810	1900	1565	1810	3610	1570	1810	3610	1570
Grp Volume(v), veh/h	94	139	139	23	137	89	65	950	32	100	984	85
Grp Sat Flow(s),veh/h/ln	1810	1805	1719	1810	1900	1565	1810	1805	1570	1810	1805	1570
Q Serve(g_s), s	5.1	7.2	7.6	1.2	6.9	3.0	3.5	23.2	1.1	2.3	0.0	0.0
Cycle Q Clear(g_c), s	5.1	7.2	7.6	1.2	6.9	3.0	3.5	23.2	1.1	2.3	0.0	0.0
Prop In Lane	1.00		0.55	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	122	254	242	60	202	166	106	1264	549	572	2194	954
V/C Ratio(X)	0.77	0.55	0.58	0.38	0.68	0.54	0.61	0.75	0.06	0.17	0.45	0.09
Avail Cap(c_a), veh/h	407	433	413	308	352	290	127	1264	549	572	2194	954
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.91	0.91	0.91
Uniform Delay (d), s/veh	45.9	40.0	40.2	47.4	43.1	13.1	46.0	28.7	14.7	13.0	0.0	0.0
Incr Delay (d2), s/veh	9.7	1.8	2.2	4.0	4.0	2.7	6.2	4.2	0.2	0.1	0.6	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.6	3.3	3.3	0.6	3.5	2.1	1.8	10.5	0.5	0.9	0.2	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	55.6	41.8	42.3	51.4	47.0	15.8	52.2	32.8	14.9	13.1	0.6	0.2
LnGrp LOS	E	D	D	D	D	B	D	C	B	B	A	A
Approach Vol, veh/h		372			249			1047			1169	
Approach Delay, s/veh		45.5			36.3			33.5			1.6	
Approach LOS		D			D			C			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	35.6	39.0	7.3	18.1	9.8	64.8	10.7	14.6				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	35.0	35.0	17.0	24.0	7.0	36.0	22.5	18.5				
Max Q Clear Time (g_c+1), s	25.2	25.2	3.2	9.6	5.5	2.0	7.1	8.9				
Green Ext Time (p_c), s	0.1	4.6	0.0	1.3	0.0	9.0	0.2	0.7				

Intersection Summary												
HCM 6th Ctrl Delay											22.2	
HCM 6th LOS											C	

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	42	2	0	16	1	0
Future Vol, veh/h	42	2	0	16	1	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	43	2	0	16	1	0

























Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	45	0	60
Stage 1	-	-	-	-	44
Stage 2	-	-	-	-	16
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1576	-	952
Stage 1	-	-	-	-	984
Stage 2	-	-	-	-	1012
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1576	-	952
Mov Cap-2 Maneuver	-	-	-	-	952
Stage 1	-	-	-	-	984
Stage 2	-	-	-	-	1012

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	952	-	-	1576	-
HCM Lane V/C Ratio	0.001	-	-	-	-
HCM Control Delay (s)	8.8	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - AM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	24	77	60	88	230	206	53	1177	61	76	1114	34
Future Volume (veh/h)	24	77	60	88	230	206	53	1177	61	76	1114	34
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.99	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	28	89	69	101	264	237	61	1353	70	87	1280	39
Peak Hour Factor	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	73	175	145	224	333	282	118	1421	631	345	1874	834
Arrive On Green	0.04	0.09	0.09	0.12	0.18	0.18	0.13	0.79	0.79	0.19	0.52	0.52
Sat Flow, veh/h	1810	1900	1577	1810	1900	1607	1810	3610	1602	1810	3610	1607
Grp Volume(v), veh/h	28	89	69	101	264	237	61	1353	70	87	1280	39
Grp Sat Flow(s),veh/h/ln	1810	1900	1577	1810	1900	1607	1810	1805	1602	1810	1805	1607
Q Serve(g_s), s	1.2	3.6	2.7	4.1	10.6	7.4	2.5	25.4	0.8	3.3	21.1	1.0
Cycle Q Clear(g_c), s	1.2	3.6	2.7	4.1	10.6	7.4	2.5	25.4	0.8	3.3	21.1	1.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	73	175	145	224	333	282	118	1421	631	345	1874	834
V/C Ratio(X)	0.38	0.51	0.48	0.45	0.79	0.84	0.52	0.95	0.11	0.25	0.68	0.05
Avail Cap(c_a), veh/h	158	439	365	224	439	372	158	1421	631	345	1874	834
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.4	34.6	23.1	32.5	31.6	13.4	33.6	7.9	5.2	27.5	14.3	9.5
Incr Delay (d2), s/veh	3.2	2.3	2.4	1.4	7.2	12.4	3.5	14.9	0.4	0.4	2.0	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.6	1.7	1.3	1.9	5.4	3.5	1.2	5.8	0.3	1.4	8.3	0.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	40.6	36.9	25.5	34.0	38.8	25.8	37.2	22.7	5.6	27.9	16.4	9.6
LnGrp LOS	D	D	C	C	D	C	D	C	A	C	B	A
Approach Vol, veh/h		186			602			1484			1406	
Approach Delay, s/veh		33.2			32.9			22.5			16.9	
Approach LOS		C			C			C			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	19.2	35.5	13.9	11.4	9.2	45.5	7.2	18.0				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	31.5	7.0	18.5	7.0	31.5	7.0	18.5				
Max Q Clear Time (g_c+I1), s	5.3	27.4	6.1	5.6	4.5	23.1	3.2	12.6				
Green Ext Time (p_c), s	0.0	3.1	0.0	0.5	0.0	5.4	0.0	1.2				
Intersection Summary												
HCM 6th Ctrl Delay				22.6								
HCM 6th LOS				C								

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaena Commercial Project
Project Completion (2022) NP - AM Peak Hour

Intersection												
Int Delay, s/veh	16.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕	↕	↕	↕	↕
Traffic Vol, veh/h	15	1	40	4	2	10	55	1245	3	17	1252	17
Future Vol, veh/h	15	1	40	4	2	10	55	1245	3	17	1252	17
Conflicting Peds, #/hr	2	0	0	0	0	2	7	0	0	0	0	7
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	50	50	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	83	83	83	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	18	1	48	5	2	12	66	1500	4	20	1508	20

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2440	3191	761	2427	3207	752	1535	0	0	1504	0	0
Stage 1	1555	1555	-	1632	1632	-	-	-	-	-	-	-
Stage 2	885	1636	-	795	1575	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 17	10	352	17	10	357	439	-	-	451	-	-
Stage 1	120	176	-	108	161	-	-	-	-	-	-	-
Stage 2	310	160	-	351	172	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	~ 11	8	350	11	8	356	436	-	-	451	-	-
Mov Cap-2 Maneuver	~ 11	8	-	11	8	-	-	-	-	-	-	-
Stage 1	101	167	-	92	137	-	-	-	-	-	-	-
Stage 2	249	136	-	287	163	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	\$ 681		\$ 326.9		0.6		0.2	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	436	-	-	35	25	451	-
HCM Lane V/C Ratio	0.152	-	-	1.928	0.771	0.045	-
HCM Control Delay (s)	14.7	-	-	\$ 681	\$ 326.9	13.4	-
HCM Lane LOS	B	-	-	F	F	B	-
HCM 95th %tile Q(veh)	0.5	-	-	7.5	2.4	0.1	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
3: Perris Boulevard & Auto Center Driveway

Perris/Draacea Commercial Project
Project Completion (2022) NP - AM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T T T			
Traffic Vol, veh/h	0	1	0	1319	1276	7
Future Vol, veh/h	0	1	0	1319	1276	7
Conflicting Peds, #/hr	0	0	11	0	0	11
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	25
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	1	0	1589	1537	8

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2347	784	1556	0	-	0
Stage 1	1552	-	-	-	-	-
Stage 2	795	-	-	-	-	-
Critical Hdwy	6.25	7.1	5.3	-	-	-
Critical Hdwy Stg 1	6.6	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.65	3.9	3.1	-	-	-
Pot Cap-1 Maneuver	*183	*646	594	-	-	-
Stage 1	*466	-	-	-	-	-
Stage 2	*477	-	-	-	-	-
Platoon blocked, %	1	1	1	-	-	-
Mov Cap-1 Maneuver	*179	*639	587	-	-	-
Mov Cap-2 Maneuver	*295	-	-	-	-	-
Stage 1	*461	-	-	-	-	-
Stage 2	*472	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.6	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	587	-	639	-	-
HCM Lane V/C Ratio	-	-	0.002	-	-
HCM Control Delay (s)	0	-	10.6	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕		↖	↕	↗	↘	↕	↖
Traffic Volume (veh/h)	69	75	55	74	102	71	53	1179	21	35	1151	91
Future Volume (veh/h)	69	75	55	74	102	71	53	1179	21	35	1151	91
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		0.99	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	82	89	65	88	121	85	63	1404	25	42	1370	108
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	197	193	440	133	165	97	295	1497	27	295	1489	642
Arrive On Green	0.27	0.27	0.27	0.27	0.27	0.27	0.05	0.14	0.14	0.22	0.55	0.55
Sat Flow, veh/h	475	703	1603	271	600	354	1810	3628	65	1810	3610	1557
Grp Volume(v), veh/h	171	0	65	294	0	0	63	698	731	42	1370	108
Grp Sat Flow(s),veh/h/ln	1178	0	1603	1226	0	0	1810	1805	1887	1810	1805	1557
Q Serve(g_s), s	0.0	0.0	2.5	10.0	0.0	0.0	2.7	30.6	30.7	1.5	27.7	2.8
Cycle Q Clear(g_c), s	9.3	0.0	2.5	19.3	0.0	0.0	2.7	30.6	30.7	1.5	27.7	2.8
Prop In Lane	0.48		1.00	0.30		0.29	1.00		0.03	1.00		1.00
Lane Grp Cap(c), veh/h	390	0	440	395	0	0	295	745	779	295	1489	642
V/C Ratio(X)	0.44	0.00	0.15	0.74	0.00	0.00	0.21	0.94	0.94	0.14	0.92	0.17
Avail Cap(c_a), veh/h	457	0	513	466	0	0	295	745	779	295	1489	642
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.33	1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.52	0.52	0.52	1.00	1.00	1.00
Uniform Delay (d), s/veh	23.9	0.0	21.9	28.8	0.0	0.0	32.9	33.5	33.6	26.8	16.9	11.2
Incr Delay (d2), s/veh	0.8	0.0	0.2	5.3	0.0	0.0	0.2	12.9	12.6	0.2	10.7	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.7	0.0	0.9	5.9	0.0	0.0	1.2	17.3	18.0	0.6	10.8	1.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	24.7	0.0	22.1	34.2	0.0	0.0	33.1	46.5	46.2	27.0	27.6	11.8
LnGrp LOS	C	A	C	C	A	A	C	D	D	C	C	B
Approach Vol, veh/h		236			294			1492			1520	
Approach Delay, s/veh		24.0			34.2			45.8			26.4	
Approach LOS		C			C			D			C	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	17.0	37.0		26.0	17.0	37.0		26.0				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	9.4	33.0		25.6	9.4	33.0		25.6				
Max Q Clear Time (g_c+I1), s	3.5	32.7		11.3	4.7	29.7		21.3				
Green Ext Time (p_c), s	0.0	0.3		1.0	0.0	2.6		0.7				

Intersection Summary

HCM 6th Ctrl Delay	35.1
HCM 6th LOS	D

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	92	237	57	65	293	169	63	1035	52	139	1036	116
Future Volume (veh/h)	92	237	57	65	293	169	63	1035	52	139	1036	116
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	111	286	69	78	353	204	76	1247	63	167	1248	140
Peak Hour Factor	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	145	663	157	130	420	355	129	1173	522	314	1543	682
Arrive On Green	0.08	0.23	0.23	0.07	0.22	0.22	0.07	0.32	0.32	0.35	0.85	0.85
Sat Flow, veh/h	1810	2892	686	1810	1900	1604	1810	3610	1605	1810	3610	1597
Grp Volume(v), veh/h	111	177	178	78	353	204	76	1247	63	167	1248	140
Grp Sat Flow(s),veh/h/ln	1810	1805	1773	1810	1900	1604	1810	1805	1605	1810	1805	1597
Q Serve(g_s), s	4.8	6.7	6.9	3.3	14.2	5.9	3.3	26.0	1.6	5.9	13.0	1.2
Cycle Q Clear(g_c), s	4.8	6.7	6.9	3.3	14.2	5.9	3.3	26.0	1.6	5.9	13.0	1.2
Prop In Lane	1.00		0.39	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	145	414	407	130	420	355	129	1173	522	314	1543	682
V/C Ratio(X)	0.77	0.43	0.44	0.60	0.84	0.57	0.59	1.06	0.12	0.53	0.81	0.21
Avail Cap(c_a), veh/h	204	496	488	204	523	441	158	1173	522	314	1543	682
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.68	0.68	0.68
Uniform Delay (d), s/veh	36.1	26.3	26.4	36.0	29.8	11.7	36.0	27.0	10.5	23.5	4.3	3.4
Incr Delay (d2), s/veh	10.6	0.7	0.7	4.3	9.7	1.5	4.2	44.7	0.5	1.2	3.2	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.5	2.9	2.9	1.6	7.4	3.3	1.6	17.8	0.9	2.3	2.4	0.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	46.7	27.0	27.2	40.3	39.5	13.2	40.2	71.7	11.0	24.7	7.5	3.9
LnGrp LOS	D	C	C	D	D	B	D	F	B	C	A	A
Approach Vol, veh/h		466			635			1386			1555	
Approach Delay, s/veh		31.8			31.1			67.2			9.0	
Approach LOS		C			C			E			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.9	30.0	9.8	22.3	9.7	38.2	10.4	21.7				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	26.0	9.0	22.0	7.0	26.0	9.0	22.0					
Max Q Clear Time (g_c+1), s	28.0	5.3	8.9	5.3	15.0	6.8	16.2					
Green Ext Time (p_c), s	0.0	0.0	0.0	1.7	0.0	6.7	0.0	1.5				

Intersection Summary

HCM 6th Ctrl Delay	35.1
HCM 6th LOS	D

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - AM Peak Hour

Intersection						
Int Delay, s/veh	0.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	20	1	0	14	2	0
Future Vol, veh/h	20	1	0	14	2	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	24	1	0	17	2	0

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	25	0	42
Stage 1	-	-	-	-	25
Stage 2	-	-	-	-	17
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1603	-	974
Stage 1	-	-	-	-	1003
Stage 2	-	-	-	-	1011
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1603	-	974
Mov Cap-2 Maneuver	-	-	-	-	974
Stage 1	-	-	-	-	1003
Stage 2	-	-	-	-	1011

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	974	-	-	1603	-
HCM Lane V/C Ratio	0.002	-	-	-	-
HCM Control Delay (s)	8.7	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘	↖	↗	↘	↖	↗	↘
Traffic Volume (veh/h)	54	200	84	55	135	114	55	1123	63	103	1250	49
Future Volume (veh/h)	54	200	84	55	135	114	55	1123	63	103	1250	49
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	57	211	88	58	142	120	58	1182	66	108	1316	52
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	101	266	224	101	267	226	101	1624	708	351	2123	922
Arrive On Green	0.06	0.14	0.14	0.06	0.14	0.14	0.02	0.15	0.15	0.19	0.59	0.59
Sat Flow, veh/h	1810	1900	1603	1810	1900	1610	1810	3610	1573	1810	3610	1569
Grp Volume(v), veh/h	57	211	88	58	142	120	58	1182	66	108	1316	52
Grp Sat Flow(s),veh/h/ln	1810	1900	1603	1810	1900	1610	1810	1805	1573	1810	1805	1569
Q Serve(g_s), s	3.1	10.7	5.0	3.1	6.9	4.7	3.2	31.3	2.7	5.1	23.6	1.4
Cycle Q Clear(g_c), s	3.1	10.7	5.0	3.1	6.9	4.7	3.2	31.3	2.7	5.1	23.6	1.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	101	266	224	101	267	226	101	1625	708	351	2123	922
V/C Ratio(X)	0.57	0.79	0.39	0.57	0.53	0.53	0.57	0.73	0.09	0.31	0.62	0.06
Avail Cap(c_a), veh/h	199	475	401	127	399	338	163	1625	708	351	2123	922
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	46.0	41.6	39.1	46.0	39.9	18.5	47.9	36.7	14.1	34.5	13.4	8.8
Incr Delay (d2), s/veh	4.9	5.3	1.1	5.0	1.6	1.9	5.0	2.9	0.3	0.5	1.4	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.5	5.4	2.0	1.5	3.3	2.8	1.6	15.7	1.4	2.3	9.3	0.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	51.0	46.9	40.2	51.0	41.6	20.5	52.9	39.6	14.4	35.0	14.7	8.9
LnGrp LOS	D	D	D	D	D	C	D	D	B	D	B	A
Approach Vol, veh/h		356			320			1306			1476	
Approach Delay, s/veh		45.9			35.4			38.9			16.0	
Approach LOS		D			D			D			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	23.4	49.0	9.6	18.0	9.6	62.8	9.6	18.0				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	45.0	7.0	25.0	9.0	43.0	11.0	21.0				
Max Q Clear Time (g_c+I1), s	7.1	33.3	5.1	12.7	5.2	25.6	5.1	8.9				
Green Ext Time (p_c), s	0.0	6.6	0.0	1.1	0.0	9.4	0.0	0.9				

Intersection Summary

HCM 6th Ctrl Delay	29.5
HCM 6th LOS	C

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaena Commercial Project
Project Completion (2022) NP - PM Peak Hour

Intersection												
Int Delay, s/veh	12.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕	↕	↕	↕	↕
Traffic Vol, veh/h	29	1	38	3	1	14	47	1210	1	45	1309	17
Future Vol, veh/h	29	1	38	3	1	14	47	1210	1	45	1309	17
Conflicting Peds, #/hr	0	0	0	0	0	0	4	0	0	0	0	4
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	50	50	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	30	1	39	3	1	14	48	1247	1	46	1349	18

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2165	2789	679	2110	2806	624	1371	0	0	1248	0	0
Stage 1	1445	1445	-	1343	1343	-	-	-	-	-	-	-
Stage 2	720	1344	-	767	1463	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 27	19	399	30	19	433	507	-	-	565	-	-
Stage 1	141	199	-	163	223	-	-	-	-	-	-	-
Stage 2	390	222	-	365	195	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	~ 22	16	397	22	16	433	505	-	-	565	-	-
Mov Cap-2 Maneuver	~ 22	16	-	22	16	-	-	-	-	-	-	-
Stage 1	127	182	-	148	202	-	-	-	-	-	-	-
Stage 2	339	201	-	300	178	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s\$	461.1		65		0.5		0.4	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	505	-	-	46	78	565	-	-
HCM Lane V/C Ratio	0.096	-	-	1.524	0.238	0.082	-	-
HCM Control Delay (s)	12.9	-	-	\$ 461.1	65	11.9	-	-
HCM Lane LOS	B	-	-	F	F	B	-	-
HCM 95th %tile Q(veh)	0.3	-	-	6.8	0.8	0.3	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
3: Perris Boulevard & Auto Center Driveway

Perris/Draacea Commercial Project
Project Completion (2022) NP - PM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T T		
Traffic Vol, veh/h	0	0	0	1259	1342	2
Future Vol, veh/h	0	0	0	1259	1342	2
Conflicting Peds, #/hr	0	0	3	0	0	3
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	25
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	0	0	1311	1398	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	2058	703	1403	0	0
Stage 1	1402	-	-	-	-
Stage 2	656	-	-	-	-
Critical Hdwy	6.25	7.1	5.3	-	-
Critical Hdwy Stg 1	6.6	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-
Follow-up Hdwy	3.65	3.9	3.1	-	-
Pot Cap-1 Maneuver	*487	*622	*782	-	-
Stage 1	*664	-	-	-	-
Stage 2	*493	-	-	-	-
Platoon blocked, %	1	1	1	-	-
Mov Cap-1 Maneuver	*484	*620	*780	-	-
Mov Cap-2 Maneuver	*449	-	-	-	-
Stage 1	*662	-	-	-	-
Stage 2	*492	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	* 780	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕		↖	↕		↖	↕	↗
Traffic Volume (veh/h)	74	108	23	34	77	50	31	1134	38	50	1219	73
Future Volume (veh/h)	74	108	23	34	77	50	31	1134	38	50	1219	73
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	77	112	24	35	80	52	32	1181	40	52	1270	76
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	133	166	327	61	124	64	320	2218	75	97	1805	784
Arrive On Green	0.20	0.20	0.20	0.20	0.20	0.20	0.35	1.00	1.00	0.11	1.00	1.00
Sat Flow, veh/h	403	814	1608	87	610	315	1810	3559	120	1810	3610	1568
Grp Volume(v), veh/h	189	0	24	167	0	0	32	599	622	52	1270	76
Grp Sat Flow(s),veh/h/ln	1218	0	1608	1012	0	0	1810	1805	1875	1810	1805	1568
Q Serve(g_s), s	0.0	0.0	1.2	2.5	0.0	0.0	1.2	0.0	0.0	2.7	0.0	0.0
Cycle Q Clear(g_c), s	15.2	0.0	1.2	17.7	0.0	0.0	1.2	0.0	0.0	2.7	0.0	0.0
Prop In Lane	0.41		1.00	0.21		0.31	1.00		0.06	1.00		1.00
Lane Grp Cap(c), veh/h	298	0	327	249	0	0	320	1125	1168	97	1805	784
V/C Ratio(X)	0.63	0.00	0.07	0.67	0.00	0.00	0.10	0.53	0.53	0.54	0.70	0.10
Avail Cap(c_a), veh/h	467	0	498	424	0	0	320	1125	1168	127	1805	784
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.83	0.83	0.83	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.1	0.0	32.2	36.7	0.0	0.0	27.0	0.0	0.0	43.5	0.0	0.0
Incr Delay (d2), s/veh	2.2	0.0	0.1	3.1	0.0	0.0	0.1	1.5	1.4	4.6	2.3	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.5	0.0	0.5	4.2	0.0	0.0	0.5	0.5	0.5	1.3	0.6	0.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.3	0.0	32.3	39.8	0.0	0.0	27.1	1.5	1.4	48.0	2.3	0.2
LnGrp LOS	D	A	C	D	A	A	C	A	A	D	A	A
Approach Vol, veh/h		213			167			1253			1398	
Approach Delay, s/veh		38.5			39.8			2.1			3.9	
Approach LOS		D			D			A			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.3	66.3		24.3	21.7	54.0		24.3				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	7.0	50.0		31.0	7.0	50.0		31.0				
Max Q Clear Time (g_c+I1), s	4.7	2.0		17.2	3.2	2.0		19.7				
Green Ext Time (p_c), s	0.0	11.3		0.9	0.0	14.0		0.6				

Intersection Summary

HCM 6th Ctrl Delay	7.6
HCM 6th LOS	A

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) NP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗		↘	↗	↘	↘	↗	↗	↘	↗	↘
Traffic Volume (veh/h)	98	209	80	24	142	92	68	1074	33	104	1143	88
Future Volume (veh/h)	98	209	80	24	142	92	68	1074	33	104	1143	88
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.97	1.00		0.97	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	100	213	82	24	145	94	69	1096	34	106	1166	90
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	129	378	141	62	209	172	108	1264	549	558	2162	940
Arrive On Green	0.07	0.15	0.15	0.03	0.11	0.11	0.06	0.35	0.35	0.62	1.00	1.00
Sat Flow, veh/h	1810	2567	956	1810	1900	1566	1810	3610	1570	1810	3610	1570
Grp Volume(v), veh/h	100	148	147	24	145	94	69	1096	34	106	1166	90
Grp Sat Flow(s),veh/h/ln	1810	1805	1719	1810	1900	1566	1810	1805	1570	1810	1805	1570
Q Serve(g_s), s	5.4	7.6	8.0	1.3	7.4	3.2	3.7	28.3	1.2	2.5	0.0	0.0
Cycle Q Clear(g_c), s	5.4	7.6	8.0	1.3	7.4	3.2	3.7	28.3	1.2	2.5	0.0	0.0
Prop In Lane	1.00		0.56	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	129	266	253	62	209	172	108	1264	549	558	2162	940
V/C Ratio(X)	0.77	0.55	0.58	0.39	0.69	0.55	0.64	0.87	0.06	0.19	0.54	0.10
Avail Cap(c_a), veh/h	407	433	412	308	352	290	127	1264	549	558	2162	940
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.85	0.85	0.85
Uniform Delay (d), s/veh	45.6	39.6	39.8	47.3	42.9	13.4	46.0	30.3	14.7	13.7	0.0	0.0
Incr Delay (d2), s/veh	9.4	1.8	2.1	4.0	4.1	2.7	8.0	8.2	0.2	0.1	0.8	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.8	3.5	3.5	0.6	3.7	2.2	1.9	13.4	0.6	1.0	0.2	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	55.0	41.4	41.9	51.2	47.0	16.1	54.0	38.5	14.9	13.9	0.8	0.2
LnGrp LOS	E	D	D	D	D	B	D	D	B	B	A	A
Approach Vol, veh/h		395			263			1199			1362	
Approach Delay, s/veh		45.0			36.3			38.7			1.8	
Approach LOS		D			D			D			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	34.9	39.0	7.4	18.7	10.0	63.9	11.1	15.0				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	35.0	35.0	17.0	24.0	7.0	36.0	22.5	18.5				
Max Q Clear Time (g_c+1), s	11.5	30.3	3.3	10.0	5.7	2.0	7.4	9.4				
Green Ext Time (p_c), s	0.1	3.0	0.0	1.4	0.0	11.3	0.2	0.7				

Intersection Summary

HCM 6th Ctrl Delay	23.7
HCM 6th LOS	C

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - PM Peak Hour

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	45	2	0	17	1	0
Future Vol, veh/h	45	2	0	17	1	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	46	2	0	18	1	0
























Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	48	0	65
Stage 1	-	-	-	-	47
Stage 2	-	-	-	-	18
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1572	-	946
Stage 1	-	-	-	-	981
Stage 2	-	-	-	-	1010
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1572	-	946
Mov Cap-2 Maneuver	-	-	-	-	946
Stage 1	-	-	-	-	981
Stage 2	-	-	-	-	1010

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	946	-	-	1572	-
HCM Lane V/C Ratio	0.001	-	-	-	-
HCM Control Delay (s)	8.8	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - AM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBL	SBT
Lane Configurations												
Traffic Volume (veh/h)	27	77	77	105	230	206	6	69	1182	77	76	1122
Future Volume (veh/h)	27	77	77	105	230	206	6	69	1182	77	76	1122
Initial Q (Qb), veh	0	0	0	0	0	0		0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00		1.00		0.99	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No				No			No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900		1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	31	89	89	121	264	237		79	1359	89	87	1290
Peak Hour Factor	0.87	0.87	0.87	0.87	0.87	0.87		0.87	0.87	0.87	0.87	0.87
Percent Heavy Veh, %	0	0	0	0	0	0		0	0	0	0	0
Cap, veh/h	79	175	145	229	333	282		131	1421	631	339	1837
Arrive On Green	0.04	0.09	0.09	0.13	0.18	0.18		0.14	0.79	0.79	0.19	0.51
Sat Flow, veh/h	1810	1900	1577	1810	1900	1607		1810	3610	1602	1810	3610
Grp Volume(v), veh/h	31	89	89	121	264	237		79	1359	89	87	1290
Grp Sat Flow(s),veh/h/ln	1810	1900	1577	1810	1900	1607		1810	1805	1602	1810	1805
Q Serve(g_s), s	1.3	3.6	3.5	5.0	10.6	7.4		3.3	25.9	1.1	3.3	21.9
Cycle Q Clear(g_c), s	1.3	3.6	3.5	5.0	10.6	7.4		3.3	25.9	1.1	3.3	21.9
Prop In Lane	1.00		1.00	1.00		1.00		1.00		1.00	1.00	
Lane Grp Cap(c), veh/h	79	175	145	229	333	282		131	1421	631	339	1837
V/C Ratio(X)	0.39	0.51	0.61	0.53	0.79	0.84		0.60	0.96	0.14	0.26	0.70
Avail Cap(c_a), veh/h	158	439	365	229	439	372		158	1421	631	339	1837
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00		2.00	2.00	2.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.2	34.6	22.9	32.7	31.6	13.5		33.1	7.9	5.3	27.7	15.0
Incr Delay (d2), s/veh	3.2	2.3	4.1	2.2	7.2	12.4		4.4	15.5	0.5	0.4	2.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.6	1.7	1.8	2.3	5.4	3.5		1.5	6.0	0.4	1.4	8.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	40.4	36.9	27.1	34.9	38.8	26.0		37.5	23.4	5.7	28.1	17.3
LnGrp LOS	D	D	C	C	D	C		D	C	A	C	B
Approach Vol, veh/h		209			622				1527			1416
Approach Delay, s/veh		33.2			33.2				23.1			17.8
Approach LOS		C			C				C			B
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	19.0	35.5	14.1	11.4	9.8	44.7	7.5	18.0				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	31.5	7.0	18.5	7.0	31.5	7.0	18.5				
Max Q Clear Time (g_c+I1), s	5.3	27.9	7.0	5.6	5.3	23.9	3.3	12.6				
Green Ext Time (p_c), s	0.0	2.8	0.0	0.6	0.0	5.1	0.0	1.2				
Intersection Summary												
HCM 6th Ctrl Delay				23.3								
HCM 6th LOS				C								
Notes												
User approved ignoring U-Turning movement.												

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
 1: Perris Boulevard & Eucalyptus Avenue

Perris/Draacea Commercial Project
 Project Completion (2022) WP - AM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	34
Future Volume (veh/h)	34
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	1.00
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	39
Peak Hour Factor	0.87
Percent Heavy Veh, %	0
Cap, veh/h	818
Arrive On Green	0.51
Sat Flow, veh/h	1607
Grp Volume(v), veh/h	39
Grp Sat Flow(s),veh/h/ln	1607
Q Serve(g_s), s	1.0
Cycle Q Clear(g_c), s	1.0
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	818
V/C Ratio(X)	0.05
Avail Cap(c_a), veh/h	818
HCM Platoon Ratio	1.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	9.9
Incr Delay (d2), s/veh	0.1
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	0.3
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	10.0
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - AM Peak Hour

Intersection												
Int Delay, s/veh	1.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗	↗	↗↗	↗	↗	↗↗	↗
Traffic Vol, veh/h	0	0	61	0	0	32	63	1281	4	63	1252	19
Future Vol, veh/h	0	0	61	0	0	32	63	1281	4	63	1252	19
Conflicting Peds, #/hr	2	0	0	0	0	2	7	0	0	0	0	7
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	150	-	50	125	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	83	83	83	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	73	0	0	39	76	1543	5	76	1508	23

Major/Minor	Minor2	Minor1	Major1	Major2								
Conflicting Flow All	-	-	761	-	-	774	1538	0	0	1548	0	0
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	6.9	-	-	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.3	-	-	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	0	0	352	0	0	346	438	-	-	434	-	-
Stage 1	0	0	-	0	0	-	-	-	-	-	-	-
Stage 2	0	0	-	0	0	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	350	-	-	345	435	-	-	434	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	18	16.7	0.7	0.7
HCM LOS	C	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	435	-	-	350	345	434	-
HCM Lane V/C Ratio	0.174	-	-	0.21	0.112	0.175	-
HCM Control Delay (s)	15	-	-	18	16.7	15	-
HCM Lane LOS	C	-	-	C	C	C	-
HCM 95th %tile Q(veh)	0.6	-	-	0.8	0.4	0.6	-

HCM 6th TWSC Perris/Dracaesa Commercial Project
3: Perris Boulevard & Auto Center Driveway/Project Driveway Project Completion (2022) WP - AM Peak Hour

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗		↕↔			↕↔↔	
Traffic Vol, veh/h	0	0	1	0	0	50	0	1313	19	0	1293	7
Future Vol, veh/h	0	0	1	0	0	50	0	1313	19	0	1293	7
Conflicting Peds, #/hr	0	0	0	0	0	0	11	0	0	0	0	11
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	-	-	-	-	-	25
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	83	83	83	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	1	0	0	60	0	1582	23	0	1558	8

Major/Minor	Minor2		Minor1		Major1		Major2	
Conflicting Flow All	-	-	794	-	-	803	-	0
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	7.1	-	-	6.9	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.9	-	-	3.3	-	-
Pot Cap-1 Maneuver	0	0	*613	0	0	*528	0	-
Stage 1	0	0	-	0	0	-	0	-
Stage 2	0	0	-	0	0	-	0	-
Platoon blocked, %			1			1		
Mov Cap-1 Maneuver	-	-	*607	-	-	*528	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	10.9	12.7	0	0
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBT	NBR	EBLn1WBLn1	SBT	SBR
Capacity (veh/h)	-	-	607	528	-
HCM Lane V/C Ratio	-	-	0.002	0.114	-
HCM Control Delay (s)	-	-	10.9	12.7	-
HCM Lane LOS	-	-	B	B	-
HCM 95th %tile Q(veh)	-	-	0	0.4	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBU	SBL	SBT
Lane Configurations		↕	↗		↕		↗	↕			↘	↕
Traffic Volume (veh/h)	69	109	54	141	136	71	52	1179	72	13	59	1132
Future Volume (veh/h)	69	109	54	141	136	71	52	1179	72	13	59	1132
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		0.99		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900		1900	1900
Adj Flow Rate, veh/h	82	130	64	168	162	85	62	1404	86		70	1348
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84		0.84	0.84
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0		0	0
Cap, veh/h	197	289	513	187	148	70	213	1424	87		213	1489
Arrive On Green	0.32	0.32	0.32	0.32	0.32	0.32	0.16	0.55	0.55		0.16	0.55
Sat Flow, veh/h	421	902	1604	387	463	219	1810	3453	211		1810	3610
Grp Volume(v), veh/h	212	0	64	415	0	0	62	732	758		70	1348
Grp Sat Flow(s),veh/h/ln	1322	0	1604	1069	0	0	1810	1805	1859		1810	1805
Q Serve(g_s), s	0.0	0.0	2.3	16.4	0.0	0.0	2.4	31.8	32.2		2.8	26.8
Cycle Q Clear(g_c), s	9.2	0.0	2.3	25.6	0.0	0.0	2.4	31.8	32.2		2.8	26.8
Prop In Lane	0.39		1.00	0.40		0.20	1.00		0.11		1.00	
Lane Grp Cap(c), veh/h	486	0	513	405	0	0	213	745	767		213	1489
V/C Ratio(X)	0.44	0.00	0.12	1.02	0.00	0.00	0.29	0.98	0.99		0.33	0.91
Avail Cap(c_a), veh/h	486	0	513	405	0	0	213	745	767		213	1489
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33		1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.51	0.51	0.51		1.00	1.00
Uniform Delay (d), s/veh	21.2	0.0	19.3	31.6	0.0	0.0	30.8	17.8	17.9		30.9	16.7
Incr Delay (d2), s/veh	0.6	0.0	0.1	50.9	0.0	0.0	0.4	19.8	20.7		0.9	9.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0
%ile BackOfQ(50%),veh/ln	3.1	0.0	0.8	13.6	0.0	0.0	1.0	13.9	14.6		1.2	10.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	21.9	0.0	19.4	82.5	0.0	0.0	31.2	37.6	38.6		31.8	26.1
LnGrp LOS	C	A	B	F	A	A	C	D	D		C	C
Approach Vol, veh/h		276			415			1552				1524
Approach Delay, s/veh		21.3			82.5			37.8				25.4
Approach LOS		C			F			D				C
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	13.4	37.0		29.6	13.4	37.0		29.6				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	9.4	33.0		25.6	9.4	33.0		25.6				
Max Q Clear Time (g_c+I1), s	4.8	34.2		11.2	4.4	28.8		27.6				
Green Ext Time (p_c), s	0.0	0.0		1.2	0.0	3.2		0.0				

Intersection Summary												
HCM 6th Ctrl Delay				36.5								
HCM 6th LOS				D								

Notes
User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
 Project Completion (2022) WP - AM Peak Hour

Movement	SBR
Lane Configurations	7
Traffic Volume (veh/h)	89
Future Volume (veh/h)	89
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	106
Peak Hour Factor	0.84
Percent Heavy Veh, %	0
Cap, veh/h	642
Arrive On Green	0.55
Sat Flow, veh/h	1557
Grp Volume(v), veh/h	106
Grp Sat Flow(s),veh/h/ln	1557
Q Serve(g_s), s	2.7
Cycle Q Clear(g_c), s	2.7
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	642
V/C Ratio(X)	0.16
Avail Cap(c_a), veh/h	642
HCM Platoon Ratio	1.33
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	11.2
Incr Delay (d2), s/veh	0.6
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	1.0
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	11.8
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	109	237	57	65	293	186	63	1052	52	155	1052	132
Future Volume (veh/h)	109	237	57	65	293	186	63	1052	52	155	1052	132
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	131	286	69	78	353	224	76	1267	63	187	1267	159
Peak Hour Factor	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83	0.83
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	166	675	160	130	406	343	129	1241	552	273	1528	676
Arrive On Green	0.09	0.23	0.23	0.07	0.21	0.21	0.07	0.34	0.34	0.15	0.42	0.42
Sat Flow, veh/h	1810	2892	686	1810	1900	1603	1810	3610	1605	1810	3610	1597
Grp Volume(v), veh/h	131	177	178	78	353	224	76	1267	63	187	1267	159
Grp Sat Flow(s),veh/h/ln	1810	1805	1773	1810	1900	1603	1810	1805	1605	1810	1805	1597
Q Serve(g_s), s	5.7	6.7	6.9	3.3	14.4	7.0	3.3	27.5	1.6	7.8	24.9	5.1
Cycle Q Clear(g_c), s	5.7	6.7	6.9	3.3	14.4	7.0	3.3	27.5	1.6	7.8	24.9	5.1
Prop In Lane	1.00		0.39	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	166	421	414	130	406	343	129	1241	552	273	1528	676
V/C Ratio(X)	0.79	0.42	0.43	0.60	0.87	0.65	0.59	1.02	0.11	0.68	0.83	0.24
Avail Cap(c_a), veh/h	249	421	414	249	439	371	192	1241	552	273	1528	676
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.44	0.44	0.44
Uniform Delay (d), s/veh	35.6	26.1	26.1	36.0	30.4	13.3	36.0	26.3	9.8	32.2	20.5	14.8
Incr Delay (d2), s/veh	9.5	0.7	0.7	4.3	16.1	3.7	4.2	31.0	0.4	3.1	2.4	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.9	2.9	2.9	1.6	8.1	0.4	1.6	16.4	0.8	3.6	10.2	1.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	45.1	26.7	26.8	40.3	46.4	17.0	40.2	57.2	10.2	35.3	22.9	15.1
LnGrp LOS	D	C	C	D	D	B	D	F	B	D	C	B
Approach Vol, veh/h		486			655			1406			1613	
Approach Delay, s/veh		31.7			35.6			54.2			23.6	
Approach LOS		C			D			D			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	31.1	31.5	9.8	22.7	9.7	37.9	11.3	21.1				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	27.5	11.0	18.5	8.5	26.0	11.0	18.5					
Max Q Clear Time (g_c+1/3), s	29.5	5.3	8.9	5.3	26.9	7.7	16.4					
Green Ext Time (p_c), s	0.0	0.0	0.1	1.4	0.0	0.0	0.1	0.7				

Intersection Summary

HCM 6th Ctrl Delay	36.8
HCM 6th LOS	D

HCM 6th TWSC
6: Dracaea Avenue & Project Driveway 2

Perris/Dracaea Commercial Project
Project Completion (2022) WP - AM Peak Hour

Intersection						
Int Delay, s/veh	3.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔	↔		↔	
Traffic Vol, veh/h	110	129	243	37	34	105
Future Vol, veh/h	110	129	243	37	34	105
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	110	129	243	37	34	105

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	280	0	-	0	611
Stage 1	-	-	-	-	262
Stage 2	-	-	-	-	349
Critical Hdwy	4.1	-	-	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	2.2	-	-	-	3.5
Pot Cap-1 Maneuver	1294	-	-	-	460
Stage 1	-	-	-	-	786
Stage 2	-	-	-	-	719
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1294	-	-	-	418
Mov Cap-2 Maneuver	-	-	-	-	418
Stage 1	-	-	-	-	714
Stage 2	-	-	-	-	719

Approach	EB	WB	SB
HCM Control Delay, s	3.7	0	12.1
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1294	-	-	-	645
HCM Lane V/C Ratio	0.085	-	-	-	0.216
HCM Control Delay (s)	8	0	-	-	12.1
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.3	-	-	-	0.8

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - AM Peak Hour

Intersection						
Int Delay, s/veh	1.6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	20	47	0	14	18	0
Future Vol, veh/h	20	47	0	14	18	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	24	57	0	17	22	0
























Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	81	0	70
Stage 1	-	-	-	-	53
Stage 2	-	-	-	-	17
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1529	-	939
Stage 1	-	-	-	-	975
Stage 2	-	-	-	-	1011
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1529	-	939
Mov Cap-2 Maneuver	-	-	-	-	939
Stage 1	-	-	-	-	975
Stage 2	-	-	-	-	1011

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	939	-	-	1529	-
HCM Lane V/C Ratio	0.023	-	-	-	-
HCM Control Delay (s)	8.9	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - PM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBL	SBT
Lane Configurations												
Traffic Volume (veh/h)	60	200	94	65	135	114	4	65	1122	73	103	1255
Future Volume (veh/h)	60	200	94	65	135	114	4	65	1122	73	103	1255
Initial Q (Qb), veh	0	0	0	0	0	0		0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00		1.00		0.98	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No				No			No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900		1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	63	211	99	68	142	120		68	1181	77	108	1321
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95		0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	0	0	0	0	0	0		0	0	0	0	0
Cap, veh/h	105	267	225	108	270	228		108	1624	708	344	2097
Arrive On Green	0.06	0.14	0.14	0.06	0.14	0.14		0.02	0.15	0.15	0.19	0.58
Sat Flow, veh/h	1810	1900	1603	1810	1900	1610		1810	3610	1573	1810	3610
Grp Volume(v), veh/h	63	211	99	68	142	120		68	1181	77	108	1321
Grp Sat Flow(s),veh/h/ln	1810	1900	1603	1810	1900	1610		1810	1805	1573	1810	1805
Q Serve(g_s), s	3.4	10.7	5.7	3.7	6.9	4.7		3.7	31.2	3.2	5.1	24.2
Cycle Q Clear(g_c), s	3.4	10.7	5.7	3.7	6.9	4.7		3.7	31.2	3.2	5.1	24.2
Prop In Lane	1.00		1.00	1.00		1.00		1.00		1.00	1.00	
Lane Grp Cap(c), veh/h	105	267	225	108	270	228		108	1625	708	344	2097
V/C Ratio(X)	0.60	0.79	0.44	0.63	0.53	0.53		0.63	0.73	0.11	0.31	0.63
Avail Cap(c_a), veh/h	199	475	401	127	399	338		163	1625	708	344	2097
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00		0.33	0.33	0.33	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	46.0	41.6	39.4	46.0	39.8	18.7		47.9	36.7	14.1	34.9	13.9
Incr Delay (d2), s/veh	5.5	5.2	1.4	7.5	1.6	1.9		6.0	2.9	0.3	0.5	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.7	5.4	2.3	1.9	3.3	2.8		1.9	15.7	1.6	2.3	9.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	51.4	46.8	40.7	53.5	41.4	20.5		53.9	39.6	14.4	35.4	15.3
LnGrp LOS	D	D	D	D	D	C		D	D	B	D	B
Approach Vol, veh/h		373			330				1326			1481
Approach Delay, s/veh		46.0			36.3				38.9			16.6
Approach LOS		D			D				D			B
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	23.0	49.0	9.9	18.0	9.9	62.1	9.8	18.2				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	45.0	7.0	25.0	9.0	43.0	11.0	21.0				
Max Q Clear Time (g_c+I1), s	7.1	33.2	5.7	12.7	5.7	26.2	5.4	8.9				
Green Ext Time (p_c), s	0.0	6.6	0.0	1.2	0.0	9.3	0.0	0.9				
Intersection Summary												
HCM 6th Ctrl Delay			30.0									
HCM 6th LOS			C									
Notes												
User approved ignoring U-Turning movement.												

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
 1: Perris Boulevard & Eucalyptus Avenue

Perris/Draacea Commercial Project
 Project Completion (2022) WP - PM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	49
Future Volume (veh/h)	49
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	52
Peak Hour Factor	0.95
Percent Heavy Veh, %	0
Cap, veh/h	911
Arrive On Green	0.58
Sat Flow, veh/h	1569
Grp Volume(v), veh/h	52
Grp Sat Flow(s),veh/h/ln	1569
Q Serve(g_s), s	1.4
Cycle Q Clear(g_c), s	1.4
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	911
V/C Ratio(X)	0.06
Avail Cap(c_a), veh/h	911
HCM Platoon Ratio	1.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	9.1
Incr Delay (d2), s/veh	0.1
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	0.5
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	9.2
LnGrp LOS	A
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaena Commercial Project
Project Completion (2022) WP - PM Peak Hour

Intersection												
Int Delay, s/veh	1.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗	↗	↕	↗	↗	↕	↗
Traffic Vol, veh/h	0	0	67	0	0	28	52	1248	2	82	1300	18
Future Vol, veh/h	0	0	67	0	0	28	52	1248	2	82	1300	18
Conflicting Peds, #/hr	0	0	0	0	0	0	4	0	0	0	0	4
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	150	-	50	125	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	69	0	0	29	54	1287	2	85	1340	19

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	-	-	674	-	-	644	1363	0	0	1289	0	0
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	6.9	-	-	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.3	-	-	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	0	0	402	0	0	420	511	-	-	545	-	-
Stage 1	0	0	-	0	0	-	-	-	-	-	-	-
Stage 2	0	0	-	0	0	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	400	-	-	420	509	-	-	545	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	15.9		14.2		0.5		0.8	
HCM LOS	C		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	509	-	-	400	420	545	-	-
HCM Lane V/C Ratio	0.105	-	-	0.173	0.069	0.155	-	-
HCM Control Delay (s)	12.9	-	-	15.9	14.2	12.8	-	-
HCM Lane LOS	B	-	-	C	B	B	-	-
HCM 95th %tile Q(veh)	0.4	-	-	0.6	0.2	0.5	-	-

HCM 6th TWSC Perris/Dracaean Commercial Project
 3: Perris Boulevard & Auto Center Driveway/Project Driveway Project Completion (2022) WP - PM Peak Hour

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗		↕↔			↕↔↔	
Traffic Vol, veh/h	0	0	0	0	0	39	0	1263	20	0	1359	2
Future Vol, veh/h	0	0	0	0	0	39	0	1263	20	0	1359	2
Conflicting Peds, #/hr	0	0	0	0	0	0	3	0	0	0	0	3
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	-	-	-	-	-	25
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	0	0	0	41	0	1316	21	0	1416	2

Major/Minor	Minor2		Minor1		Major1		Major2	
Conflicting Flow All	-	-	712	-	-	669	-	0
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	7.1	-	-	6.9	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.9	-	-	3.3	-	-
Pot Cap-1 Maneuver	0	0	*595	0	0	*546	0	-
Stage 1	0	0	-	0	0	-	0	-
Stage 2	0	0	-	0	0	-	0	-
Platoon blocked, %			1			1		-
Mov Cap-1 Maneuver	-	-	*593	-	-	*546	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	12.1	0	0
HCM LOS	A	B		

Minor Lane/Major Mvmt	NBT	NBR	EBLn1WBLn1	SBT	SBR
Capacity (veh/h)	-	-	-	546	-
HCM Lane V/C Ratio	-	-	-	0.074	-
HCM Control Delay (s)	-	-	0	12.1	-
HCM Lane LOS	-	-	A	B	-
HCM 95th %tile Q(veh)	-	-	-	0.2	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBU	SBL	SBT
Lane Configurations		↕	↗		↔		↖	↕			↘	↕
Traffic Volume (veh/h)	74	129	23	83	99	50	30	1134	70	24	64	1199
Future Volume (veh/h)	74	129	23	83	99	50	30	1134	70	24	64	1199
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900		1900	1900
Adj Flow Rate, veh/h	77	134	24	86	103	52	31	1181	73		67	1249
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96		0.96	0.96
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0		0	0
Cap, veh/h	155	250	428	123	141	59	206	1912	118		107	1805
Arrive On Green	0.27	0.27	0.27	0.27	0.27	0.27	0.23	1.00	1.00		0.12	1.00
Sat Flow, veh/h	397	938	1608	280	529	223	1810	3447	213		1810	3610
Grp Volume(v), veh/h	211	0	24	241	0	0	31	618	636		67	1249
Grp Sat Flow(s),veh/h/ln	1334	0	1608	1032	0	0	1810	1805	1855		1810	1805
Q Serve(g_s), s	0.0	0.0	1.1	10.9	0.0	0.0	1.4	0.0	0.0		3.5	0.0
Cycle Q Clear(g_c), s	13.1	0.0	1.1	24.0	0.0	0.0	1.4	0.0	0.0		3.5	0.0
Prop In Lane	0.36		1.00	0.36		0.22	1.00		0.11		1.00	
Lane Grp Cap(c), veh/h	404	0	428	324	0	0	206	1001	1029		107	1805
V/C Ratio(X)	0.52	0.00	0.06	0.74	0.00	0.00	0.15	0.62	0.62		0.63	0.69
Avail Cap(c_a), veh/h	474	0	499	391	0	0	206	1001	1029		127	1805
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00		2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.80	0.80	0.80		1.00	1.00
Uniform Delay (d), s/veh	31.2	0.0	27.3	37.6	0.0	0.0	34.8	0.0	0.0		43.0	0.0
Incr Delay (d2), s/veh	1.0	0.0	0.1	6.1	0.0	0.0	0.3	2.3	2.2		7.1	2.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0
%ile BackOfQ(50%),veh/ln	4.4	0.0	0.4	6.3	0.0	0.0	0.6	0.6	0.6		1.7	0.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	32.2	0.0	27.4	43.7	0.0	0.0	35.0	2.3	2.2		50.1	2.2
LnGrp LOS	C	A	C	D	A	A	D	A	A		D	A
Approach Vol, veh/h		235			241			1285				1391
Approach Delay, s/veh		31.7			43.7			3.1				4.4
Approach LOS		C			D			A				A
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.9	59.5		30.6	15.4	54.0		30.6				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	7.0	50.0		31.0	7.0	50.0		31.0				
Max Q Clear Time (g_c+I1), s	5.5	2.0		15.1	3.4	2.0		26.0				
Green Ext Time (p_c), s	0.0	11.9		1.1	0.0	13.6		0.6				

Intersection Summary

HCM 6th Ctrl Delay	8.9
HCM 6th LOS	A

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
 Project Completion (2022) WP - PM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	72
Future Volume (veh/h)	72
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	75
Peak Hour Factor	0.96
Percent Heavy Veh, %	0
Cap, veh/h	784
Arrive On Green	1.00
Sat Flow, veh/h	1568
Grp Volume(v), veh/h	75
Grp Sat Flow(s),veh/h/ln	1568
Q Serve(g_s), s	0.0
Cycle Q Clear(g_c), s	0.0
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	784
V/C Ratio(X)	0.10
Avail Cap(c_a), veh/h	784
HCM Platoon Ratio	2.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	0.0
Incr Delay (d2), s/veh	0.2
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	0.1
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	0.2
LnGrp LOS	A
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	108	209	80	24	142	102	68	1084	33	114	1153	98
Future Volume (veh/h)	108	209	80	24	142	102	68	1084	33	114	1153	98
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.97	1.00		0.97	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	110	213	82	24	145	104	69	1106	34	116	1177	100
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	141	396	147	62	209	173	108	1264	549	546	2138	930
Arrive On Green	0.08	0.15	0.15	0.03	0.11	0.11	0.06	0.35	0.35	0.60	1.00	1.00
Sat Flow, veh/h	1810	2568	956	1810	1900	1566	1810	3610	1570	1810	3610	1570
Grp Volume(v), veh/h	110	148	147	24	145	104	69	1106	34	116	1177	100
Grp Sat Flow(s),veh/h/ln	1810	1805	1719	1810	1900	1566	1810	1805	1570	1810	1805	1570
Q Serve(g_s), s	6.0	7.5	7.9	1.3	7.4	3.6	3.7	28.7	1.2	2.9	0.0	0.0
Cycle Q Clear(g_c), s	6.0	7.5	7.9	1.3	7.4	3.6	3.7	28.7	1.2	2.9	0.0	0.0
Prop In Lane	1.00		0.56	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	141	278	265	62	209	173	108	1264	549	546	2138	930
V/C Ratio(X)	0.78	0.53	0.56	0.39	0.69	0.60	0.64	0.88	0.06	0.21	0.55	0.11
Avail Cap(c_a), veh/h	407	433	413	308	352	290	127	1264	549	546	2138	930
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.83	0.83	0.83
Uniform Delay (d), s/veh	45.3	39.0	39.1	47.3	42.9	13.8	46.0	30.5	14.7	14.4	0.0	0.0
Incr Delay (d2), s/veh	9.0	1.6	1.8	4.0	4.1	3.3	8.0	8.7	0.2	0.2	0.9	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	8.0	3.4	3.5	0.6	3.7	2.5	1.9	13.6	0.6	1.2	0.3	0.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	54.2	40.5	41.0	51.2	46.9	17.2	54.0	39.1	14.9	14.6	0.9	0.2
LnGrp LOS	D	D	D	D	D	B	D	D	B	B	A	A
Approach Vol, veh/h		405			273			1209			1393	
Approach Delay, s/veh		44.4			36.0			39.3			1.9	
Approach LOS		D			D			D			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	34.2	39.0	7.4	19.4	10.0	63.2	11.8	15.0				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	35.0	35.0	17.0	24.0	7.0	36.0	22.5	18.5				
Max Q Clear Time (g_c+1), s	30.7	30.7	3.3	9.9	5.7	2.0	8.0	9.4				
Green Ext Time (p_c), s	0.1	2.8	0.0	1.4	0.0	11.5	0.2	0.7				

Intersection Summary

HCM 6th Ctrl Delay	23.8
HCM 6th LOS	C

HCM 6th TWSC
6: Dracaea Avenue & Project Driveway 2

Perris/Dracaea Commercial Project
Project Completion (2022) WP - PM Peak Hour

Intersection						
Int Delay, s/veh	2.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔	↔		↔	
Traffic Vol, veh/h	71	192	158	24	24	75
Future Vol, veh/h	71	192	158	24	24	75
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	71	192	158	24	24	75

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	182	0	0	504	170
Stage 1	-	-	-	170	-
Stage 2	-	-	-	334	-
Critical Hdwy	4.1	-	-	6.4	6.2
Critical Hdwy Stg 1	-	-	-	5.4	-
Critical Hdwy Stg 2	-	-	-	5.4	-
Follow-up Hdwy	2.2	-	-	3.5	3.3
Pot Cap-1 Maneuver	1405	-	-	531	879
Stage 1	-	-	-	865	-
Stage 2	-	-	-	730	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	1405	-	-	501	879
Mov Cap-2 Maneuver	-	-	-	501	-
Stage 1	-	-	-	816	-
Stage 2	-	-	-	730	-

Approach	EB	WB	SB
HCM Control Delay, s	2.1	0	10.6
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1405	-	-	-	743
HCM Lane V/C Ratio	0.051	-	-	-	0.133
HCM Control Delay (s)	7.7	0	-	-	10.6
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.2	-	-	-	0.5

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - PM Peak Hour

Intersection						
Int Delay, s/veh	0.9					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	45	39	0	17	11	0
Future Vol, veh/h	45	39	0	17	11	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	46	40	0	18	11	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	86	0	84 66
Stage 1	-	-	-	-	66 -
Stage 2	-	-	-	-	18 -
Critical Hdwy	-	-	4.1	-	6.4 6.2
Critical Hdwy Stg 1	-	-	-	-	5.4 -
Critical Hdwy Stg 2	-	-	-	-	5.4 -
Follow-up Hdwy	-	-	2.2	-	3.5 3.3
Pot Cap-1 Maneuver	-	-	1523	-	923 1003
Stage 1	-	-	-	-	962 -
Stage 2	-	-	-	-	1010 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1523	-	923 1003
Mov Cap-2 Maneuver	-	-	-	-	923 -
Stage 1	-	-	-	-	962 -
Stage 2	-	-	-	-	1010 -

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	923	-	-	1523	-
HCM Lane V/C Ratio	0.012	-	-	-	-
HCM Control Delay (s)	8.9	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘	↖	↑↑	↘	↖	↑↑	↘
Traffic Volume (veh/h)	58	170	150	92	401	216	129	1274	65	90	1438	92
Future Volume (veh/h)	58	170	150	92	401	216	129	1274	65	90	1438	92
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.99	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	58	170	150	92	401	216	129	1274	65	90	1438	92
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	115	241	200	303	439	371	158	1421	631	203	1510	672
Arrive On Green	0.06	0.13	0.13	0.17	0.23	0.23	0.17	0.79	0.79	0.11	0.42	0.42
Sat Flow, veh/h	1810	1900	1581	1810	1900	1608	1810	3610	1602	1810	3610	1606
Grp Volume(v), veh/h	58	170	150	92	401	216	129	1274	65	90	1438	92
Grp Sat Flow(s),veh/h/ln	1810	1900	1581	1810	1900	1608	1810	1805	1602	1810	1805	1606
Q Serve(g_s), s	2.5	6.9	5.8	3.6	16.5	6.9	5.5	20.4	0.8	3.7	30.8	2.8
Cycle Q Clear(g_c), s	2.5	6.9	5.8	3.6	16.5	6.9	5.5	20.4	0.8	3.7	30.8	2.8
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	115	241	200	303	439	371	158	1421	631	203	1510	672
V/C Ratio(X)	0.51	0.71	0.75	0.30	0.91	0.58	0.81	0.90	0.10	0.44	0.95	0.14
Avail Cap(c_a), veh/h	158	439	366	303	439	372	158	1421	631	203	1510	672
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	36.3	33.5	20.8	29.2	30.0	14.3	32.4	7.3	5.2	33.2	22.5	14.4
Incr Delay (d2), s/veh	3.4	3.8	5.5	0.6	23.4	2.3	26.8	9.1	0.3	1.5	14.3	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	3.3	3.0	1.6	10.1	2.6	3.3	4.5	0.3	1.7	15.0	1.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.7	37.3	26.3	29.8	53.4	16.6	59.2	16.5	5.6	34.7	36.8	14.8
LnGrp LOS	D	D	C	C	D	B	E	B	A	C	D	B
Approach Vol, veh/h		378			709			1468			1620	
Approach Delay, s/veh		33.3			39.1			19.7			35.5	
Approach LOS		C			D			B			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	13.0	35.5	17.4	14.1	11.0	37.5	9.1	22.5				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	31.5	7.0	18.5	7.0	31.5	7.0	18.5				
Max Q Clear Time (g_c+I1), s	5.7	22.4	5.6	8.9	7.5	32.8	4.5	18.5				
Green Ext Time (p_c), s	0.0	5.8	0.0	1.0	0.0	0.0	0.0	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			30.4									
HCM 6th LOS			C									

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour

Intersection												
Int Delay, s/veh	163.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑	↕	↕	↑↑	↕
Traffic Vol, veh/h	16	3	43	50	11	59	61	1445	19	56	1654	18
Future Vol, veh/h	16	3	43	50	11	59	61	1445	19	56	1654	18
Conflicting Peds, #/hr	2	0	0	0	0	2	7	0	0	0	0	7
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	50	50	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	16	3	43	50	11	59	61	1445	19	56	1654	18

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2625	3359	834	2508	3358	725	1679	0	0	1464	0	0
Stage 1	1773	1773	-	1567	1567	-	-	-	-	-	-	-
Stage 2	852	1586	-	941	1791	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 12	8	316	~ 15	~ 8	372	387	-	-	467	-	-
Stage 1	88	137	-	118	173	-	-	-	-	-	-	-
Stage 2	325	170	-	287	134	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	-	6	314	~ 6	~ 6	371	384	-	-	467	-	-
Mov Cap-2 Maneuver	-	6	-	~ 6	~ 6	-	-	-	-	-	-	-
Stage 1	74	120	-	99	145	-	-	-	-	-	-	-
Stage 2	212	143	-	213	117	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s		\$ 4664.7	0.6	0.4
HCM LOS	-	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	384	-	-	-	12	467	-
HCM Lane V/C Ratio	0.159	-	-	-	10	0.12	-
HCM Control Delay (s)	16.1	-	-	\$ 4664.7	13.8	-	-
HCM Lane LOS	C	-	-	-	F	B	-
HCM 95th %tile Q(veh)	0.6	-	-	-	16.3	0.4	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
3: Perris Boulevard & Auto Center Driveway

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T T T		
Traffic Vol, veh/h	0	1	0	1525	1747	7
Future Vol, veh/h	0	1	0	1525	1747	7
Conflicting Peds, #/hr	0	0	11	0	0	11
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	25
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	1	0	1525	1747	7

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2525	888	1765	0	-	0
Stage 1	1762	-	-	-	-	-
Stage 2	763	-	-	-	-	-
Critical Hdwy	6.25	7.1	5.3	-	-	-
Critical Hdwy Stg 1	6.6	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.65	3.9	3.1	-	-	-
Pot Cap-1 Maneuver	34	249	168	-	-	-
Stage 1	84	-	-	-	-	-
Stage 2	414	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	33	246	166	-	-	-
Mov Cap-2 Maneuver	70	-	-	-	-	-
Stage 1	83	-	-	-	-	-
Stage 2	410	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	19.7	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	166	-	246	-	-
HCM Lane V/C Ratio	-	-	0.004	-	-
HCM Control Delay (s)	0	-	19.7	-	-
HCM Lane LOS	A	-	C	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↖	↗		↔		↖	↕		↗	↕	↗
Traffic Volume (veh/h)	92	79	60	77	147	91	75	1342	22	43	1527	177
Future Volume (veh/h)	92	79	60	77	147	91	75	1342	22	43	1527	177
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		0.99	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	92	79	60	77	147	91	75	1342	22	43	1527	177
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	206	158	468	116	193	103	264	1499	25	264	1489	642
Arrive On Green	0.29	0.29	0.29	0.29	0.29	0.29	0.19	0.55	0.55	0.19	0.55	0.55
Sat Flow, veh/h	468	542	1604	204	663	352	1810	3634	60	1810	3610	1557
Grp Volume(v), veh/h	171	0	60	315	0	0	75	666	698	43	1527	177
Grp Sat Flow(s),veh/h/ln	1010	0	1604	1220	0	0	1810	1805	1888	1810	1805	1557
Q Serve(g_s), s	0.0	0.0	2.2	9.6	0.0	0.0	2.8	26.2	26.2	1.6	33.0	4.8
Cycle Q Clear(g_c), s	11.2	0.0	2.2	20.8	0.0	0.0	2.8	26.2	26.2	1.6	33.0	4.8
Prop In Lane	0.54		1.00	0.24		0.29	1.00		0.03	1.00		1.00
Lane Grp Cap(c), veh/h	364	0	468	412	0	0	264	745	779	264	1489	642
V/C Ratio(X)	0.47	0.00	0.13	0.77	0.00	0.00	0.28	0.89	0.90	0.16	1.03	0.28
Avail Cap(c_a), veh/h	404	0	513	457	0	0	264	745	779	264	1489	642
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33	1.33	1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.58	0.58	0.58	1.00	1.00	1.00
Uniform Delay (d), s/veh	23.4	0.0	20.9	28.1	0.0	0.0	28.7	16.5	16.5	28.2	18.1	11.7
Incr Delay (d2), s/veh	0.9	0.0	0.1	6.9	0.0	0.0	0.3	9.9	9.6	0.3	30.0	1.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.7	0.0	0.8	6.4	0.0	0.0	1.2	10.1	10.5	0.7	16.4	1.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	24.4	0.0	21.0	34.9	0.0	0.0	29.0	26.4	26.1	28.5	48.1	12.8
LnGrp LOS	C	A	C	C	A	A	C	C	C	C	F	B
Approach Vol, veh/h		231			315			1439			1747	
Approach Delay, s/veh		23.5			34.9			26.4			44.0	
Approach LOS		C			C			C			D	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	15.7	37.0		27.3	15.7	37.0		27.3				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	9.4	33.0		25.6	9.4	33.0		25.6				
Max Q Clear Time (g_c+I1), s	3.6	28.2		13.2	4.8	35.0		22.8				
Green Ext Time (p_c), s	0.0	3.4		0.9	0.0	0.0		0.5				

Intersection Summary

HCM 6th Ctrl Delay	35.2
HCM 6th LOS	D

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) NP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↗	↕		↖	↕	↗	↖	↕	↗	↖	↕	↗
Traffic Volume (veh/h)	97	239	69	90	349	172	96	1213	78	162	1383	139
Future Volume (veh/h)	97	239	69	90	349	172	96	1213	78	162	1383	139
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	97	239	69	90	349	172	96	1213	78	162	1383	139
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	140	617	174	137	419	353	140	1151	512	332	1535	679
Arrive On Green	0.08	0.22	0.22	0.08	0.22	0.22	0.08	0.32	0.32	0.24	0.57	0.57
Sat Flow, veh/h	1810	2777	783	1810	1900	1604	1810	3610	1605	1810	3610	1597
Grp Volume(v), veh/h	97	153	155	90	349	172	96	1213	78	162	1383	139
Grp Sat Flow(s),veh/h/ln	1810	1805	1755	1810	1900	1604	1810	1805	1605	1810	1805	1597
Q Serve(g_s), s	4.2	5.8	6.0	3.9	14.0	4.8	4.1	25.5	2.1	6.1	27.2	3.4
Cycle Q Clear(g_c), s	4.2	5.8	6.0	3.9	14.0	4.8	4.1	25.5	2.1	6.1	27.2	3.4
Prop In Lane	1.00		0.45	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	140	401	390	137	419	353	140	1151	512	332	1535	679
V/C Ratio(X)	0.69	0.38	0.40	0.66	0.83	0.49	0.69	1.05	0.15	0.49	0.90	0.20
Avail Cap(c_a), veh/h	192	519	505	192	546	461	158	1151	512	332	1535	679
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.54	0.54	0.54
Uniform Delay (d), s/veh	36.0	26.5	26.5	36.0	29.8	11.0	36.0	27.2	10.7	27.0	15.9	10.7
Incr Delay (d2), s/veh	6.2	0.6	0.7	5.3	8.4	1.0	10.1	42.0	0.6	0.6	5.2	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.0	2.5	2.5	1.9	7.2	2.7	2.2	17.1	1.1	2.6	9.3	1.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	42.2	27.1	27.2	41.2	38.2	12.1	46.1	69.3	11.4	27.6	21.1	11.1
LnGrp LOS	D	C	C	D	D	B	D	F	B	C	C	B
Approach Vol, veh/h		405			611			1387			1684	
Approach Delay, s/veh		30.7			31.3			64.4			20.9	
Approach LOS		C			C			E			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	18.7	29.5	10.1	21.8	10.2	38.0	10.2	21.6				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	25.5	8.5	23.0	7.0	25.5	8.5	23.0				
Max Q Clear Time (g_c+I1), s	8.1	27.5	5.9	8.0	6.1	29.2	6.2	16.0				
Green Ext Time (p_c), s	0.0	0.0	0.0	1.5	0.0	0.0	0.0	1.6				

Intersection Summary												
HCM 6th Ctrl Delay											38.2	
HCM 6th LOS											D	

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	76	1	0	119	2	0
Future Vol, veh/h	76	1	0	119	2	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	76	1	0	119	2	0

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	77	0	196 77
Stage 1	-	-	-	-	77 -
Stage 2	-	-	-	-	119 -
Critical Hdwy	-	-	4.1	-	6.4 6.2
Critical Hdwy Stg 1	-	-	-	-	5.4 -
Critical Hdwy Stg 2	-	-	-	-	5.4 -
Follow-up Hdwy	-	-	2.2	-	3.5 3.3
Pot Cap-1 Maneuver	-	-	1535	-	797 990
Stage 1	-	-	-	-	951 -
Stage 2	-	-	-	-	911 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1535	-	797 990
Mov Cap-2 Maneuver	-	-	-	-	797 -
Stage 1	-	-	-	-	951 -
Stage 2	-	-	-	-	911 -

Approach	EB	WB	NB
HCM Control Delay, s	0	0	9.5
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	797	-	-	1535	-
HCM Lane V/C Ratio	0.003	-	-	-	-
HCM Control Delay (s)	9.5	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	149	516	265	78	301	140	147	1551	88	111	1490	100
Future Volume (veh/h)	149	516	265	78	301	140	147	1551	88	111	1490	100
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	149	516	265	78	301	140	147	1551	88	111	1490	100
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	180	475	402	112	404	342	163	1624	708	141	1581	686
Arrive On Green	0.10	0.25	0.25	0.06	0.21	0.21	0.03	0.15	0.15	0.08	0.44	0.44
Sat Flow, veh/h	1810	1900	1606	1810	1900	1610	1810	3610	1573	1810	3610	1566
Grp Volume(v), veh/h	149	516	265	78	301	140	147	1551	88	111	1490	100
Grp Sat Flow(s),veh/h/ln	1810	1900	1606	1810	1900	1610	1810	1805	1573	1810	1805	1566
Q Serve(g_s), s	8.1	25.0	14.8	4.2	14.8	6.0	8.1	42.6	3.6	6.0	39.5	3.8
Cycle Q Clear(g_c), s	8.1	25.0	14.8	4.2	14.8	6.0	8.1	42.6	3.6	6.0	39.5	3.8
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	180	475	402	112	404	342	163	1625	708	141	1581	686
V/C Ratio(X)	0.83	1.09	0.66	0.70	0.75	0.41	0.90	0.95	0.12	0.79	0.94	0.15
Avail Cap(c_a), veh/h	199	475	402	127	404	342	163	1625	708	141	1581	686
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	44.2	37.5	33.7	46.0	36.8	21.7	48.1	41.6	14.0	45.3	26.9	16.9
Incr Delay (d2), s/veh	22.5	66.6	4.0	13.3	7.4	0.8	43.5	13.9	0.4	24.8	12.5	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.7	20.2	6.1	2.3	7.6	2.9	5.8	23.6	1.9	3.7	18.9	1.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	66.7	104.1	37.6	59.3	44.2	22.5	91.6	55.5	14.4	70.1	39.4	17.3
LnGrp LOS	E	F	D	E	D	C	F	E	B	E	D	B
Approach Vol, veh/h		930			519			1786			1701	
Approach Delay, s/veh		79.2			40.6			56.4			40.1	
Approach LOS		E			D			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.8	49.0	10.2	29.0	13.0	47.8	14.0	25.2				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	45.0	7.0	25.0	9.0	43.0	11.0	21.0				
Max Q Clear Time (g_c+I1), s	8.0	44.6	6.2	27.0	10.1	41.5	10.1	16.8				
Green Ext Time (p_c), s	0.0	0.3	0.0	0.0	0.0	1.3	0.0	0.9				
Intersection Summary												
HCM 6th Ctrl Delay			53.4									
HCM 6th LOS			D									

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour

Intersection												
Int Delay, s/veh	1.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↑↑	↕	↕	↑↑	↕
Traffic Vol, veh/h	30	3	51	40	4	58	74	1855	12	153	1776	18
Future Vol, veh/h	30	3	51	40	4	58	74	1855	12	153	1776	18
Conflicting Peds, #/hr	0	0	0	0	0	0	4	0	0	0	0	4
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	50	-	50	50	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	30	3	51	40	4	58	74	1855	12	153	1776	18

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	3164	4101	892	3199	4107	928	1798	0	0	1867	0	0
Stage 1	2086	2086	-	2003	2003	-	-	-	-	-	-	-
Stage 2	1078	2015	-	1196	2104	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 5	3	289	~ 4	~ 2	274	348	-	-	327	-	-
Stage 1	56	96	-	63	105	-	-	-	-	-	-	-
Stage 2	237	104	-	201	94	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	-	~ 1	288	-	~ 1	274	347	-	-	327	-	-
Mov Cap-2 Maneuver	-	~ 1	-	-	~ 1	-	-	-	-	-	-	-
Stage 1	44	51	-	50	83	-	-	-	-	-	-	-
Stage 2	140	82	-	83	50	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s					0.7		2	
HCM LOS								

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	347	-	-	-	327	-	-
HCM Lane V/C Ratio	0.213	-	-	-	0.468	-	-
HCM Control Delay (s)	18.2	-	-	-	25.3	-	-
HCM Lane LOS	C	-	-	-	D	-	-
HCM 95th %tile Q(veh)	0.8	-	-	-	2.4	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
3: Perris Boulevard & Auto Center Driveway

Perris/Draacea Commercial Project
GPBO (2040) NP - PM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T T		
Traffic Vol, veh/h	0	0	0	1941	1867	2
Future Vol, veh/h	0	0	0	1941	1867	2
Conflicting Peds, #/hr	0	0	3	0	0	3
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	25
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	0	0	1941	1867	2

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2842	938	1872	0	-	0
Stage 1	1871	-	-	-	-	-
Stage 2	971	-	-	-	-	-
Critical Hdwy	6.25	7.1	5.3	-	-	-
Critical Hdwy Stg 1	6.6	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.65	3.9	3.1	-	-	-
Pot Cap-1 Maneuver	*345	*488	*615	-	-	-
Stage 1	*522	-	-	-	-	-
Stage 2	*238	-	-	-	-	-
Platoon blocked, %	1	1	1	-	-	-
Mov Cap-1 Maneuver	*343	*487	*613	-	-	-
Mov Cap-2 Maneuver	*72	-	-	-	-	-
Stage 1	*520	-	-	-	-	-
Stage 2	*237	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	* 613	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↖	↗		↔		↖	↗		↖	↗	↗
Traffic Volume (veh/h)	258	344	81	48	118	72	50	1611	53	70	1679	118
Future Volume (veh/h)	258	344	81	48	118	72	50	1611	53	70	1679	118
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.97	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	258	344	81	48	118	72	50	1611	53	70	1679	118
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	157	141	499	43	96	42	127	1817	60	109	1805	784
Arrive On Green	0.31	0.31	0.31	0.31	0.31	0.31	0.07	0.51	0.51	0.12	1.00	1.00
Sat Flow, veh/h	341	454	1609	0	311	135	1810	3564	117	1810	3610	1568
Grp Volume(v), veh/h	602	0	81	238	0	0	50	813	851	70	1679	118
Grp Sat Flow(s),veh/h/ln	795	0	1609	446	0	0	1810	1805	1875	1810	1805	1568
Q Serve(g_s), s	0.0	0.0	3.7	0.0	0.0	0.0	2.6	40.2	40.7	3.7	0.0	0.0
Cycle Q Clear(g_c), s	31.0	0.0	3.7	31.0	0.0	0.0	2.6	40.2	40.7	3.7	0.0	0.0
Prop In Lane	0.43		1.00	0.20		0.30	1.00		0.06	1.00		1.00
Lane Grp Cap(c), veh/h	298	0	499	182	0	0	127	921	956	109	1805	784
V/C Ratio(X)	2.02	0.00	0.16	1.31	0.00	0.00	0.39	0.88	0.89	0.64	0.93	0.15
Avail Cap(c_a), veh/h	298	0	499	182	0	0	127	921	956	127	1805	784
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.47	0.47	0.47	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.2	0.0	25.1	29.3	0.0	0.0	44.5	21.9	22.0	43.0	0.0	0.0
Incr Delay (d2), s/veh	471.4	0.0	0.2	173.7	0.0	0.0	0.9	6.2	6.3	8.5	10.1	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	46.4	0.0	1.4	12.0	0.0	0.0	1.2	17.5	18.4	1.8	2.5	0.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	508.6	0.0	25.2	203.0	0.0	0.0	45.4	28.1	28.3	51.5	10.1	0.4
LnGrp LOS	F	A	C	F	A	A	D	C	C	D	B	A
Approach Vol, veh/h		683			238			1714			1867	
Approach Delay, s/veh		451.3			203.0			28.7			11.0	
Approach LOS		F			F			C			B	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.0	55.0		35.0	11.0	54.0		35.0				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	7.0	50.0		31.0	7.0	50.0		31.0				
Max Q Clear Time (g_c+I1), s	5.7	42.7		33.0	4.6	2.0		33.0				
Green Ext Time (p_c), s	0.0	5.7		0.0	0.0	22.5		0.0				

Intersection Summary

HCM 6th Ctrl Delay	94.7
HCM 6th LOS	F

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	190	374	137	41	221	173	80	1408	43	165	1567	126
Future Volume (veh/h)	190	374	137	41	221	173	80	1408	43	165	1567	126
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.98	1.00		0.97	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	190	374	137	41	221	173	80	1408	43	165	1567	126
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	227	581	210	86	277	229	113	1264	549	396	1827	794
Arrive On Green	0.13	0.22	0.22	0.05	0.15	0.15	0.06	0.35	0.35	0.29	0.67	0.67
Sat Flow, veh/h	1810	2594	937	1810	1900	1572	1810	3610	1570	1810	3610	1569
Grp Volume(v), veh/h	190	259	252	41	221	173	80	1408	43	165	1567	126
Grp Sat Flow(s),veh/h/ln	1810	1805	1725	1810	1900	1572	1810	1805	1570	1810	1805	1569
Q Serve(g_s), s	10.3	13.0	13.3	2.2	11.2	6.9	4.3	35.0	1.5	7.4	33.6	2.9
Cycle Q Clear(g_c), s	10.3	13.0	13.3	2.2	11.2	6.9	4.3	35.0	1.5	7.4	33.6	2.9
Prop In Lane	1.00		0.54	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	227	404	386	86	277	229	113	1264	549	396	1827	794
V/C Ratio(X)	0.84	0.64	0.65	0.48	0.80	0.75	0.71	1.11	0.08	0.42	0.86	0.16
Avail Cap(c_a), veh/h	407	433	414	308	352	291	127	1264	549	396	1827	794
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.40	0.40	0.40
Uniform Delay (d), s/veh	42.7	35.2	35.3	46.4	41.3	17.3	46.0	32.5	14.0	30.3	13.5	8.5
Incr Delay (d2), s/veh	7.9	2.9	3.3	4.0	9.7	8.2	14.7	62.8	0.3	0.3	2.3	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	6.0	5.9	1.1	6.0	3.0	2.4	25.8	0.7	3.1	10.3	1.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	50.6	38.0	38.6	50.4	50.9	25.5	60.7	95.3	14.3	30.6	15.8	8.7
LnGrp LOS	D	D	D	D	D	C	E	F	B	C	B	A
Approach Vol, veh/h		701			435			1531			1858	
Approach Delay, s/veh		41.7			40.8			91.2			16.7	
Approach LOS		D			D			F			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	35.9	39.0	8.8	26.4	10.2	54.6	16.6	18.6				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	35.0	35.0	17.0	24.0	7.0	36.0	22.5	18.5				
Max Q Clear Time (g_c+1), s	19.4	37.0	4.2	15.3	6.3	35.6	12.3	13.2				
Green Ext Time (p_c), s	0.0	0.0	0.0	2.0	0.0	0.4	0.4	0.9				

Intersection Summary

HCM 6th Ctrl Delay	48.1
HCM 6th LOS	D

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	165	2	0	101	1	0
Future Vol, veh/h	165	2	0	101	1	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	165	2	0	101	1	0
























Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	167	0	267
Stage 1	-	-	-	-	166
Stage 2	-	-	-	-	101
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1423	-	727
Stage 1	-	-	-	-	868
Stage 2	-	-	-	-	928
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1423	-	727
Mov Cap-2 Maneuver	-	-	-	-	727
Stage 1	-	-	-	-	868
Stage 2	-	-	-	-	928

Approach	EB	WB	NB
HCM Control Delay, s	0	0	10
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	727	-	-	1423	-
HCM Lane V/C Ratio	0.001	-	-	-	-
HCM Control Delay (s)	10	-	-	0	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBL	SBT
Lane Configurations												
Traffic Volume (veh/h)	61	170	167	109	401	216	61	145	1279	81	90	1446
Future Volume (veh/h)	61	170	167	109	401	216	61	145	1279	81	90	1446
Initial Q (Qb), veh	0	0	0	0	0	0		0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00		1.00		0.99	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No				No			No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900		1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	61	170	167	109	401	216		145	1279	81	90	1446
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0		0	0	0	0	0
Cap, veh/h	118	242	201	305	439	371		158	1421	631	200	1504
Arrive On Green	0.06	0.13	0.13	0.17	0.23	0.23		0.17	0.79	0.79	0.11	0.42
Sat Flow, veh/h	1810	1900	1581	1810	1900	1608		1810	3610	1602	1810	3610
Grp Volume(v), veh/h	61	170	167	109	401	216		145	1279	81	90	1446
Grp Sat Flow(s),veh/h/ln	1810	1900	1581	1810	1900	1608		1810	1805	1602	1810	1805
Q Serve(g_s), s	2.6	6.9	6.5	4.3	16.5	6.9		6.3	20.7	1.0	3.7	31.2
Cycle Q Clear(g_c), s	2.6	6.9	6.5	4.3	16.5	6.9		6.3	20.7	1.0	3.7	31.2
Prop In Lane	1.00		1.00	1.00		1.00		1.00		1.00	1.00	
Lane Grp Cap(c), veh/h	118	242	201	305	439	371		158	1421	631	200	1504
V/C Ratio(X)	0.52	0.70	0.83	0.36	0.91	0.58		0.92	0.90	0.13	0.45	0.96
Avail Cap(c_a), veh/h	158	439	366	305	439	372		158	1421	631	200	1504
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00		2.00	2.00	2.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	36.2	33.5	21.0	29.4	30.0	14.4		32.7	7.3	5.3	33.3	22.7
Incr Delay (d2), s/veh	3.5	3.7	8.5	0.7	23.4	2.3		47.6	9.4	0.4	1.6	15.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	3.3	0.5	1.9	10.1	2.6		4.5	4.6	0.4	1.7	15.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.7	37.2	29.5	30.1	53.4	16.7		80.3	16.8	5.7	34.9	38.4
LnGrp LOS	D	D	C	C	D	B		F	B	A	C	D
Approach Vol, veh/h		398			726				1505			1628
Approach Delay, s/veh		34.3			39.0				22.3			36.9
Approach LOS		C			D				C			D
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.8	35.5	17.5	14.2	11.0	37.3	9.2	22.5				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	31.5	7.0	18.5	7.0	31.5	7.0	18.5				
Max Q Clear Time (g_c+I1), s	5.7	22.7	6.3	8.9	8.3	33.2	4.6	18.5				
Green Ext Time (p_c), s	0.0	5.7	0.0	1.1	0.0	0.0	0.0	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			31.8									
HCM 6th LOS			C									
Notes												
User approved ignoring U-Turning movement.												

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
 1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
 GPBO (2040) WP - AM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	92
Future Volume (veh/h)	92
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	1.00
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	92
Peak Hour Factor	1.00
Percent Heavy Veh, %	0
Cap, veh/h	669
Arrive On Green	0.42
Sat Flow, veh/h	1606
Grp Volume(v), veh/h	92
Grp Sat Flow(s),veh/h/ln	1606
Q Serve(g_s), s	2.8
Cycle Q Clear(g_c), s	2.8
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	669
V/C Ratio(X)	0.14
Avail Cap(c_a), veh/h	669
HCM Platoon Ratio	1.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	14.4
Incr Delay (d2), s/veh	0.4
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	1.1
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	14.9
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour

Intersection												
Int Delay, s/veh	2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗	↗	↗↗	↗	↗	↗↗	↗
Traffic Vol, veh/h	0	0	67	0	0	136	69	1482	22	102	1700	29
Future Vol, veh/h	0	0	67	0	0	136	69	1482	22	102	1700	29
Conflicting Peds, #/hr	2	0	0	0	0	2	7	0	0	0	0	7
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	150	-	50	125	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	67	0	0	136	69	1482	22	102	1700	29

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	-	-	857	-	-	743	1736	0	0	1504	0	0
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	6.9	-	-	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.3	-	-	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	0	0	305	0	0	362	368	-	-	451	-	-
Stage 1	0	0	-	0	0	-	-	-	-	-	-	-
Stage 2	0	0	-	0	0	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	303	-	-	361	366	-	-	451	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	20.2		20.9		0.8		0.9	
HCM LOS	C		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	366	-	-	303	361	451	-	-
HCM Lane V/C Ratio	0.189	-	-	0.221	0.377	0.226	-	-
HCM Control Delay (s)	17.1	-	-	20.2	20.9	15.3	-	-
HCM Lane LOS	C	-	-	C	C	C	-	-
HCM 95th %tile Q(veh)	0.7	-	-	0.8	1.7	0.9	-	-

HCM 6th TWSC

Perris/Dracaean Commercial Project

3: Perris Boulevard & Auto Center Driveway/Project Driveway 1

GPBO (2040) WP - AM Peak Hour

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗		↕↔			↕↔↔	
Traffic Vol, veh/h	0	0	1	0	0	50	0	1522	19	0	1767	7
Future Vol, veh/h	0	0	1	0	0	50	0	1522	19	0	1767	7
Conflicting Peds, #/hr	0	0	0	0	0	0	11	0	0	0	0	11
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	-	-	-	-	-	25
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	1	0	0	50	0	1522	19	0	1767	7

Major/Minor	Minor2		Minor1		Major1		Major2	
Conflicting Flow All	-	-	898	-	-	771	-	0
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	7.1	-	-	6.9	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.9	-	-	3.3	-	-
Pot Cap-1 Maneuver	0	0	*513	0	0	*449	0	-
Stage 1	0	0	-	0	0	-	0	-
Stage 2	0	0	-	0	0	-	0	-
Platoon blocked, %			1			1		
Mov Cap-1 Maneuver	-	-	*508	-	-	*449	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	12.1	14	0	0
HCM LOS	B	B		

Minor Lane/Major Mvmt	NBT	NBR	EBLn1WBLn1	SBT	SBR
Capacity (veh/h)	-	-	508	449	-
HCM Lane V/C Ratio	-	-	0.002	0.111	-
HCM Control Delay (s)	-	-	12.1	14	-
HCM Lane LOS	-	-	B	B	-
HCM 95th %tile Q(veh)	-	-	0	0.4	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBU	SBL	SBT
Lane Configurations		↕	↗		↕		↖	↕			↖	↕
Traffic Volume (veh/h)	92	113	59	144	181	91	74	1342	73	16	67	1508
Future Volume (veh/h)	92	113	59	144	181	91	74	1342	73	16	67	1508
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		0.99		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900		1900	1900
Adj Flow Rate, veh/h	92	113	59	144	181	91	74	1342	73		67	1508
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0		0	0
Cap, veh/h	215	242	552	177	192	86	204	1370	74		204	1421
Arrive On Green	0.34	0.34	0.34	0.34	0.34	0.34	0.15	0.52	0.52		0.15	0.52
Sat Flow, veh/h	436	705	1605	338	559	251	1810	3479	189		1810	3610
Grp Volume(v), veh/h	205	0	59	416	0	0	74	695	720		67	1508
Grp Sat Flow(s),veh/h/ln	1142	0	1605	1148	0	0	1810	1805	1863		1810	1805
Q Serve(g_s), s	0.0	0.0	2.0	17.4	0.0	0.0	2.9	30.1	30.3		2.6	31.5
Cycle Q Clear(g_c), s	10.1	0.0	2.0	27.5	0.0	0.0	2.9	30.1	30.3		2.6	31.5
Prop In Lane	0.45		1.00	0.35		0.22	1.00		0.10		1.00	
Lane Grp Cap(c), veh/h	458	0	552	455	0	0	204	711	734		204	1421
V/C Ratio(X)	0.45	0.00	0.11	0.91	0.00	0.00	0.36	0.98	0.98		0.33	1.06
Avail Cap(c_a), veh/h	458	0	552	455	0	0	204	711	734		204	1421
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33		1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.53	0.53	0.53		1.00	1.00
Uniform Delay (d), s/veh	20.1	0.0	17.9	29.3	0.0	0.0	31.4	18.7	18.8		31.3	19.1
Incr Delay (d2), s/veh	0.7	0.0	0.1	22.8	0.0	0.0	0.6	19.8	20.2		0.9	41.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0
%ile BackOfQ(50%),veh/ln	2.9	0.0	0.7	10.7	0.0	0.0	1.3	13.5	14.1		1.2	18.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	20.8	0.0	18.0	52.1	0.0	0.0	32.0	38.5	39.0		32.2	60.9
LnGrp LOS	C	A	B	D	A	A	C	D	D		C	F
Approach Vol, veh/h		264			416			1489				1750
Approach Delay, s/veh		20.1			52.1			38.4				55.1
Approach LOS		C			D			D				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	13.0	35.5		31.5	13.0	35.5		31.5				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	9.0	31.5		27.5	9.0	31.5		27.5				
Max Q Clear Time (g_c+I1), s	4.6	32.3		12.1	4.9	33.5		29.5				
Green Ext Time (p_c), s	0.0	0.0		1.2	0.0	0.0		0.0				

Intersection Summary

HCM 6th Ctrl Delay	46.1
HCM 6th LOS	D

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
 GPBO (2040) WP - AM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	175
Future Volume (veh/h)	175
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	175
Peak Hour Factor	1.00
Percent Heavy Veh, %	0
Cap, veh/h	613
Arrive On Green	0.52
Sat Flow, veh/h	1556
Grp Volume(v), veh/h	175
Grp Sat Flow(s),veh/h/ln	1556
Q Serve(g_s), s	5.0
Cycle Q Clear(g_c), s	5.0
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	613
V/C Ratio(X)	0.29
Avail Cap(c_a), veh/h	613
HCM Platoon Ratio	1.33
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	12.8
Incr Delay (d2), s/veh	1.2
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	1.8
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	13.9
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) WP - AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗		↘	↗	↘	↘	↗	↘	↘	↗	↘
Traffic Volume (veh/h)	114	239	69	90	349	189	96	1230	78	178	1399	155
Future Volume (veh/h)	114	239	69	90	349	189	96	1230	78	178	1399	155
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	114	239	69	90	349	189	96	1230	78	178	1399	155
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	146	627	177	137	419	354	140	1151	512	326	1522	673
Arrive On Green	0.08	0.23	0.23	0.08	0.22	0.22	0.08	0.32	0.32	0.24	0.56	0.56
Sat Flow, veh/h	1810	2777	783	1810	1900	1604	1810	3610	1605	1810	3610	1597
Grp Volume(v), veh/h	114	153	155	90	349	189	96	1230	78	178	1399	155
Grp Sat Flow(s),veh/h/ln	1810	1805	1755	1810	1900	1604	1810	1805	1605	1810	1805	1597
Q Serve(g_s), s	4.9	5.8	6.0	3.9	14.0	5.3	4.1	25.5	2.1	6.9	28.1	3.9
Cycle Q Clear(g_c), s	4.9	5.8	6.0	3.9	14.0	5.3	4.1	25.5	2.1	6.9	28.1	3.9
Prop In Lane	1.00		0.45	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	146	407	396	137	419	354	140	1151	512	326	1522	673
V/C Ratio(X)	0.78	0.38	0.39	0.66	0.83	0.53	0.69	1.07	0.15	0.55	0.92	0.23
Avail Cap(c_a), veh/h	192	519	505	192	546	461	158	1151	512	326	1522	673
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.32	0.32	0.32
Uniform Delay (d), s/veh	36.1	26.2	26.3	36.0	29.8	11.3	36.0	27.2	10.7	27.6	16.3	11.0
Incr Delay (d2), s/veh	14.0	0.6	0.6	5.3	8.3	1.3	10.1	47.0	0.6	0.6	3.9	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.7	2.5	2.5	1.9	7.1	3.1	2.2	17.9	1.1	2.8	9.4	1.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	50.1	26.8	26.9	41.2	38.1	12.6	46.1	74.3	11.4	28.2	20.2	11.3
LnGrp LOS	D	C	C	D	D	B	D	F	B	C	C	B
Approach Vol, veh/h		422			628			1404			1732	
Approach Delay, s/veh		33.1			30.8			68.9			20.3	
Approach LOS		C			C			E			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	18.4	29.5	10.1	22.1	10.2	37.7	10.4	21.7				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	25.5	8.5	23.0	7.0	25.5	8.5	23.0					
Max Q Clear Time (g_c+1), s	27.5	5.9	8.0	6.1	30.1	6.9	16.0					
Green Ext Time (p_c), s	0.0	0.0	0.0	1.5	0.0	0.0	0.0	1.6				

Intersection Summary

HCM 6th Ctrl Delay	39.4
HCM 6th LOS	D

HCM 6th TWSC
6: Dracaea Avenue & Project Driveway 2

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour

Intersection						
Int Delay, s/veh	3.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	110	142	311	37	34	105
Future Vol, veh/h	110	142	311	37	34	105
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	110	142	311	37	34	105

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	348	0	-	0	692
Stage 1	-	-	-	-	330
Stage 2	-	-	-	-	362
Critical Hdwy	4.1	-	-	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	2.2	-	-	-	3.5
Pot Cap-1 Maneuver	1222	-	-	-	413
Stage 1	-	-	-	-	733
Stage 2	-	-	-	-	709
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1222	-	-	-	373
Mov Cap-2 Maneuver	-	-	-	-	373
Stage 1	-	-	-	-	661
Stage 2	-	-	-	-	709

Approach	EB	WB	SB
HCM Control Delay, s	3.6	0	13.1
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1222	-	-	-	585
HCM Lane V/C Ratio	0.09	-	-	-	0.238
HCM Control Delay (s)	8.2	0	-	-	13.1
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.3	-	-	-	0.9

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour

Intersection						
Int Delay, s/veh	0.7					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	76	47	0	119	18	0
Future Vol, veh/h	76	47	0	119	18	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	76	47	0	119	18	0

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	123	0	219
Stage 1	-	-	-	-	100
Stage 2	-	-	-	-	119
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1477	-	774
Stage 1	-	-	-	-	929
Stage 2	-	-	-	-	911
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1477	-	774
Mov Cap-2 Maneuver	-	-	-	-	774
Stage 1	-	-	-	-	929
Stage 2	-	-	-	-	911

Approach	EB	WB	NB
HCM Control Delay, s	0	0	9.8
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	774	-	-	1477	-
HCM Lane V/C Ratio	0.023	-	-	-	-
HCM Control Delay (s)	9.8	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0	-

HCM 6th Signalized Intersection Summary
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBL	SBT
Lane Configurations	↗	↑	↖	↗	↑	↖		↘	↑↑	↖	↗	↑↑
Traffic Volume (veh/h)	155	516	275	88	301	140	44	157	1550	98	111	1495
Future Volume (veh/h)	155	516	275	88	301	140	44	157	1550	98	111	1495
Initial Q (Qb), veh	0	0	0	0	0	0		0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00		1.00		0.98	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900		1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	155	516	275	88	301	140		157	1550	98	111	1495
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0		0	0	0	0	0
Cap, veh/h	186	475	402	116	401	340		163	1624	708	138	1574
Arrive On Green	0.10	0.25	0.25	0.06	0.21	0.21		0.03	0.15	0.15	0.08	0.44
Sat Flow, veh/h	1810	1900	1606	1810	1900	1610		1810	3610	1573	1810	3610
Grp Volume(v), veh/h	155	516	275	88	301	140		157	1550	98	111	1495
Grp Sat Flow(s),veh/h/ln	1810	1900	1606	1810	1900	1610		1810	1805	1573	1810	1805
Q Serve(g_s), s	8.4	25.0	15.5	4.8	14.9	6.0		8.7	42.6	4.0	6.0	39.9
Cycle Q Clear(g_c), s	8.4	25.0	15.5	4.8	14.9	6.0		8.7	42.6	4.0	6.0	39.9
Prop In Lane	1.00		1.00	1.00		1.00		1.00		1.00	1.00	
Lane Grp Cap(c), veh/h	186	475	402	116	401	340		163	1625	708	138	1574
V/C Ratio(X)	0.83	1.09	0.68	0.76	0.75	0.41		0.96	0.95	0.14	0.81	0.95
Avail Cap(c_a), veh/h	199	475	402	127	401	340		163	1625	708	138	1574
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00		0.33	0.33	0.33	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	44.0	37.5	33.9	46.1	37.0	21.9		48.4	41.6	14.0	45.5	27.1
Incr Delay (d2), s/veh	23.9	66.6	4.8	21.5	7.7	0.8		59.7	13.8	0.4	28.6	13.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.0	20.2	6.5	2.8	7.7	2.9		6.8	23.6	2.1	3.8	19.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	67.9	104.1	38.7	67.6	44.7	22.7		108.0	55.4	14.4	74.0	40.7
LnGrp LOS	E	F	D	E	D	C		F	E	B	E	D
Approach Vol, veh/h		946			529				1805			1706
Approach Delay, s/veh		79.2			42.7				57.7			41.5
Approach LOS		E			D				E			D
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.6	49.0	10.4	29.0	13.0	47.6	14.3	25.1				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	7.0	45.0	7.0	25.0	9.0	43.0	11.0	21.0				
Max Q Clear Time (g_c+I1), s	8.0	44.6	6.8	27.0	10.7	41.9	10.4	16.9				
Green Ext Time (p_c), s	0.0	0.4	0.0	0.0	0.0	1.0	0.0	0.9				

Intersection Summary

HCM 6th Ctrl Delay	54.6
HCM 6th LOS	D

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
 GPBO (2040) WP - PM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	100
Future Volume (veh/h)	100
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	100
Peak Hour Factor	1.00
Percent Heavy Veh, %	0
Cap, veh/h	683
Arrive On Green	0.44
Sat Flow, veh/h	1566
Grp Volume(v), veh/h	100
Grp Sat Flow(s),veh/h/ln	1566
Q Serve(g_s), s	3.8
Cycle Q Clear(g_c), s	3.8
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	683
V/C Ratio(X)	0.15
Avail Cap(c_a), veh/h	683
HCM Platoon Ratio	1.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	17.0
Incr Delay (d2), s/veh	0.5
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	1.4
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	17.4
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th TWSC
2: Perris Boulevard & Atwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour

Intersection												
Int Delay, s/veh	3.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗	↗	↗↗	↗	↗	↗↗	↗
Traffic Vol, veh/h	0	0	83	0	0	112	79	1894	15	190	1804	22
Future Vol, veh/h	0	0	83	0	0	112	79	1894	15	190	1804	22
Conflicting Peds, #/hr	0	0	0	0	0	0	4	0	0	0	0	4
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	150	-	50	125	-	50
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	83	0	0	112	79	1894	15	190	1804	22

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	-	-	906	-	-	947	1830	0	0	1909	0	0
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	6.9	-	-	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.3	-	-	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	0	0	283	0	0	266	338	-	-	315	-	-
Stage 1	0	0	-	0	0	-	-	-	-	-	-	-
Stage 2	0	0	-	0	0	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	282	-	-	266	337	-	-	315	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB			
HCM Control Delay, s	23		28		0.8		3.1			
HCM LOS	C		D							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	337	-	-	282	266	315	-
HCM Lane V/C Ratio	0.234	-	-	0.294	0.421	0.603	-
HCM Control Delay (s)	18.9	-	-	23	28	32.4	-
HCM Lane LOS	C	-	-	C	D	D	-
HCM 95th %tile Q(veh)	0.9	-	-	1.2	2	3.7	-

HCM 6th TWSC

Perris/Dracaean Commercial Project

3: Perris Boulevard & Auto Center Driveway/Project Driveway 1

GPBO (2040) WP - PM Peak Hour

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations			↗			↗		↕↔			↕↔↔	
Traffic Vol, veh/h	0	0	0	0	0	39	0	1948	20	0	1887	2
Future Vol, veh/h	0	0	0	0	0	39	0	1948	20	0	1887	2
Conflicting Peds, #/hr	0	0	0	0	0	0	3	0	0	0	0	3
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	0	-	-	0	-	-	-	-	-	25
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	0	0	0	0	0	39	0	1948	20	0	1887	2

Major/Minor	Minor2		Minor1		Major1		Major2	
Conflicting Flow All	-	-	948	-	-	984	-	0
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	7.1	-	-	6.9	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	3.9	-	-	3.3	-	-
Pot Cap-1 Maneuver	0	0	*488	0	0	*263	0	-
Stage 1	0	0	-	0	0	-	0	-
Stage 2	0	0	-	0	0	-	0	-
Platoon blocked, %			1			1		
Mov Cap-1 Maneuver	-	-	*487	-	-	*263	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	21.1	0	0
HCM LOS	A	C		

Minor Lane/Major Mvmt	NBT	NBR	EBLn1WBLn1	SBT	SBR
Capacity (veh/h)	-	-	-	263	-
HCM Lane V/C Ratio	-	-	-	0.148	-
HCM Control Delay (s)	-	-	0	21.1	-
HCM Lane LOS	-	-	A	C	-
HCM 95th %tile Q(veh)	-	-	-	0.5	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBU	SBL	SBT
Lane Configurations		↕	↗		↕		↖	↕			↖	↕
Traffic Volume (veh/h)	258	365	81	97	140	72	49	1611	85	27	84	1659
Future Volume (veh/h)	258	365	81	97	140	72	49	1611	85	27	84	1659
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.97		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900		1900	1900
Adj Flow Rate, veh/h	258	365	81	97	140	72	49	1611	85		84	1659
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0		0	0
Cap, veh/h	182	186	499	47	59	18	127	1766	93		114	1805
Arrive On Green	0.31	0.31	0.31	0.31	0.31	0.31	0.07	0.51	0.51		0.13	1.00
Sat Flow, veh/h	424	601	1609	0	191	58	1810	3484	183		1810	3610
Grp Volume(v), veh/h	623	0	81	309	0	0	49	830	866		84	1659
Grp Sat Flow(s),veh/h/ln	1025	0	1609	249	0	0	1810	1805	1862		1810	1805
Q Serve(g_s), s	0.0	0.0	3.7	0.0	0.0	0.0	2.6	42.0	42.9		4.5	0.0
Cycle Q Clear(g_c), s	31.0	0.0	3.7	31.0	0.0	0.0	2.6	42.0	42.9		4.5	0.0
Prop In Lane	0.41		1.00	0.31		0.23	1.00		0.10		1.00	
Lane Grp Cap(c), veh/h	369	0	499	124	0	0	127	915	943		114	1805
V/C Ratio(X)	1.69	0.00	0.16	2.48	0.00	0.00	0.39	0.91	0.92		0.73	0.92
Avail Cap(c_a), veh/h	369	0	499	124	0	0	127	915	943		127	1805
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.44	0.44	0.44		1.00	1.00
Uniform Delay (d), s/veh	36.6	0.0	25.1	32.1	0.0	0.0	44.4	22.5	22.7		42.9	0.0
Incr Delay (d2), s/veh	322.0	0.0	0.2	690.6	0.0	0.0	0.8	7.2	7.7		17.9	9.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0
%ile BackOfQ(50%),veh/ln	42.1	0.0	1.4	26.0	0.0	0.0	1.2	18.5	19.6		2.5	2.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	358.6	0.0	25.2	722.7	0.0	0.0	45.3	29.8	30.5		60.7	9.1
LnGrp LOS	F	A	C	F	A	A	D	C	C		E	A
Approach Vol, veh/h		704			309			1745				1860
Approach Delay, s/veh		320.3			722.7			30.6				10.9
Approach LOS		F			F			C				B
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.3	54.7		35.0	11.0	54.0		35.0				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	7.0	50.0		31.0	7.0	50.0		31.0				
Max Q Clear Time (g_c+I1), s	6.5	44.9		33.0	4.6	2.0		33.0				
Green Ext Time (p_c), s	0.0	4.2		0.0	0.0	22.1		0.0				

Intersection Summary

HCM 6th Ctrl Delay	113.1
HCM 6th LOS	F

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
 GPBO (2040) WP - PM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	117
Future Volume (veh/h)	117
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	117
Peak Hour Factor	1.00
Percent Heavy Veh, %	0
Cap, veh/h	784
Arrive On Green	1.00
Sat Flow, veh/h	1568
Grp Volume(v), veh/h	117
Grp Sat Flow(s),veh/h/ln	1568
Q Serve(g_s), s	0.0
Cycle Q Clear(g_c), s	0.0
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	784
V/C Ratio(X)	0.15
Avail Cap(c_a), veh/h	784
HCM Platoon Ratio	2.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	0.0
Incr Delay (d2), s/veh	0.4
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	0.1
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	0.4
LnGrp LOS	A
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
5: Perris Boulevard & Cottonwood Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗		↘	↗	↘	↘	↗	↗	↘	↗	↘
Traffic Volume (veh/h)	200	374	137	41	221	183	80	1418	43	175	1577	136
Future Volume (veh/h)	200	374	137	41	221	183	80	1418	43	175	1577	136
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.98	1.00		0.97	1.00		0.97
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	200	374	137	41	221	183	80	1418	43	175	1577	136
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	237	596	215	86	277	230	113	1264	549	385	1806	785
Arrive On Green	0.13	0.23	0.23	0.05	0.15	0.15	0.06	0.35	0.35	0.28	0.67	0.67
Sat Flow, veh/h	1810	2594	937	1810	1900	1572	1810	3610	1570	1810	3610	1568
Grp Volume(v), veh/h	200	259	252	41	221	183	80	1418	43	175	1577	136
Grp Sat Flow(s),veh/h/ln	1810	1805	1726	1810	1900	1572	1810	1805	1570	1810	1805	1568
Q Serve(g_s), s	10.8	12.9	13.2	2.2	11.2	7.4	4.3	35.0	1.5	8.0	34.9	3.3
Cycle Q Clear(g_c), s	10.8	12.9	13.2	2.2	11.2	7.4	4.3	35.0	1.5	8.0	34.9	3.3
Prop In Lane	1.00		0.54	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	237	414	396	86	277	230	113	1264	549	385	1806	785
V/C Ratio(X)	0.84	0.62	0.64	0.48	0.80	0.80	0.71	1.12	0.08	0.45	0.87	0.17
Avail Cap(c_a), veh/h	407	433	414	308	352	291	127	1264	549	385	1806	785
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.42	0.42	0.42
Uniform Delay (d), s/veh	42.4	34.6	34.8	46.4	41.3	17.8	46.0	32.5	14.0	31.1	14.2	8.9
Incr Delay (d2), s/veh	7.9	2.6	3.0	4.0	9.6	11.5	14.7	65.9	0.3	0.4	2.7	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.3	5.9	5.8	1.1	5.9	3.4	2.4	26.3	0.7	3.4	10.9	1.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	50.3	37.2	37.8	50.4	50.9	29.3	60.7	98.4	14.3	31.4	16.9	9.1
LnGrp LOS	D	D	D	D	D	C	E	F	B	C	B	A
Approach Vol, veh/h		711			445			1541			1888	
Approach Delay, s/veh		41.1			42.0			94.1			17.7	
Approach LOS		D			D			F			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	35.3	39.0	8.8	27.0	10.2	54.0	17.1	18.6				
Change Period (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0				
Max Green Setting (Gmax), s	35.0	35.0	17.0	24.0	7.0	36.0	22.5	18.5				
Max Q Clear Time (g_c+10), s	37.0	37.0	4.2	15.2	6.3	36.9	12.8	13.2				
Green Ext Time (p_c), s	0.0	0.0	0.0	2.1	0.0	0.0	0.4	0.9				

Intersection Summary

HCM 6th Ctrl Delay	49.4
HCM 6th LOS	D

HCM 6th TWSC
6: Dracaea Avenue & Project Driveway 2

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour

Intersection						
Int Delay, s/veh	2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	71	463	235	24	24	75
Future Vol, veh/h	71	463	235	24	24	75
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	71	463	235	24	24	75

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	259	0	-	0	852
Stage 1	-	-	-	-	247
Stage 2	-	-	-	-	605
Critical Hdwy	4.1	-	-	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	2.2	-	-	-	3.5
Pot Cap-1 Maneuver	1317	-	-	-	333
Stage 1	-	-	-	-	799
Stage 2	-	-	-	-	549
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1317	-	-	-	309
Mov Cap-2 Maneuver	-	-	-	-	309
Stage 1	-	-	-	-	741
Stage 2	-	-	-	-	549

Approach	EB	WB	SB
HCM Control Delay, s	1	0	12.5
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1317	-	-	-	576
HCM Lane V/C Ratio	0.054	-	-	-	0.172
HCM Control Delay (s)	7.9	0	-	-	12.5
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.2	-	-	-	0.6

HCM 6th TWSC
7: Sunset Lane & Atwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) WP - PM Peak Hour

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	165	39	0	101	11	0
Future Vol, veh/h	165	39	0	101	11	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	100	100	100	100	100	100
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	165	39	0	101	11	0

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	204	0	286
Stage 1	-	-	-	-	185
Stage 2	-	-	-	-	101
Critical Hdwy	-	-	4.1	-	6.4
Critical Hdwy Stg 1	-	-	-	-	5.4
Critical Hdwy Stg 2	-	-	-	-	5.4
Follow-up Hdwy	-	-	2.2	-	3.5
Pot Cap-1 Maneuver	-	-	1380	-	709
Stage 1	-	-	-	-	852
Stage 2	-	-	-	-	928
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1380	-	709
Mov Cap-2 Maneuver	-	-	-	-	709
Stage 1	-	-	-	-	852
Stage 2	-	-	-	-	928

Approach	EB	WB	NB
HCM Control Delay, s	0	0	10.2
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	709	-	-	1380	-
HCM Lane V/C Ratio	0.016	-	-	-	-
HCM Control Delay (s)	10.2	-	-	0	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP MIT- AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBU	SBL	SBT
Lane Configurations	↶	↷		↶	↷		↶	↷			↶	↷
Traffic Volume (veh/h)	92	113	59	144	181	91	74	1342	73	16	67	1508
Future Volume (veh/h)	92	113	59	144	181	91	74	1342	73	16	67	1508
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		0.99		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900		1900	1900
Adj Flow Rate, veh/h	92	113	59	144	181	91	74	1342	73		67	1508
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0		0	0
Cap, veh/h	215	290	151	294	293	147	300	1522	83		300	1579
Arrive On Green	0.25	0.25	0.25	0.25	0.25	0.25	0.22	0.58	0.58		0.22	0.58
Sat Flow, veh/h	1124	1173	613	1228	1186	596	1810	3480	189		1810	3610
Grp Volume(v), veh/h	92	0	172	144	0	272	74	695	720		67	1508
Grp Sat Flow(s),veh/h/ln	1124	0	1786	1228	0	1782	1810	1805	1863		1810	1805
Q Serve(g_s), s	6.3	0.0	6.4	8.9	0.0	10.9	2.7	26.4	26.6		2.4	31.4
Cycle Q Clear(g_c), s	17.2	0.0	6.4	15.3	0.0	10.9	2.7	26.4	26.6		2.4	31.4
Prop In Lane	1.00		0.34	1.00		0.33	1.00		0.10		1.00	
Lane Grp Cap(c), veh/h	215	0	441	294	0	440	300	790	815		300	1579
V/C Ratio(X)	0.43	0.00	0.39	0.49	0.00	0.62	0.25	0.88	0.88		0.22	0.95
Avail Cap(c_a), veh/h	275	0	536	360	0	535	300	790	815		300	1579
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33		1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.53	0.53	0.53		1.00	1.00
Uniform Delay (d), s/veh	34.4	0.0	25.1	31.5	0.0	26.8	27.1	14.9	15.0		27.0	16.0
Incr Delay (d2), s/veh	1.3	0.0	0.6	1.3	0.0	1.5	0.2	7.7	7.7		0.4	14.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0
%ile BackOfQ(50%),veh/ln	1.8	0.0	2.7	2.7	0.0	4.6	1.1	9.4	9.8		1.0	12.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	35.8	0.0	25.7	32.7	0.0	28.3	27.3	22.7	22.7		27.3	30.2
LnGrp LOS	D	A	C	C	A	C	C	C	C		C	C
Approach Vol, veh/h		264			416			1489				1750
Approach Delay, s/veh		29.2			29.8			22.9				28.2
Approach LOS		C			C			C				C
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	17.3	39.0		23.7	17.3	39.0		23.7				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	9.0	35.0		24.0	9.0	35.0		24.0				
Max Q Clear Time (g_c+I1), s	4.4	28.6		19.2	4.7	33.4		17.3				
Green Ext Time (p_c), s	0.0	4.5		0.5	0.0	1.3		1.2				

Intersection Summary

HCM 6th Ctrl Delay	26.4
HCM 6th LOS	C

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
 GPBO (2040) WP MIT- AM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	175
Future Volume (veh/h)	175
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	175
Peak Hour Factor	1.00
Percent Heavy Veh, %	0
Cap, veh/h	682
Arrive On Green	0.58
Sat Flow, veh/h	1559
Grp Volume(v), veh/h	175
Grp Sat Flow(s),veh/h/ln	1559
Q Serve(g_s), s	4.4
Cycle Q Clear(g_c), s	4.4
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	682
V/C Ratio(X)	0.26
Avail Cap(c_a), veh/h	682
HCM Platoon Ratio	1.33
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	10.3
Incr Delay (d2), s/veh	0.9
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	1.5
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	11.2
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

HCM 6th Signalized Intersection Summary
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP MIT- PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBU	SBL	SBT
Lane Configurations												
Traffic Volume (veh/h)	258	365	81	97	140	72	49	1611	85	27	84	1659
Future Volume (veh/h)	258	365	81	97	140	72	49	1611	85	27	84	1659
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.97		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Work Zone On Approach		No			No			No				No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900		1900	1900
Adj Flow Rate, veh/h	258	365	81	97	140	72	49	1611	85		84	1659
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0		0	0
Cap, veh/h	294	428	95	124	336	173	124	1867	98		113	1911
Arrive On Green	0.28	0.28	0.28	0.28	0.28	0.28	0.07	0.54	0.54		0.06	0.53
Sat Flow, veh/h	1188	1505	334	959	1182	608	1810	3484	183		1810	3610
Grp Volume(v), veh/h	258	0	446	97	0	212	49	830	866		84	1659
Grp Sat Flow(s),veh/h/ln	1188	0	1839	959	0	1790	1810	1805	1862		1810	1805
Q Serve(g_s), s	19.2	0.0	23.4	5.6	0.0	9.8	2.6	40.3	41.2		4.7	40.8
Cycle Q Clear(g_c), s	29.0	0.0	23.4	29.0	0.0	9.8	2.6	40.3	41.2		4.7	40.8
Prop In Lane	1.00		0.18	1.00		0.34	1.00		0.10		1.00	
Lane Grp Cap(c), veh/h	294	0	523	124	0	509	124	967	997		113	1911
V/C Ratio(X)	0.88	0.00	0.85	0.79	0.00	0.42	0.39	0.86	0.87		0.75	0.87
Avail Cap(c_a), veh/h	294	0	523	124	0	509	124	967	997		124	1911
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00		1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.44	0.44	0.44		1.00	1.00
Uniform Delay (d), s/veh	42.7	0.0	34.5	49.5	0.0	29.6	45.5	20.4	20.5		47.0	20.9
Incr Delay (d2), s/veh	24.5	0.0	12.8	27.5	0.0	0.5	0.9	4.6	4.8		19.7	5.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0
%ile BackOfQ(50%),veh/ln	8.6	0.0	12.1	3.4	0.0	4.3	1.2	17.0	18.0		2.7	17.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	67.2	0.0	47.3	76.9	0.0	30.2	46.4	25.0	25.4		66.7	26.6
LnGrp LOS	E	A	D	E	A	C	D	C	C		E	C
Approach Vol, veh/h		704			309			1745				1860
Approach Delay, s/veh		54.6			44.9			25.8				27.5
Approach LOS		D			D			C				C
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.4	58.6		33.0	11.0	58.0		33.0				
Change Period (Y+Rc), s	4.0	4.0		4.0	4.0	4.0		4.0				
Max Green Setting (Gmax), s	7.0	54.0		29.0	7.0	54.0		29.0				
Max Q Clear Time (g_c+I1), s	6.7	43.2		31.0	4.6	42.8		31.0				
Green Ext Time (p_c), s	0.0	8.1		0.0	0.0	8.5		0.0				

Intersection Summary

HCM 6th Ctrl Delay	32.1
HCM 6th LOS	C

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary
 4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
 GPBO (2040) WP MIT- PM Peak Hour

Movement	SBR
Lane Configurations	
Traffic Volume (veh/h)	117
Future Volume (veh/h)	117
Initial Q (Qb), veh	0
Ped-Bike Adj(A_pbT)	0.97
Parking Bus, Adj	1.00
Work Zone On Approach	
Adj Sat Flow, veh/h/ln	1900
Adj Flow Rate, veh/h	117
Peak Hour Factor	1.00
Percent Heavy Veh, %	0
Cap, veh/h	830
Arrive On Green	0.53
Sat Flow, veh/h	1569
Grp Volume(v), veh/h	117
Grp Sat Flow(s),veh/h/ln	1569
Q Serve(g_s), s	3.9
Cycle Q Clear(g_c), s	3.9
Prop In Lane	1.00
Lane Grp Cap(c), veh/h	830
V/C Ratio(X)	0.14
Avail Cap(c_a), veh/h	830
HCM Platoon Ratio	1.00
Upstream Filter(l)	1.00
Uniform Delay (d), s/veh	12.2
Incr Delay (d2), s/veh	0.4
Initial Q Delay(d3),s/veh	0.0
%ile BackOfQ(50%),veh/ln	1.4
Unsig. Movement Delay, s/veh	
LnGrp Delay(d),s/veh	12.6
LnGrp LOS	B
Approach Vol, veh/h	
Approach Delay, s/veh	
Approach LOS	
Timer - Assigned Phs	

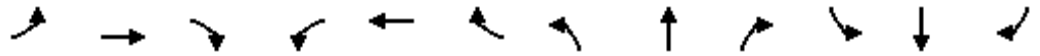
Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

APPENDIX E:

QUEUING WORKSHEETS

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	25	84	64	95	249	223	57	1152	67	83	1156	37
v/c Ratio	0.16	0.36	0.18	0.39	0.59	0.42	0.35	0.62	0.08	0.53	0.60	0.04
Control Delay	36.4	36.7	1.1	35.5	33.4	6.6	30.3	17.3	0.8	48.0	17.6	0.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	36.4	36.7	1.1	35.5	33.4	6.6	30.3	17.3	0.8	48.0	17.6	0.1
Queue Length 50th (ft)	12	40	0	45	102	0	19	314	0	41	227	0
Queue Length 95th (ft)	34	76	0	83	176	48	m41	386	m3	#83	327	0
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	157	439	503	242	465	559	161	1844	888	157	1936	927
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.16	0.19	0.13	0.39	0.54	0.40	0.35	0.62	0.08	0.53	0.60	0.04

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Queuing and Blocking Report

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	L	L
Maximum Queue (ft)	69	44	47	31
Average Queue (ft)	27	12	23	10
95th Queue (ft)	58	35	47	33
Link Distance (ft)	424	42		
Upstream Blk Time (%)		4		
Queuing Penalty (veh)		1		
Storage Bay Dist (ft)			50	50
Storage Blk Time (%)			0	0
Queuing Penalty (veh)			1	0

Queuing and Blocking Report

Intersection: 3: Perris Boulevard & Auto Center Driveway

Movement	EB	SB	SB	SB
Directions Served	LR	T	T	TR
Maximum Queue (ft)	30	29	78	75
Average Queue (ft)	3	1	5	3
95th Queue (ft)	17	11	33	29
Link Distance (ft)	134	400	400	
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				25
Storage Blk Time (%)			0	
Queuing Penalty (veh)			2	

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Existing NP - AM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	161	62	277	60	1222	39	1237	102
v/c Ratio	0.57	0.15	0.78	0.33	0.59	0.21	0.59	0.11
Control Delay	34.5	6.1	41.1	41.4	12.3	28.5	5.7	0.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	34.5	6.1	41.1	41.4	12.3	28.5	5.7	0.4
Queue Length 50th (ft)	72	0	118	32	298	21	57	0
Queue Length 95th (ft)	108	20	164	m48	377	m37	86	m0
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		95		
Base Capacity (vph)	397	554	488	212	2075	212	2081	934
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.41	0.11	0.57	0.28	0.59	0.18	0.59	0.11

Intersection Summary

m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Existing NP - AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	105	335	73	333	192	71	1048	59	158	1120	131
v/c Ratio	0.67	0.43	0.46	0.82	0.39	0.40	0.66	0.08	1.01	0.68	0.17
Control Delay	57.8	25.9	45.0	47.4	6.8	40.6	21.5	0.3	106.5	20.4	3.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	57.8	25.9	45.0	47.4	6.8	40.6	21.5	0.3	106.5	20.4	3.9
Queue Length 50th (ft)	52	66	35	156	0	34	229	0	-88	324	21
Queue Length 95th (ft)	#108	94	70	#225	39	67	267	0	#188	360	30
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	157	834	157	439	515	191	1582	760	157	1639	779
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.67	0.40	0.46	0.76	0.37	0.37	0.66	0.08	1.01	0.68	0.17

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

Queuing and Blocking Report

Intersection: 7: Sunset Lane & Atwood Avenue

Movement	WB	NB
Directions Served	LT	LR
Maximum Queue (ft)	31	32
Average Queue (ft)	3	3
95th Queue (ft)	17	17
Link Distance (ft)	409	273
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	54	199	83	55	134	114	54	1029	62	102	1119	47
v/c Ratio	0.35	0.67	0.22	0.44	0.50	0.33	0.34	0.50	0.07	0.81	0.54	0.05
Control Delay	48.8	50.2	2.2	55.9	45.9	6.2	33.7	22.5	4.5	88.7	17.4	0.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	48.8	50.2	2.2	55.9	45.9	6.2	33.7	22.5	4.5	88.7	17.4	0.2
Queue Length 50th (ft)	33	121	0	34	80	0	34	333	6	65	243	0
Queue Length 95th (ft)	70	183	7	74	135	30	60	412	23	#159	371	2
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	198	475	504	126	399	451	173	2039	931	126	2057	928
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.27	0.42	0.16	0.44	0.34	0.25	0.31	0.50	0.07	0.81	0.54	0.05

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

Queuing and Blocking Report

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	SB	SB	SB
Directions Served	LTR	LTR	L	L	T	R
Maximum Queue (ft)	108	44	53	59	94	21
Average Queue (ft)	40	13	24	21	4	1
95th Queue (ft)	85	34	53	50	36	8
Link Distance (ft)	424	32			599	
Upstream Blk Time (%)		1				
Queuing Penalty (veh)		0				
Storage Bay Dist (ft)			50	50		50
Storage Blk Time (%)				2		
Queuing Penalty (veh)				10		

Queuing and Blocking Report

Intersection: 3: Perris Boulevard & Auto Center Driveway

Movement	SB	SB
Directions Served	T	T
Maximum Queue (ft)	53	95
Average Queue (ft)	4	6
95th Queue (ft)	24	40
Link Distance (ft)	400	400
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		1
Queuing Penalty (veh)		4

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Existing NP - PM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	178	23	158	30	1067	49	1079	71
v/c Ratio	0.83	0.07	0.61	0.24	0.46	0.32	0.43	0.06
Control Delay	69.6	0.4	42.1	34.4	5.4	54.6	7.1	2.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	69.6	0.4	42.1	34.4	5.4	54.6	7.1	2.8
Queue Length 50th (ft)	111	0	80	18	82	34	93	1
Queue Length 95th (ft)	175	0	137	m37	100	m63	168	m16
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		95		
Base Capacity (vph)	401	561	467	126	2341	153	2485	1097
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.44	0.04	0.34	0.24	0.46	0.32	0.43	0.06

Intersection Summary

m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Existing NP - PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	94	278	23	137	89	65	950	32	100	984	85
v/c Ratio	0.49	0.38	0.17	0.58	0.25	0.39	0.48	0.03	0.69	0.49	0.09
Control Delay	50.2	28.5	46.2	50.3	1.6	49.1	16.5	0.1	74.2	23.5	6.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	50.2	28.5	46.2	50.3	1.6	49.1	16.5	0.1	74.2	23.5	6.0
Queue Length 50th (ft)	58	58	14	84	0	40	193	0	51	171	4
Queue Length 95th (ft)	104	100	39	139	0	80	298	0	#145	407	32
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	406	886	306	351	443	166	1985	926	144	2021	937
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.23	0.31	0.08	0.39	0.20	0.39	0.48	0.03	0.69	0.49	0.09

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

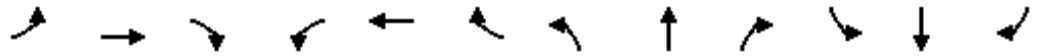
Queuing and Blocking Report

Intersection: 7: Sunset Lane & Atwood Avenue

Movement	NB
Directions Served	LR
Maximum Queue (ft)	30
Average Queue (ft)	1
95th Queue (ft)	12
Link Distance (ft)	199
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Draacea Commercial Project
Project Completion (2022) NP - AM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	28	89	69	101	264	237	61	1353	70	87	1280	39
v/c Ratio	0.18	0.37	0.19	0.42	0.61	0.43	0.38	0.74	0.08	0.55	0.67	0.04
Control Delay	36.8	36.7	1.2	36.2	34.0	6.5	33.6	20.0	0.6	49.5	19.5	0.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	36.8	36.7	1.2	36.2	34.0	6.5	33.6	20.0	0.6	49.5	19.5	0.1
Queue Length 50th (ft)	13	42	0	47	108	0	22	391	0	43	270	0
Queue Length 95th (ft)	36	79	0	87	186	49	m37	#478	m2	#91	#395	0
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	157	439	503	243	470	572	162	1832	883	157	1921	921
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.18	0.20	0.14	0.42	0.56	0.41	0.38	0.74	0.08	0.55	0.67	0.04

Intersection Summary

- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queuing and Blocking Report

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	SB	SB
Directions Served	LTR	LTR	L	L	T
Maximum Queue (ft)	44	44	52	31	73
Average Queue (ft)	19	13	25	6	3
95th Queue (ft)	41	35	49	25	28
Link Distance (ft)	424	42			599
Upstream Blk Time (%)		1			
Queuing Penalty (veh)		0			
Storage Bay Dist (ft)			50	50	
Storage Blk Time (%)			0	0	0
Queuing Penalty (veh)			3	0	0

Queuing and Blocking Report

Intersection: 3: Perris Boulevard & Auto Center Driveway

Movement	SB	SB
Directions Served	T	T
Maximum Queue (ft)	91	116
Average Queue (ft)	10	10
95th Queue (ft)	50	52
Link Distance (ft)	400	400
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		1
Queuing Penalty (veh)		4

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - AM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	171	65	294	63	1429	42	1370	108
v/c Ratio	0.60	0.15	0.82	0.34	0.70	0.23	0.70	0.12
Control Delay	35.0	6.2	43.9	50.7	22.4	27.9	9.1	0.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	35.0	6.2	43.9	50.7	22.4	27.9	9.1	0.5
Queue Length 50th (ft)	76	0	127	34	375	22	63	1
Queue Length 95th (ft)	114	21	176	m45	#457	m36	#96	m1
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		95		
Base Capacity (vph)	387	554	476	212	2038	212	1945	882
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.44	0.12	0.62	0.30	0.70	0.20	0.70	0.12

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) NP - AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	111	355	78	353	204	76	1247	63	167	1248	140
v/c Ratio	0.58	0.41	0.42	0.78	0.38	0.46	0.87	0.09	1.06	0.83	0.19
Control Delay	46.5	23.7	40.6	41.0	5.9	44.1	33.9	0.7	109.0	18.9	1.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	46.5	23.7	40.6	41.0	5.9	44.1	33.9	0.7	109.0	18.9	1.3
Queue Length 50th (ft)	53	68	37	161	0	36	-328	0	-93	-371	0
Queue Length 95th (ft)	95	93	71	222	37	72	#429	1	m#165	#410	m0
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	203	986	203	522	584	165	1428	698	157	1511	726
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.55	0.36	0.38	0.68	0.35	0.46	0.87	0.09	1.06	0.83	0.19

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queuing and Blocking Report

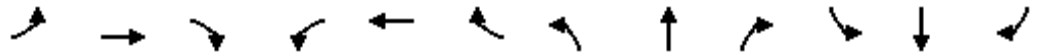
Intersection: 7: Sunset Lane & Atwood Avenue

Movement	NB
Directions Served	LR
Maximum Queue (ft)	31
Average Queue (ft)	3
95th Queue (ft)	17
Link Distance (ft)	273
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Draacea Commercial Project
Project Completion (2022) NP - PM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	57	211	88	58	142	120	58	1182	66	108	1316	52
v/c Ratio	0.37	0.67	0.23	0.46	0.51	0.33	0.37	0.59	0.07	0.86	0.65	0.06
Control Delay	49.2	49.4	2.6	57.0	45.1	6.8	32.6	24.7	4.5	96.7	20.2	0.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	49.2	49.4	2.6	57.0	45.1	6.8	32.6	24.7	4.5	96.7	20.2	0.7
Queue Length 50th (ft)	35	128	0	36	84	0	35	392	7	69	318	0
Queue Length 95th (ft)	73	189	11	78	139	34	m57	485	m23	#169	482	5
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	198	475	504	126	399	451	173	2009	919	126	2025	915
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.29	0.44	0.17	0.46	0.36	0.27	0.34	0.59	0.07	0.86	0.65	0.06

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Queuing and Blocking Report

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	NB	NB	SB	SB
Directions Served	LTR	LTR	L	T	T	L	T
Maximum Queue (ft)	63	42	58	84	21	52	40
Average Queue (ft)	28	11	21	4	1	23	2
95th Queue (ft)	54	35	50	33	8	51	15
Link Distance (ft)	424	32		400	400		599
Upstream Blk Time (%)		2					
Queuing Penalty (veh)		0					
Storage Bay Dist (ft)			50			50	
Storage Blk Time (%)			2			1	0
Queuing Penalty (veh)			11			6	0

Queuing and Blocking Report

Intersection: 3: Perris Boulevard & Auto Center Driveway

Movement	SB	SB
Directions Served	T	T
Maximum Queue (ft)	94	97
Average Queue (ft)	6	11
95th Queue (ft)	39	51
Link Distance (ft)	400	400
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		2
Queuing Penalty (veh)		9

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) NP - PM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	190	24	167	32	1221	52	1270	76
v/c Ratio	0.85	0.07	0.62	0.25	0.53	0.34	0.52	0.07
Control Delay	70.1	0.3	41.9	34.4	6.3	53.4	9.1	4.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	70.1	0.3	41.9	34.4	6.3	53.4	9.1	4.3
Queue Length 50th (ft)	119	0	85	20	92	36	108	1
Queue Length 95th (ft)	184	0	143	m32	130	m57	246	m17
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		95		
Base Capacity (vph)	398	561	459	126	2304	155	2452	1085
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.48	0.04	0.36	0.25	0.53	0.34	0.52	0.07

Intersection Summary

m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) NP - PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	100	295	24	145	94	69	1096	34	106	1166	90
v/c Ratio	0.51	0.39	0.18	0.59	0.26	0.40	0.56	0.04	0.74	0.59	0.10
Control Delay	50.3	28.5	46.2	50.3	1.7	49.1	18.3	0.1	77.1	25.8	5.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	50.3	28.5	46.2	50.3	1.7	49.1	18.3	0.1	77.1	25.8	5.4
Queue Length 50th (ft)	61	63	15	88	0	42	240	0	59	230	7
Queue Length 95th (ft)	109	105	40	144	0	83	367	0	#159	490	28
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	406	892	306	351	443	171	1959	916	144	1985	923
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.25	0.33	0.08	0.41	0.21	0.40	0.56	0.04	0.74	0.59	0.10

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

Queuing and Blocking Report

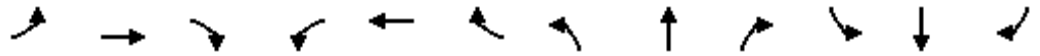
Intersection: 7: Sunset Lane & Atwood Avenue

Movement	NB
Directions Served	LR
Maximum Queue (ft)	30
Average Queue (ft)	3
95th Queue (ft)	17
Link Distance (ft)	199
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - AM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	31	89	89	121	264	237	85	1359	89	87	1290	39
v/c Ratio	0.20	0.40	0.26	0.45	0.59	0.43	no cap	0.75	0.10	0.55	0.91	0.05
Control Delay	37.2	37.6	1.9	36.2	33.1	6.4		14.8	0.8	49.5	33.9	0.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
Total Delay	37.2	37.6	1.9	36.2	33.1	6.4	Error	14.8	0.8	49.5	33.9	0.1
Queue Length 50th (ft)	15	42	0	56	105	0	-89	330	0	43	312	0
Queue Length 95th (ft)	39	79	0	101	186	49	m#103	m#414	m2	#91	#420	0
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	157	439	503	271	483	581	1	1807	874	157	1421	727
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.20	0.20	0.18	0.45	0.55	0.41	85.00	0.75	0.10	0.55	0.91	0.05

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queuing and Blocking Report

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	SB	SB	SB
Directions Served	R	R	L	L	T	T
Maximum Queue (ft)	64	44	53	69	31	95
Average Queue (ft)	27	18	22	21	1	7
95th Queue (ft)	55	39	52	46	12	47
Link Distance (ft)	424	42			599	599
Upstream Blk Time (%)		2				
Queuing Penalty (veh)		1				
Storage Bay Dist (ft)			150	125		
Storage Blk Time (%)						0
Queuing Penalty (veh)						0

Queuing and Blocking Report

Intersection: 3: Perris Boulevard & Auto Center Driveway/Project Driveway 1

Movement	EB	WB	SB	SB	SB
Directions Served	R	R	T	T	TR
Maximum Queue (ft)	28	54	73	76	70
Average Queue (ft)	3	27	5	8	3
95th Queue (ft)	17	45	31	43	27
Link Distance (ft)	135	110	405	405	
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					25
Storage Blk Time (%)				2	
Queuing Penalty (veh)				8	

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - AM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	212	64	415	62	1490	85	1348	106
v/c Ratio	0.45	0.11	0.94	0.32	0.97	0.44	0.87	0.15
Control Delay	25.0	5.6	57.7	39.0	32.5	30.3	13.4	0.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	25.0	5.6	57.7	39.0	32.5	30.3	13.4	0.6
Queue Length 50th (ft)	84	0	197	33	-414	45	76	1
Queue Length 95th (ft)	136	21	#345	m40	#490	m59	102	m0
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		100		
Base Capacity (vph)	474	586	443	212	1540	212	1551	725
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.45	0.11	0.94	0.29	0.97	0.40	0.87	0.15

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	131	355	78	353	224	76	1267	63	187	1267	159
v/c Ratio	0.83	0.40	0.50	0.85	0.43	0.42	0.86	0.09	1.19	0.83	0.21
Control Delay	77.3	24.8	46.3	50.3	7.2	41.4	30.0	0.6	147.0	20.7	2.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	77.3	24.8	46.3	50.3	7.2	41.4	30.0	0.6	147.0	20.7	2.9
Queue Length 50th (ft)	66	71	38	167	2	36	303	0	~120	367	17
Queue Length 95th (ft)	#142	100	73	#259	43	71	346	1	m#154	m#407	m19
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	157	891	157	439	535	191	1466	713	157	1521	745
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.83	0.40	0.50	0.80	0.42	0.40	0.86	0.09	1.19	0.83	0.21

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queuing and Blocking Report

Intersection: 6: Draacea Avenue & Project Driveway 2

Movement	EB	WB	SB
Directions Served	LT	TR	LR
Maximum Queue (ft)	55	99	77
Average Queue (ft)	23	12	47
95th Queue (ft)	60	58	71
Link Distance (ft)	210	274	106
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Queuing and Blocking Report

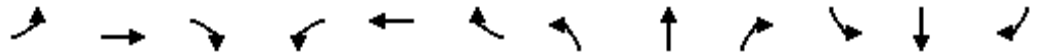
Intersection: 7: Sunset Lane & Atwood Avenue

Movement	NB
Directions Served	LR
Maximum Queue (ft)	32
Average Queue (ft)	14
95th Queue (ft)	39
Link Distance (ft)	273
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - PM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	63	211	99	68	142	120	72	1181	77	108	1321	52
v/c Ratio	0.39	0.67	0.26	0.54	0.51	0.34	no cap	0.59	0.08	0.86	0.85	0.07
Control Delay	49.8	49.4	3.7	61.4	45.5	6.9		23.9	5.4	96.7	32.2	0.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
Total Delay	49.8	49.4	3.7	61.4	45.5	6.9	Error	23.9	5.4	96.7	32.2	0.8
Queue Length 50th (ft)	39	128	0	43	85	0	-99	378	10	69	388	0
Queue Length 95th (ft)	79	189	18	#95	139	34	m#163	475	m24	#169	484	5
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	198	475	504	126	399	451	1	2009	919	126	1552	724
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.32	0.44	0.20	0.54	0.36	0.27	72.00	0.59	0.08	0.86	0.85	0.07

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queuing and Blocking Report

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	SB
Directions Served	R	R	L	L
Maximum Queue (ft)	66	44	51	74
Average Queue (ft)	22	12	24	32
95th Queue (ft)	51	33	47	69
Link Distance (ft)	424	32		
Upstream Blk Time (%)		2		
Queuing Penalty (veh)		0		
Storage Bay Dist (ft)			150	125
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Intersection: 3: Perris Boulevard & Auto Center Driveway/Project Driveway 1

Movement	WB	NB	SB	SB	SB
Directions Served	R	T	T	T	TR
Maximum Queue (ft)	54	40	182	180	71
Average Queue (ft)	25	2	42	46	6
95th Queue (ft)	49	15	129	131	40
Link Distance (ft)	110	141	405	405	
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					25
Storage Blk Time (%)				14	
Queuing Penalty (veh)				64	

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
Project Completion (2022) WP - PM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	211	24	241	31	1254	92	1249	75
v/c Ratio	0.71	0.06	0.97	0.25	0.62	0.53	0.54	0.07
Control Delay	48.8	0.3	85.0	34.6	8.5	57.2	12.9	6.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	48.8	0.3	85.0	34.6	8.5	57.2	12.9	6.3
Queue Length 50th (ft)	125	0	145	19	99	63	169	2
Queue Length 95th (ft)	185	0	#241	m29	148	m81	251	m12
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		100		
Base Capacity (vph)	425	561	352	126	2031	174	2303	1023
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.50	0.04	0.68	0.25	0.62	0.53	0.54	0.07

Intersection Summary

- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
Project Completion (2022) WP - PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	110	295	24	145	104	69	1106	34	116	1177	100
v/c Ratio	0.54	0.35	0.18	0.59	0.29	0.40	0.59	0.04	0.81	0.62	0.11
Control Delay	50.7	27.1	46.2	50.6	2.0	49.1	19.7	0.1	88.5	30.0	5.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	50.7	27.1	46.2	50.6	2.0	49.1	19.7	0.1	88.5	30.0	5.8
Queue Length 50th (ft)	67	62	15	88	0	42	246	0	80	267	11
Queue Length 95th (ft)	117	104	40	144	0	83	377	0	m#173	451	m28
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	406	902	306	351	443	171	1867	879	144	1893	887
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.27	0.33	0.08	0.41	0.23	0.40	0.59	0.04	0.81	0.62	0.11

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Queuing and Blocking Report

Intersection: 6: Draacea Avenue & Project Driveway 2

Movement	EB	SB
Directions Served	LT	LR
Maximum Queue (ft)	76	55
Average Queue (ft)	17	33
95th Queue (ft)	52	52
Link Distance (ft)	210	106
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queuing and Blocking Report

Intersection: 7: Sunset Lane & Atwood Avenue

Movement	NB
Directions Served	LR
Maximum Queue (ft)	31
Average Queue (ft)	16
95th Queue (ft)	40
Link Distance (ft)	199
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	58	170	150	92	401	216	129	1274	65	90	1438	92
v/c Ratio	0.37	0.49	0.35	0.43	0.88	0.40	0.73	0.75	0.08	0.57	0.93	0.12
Control Delay	41.6	34.9	5.4	39.6	53.1	6.4	48.4	21.5	0.6	50.8	35.3	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	41.6	34.9	5.4	39.6	53.1	6.4	48.4	21.5	0.6	50.8	35.3	0.3
Queue Length 50th (ft)	28	80	0	41	196	0	54	370	0	44	~378	0
Queue Length 95th (ft)	64	130	33	#93	#360	52	m#118	#461	m1	#103	#533	0
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	157	439	503	212	458	548	177	1691	829	157	1551	778
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.37	0.39	0.30	0.43	0.88	0.39	0.73	0.75	0.08	0.57	0.93	0.12

Intersection Summary

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Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queuing and Blocking Report
 GPBO (2040) NP - AM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) NP - AM Peak Hour

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	NB	NB	SB	SB	SB	SB
Directions Served	LTR	LTR	L	T	T	L	T	T	R
Maximum Queue (ft)	432	45	82	115	70	100	604	618	100
Average Queue (ft)	169	43	44	8	3	39	200	204	9
95th Queue (ft)	424	45	80	50	27	84	599	608	56
Link Distance (ft)	424	42		400	400		599	599	
Upstream Blk Time (%)	18	75					1	2	
Queuing Penalty (veh)	0	91					5	13	
Storage Bay Dist (ft)			50			50			50
Storage Blk Time (%)			10	0	0	6	21	23	
Queuing Penalty (veh)			75	0	0	49	12	4	

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Queuing and Blocking Report
 GPBO (2040) NP - AM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) NP - AM Peak Hour

Intersection: 3: Perris Boulevard & Auto Center Driveway

Movement	EB	SB	SB	SB
Directions Served	LR	T	T	TR
Maximum Queue (ft)	30	420	406	75
Average Queue (ft)	10	271	286	38
95th Queue (ft)	27	490	493	95
Link Distance (ft)	134	400	400	
Upstream Blk Time (%)		13	14	
Queuing Penalty (veh)		111	119	
Storage Bay Dist (ft)				25
Storage Blk Time (%)			49	
Queuing Penalty (veh)			287	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - AM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	171	60	315	75	1364	43	1527	177
v/c Ratio	0.66	0.14	0.80	0.40	0.68	0.23	0.80	0.20
Control Delay	38.9	5.6	40.4	42.9	14.8	24.8	10.8	0.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	38.9	5.6	40.4	42.9	14.8	24.8	10.8	0.5
Queue Length 50th (ft)	77	0	135	41	362	22	91	1
Queue Length 95th (ft)	130	22	206	m54	#479	m29	m#491	m0
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		95		
Base Capacity (vph)	342	554	514	212	2012	212	1913	902
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.50	0.11	0.61	0.35	0.68	0.20	0.80	0.20

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) NP - AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	97	308	90	349	172	96	1213	78	162	1383	139
v/c Ratio	0.62	0.35	0.57	0.76	0.33	0.55	0.82	0.11	1.03	0.89	0.19
Control Delay	53.8	21.5	50.8	38.9	5.5	48.2	29.6	1.7	99.1	25.8	3.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	53.8	21.5	50.8	38.9	5.5	48.2	29.6	1.7	99.1	25.8	3.4
Queue Length 50th (ft)	48	55	44	159	0	46	298	0	-93	-451	12
Queue Length 95th (ft)	#112	85	#103	240	41	#111	#460	12	m#145	#586	m24
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	157	1032	157	546	582	174	1480	719	157	1546	737
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.62	0.30	0.57	0.64	0.30	0.55	0.82	0.11	1.03	0.89	0.19

Intersection Summary

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Queue shown is maximum after two cycles.
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Queuing and Blocking Report
 GPBO (2040) NP - AM Peak Hour

Perris/Dracaea Commercial Project
 GPBO (2040) NP - AM Peak Hour

Intersection: 7: Sunset Lane & Atwood Avenue

Movement	WB	NB
Directions Served	LT	LR
Maximum Queue (ft)	402	53
Average Queue (ft)	157	17
95th Queue (ft)	372	43
Link Distance (ft)	409	273
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	149	516	265	78	301	140	147	1551	88	111	1490	100
v/c Ratio	0.78	1.00	0.49	0.62	0.74	0.31	0.91	0.96	0.12	0.88	0.96	0.14
Control Delay	71.1	78.1	18.2	67.1	49.3	7.5	57.3	37.4	6.4	101.2	43.6	4.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	71.1	78.1	18.2	67.1	49.3	7.5	57.3	37.4	6.4	101.2	43.6	4.3
Queue Length 50th (ft)	94	~372	64	49	181	0	88	536	9	71	473	1
Queue Length 95th (ft)	#190	#572	144	#115	#301	48	m96	m#572	m14	#175	#637	30
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	198	517	536	126	407	457	162	1624	761	126	1552	724
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.75	1.00	0.49	0.62	0.74	0.31	0.91	0.96	0.12	0.88	0.96	0.14

Intersection Summary

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Queuing and Blocking Report
 GPBO (2040) NP - PM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) NP - PM Peak Hour

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	NB	NB	SB	SB	SB	SB
Directions Served	LTR	LTR	L	T	T	L	T	T	R
Maximum Queue (ft)	439	43	138	252	203	100	603	599	100
Average Queue (ft)	377	35	45	50	39	73	319	309	21
95th Queue (ft)	482	43	99	174	155	115	705	703	87
Link Distance (ft)	424	32		400	400		599	599	
Upstream Blk Time (%)	44	97					1	0	
Queuing Penalty (veh)	0	99					5	4	
Storage Bay Dist (ft)			50			50			50
Storage Blk Time (%)			1	6	9	29	27	30	
Queuing Penalty (veh)			12	4	1	257	41	5	

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Queuing and Blocking Report
 GPBO (2040) NP - PM Peak Hour

Perris/Dracaean Commercial Project
 GPBO (2040) NP - PM Peak Hour

Intersection: 3: Perris Boulevard & Auto Center Driveway

Movement	SB	SB	SB
Directions Served	T	T	TR
Maximum Queue (ft)	428	418	75
Average Queue (ft)	360	363	32
95th Queue (ft)	476	475	92
Link Distance (ft)	400	400	
Upstream Blk Time (%)	19	20	
Queuing Penalty (veh)	181	190	
Storage Bay Dist (ft)			25
Storage Blk Time (%)		51	
Queuing Penalty (veh)		321	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) NP - PM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	602	81	238	50	1664	70	1679	118
v/c Ratio	1.53	0.14	1.61	0.40	0.89	0.56	0.89	0.14
Control Delay	278.5	4.4	328.3	41.7	20.8	61.2	24.5	6.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	278.5	4.4	328.3	41.7	20.8	61.2	24.5	6.7
Queue Length 50th (ft)	~540	0	~210	27	193	47	312	11
Queue Length 95th (ft)	#751	25	#364	m35	#400	m61	m350	m15
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		95		
Base Capacity (vph)	394	561	148	126	1876	126	1884	866
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	1.53	0.14	1.61	0.40	0.89	0.56	0.89	0.14

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) NP - PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	190	511	41	221	173	80	1408	43	165	1567	126
v/c Ratio	0.67	0.50	0.28	0.71	0.42	0.52	0.89	0.06	1.15	0.95	0.16
Control Delay	50.9	28.7	47.6	52.2	7.8	57.1	36.1	0.1	142.3	39.9	6.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	50.9	28.7	47.6	52.2	7.8	57.1	36.1	0.1	142.3	39.9	6.8
Queue Length 50th (ft)	115	135	25	134	0	49	426	0	~121	-564	12
Queue Length 95th (ft)	177	174	58	207	48	#117	#680	0	m#151	m#749	m19
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	406	1012	306	361	450	153	1585	768	144	1646	789
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.47	0.50	0.13	0.61	0.38	0.52	0.89	0.06	1.15	0.95	0.16

Intersection Summary

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Queuing and Blocking Report
 GPBO (2040) NP - PM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) NP - PM Peak Hour

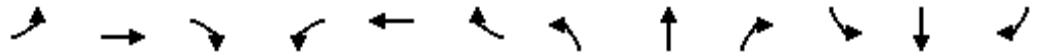
Intersection: 7: Sunset Lane & Atwood Avenue

Movement	WB
Directions Served	LT
Maximum Queue (ft)	433
Average Queue (ft)	416
95th Queue (ft)	487
Link Distance (ft)	418
Upstream Blk Time (%)	86
Queuing Penalty (veh)	0
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	61	170	167	109	401	216	206	1279	81	90	1446	92
v/c Ratio	0.39	0.49	0.39	0.50	0.88	0.39	no cap	0.76	0.10	0.57	1.02	0.13
Control Delay	42.2	34.9	6.9	42.6	51.9	6.3		19.0	0.7	50.8	54.1	0.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
Total Delay	42.2	34.9	6.9	42.6	51.9	6.3	Error	19.0	0.7	50.8	54.1	0.4
Queue Length 50th (ft)	29	80	0	49	196	0	~217	368	0	44	~386	0
Queue Length 95th (ft)	66	130	43	#122	#360	52	m#264	m#438	m2	#103	#538	0
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	157	439	503	216	463	551	1	1683	826	157	1421	727
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.39	0.39	0.33	0.50	0.87	0.39	206.00	0.76	0.10	0.57	1.02	0.13

Intersection Summary

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Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queuing and Blocking Report
 GPBO (2040) WP - AM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) WP - AM Peak Hour

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	NB	NB	NB	SB	SB	
Directions Served	R	R	L	T	T	R	L	T	
Maximum Queue (ft)	63	48	199	322	315	100	74	30	
Average Queue (ft)	23	41	50	91	94	14	46	1	
95th Queue (ft)	45	52	150	285	286	70	81	12	
Link Distance (ft)	424	42		405	405			599	
Upstream Blk Time (%)	63								
Queuing Penalty (veh)	86								
Storage Bay Dist (ft)			150			50	125		
Storage Blk Time (%)				14	27			0	
Queuing Penalty (veh)				10	6			0	

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Queuing and Blocking Report
 GPBO (2040) WP - AM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) WP - AM Peak Hour

Intersection: 3: Perris Boulevard & Auto Center Driveway/Project Driveway 1

Movement	WB	NB	NB	SB	SB	SB
Directions Served	R	T	TR	T	T	TR
Maximum Queue (ft)	53	145	171	418	446	75
Average Queue (ft)	31	27	29	193	213	42
95th Queue (ft)	60	114	125	430	447	101
Link Distance (ft)	110	141	141	405	405	
Upstream Blk Time (%)		0	1	1	2	
Queuing Penalty (veh)		4	7	9	16	
Storage Bay Dist (ft)						25
Storage Blk Time (%)					39	
Queuing Penalty (veh)					234	

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - AM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	205	59	416	74	1415	83	1508	175
v/c Ratio	0.50	0.11	0.94	0.40	0.88	0.44	0.93	0.22
Control Delay	26.2	4.8	56.2	40.5	23.0	27.0	20.6	0.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	26.2	4.8	56.2	40.5	23.0	27.0	20.6	0.9
Queue Length 50th (ft)	79	0	185	40	-380	42	-420	1
Queue Length 95th (ft)	143	21	#357	m49	m#500	m48	m#221	m0
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		100		
Base Capacity (vph)	434	590	470	203	1613	203	1623	791
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.47	0.10	0.89	0.36	0.88	0.41	0.93	0.22

Intersection Summary

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Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) WP - AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	114	308	90	349	189	96	1230	78	178	1399	155
v/c Ratio	0.73	0.32	0.57	0.76	0.36	0.55	0.89	0.11	1.13	0.97	0.22
Control Delay	63.2	20.6	50.8	38.9	5.7	48.2	34.7	1.8	120.7	30.6	3.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	63.2	20.6	50.8	38.9	5.7	48.2	34.7	1.8	120.7	30.6	3.3
Queue Length 50th (ft)	57	55	44	159	0	46	304	0	~110	-461	18
Queue Length 95th (ft)	#137	85	#103	240	45	#111	#470	12	m#132	m#531	m22
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	157	1042	157	546	591	174	1381	679	157	1447	702
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.73	0.30	0.57	0.64	0.32	0.55	0.89	0.11	1.13	0.97	0.22

Intersection Summary

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Queuing and Blocking Report
 GPBO (2040) WP - AM Peak Hour

Perris/Dracaea Commercial Project
 GPBO (2040) WP - AM Peak Hour

Intersection: 6: Dracaea Avenue & Project Driveway 2

Movement	EB	WB	SB
Directions Served	LT	TR	LR
Maximum Queue (ft)	117	171	102
Average Queue (ft)	31	31	59
95th Queue (ft)	76	102	98
Link Distance (ft)	210	274	106
Upstream Blk Time (%)			0
Queuing Penalty (veh)			0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

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Queuing and Blocking Report
 GPBO (2040) WP - AM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) WP - AM Peak Hour

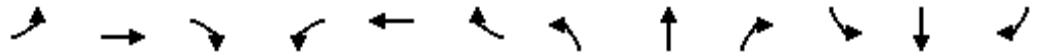
Intersection: 7: Sunset Lane & Atwood Avenue

Movement	WB	NB
Directions Served	LT	LR
Maximum Queue (ft)	424	94
Average Queue (ft)	125	28
95th Queue (ft)	394	76
Link Distance (ft)	409	273
Upstream Blk Time (%)	14	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Attachment: Exhibit A to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

Queues
1: Perris Boulevard & Eucalyptus Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	155	516	275	88	301	140	201	1550	98	111	1495	100
v/c Ratio	0.80	1.00	0.51	0.70	0.74	0.31	no cap	0.95	0.13	0.88	0.96	0.14
Control Delay	73.8	78.1	19.1	74.4	49.6	7.6		33.4	6.7	101.2	44.2	4.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0
Total Delay	73.8	78.1	19.1	74.4	49.6	7.6	Error	33.4	6.7	101.2	44.2	4.3
Queue Length 50th (ft)	98	~372	70	56	181	0	~273	536	12	71	475	1
Queue Length 95th (ft)	#201	#572	154	#133	#301	48	m#276	m523	m15	#175	#641	30
Internal Link Dist (ft)		465			496			595			643	
Turn Bay Length (ft)	50		50	100			95		95	250		100
Base Capacity (vph)	198	517	536	126	405	455	1	1624	761	126	1552	724
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.78	1.00	0.51	0.70	0.74	0.31	201.00	0.95	0.13	0.88	0.96	0.14

Intersection Summary

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Queuing and Blocking Report
 GPBO (2040) WP - PM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) WP - PM Peak Hour

Intersection: 2: Perris Boulevard & Atwood Avenue

Movement	EB	WB	NB	NB	NB	NB	SB	SB	SB	SB
Directions Served	R	R	L	T	T	R	L	T	T	R
Maximum Queue (ft)	439	44	139	446	426	100	100	594	574	99
Average Queue (ft)	153	36	73	193	194	9	88	278	268	5
95th Queue (ft)	403	47	145	492	490	53	123	596	586	39
Link Distance (ft)	424	32		405	405			599	599	
Upstream Blk Time (%)	12	67		4	5			0		
Queuing Penalty (veh)	0	74		38	50			1		
Storage Bay Dist (ft)			150			50	125			50
Storage Blk Time (%)			1	39	43		48	19	23	
Queuing Penalty (veh)			13	31	6		430	36	5	

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Queuing and Blocking Report
 GPBO (2040) WP - PM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) WP - PM Peak Hour

Intersection: 3: Perris Boulevard & Auto Center Driveway/Project Driveway 1

Movement	WB	NB	NB	SB	SB	SB
Directions Served	R	T	TR	T	T	TR
Maximum Queue (ft)	74	145	148	420	440	75
Average Queue (ft)	28	35	37	348	357	41
95th Queue (ft)	65	132	136	477	473	102
Link Distance (ft)	110	141	141	405	405	
Upstream Blk Time (%)		1	1	13	15	
Queuing Penalty (veh)		6	9	124	143	
Storage Bay Dist (ft)						25
Storage Blk Time (%)					50	
Queuing Penalty (veh)					313	

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Queues
4: Perris Boulevard & Dracaea Avenue

Perris/Dracaea Commercial Project
GPBO (2040) WP - PM Peak Hour



Lane Group	EBT	EBR	WBT	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	623	81	309	49	1696	111	1659	117
v/c Ratio	1.61	0.14	5.24	0.39	0.95	0.88	0.88	0.14
Control Delay	315.4	4.4	1954.0	41.6	25.6	90.1	24.2	6.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	315.4	4.4	1954.0	41.6	25.6	90.1	24.2	6.6
Queue Length 50th (ft)	~574	0	~342	26	208	76	313	8
Queue Length 95th (ft)	#786	25	#513	m34	m#656	m96	m341	m14
Internal Link Dist (ft)	143		198		1242		119	
Turn Bay Length (ft)		50		100		100		
Base Capacity (vph)	386	561	59	126	1792	126	1884	866
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	1.61	0.14	5.24	0.39	0.95	0.88	0.88	0.14

Intersection Summary

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Queues
5: Perris Boulevard & Cottonwood Avenue

Perris/Draacea Commercial Project
GPBO (2040) WP - PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	200	511	41	221	183	80	1418	43	175	1577	136
v/c Ratio	0.68	0.50	0.28	0.72	0.45	0.53	0.90	0.06	1.22	0.96	0.17
Control Delay	50.8	28.4	47.6	52.9	8.9	57.8	37.6	0.1	144.6	34.1	7.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	50.8	28.4	47.6	52.9	8.9	57.8	37.6	0.1	144.6	34.1	7.0
Queue Length 50th (ft)	121	134	25	134	0	49	436	0	~135	-577	13
Queue Length 95th (ft)	184	174	58	209	56	#117	#687	0	m#162	m#721	m20
Internal Link Dist (ft)		169		578			419			1242	
Turn Bay Length (ft)	115		100		355	95		175	95		200
Base Capacity (vph)	406	1025	306	358	448	151	1570	762	144	1635	785
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.49	0.50	0.13	0.62	0.41	0.53	0.90	0.06	1.22	0.96	0.17

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Queuing and Blocking Report
 GPBO (2040) WP - PM Peak Hour

Perris/Dracaea Commercial Project
 GPBO (2040) WP - PM Peak Hour

Intersection: 6: Dracaea Avenue & Project Driveway 2

Movement	EB	WB	SB
Directions Served	LT	TR	LR
Maximum Queue (ft)	97	289	121
Average Queue (ft)	15	66	63
95th Queue (ft)	59	184	112
Link Distance (ft)	210	274	106
Upstream Blk Time (%)		3	14
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

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Queuing and Blocking Report
 GPBO (2040) WP - PM Peak Hour

Perris/Draacea Commercial Project
 GPBO (2040) WP - PM Peak Hour

Intersection: 7: Sunset Lane & Atwood Avenue

Movement	WB	NB
Directions Served	LT	LR
Maximum Queue (ft)	433	71
Average Queue (ft)	186	15
95th Queue (ft)	457	44
Link Distance (ft)	418	199
Upstream Blk Time (%)	19	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

**CITY OF MORENO VALLEY
PLANNING COMMISSION
VIA TELECONFERENCE ONLY
PURSUANT TO COVID-19
GOVERNOR EXECUTIVE ORDER N-29-20
NOTICE OF PUBLIC HEARING AND
ENVIRONMENTAL DETERMINATION**

NOTICE IS HEREBY GIVEN that a teleconferenced Public Hearing will be held by the Planning Commission of the City of Moreno Valley on the date and time set forth below:

Date and Time: November 12, 2020 at 7:00 p.m.

Location: **VIA TELECONFERENCE ONLY**

Go to <http://morenovalleyca.igm2.com/Citizens/default.aspx> for instructions.

Item: PEN19-0206 General Plan Amendment

PEN19-0207 Change of Zone

PEN19-0204 Conditional Use Permit

PEN19-0205 Conditional Use Permit

Applicant: Cadence Acquisition LLC

Property Owner: John David Monjazi/Jonathan J Monjazi

APN: 479-120-027, 029, 042, and 043

Location: East side of Perris Boulevard between Dracaea Avenue and Atwood Avenue

Proposal: The applicant is requesting approval of the following entitlements for an 2.04-acre site: 1) a General Plan Amendment (GPA) amending Figure 2-2 "Land Use Map" of the Moreno Valley General Plan to change the land use designation of the project site from Residential/ Office (R/O) to Commercial (C); 2) a Change of Zone amending the City of Moreno Valley Zoning Atlas to rezone the project site from Office (O) District and Office Commercial (OC) District to Community Commercial (CC) District; 3) a Conditional Use permit for a vehicle fueling station and convenience store; and 4) a Conditional Use Permit for a drive-through restaurant with outdoor seating.

Council District: 3

Environmental Determination: The project has been evaluated against criteria set forth in the California Environmental Quality Act (CEQA) Guidelines Section 15070 and has determined that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because mitigation measures have been required of the project that will reduce potential impacts to a less than significant level. Therefore, a Mitigated Negative Declaration is recommended for the project.

PUBLIC TESTIMONY: All interested parties will be provided an opportunity to submit oral testimony during the teleconferenced Public Hearing and/or provide written testimony during or prior to the teleconferenced Public Hearing. The application file and related environmental documents may be inspected by appointment at the Community Development Department at 14177 Frederick Street, Moreno Valley, California by calling (951) 413-3206 during normal business hours (7:30 a.m. to 5:30 p.m., Monday through Thursday).

COVID-19 – IMPORTANT NOTICES: Please note that due to the COVID-19 pandemic situation, staff will attempt to make reasonable arrangements to ensure accessibility to inspect the aforementioned records. **In addition, special instructions on how to effectively participate in the teleconferenced Public Hearing, as approved by Governor Executive Order No. N-25-20, will be posted at <http://morenovalleyca.igm2.com/Citizens/default.aspx> and will be described in the Planning Commission agenda.**

PLEASE NOTE: The Planning Commission may consider and approve changes to the proposed items under consideration during the teleconferenced Public Hearing.

GOVERNMENT CODE § 65009 NOTICE: If you challenge any of the proposed actions taken by the Planning Commission in court, you may be limited to raising only those issues you or someone else raised during the teleconferenced Public Hearing described in this notice, or in written correspondence delivered to the Planning Division of the City of Moreno Valley during or prior to, the teleconferenced Public Hearing.

ACCESSIBILITY: Upon request and in compliance with the Americans with Disabilities Act of 1990, any person with a disability who requires a modification or accommodation in order to participate

should direct such request to Guy Pegan, ADA Coordinator, at (951) 413-3120 at least 48 hours before meeting. The 48-hour notification will enable the City to make reasonable arrangements to ensure accessibility to this meeting.

STAFF CONTACT: Due to the COVID-19 pandemic situation, if you have questions regarding this Public Hearing, please contact Julia Descoteaux, Associate Planner, by telephone at (951) 413-3209 or via email at juliad@moval.org.

/s/Patty Nevins	Press-Enterprise	October 23, 2020
Patty Nevins	Newspaper	Date of Publication
Planning Official		
Community Development Department		

MITIGATED NEGATIVE DECLARATION

Project Name: Perris and Dracaea Commercial Retail

Project Location: The project is located in Township 3S, Range 3W, Section 8 of the Sunnymead, CA United States Geological Survey 7.5-minute quadrangle map of the San Bernardino Base Meridian. The project site is located on the northeast corner of Perris Boulevard and Dracaea Avenue in the City of Moreno Valley (City), Riverside County. The project site is currently vacant. The site consists of four parcels, Assessor's Parcel Number (APN) 479-120-027, 029, 042, and 043. The parcels are approximately 4,000 feet south of State Route 60 (SR-60) and Interstate 215 (I-215) is approximately 3.5 miles west of the project site (Figure 1 depicts the regional and project location and Figure 2 depicts the existing setting). The March Air Reserve Base is located approximately 2.75 miles southwest of the project site.

Findings: It is hereby determined that, based on the information contained in the attached Initial Study, the project would not have a significant adverse effect on the environment.

Mitigation measures necessary to avoid the potentially significant effects on the environment are included in the attached Initial Study, which is hereby incorporated and fully made part of this Mitigated Negative Declaration. The City of Moreno Valley has hereby agreed to implement each of the identified mitigation measures, which would be adopted as part of the attached Mitigation Monitoring and Reporting Program.

MITIGATION MONITORING AND REPORTING PROGRAM PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT

This Mitigation Monitoring and Reporting Program has been prepared for use in implementing mitigation for the:

Perris and Dracaea Commercial Retail Project City Case No. PPA18-0018

The program has been prepared in compliance with State law and the Initial Study/Mitigated Negative Declaration (IS/MND) prepared for the project by the City of Moreno Valley (City).

The California Environmental Quality Act (CEQA) requires adoption of a reporting or monitoring program for those measures placed on a project to mitigate or avoid significant effects on the environment (Public Resource Code Section 21081.6). The law states the reporting or monitoring program shall be designed to ensure compliance during Project implementation.

The monitoring program contains the following elements:

- 1) The mitigation measures are recorded with the action and procedure necessary to ensure compliance. In some instances, one action may be used to verify implementation of several mitigation measures.
- 2) A procedure for compliance and verification has been outlined for each action necessary. This procedure designates who will take action, what action will be taken and when, and to whom and when compliance will be reported.
- 3) The program has been designed to be flexible. As monitoring progresses, changes to compliance procedures may be necessary based upon recommendations by those responsible for the program. As changes are made, new monitoring compliance procedures and records will be developed and incorporated into the program.

This Mitigation Monitoring and Reporting Program includes mitigation identified in the IS/MND.

MITIGATION MONITORING AND RESPONSIBILITIES

As the Lead Agency, the City is responsible for ensuring full compliance with the mitigation measures adopted for the proposed Project. The City will monitor and report on all mitigation activities. Mitigation measures will be implemented at different stages of development throughout the Project site. In this regard, the responsibilities for implementation have been assigned to the Applicant, Contractor, or a combination thereof. If during the course of Project implementation, any of the mitigation measures identified herein cannot be successfully implemented, the City shall be immediately informed, and the City will then inform any affected responsible agencies. The City, in conjunction with any affected responsible agencies, will then determine if modification to the project is required and/or whether alternative mitigation is appropriate.

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

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Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

MITIGATION MONITORING AND REPORTING PROGRAM CHECKLIST

Project Name: Perris and Dracaea Commercial Retail Project

Applicant: Cadence Capital Investments, LLC

Date: October 19, 2020

Mitigation Measure No. / Implementing Action	Responsible for Monitoring	Timing of Verification	Method of Verification	Verified Date/ Initials	Sanctions for Non-Compliance
BIOLOGICAL RESOURCES					
MITIGATION MEASURE BIO-1: A nesting bird pre-construction survey will be conducted by a qualified biologist three days prior to ground-disturbing activities. Should nesting birds be found, an exclusionary buffer will be established by the qualified biologist. The buffer may be up to 500 feet in diameter depending on the species of nesting bird found. This buffer will be clearly marked in the field by construction personnel under guidance of the qualified biologist and construction or clearing will not be conducted within this zone until the qualified biologist determines that the young have fledged or the nest is no longer active. Nesting bird habitat within the project area will be resurveyed during bird breeding season if there is a lapse in construction activities longer than seven days.	Community Development Director or designee	Prior to ground-disturbing activities and during construction	Evidence to the City: 1) the required pre-construction survey has been completed 2) the establishment and maintenance (as applicable) of appropriate buffer(s)		Issuance of a stop work order
GEOLOGY AND SOILS					
MITIGATION MEASURE GEO-1: The Project Applicant shall retain a qualified paleontologist to attend the pre-grading meeting with the City, the construction manager and any contractors. The paleontologist will conduct a mandatory Paleontological Resources Worker Sensitivity Training to those in attendance. The Training will include a brief review of the paleontological sensitivity of the Project and the surrounding area; what resources could potentially be identified during earthmoving activities; the protocols that apply in the event inadvertent discoveries of paleontological resources are identified, including who to contact	Community Development Director or designee	During the pre-grading meeting and throughout construction	Evidence to the City that a qualified paleontologist is retained and the Paleontological Resources Worker Sensitivity Training is administered as required		Issuance of a stop work order

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>and appropriate avoidance measures until the find(s) can be properly evaluated; and any other appropriate protocols. All new construction personnel that will conduct earthwork or grading activities that begin work on the Project following the initial Training must take the Paleontological Sensitivity Training prior to beginning work, and the Project paleontologist shall make themselves available to provide the training on an as-needed basis.</p>					
<p>MITIGATION MEASURE GEO-2: If paleontological resources (fossils) are discovered during project grading, work shall be halted within 100 feet of the find until a qualified paleontologist assesses the significance of the find. The project paleontologist shall monitor remaining earthmoving activities at the project site and shall be equipped to record and salvage fossil resources that may be unearthed during grading activities. The paleontologist shall be empowered to temporarily halt or divert grading equipment to allow recording and removal of the unearthed resources. Any fossils found shall be evaluated in accordance with the CEQA Guidelines and offered for curation at an accredited facility approved by the City of Moreno Valley. Once grading activities have ceased or the paleontologist determines that monitoring is no longer necessary, monitoring activities shall be discontinued. This measure shall be implemented to the satisfaction of the City Planning Division.</p>	<p>Community Development Director or designee</p>	<p>During grading</p>	<p>Evidence to the City that a qualified paleontologist is retained to evaluate unanticipated encounters and monitors ground-disturbance as needed. Evidence to the City that recovered specimens are offered for curation</p>		<p>Issuance of a stop work order</p>
<p>HYDROLOGY AND WATER QUALITY</p>					
<p>MITIGATION MEASURE HYD-1: Prior to the issuance of a grading permit, the project applicant shall file and obtain a Notice of Intent (NOI) with the Regional Water Quality Control Board (RWQCB) in order to be in compliance with the State NPDES General Construction Storm Water Permit for discharge of surface runoff associated with construction activities. Evidence that this has been obtained (i.e., a copy of the Waste Discharger's Identification Number) shall be submitted to the City</p>	<p>Public Works Department</p>	<p>Prior to issuance of grading permit</p>	<p>Copy of the Waste Discharger's Identification Number submitted to City</p>		<p>Withhold grading permit</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>for coverage under the NPDES General Construction Permit. The NOI shall address the potential for an extended and discontinuous construction period based on funding availability.</p>					
<p>MITIGATION MEASURE HYD-2: Prior to the issuance of a grading permit, the project applicant shall prepare a Storm Water Pollution Prevention Plan (SWPPP). The SWPPP shall include a surface water control plan and erosion control plan citing specific measures to control on-site and off-site erosion during the entire grading and construction period. In addition, the SWPPP shall emphasize structural and nonstructural Best Management Practices (BMPs) to control sediment and non-visible discharges from the site. The SWPPP would include inspection forms for routine monitoring of the site during construction phase to ensure NPDES compliance and additional BMPs and erosion control measures would be documented in the SWPPP and utilized if necessary. The SWPPP shall address the potential for an extended and discontinuous construction period based on funding availability. The SWPPP would be kept on site for the entire duration of project construction and would be available to the local RWQCB for inspection at any time. BMPs included in the SWPPP may include the following:</p> <ul style="list-style-type: none"> • Sediment discharges from the site may be controlled by the following: sandbags, silt fences, straw wattles and temporary basins (if deemed necessary), and other discharge control devices. The construction and condition of the BMPs would be periodically inspected during construction and repairs would be made when necessary as required by the SWPPP. • Materials that have the potential to contribute to non-visible pollutants to storm water must not be placed in drainage ways and must be contained, elevated, and placed in temporary storage containment areas. 	<p>Public Works Department</p>	<p>Prior to issuance of grading permit</p>	<p>Submit a Storm Water Pollution Prevention Plan to the City and kept on site for the entire duration of Project construction and shall be available to the local Regional Water Quality Control Board for inspection.</p>		<p>Withhold grading permit</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<ul style="list-style-type: none"> All loose piles of soil, silt, clay, sand, debris, and other earthen material shall be protected in a reasonable manner to eliminate any discharge from the site. Stockpiles would be surrounded by silt fences and covered with plastic tarps. In addition, the construction contractor shall be responsible for performing and documenting the application of BMPs identified in the SWPPP. Weekly inspections shall be performed on sandbag barriers and other sediment control measures called for in the SWPPP. Monthly reports and inspection logs shall be maintained by the Contractor and reviewed by the City and the representatives of the State Water Resources Control Board. In the event that it is not feasible to implement specific BMPs, the City can make a determination that other BMPs would provide equivalent or superior treatment either on or off site. 					
<p>MITIGATION MEASURE HYD-3: Prior to the issuance of a grading permit, the Project Applicant shall submit a Final Water Quality Management Plan (Final WQMP) to the City of Moreno Valley (City) for review and approval. The project shall implement project design features identified in the Final WQMP. The Final WQMP shall demonstrate that any proposed on-site development plan includes best management practices (BMPs) for source control, pollution prevention, site design, low impact development (LID) implementation, and structural treatment control. BMPs shall be designed and implemented to address 303(d) listed pollutants and retain the project site's minimum design capture volume and hydromodification volume to ensure post-development storm water runoff volume or time of concentration does not exceed pre-development storm water runoff by more than 10 percent of the two-year peak flow in accordance with the Santa Ana Regional Water Quality Control Board Order Number R8-2010-</p>	<p>Public Works Department</p>	<p>Prior to issuance of grading permit</p>	<p>Submit a Final Water Quality Management Plan and grading and development plans to the City detailing the low impact development best management practices.</p>		<p>Withhold grading permit</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>0033, National Pollutant Discharge Elimination System (NPDES) Permit No. CAS618033, as amended by Order No. R8-2013-0024, also known as the Municipal Separate Storm Sewer System (MS4) permit. The proposed LID BMPs specified in the Final WQMP shall be incorporated into the grading and development plans submitted to the City for review and approval. Periodic maintenance of any required BMPs and landscaped areas during project occupancy and operation shall be in accordance with the schedule outlined in the Final WQMP. This measure shall be implemented to the satisfaction of the City Public Works Department.</p>					
NOISE					
<p>MITIGATION MEASURE NOI-1: Prior to the issuance of a grading permit and during all phases of construction, the project applicant shall ensure the following measures are incorporated:</p> <ul style="list-style-type: none"> • The construction contractor shall limit all grading-related activities, including operation of grading equipment, to between the hours of 7:00 a.m. and 7:00 p.m. Monday through Friday and between the hours of 8:00 a.m. and 4:00 p.m. on Saturday in accordance with Moreno Valley Municipal Code Section 8.21.050(O). • The construction contractor shall limit the operation of any tools or equipment used in construction, drilling, repair, alteration or demolition work, to between the hours of 7:00 a.m. and 8:00 p.m. every day in accordance with Moreno Valley Municipal Code Section 11.80.030(D)(7). • During all project site excavation and grading, the project contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers consistent with manufacturers' standards. • The project contractor shall place all stationary construction equipment so that emitted noise is 	<p>City Building Inspector</p>	<p>Prior to issuance of grading and building permits and during all phases of construction</p>	<p>Evidence to the City the construction contractor complies with construction noise ordinance</p>		<p>Withhold grading and building permits, and/or issuance of stop work order</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>directed away from sensitive receptors nearest the project site.</p> <ul style="list-style-type: none"> The construction contractor shall locate equipment staging in areas that would create the greatest distance between construction-related noise sources and noise-sensitive receptors nearest the project site during all project construction. 					
<p>MITIGATION MEASURE NOI-2: The project applicant shall ensure that only small bulldozers are used within 15 feet of the residential structures immediately north and southeast of the project site to ensure vibration levels would not exceed the Federal Transit Administration’s damage threshold of 94 velocity decibels (VdB) (0.2 peak particle velocity (PPV) [in/sec]) for buildings constructed of non-engineered timber and masonry. The project Applicant shall provide evidence to the City that this measure is incorporated into the project grading plans, and directional signage is placed on the construction site to direct equipment operators. This measure shall be implemented to the satisfaction of the Director of Building and Safety or designee.</p>	<p>City Building Inspector</p>	<p>Prior to issuance of grading permits and during construction</p>	<p>Evidence to the City: 1) Construction contractor will prohibit the use of heavy construction equipment (i.e., large bulldozers) along the Project north and east-southeast construction boundaries 2) Temporary on-site signage is placed in the immediate vicinity of the Project site’s northern and eastern construction boundaries</p>		<p>Withhold grading permits and/or issuance of stop work order</p>
TRANSPORTATION					
<p>MITIGATION MEASURE TRA-1: Prior to issuance of an occupancy permit, the applicant shall provide evidence to the City that payment of fair share is made, as identified in the project-specific Transportation Impact Analysis (Appendix E) and Table 17.B of the Initial Study, for improvements at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Cottonwood Avenue under project completion (2022) conditions and at Perris Boulevard/Eucalyptus Avenue and Perris Boulevard/Dracaea Avenue under General Plan build-out (2040) conditions. This measure shall be implemented to the satisfaction of the City of Moreno Valley Director of Engineering, or designee.</p>	<p>City Director of Engineering, or designee</p>	<p>Prior to issuance of occupancy permit</p>	<p>Evidence to the City fair share payment is made</p>		<p>Withhold occupancy permit</p>

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**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

TRIBAL CULTURAL RESOURCES					
<p>MITIGATION MEASURE TCR-1: Prior to the issuance of a grading permit, the Developer shall retain a professional archaeologist to conduct monitoring of all mass grading and trenching activities. The Project Archaeologist shall have the authority to temporarily redirect earthmoving activities in the event that suspected archaeological resources are unearthed during Project construction. The Project Archaeologist, in consultation with the Consulting Tribe(s), the contractor, and the City, shall develop a Cultural Resources Management Plan (CRMP) in consultation pursuant to the definition in AB52 to address the details, timing and responsibility of all archaeological and cultural activities that will occur on the project site. A consulting tribe is defined as a tribe that initiated the AB 52 tribal consultation process for the Project, has not opted out of the AB52 consultation process, and has completed AB 52 consultation with the City as provided for in Cal Pub Res Code Section 21080.3.2(b)(1) of AB52. Details in the Plan shall include:</p> <ul style="list-style-type: none"> a. Project grading and development scheduling; b. The Project archeologist and the Consulting Tribes(s) as defined in TCR-1 shall attend the pre-grading meeting with the City, the construction manager and any contractors and will conduct a mandatory Cultural Resources Worker Sensitivity Training to those in attendance. The Training will include a brief review of the cultural sensitivity of the Project and the surrounding area; what resources could potentially be identified during earthmoving activities; the requirements of the monitoring program; the protocols that apply in the event inadvertent discoveries of cultural resources are identified, including who to contact and appropriate avoidance measures until the find(s) can be properly evaluated; and any other appropriate protocols. All new 	<p>Community Development Director or designee</p>	<p>Prior to issuance of grading and building permits and during ground-disturbing activities</p>	<p>Evidence to the City:</p> <ol style="list-style-type: none"> 1. Professional archaeologist is retained for monitoring; 2. Interested Native American Tribes are engaged for project execution; 3. Preparation of a Cultural Resources Management Plan; 4. Administer Cultural Resources Worker Sensitivity Training; 		<p>Withhold grading permits and/or issuance of stop work order</p>

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**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>construction personnel that will conduct earthwork or grading activities that begin work on the Project following the initial Training must take the Cultural Sensitivity Training prior to beginning work and the Project archaeologist and Consulting Tribe(s) shall make themselves available to provide the training on an as-needed basis;</p> <p>c. The protocols and stipulations that the contractor, City, Consulting Tribe(s) and Project archaeologist will follow in the event of inadvertent cultural resources discoveries, including any newly discovered cultural resource deposits that shall be subject to a cultural resources evaluation.</p>					
<p>MITIGATION MEASURE TCR-2: Prior to the issuance of a grading permit, the Developer shall secure agreements with the Soboba Band of Luiseño Indians (Soboba) and Pechanga Band of Luiseño Indians (Pechanga) for tribal monitoring. The Developer is also required to provide a minimum of 30 days advance notice to the tribes of all mass grading and trenching activities. The Native American Tribal Representatives shall have the authority to temporarily halt and redirect earth moving activities in the affected area in the event that suspected archaeological resources are unearthed. If the Native American Tribal Representatives suspect that an archaeological resource may have been unearthed, the Project Archaeologist or the Tribal Representatives shall immediately redirect grading operations in a 100-foot radius around the find to allow identification and evaluation of the suspected resource. In consultation with the Native American Tribal Representatives, the Project Archaeologist shall evaluate the suspected resource and make a determination of significance pursuant to California Public Resources Code Section 21083.2.</p>	<p>Community Development Director or designee</p>	<p>Prior to issuance of grading and building permits and during ground-disturbing activities</p>	<p>Evidence to the City that interested Native American Tribes and a qualified archaeologist are retained for monitoring and appropriate buffers are established as necessary.</p>		<p>Withhold grading permits and/or issuance of stop work order</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>MITIGATION MEASURE TCR-3: In the event that Native American cultural resources are discovered during the course of grading (inadvertent discoveries), the following procedures shall be carried out for final disposition of the discoveries:</p> <p>a) One or more of the following treatments, in order of preference, shall be employed with the tribes. Evidence of such shall be provided to the City of Moreno Valley Planning Department:</p> <p>i. Preservation-In-Place of the cultural resources, if feasible. Preservation in place means avoiding the resources, leaving them in the place they were found with no development affecting the integrity of the resources.</p> <p>ii. Onsite reburial of the discovered items as detailed in the treatment plan required pursuant to Mitigation Measure TCR-1. This shall include measures and provisions to protect the future reburial area from any future impacts in perpetuity. Reburial shall not occur until all legally required cataloging and basic recordation have been completed. No recordation of sacred items is permitted without the written consent of all Consulting Native American Tribal Governments as defined in TCR-1.</p>	<p>Community Development Director or designee</p>	<p>During ground-disturbing activities</p>	<p>Evidence to the City that Preservation-In-Place occurs for cultural resources, if feasible; or onsite reburial of the discovered items as detailed in the treatment plan required pursuant to Mitigation Measure TCR-1</p>		<p>Issuance of stop work order</p>
<p>MITIGATION MEASURE TCR-4: The City shall verify that the following note is included on the Grading Plan:</p> <p>"If any suspected archaeological resources are discovered during ground-disturbing activities and the Project Archaeologist or Native American Tribal Representatives are not present, the construction supervisor is obligated to halt work in a 100-foot radius around the find and call the Project Archaeologist and the Tribal Representatives to the site to assess the significance of the find."</p>	<p>Community Development Director or designee</p>	<p>Prior to issuance of grading permit</p>	<p>Evidence to the City that unanticipated cultural resources note is included in grading plans</p>		<p>Withhold grading permit</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

**MITIGATION MONITORING AND REPORTING PROGRAM
PERRIS AND DRACAEA COMMERCIAL RETAIL PROJECT**

<p>MITIGATION MEASURE TCR-5: If potential historic or cultural resources are uncovered during excavation or construction activities at the project site, work in the affected area must cease immediately and a qualified person meeting the Secretary of the Interior's standards (36 CFR 61), Tribal Representatives, and all site monitors per the Mitigation Measures, shall be consulted by the City to evaluate the find, and as appropriate recommend alternative measures to avoid, minimize or mitigate negative effects on the historic, or prehistoric resource. Determinations and recommendations by the consultant shall be immediately submitted to the Planning Division for consideration, and implemented as deemed appropriate by the Community Development Director, in consultation with the State Historic Preservation Officer (SHPO) and any and all Consulting Native American Tribes as defined in TCR-1 before any further work commences in the affected area.</p>	<p>Community Development Director or designee</p>	<p>During ground-disturbing activities</p>	<p>Qualified person meeting the Secretary of the Interior's standards (36 CFR 61), Tribal Representatives, and all site monitors per the Mitigation Measures, are consulted, and their recommendations are considered by the City</p>		<p>Issuance of stop work order</p>
<p>MITIGATION MEASURE TCR-6: If human remains are discovered, no further disturbance shall occur in the affected area until the County Coroner has made necessary findings as to origin. If the County Coroner determines that the remains are potentially Native American, the California Native American Heritage Commission shall be notified within 24 hours of the published finding to be given a reasonable opportunity to identify the "most likely descendant". The "most likely descendant" shall then make recommendations, and engage in consultations concerning the treatment of the remains (California Public Resources Code 5097.98). (GP Objective 23.3, CEQA).</p>	<p>Community Development Director or designee</p>	<p>During ground-disturbing activities</p>	<p>County Coroner is contacted for inspection and recommendations, and Native American Heritage Commission and most likely descendent are contacted as appropriate.</p>		<p>Issuance of stop work order</p>

Attachment: Exhibit D to Resolution No. 2020-___ Initial Study MND (4210 : Conditional Use Permits for a

RESOLUTION NUMBER 2020-

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, APPROVING GENERAL PLAN AMENDMENT PEN19-0206 TO AMEND THE GENERAL PLAN LAND USE MAP, CHANGING THE LAND USE DESIGNATION OF APPROXIMATELY 2.04 ACRES FROM RESIDENTIAL/OFFICE (R/O) TO COMMERCIAL (CC) FOR THE PROPERTY LOCATED AT THE NORTHEAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE (APNs 479-120-027, -029, -042, AND -043)

WHEREAS, the City of Moreno Valley (“City”) is a general law city and a municipal corporation of the State of California; and

WHEREAS, Cadence Acquisition LLC., (“Developer”) has filed an application for the approval of General Plan Amendment PEN19-0206 (“Application”) requesting an amendment to the Moreno Valley General Plan from Residential/Office (R/O) to Commercial (C) for the property located at the northeast corner of Perris Boulevard and Dracaea Avenue (APN’S 479-120-027, 029, 042, AND 043) (“Site”); and

WHEREAS, pursuant to the provisions of Section 9.02.200 (Public Hearing and Notification Procedures) of the Moreno Valley Municipal Code and Government Code section 65905, a public hearing was scheduled for December 15, 2020, and notice thereof was duly published and posted, and mailed to all property owners of record within 600 feet of the Site; and

WHEREAS, at its November 12, 2020 meeting, the Planning Commission considered the General Plan Amendment for the Perris and Dracaea commercial project and recommended approval to the City Council; and

WHEREAS, on December 15, 2020, the public hearing to consider the Application was duly conducted by the City Council at which time all interested persons were provided with an opportunity to testify and to present evidence; and

WHEREAS, on December 15, 2020, in accordance with the provisions of the California Environmental Quality Act (CEQA¹) and CEQA Guidelines,² the City Council considered and approved the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program for the project.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

Section 1. Recitals and Exhibits

¹ Public Resources Code §§ 21000-21177

² 14 California Code of Regulations §§15000-15387

That the foregoing Recitals and attached exhibits are true and correct and are hereby incorporated by this reference.

Section 2. Notice

That pursuant to Government Code section 66020(d)(1), notice is hereby given that the proposed project is subject to certain fees, dedications, reservations and other exactions as provided herein.

Section 3. Evidence

That the City Council has considered all of the evidence submitted into the administrative record for the General Plan Amendment, including, but not limited to, the following:

- (a) Moreno Valley General Plan and all other relevant provisions contained therein;
- (b) Title 9 (Planning and Zoning) of the Moreno Valley Municipal Code and all other relevant provisions referenced therein;
- (c) The Moreno Valley General Plan amendment changing the land use designation from Residential/Office (R/O) to Commercial (C) and all other relevant provisions contained therein as shown on Exhibit A;
- (d) Application for the approval of a General Plan Amendment PEN19-0206 and all documents, records and references contained therein;
- (e) Staff Report prepared for the City Council's consideration and all documents, records and references related thereto, and Staff's presentation at the public hearing;
- (f) Testimony and/or comments from Applicant and its representatives during the public hearing; and
- (g) Testimony and/or comments from all persons that was provided in written format or correspondence, at, or prior to, the public hearing.

Section 4. Findings

That based on the foregoing Recitals and the Evidence contained in the Administrative Record as set forth above, the City Council makes the following findings:

- (a) The proposed General Plan amendment are consistent with the existing goals, objectives, policies and programs of the General Plan; and
- (b) The proposed General Plan amendment will not adversely affect the public health, safety or general welfare.

Section 5. Approval

That based on the foregoing Recitals, Administrative Record and Findings, the City Council approves General Plan Amendment PEN19-0206 attached hereto as Exhibit A.

Section 6. Repeal of Conflicting Provisions

That all the provisions as heretofore adopted by the City Council that are in conflict with the provisions of this Resolution are hereby repealed.

Section 7. Severability

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Resolution be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this Resolution as hereby adopted shall remain in full force and effect.

Section 8. Effective Date

That this Resolution shall take effect immediately upon the date of adoption.

Section 9. Certification

That the City Clerk for the City Council shall certify to the passage of this Resolution.

PASSED AND ADOPTED THIS _____ day of _____, 2020.

CITY OF MORENO VALLEY
CITY COUNCIL

Dr. Yxstian A. Gutierrez
Mayor of the City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

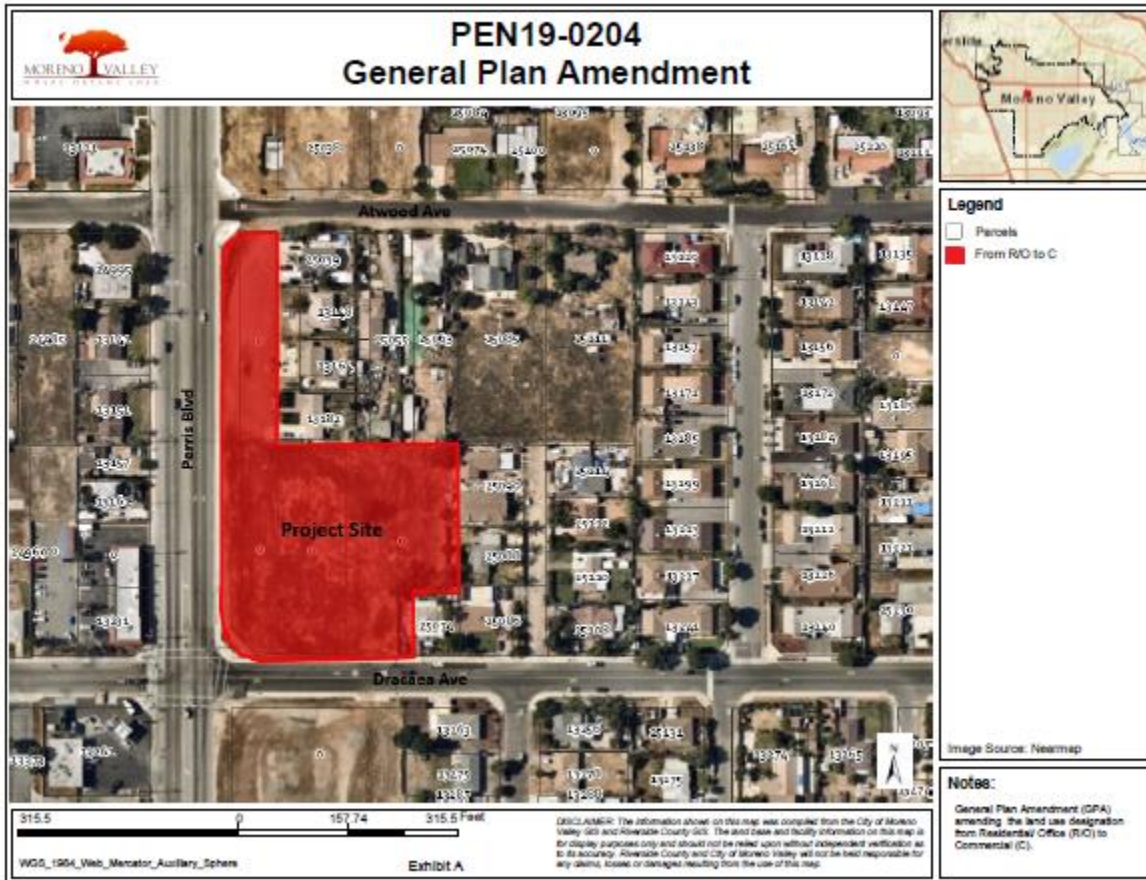
Steven B. Quintanilla, Interim City Attorney

Exhibits:
Exhibit A General Plan Land Use Designation

Attachment: Resolution No. 2020- General Plan Amendment [Revision 5] (4210 : Conditional Use Permits for a Drive Through Restaurant,

Exhibit A

General Plan Amendment Land Use Designation Map



Attachment: Resolution No. 2020- General Plan Amendment [Revision 5] (4210 : Conditional Use Permits for a Drive Through Restaurant,

ORDINANCE NO. 2020-XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, APPROVING CHANGE OF ZONE PEN19-0207 TO AMEND THE OFFICIAL ZONING ATLAS, CHANGING THE ZONING CLASSIFICATION OF APPROXIMATELY 2.04 ACRES FROM OFFICE (O) DISTRICT AND OFFICE COMMERCIAL (OC) DISTRICT TO COMMUNITY COMMERCIAL (CC) DISTRICT FOR THE PROPERTY LOCATED AT THE NORTHEAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE (APNs: 479-120-027, 029, 042 AND 043)

The City Council of the City of Moreno Valley does ordain as follows:

SECTION 1 GENERAL:

1.1 The applicant, Cadence Acquisition LLC., filed Application No. PEN19-0207, requesting an amendment to Page 72 of the Official Zoning Atlas to the zoning classification for certain properties, as described in the title of this resolution and the attached Exhibit A; and

1.2 At its November 12, 2020 meeting, the Planning Commission considered the Change of Zone for the Perris and Dracaea commercial project and recommended approval to the City Council; and

1.3 Pursuant to the provisions of the law, a public hearing was held before the City Council on December 15, 2020, for deliberations and decision.

1.4 The matter was fully discussed, and the public and other agencies were given opportunity to present testimony and documentation.

1.5 An Initial Study has been prepared for the Project for the purpose of compliance with the California Environmental Quality Act (CEQA). Based on the Initial Study including all supporting technical evidence, determined that the project impacts are expected to be less than significant with mitigation, and approval of a Mitigated Negative Declaration is an appropriate environmental determination for the Project.

SECTION 2 FINDINGS:

That based on the content of the foregoing Recitals and the Evidence contained in the Administrative Record as set forth above, the City Council makes the following findings:

- (a) The proposed amendment is consistent with the existing goals, objectives, policies and programs of the general plan.
- (b) The proposed amendment will not adversely affect the public health, safety or general welfare.

1
Ordinance No. 2020-XX
Date Adopted: December __, 2020

- (c) The proposed amendment is consistent with the purposes and intent of this title.

SECTION 3 AMENDMENT OF THE OFFICIAL ZONING ATLAS:

3.1 The City of Moreno Valley Official Zoning Atlas, as adopted by Ordinance No. 359, on April 14, 1992, of the City of Moreno Valley, and as amended thereafter from time to time by the City Council of the City of Moreno Valley, is further amended by placing in effect the zone or zone classification to Page 72 of the Official Zoning Atlas as shown on the attached map marked "Exhibit A" and included herein by reference and on file in the office of the City Clerk.

SECTION 4 EFFECT OF ENACTMENT:

4.1 Except as specifically provided herein, nothing contained in this ordinance shall be deemed to modify or supersede any prior enactment of the City Council which addresses the same subject addressed herein.

SECTION 5. NOTICE OF ADOPTION:

5.1 Within fifteen days after the date of adoption hereof, the City Clerk shall certify to the adoption of this ordinance and cause it to be posted in three public places within the city.

SECTION 6. EFFECTIVE DATE:

6.1 This ordinance shall take effect thirty days after the date of its adoption.

APPROVED AND ADOPTED this _____ day of _____, _____.

CITY OF MORENO VALLEY
CITY COUNCIL

Mayor of the City of Moreno Valley

ATTEST:

City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Attachment: Ordinance No. XXX Change of Zone [Revision 3] (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling Station

Ordinance No. 2020-XX³
Date Adopted: December __, 2020

ORDINANCE JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Ordinance No. YYYY-__ was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the ____ day of December, 2020, by the following vote:

AYES:

NOES:

ABSENT:

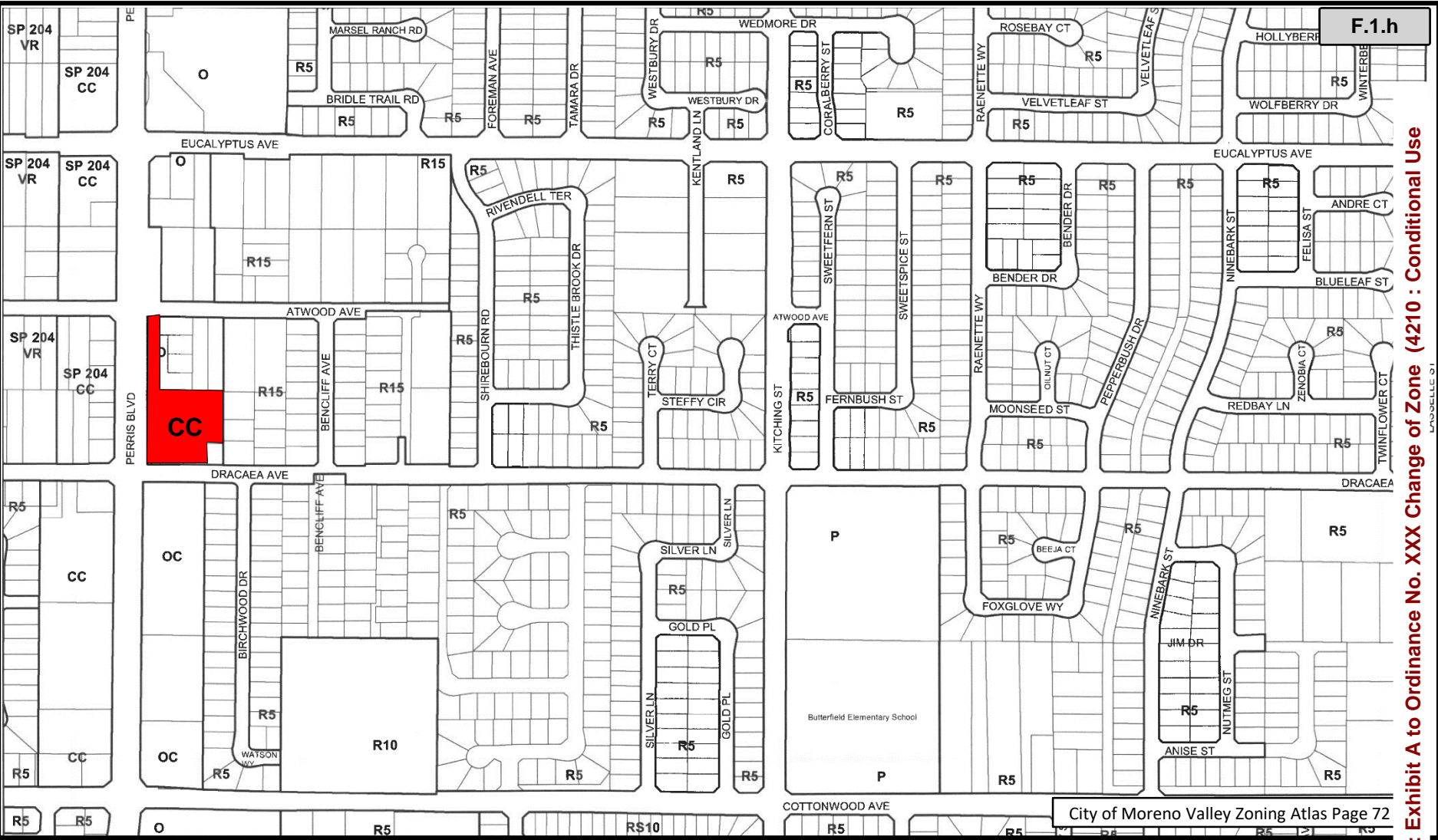
ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

4
Ordinance No. 2020-XX
Date Adopted: December __, 2020



PEN19-0205 – Change of Zone

From Office Commercial (OC) to Community Commercial (CC)

RESOLUTION NUMBER 2020-

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, APPROVING CONDITIONAL USE PERMIT PEN19-0204 FOR A 3,000 SQUARE FOOT DRIVE THROUGH RESTAURANT WITH OUTDOOR SEATING LOCATED ON THE NORTH EAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE (APNS 479-120-027, 029, 042, AND 043)

WHEREAS, the City of Moreno Valley (“City”) is a general law city and a municipal corporation of the State of California; and

WHEREAS, Cadence Acquisition LLC, (“Developer”) has filed an application for the approval of Conditional Use Permits PEN19-0204 (“Applications”) for the development of a 3,000 square foot drive through restaurant with outdoor seating (“Project”) located at the north east corner of Perris Boulevard and Dracaea Street (“Site”); and

WHEREAS, Section 9.02.060 (Conditional Use Permits) of the Moreno Valley Municipal Code acknowledges that the purpose of conditional use permit is to allow the establishment of uses that may have special impacts or uniqueness such that their effect on the surrounding environment cannot be determined in advance of the use being proposed for a particular location and that the conditional use permit application process involves the review of location, design and configuration of improvements related to the project, and the potential impact of the project on the surrounding area based on fixed and established standards; and

WHEREAS, the Application has been evaluated in accordance with Section 9.02.060 (Conditional Use Permits) of the Municipal Code with consideration given to the City’s General Plan, Zoning Ordinance, and other applicable laws and regulations; and

WHEREAS, Section 9.02.060 of the Municipal Code imposes conditions of approval upon projects for which a Conditional Use Permit (CUP) is required, which conditions may be imposed by the City Council to address on-site improvements, off-site improvements, the manner in which the site is used and any other conditions as may be deemed necessary to protect the public health, safety and welfare and ensure that the proposed Project will be developed in accordance with the purpose and intent of Title 9 (“Planning and Zoning”) of the Municipal Code; and

WHEREAS, at its November 12, 2020 meeting, the Planning Commission considered the Conditional Use Permit for the Perris and Dracaea commercial project and recommended approval to the City Council; and

WHEREAS, Staff has presented for the City Council's consideration Conditions of Approval to be imposed upon Conditional Use Permit PEN20-0204 ("CUP"), which conditions have been deemed necessary to protect the public health, safety and welfare and ensure that the proposed Project will be developed in accordance with the purpose and intent of Title 9 (Planning and Zoning) of the Municipal Code; and

WHEREAS, pursuant to the provisions of Section 9.02.200 (Public Hearing and Notification Procedures) of the Municipal Code and Government Code section 65905, a public hearing was scheduled for December 15, 2020, and notice thereof was duly published and posted, and mailed to all property owners of record within 600 feet of the Site; and

WHEREAS, on December 15, 2020, the public hearing to consider the Application was duly conducted by the City Council at which time all interested persons were provided with an opportunity to testify and to present evidence; and

WHEREAS, consistent with the requirements of Section 9.02.060 (Conditional Use Permits) of the Municipal Code, at the public hearing the City Council considered Conditions of Approval to be imposed upon Conditional Use Permit PEN20-0204 (CUP), which conditions were prepared by Planning Division staff who deemed said conditions to be necessary to protect the public health, safety and welfare and to ensure the proposed Project will be developed in accordance with the purpose and intent of Title 9 ("Planning and Zoning") of the Municipal Code; and

WHEREAS, at the public hearing, the City Council considered whether each of the requisite findings specified in Section 9.02.060 of the Municipal Code and set forth herein could be made with respect to the proposed Project as conditioned by Conditions of Approval.

WHEREAS, on December 15, 2020, in accordance with the provisions of the California Environmental Quality Act (CEQA¹) and CEQA Guidelines,² the City Council considered and approved the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program for the project.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

Section 1. Recitals and Exhibits

That the foregoing Recitals and attached Exhibits are true and correct and are hereby incorporated by this reference.

Section 2. Notice

¹ Public Resources Code §§ 21000-21177

² 14 California Code of Regulations §§15000-15387

That pursuant to Government Code section 66020(d)(1), notice is hereby given that the proposed project is subject to certain fees, dedications, reservations and other exactions as provided herein.

Section 3. Evidence

That the City Council has considered all of the evidence submitted into the administrative record for the proposed CUP, including, but not limited to, the following:

- (a) Moreno Valley General Plan and all other relevant provisions contained therein;
- (b) Title 9 (Planning and Zoning) of the Moreno Valley Municipal Code and all other relevant provisions referenced therein;
- (c) Application for the approval of Conditional Use Permit (CUP) PEN19-0204 and all documents, records and references contained therein;
- (d) Conditions of Approval for CUP PEN19-0204, attached hereto as Exhibit A and Exhibit B respectively;
- (e) Staff Report prepared for the City Council's consideration and all documents, records and references related thereto, and Staff's presentation at the public hearing;
- (f) Testimony and/or comments from Applicant and its representatives during the public hearing; and
- (g) Testimony and/or comments from all persons that was provided in written format or correspondence, at, or prior to, the public hearing.

Section 4. Findings

That based on the foregoing Recitals and the Evidence contained in the Administrative Record as set forth above, the City Council makes the following findings in approving CUP PEN19-0204

- (a) The proposed Project is consistent with the goals, objectives, policies and programs of the General Plan;
- (b) The proposed Project complies with all applicable zoning and other regulations;
- (c) The proposed Project will not be detrimental to the public health, safety or welfare or materially injurious to properties or improvements in the vicinity; and
- (d) The location, design and operation of the proposed Project will be compatible with existing and planned land uses in the vicinity.

Section 5. Approval

That based on the foregoing Recitals, Evidence contained in the Administrative Record and Findings set forth above, the City Council hereby approves CUP PEN19-0204 subject to the Conditions of Approval for CUP PEN19-0204 attached hereto as Exhibit A.

Section 6. Repeal of Conflicting Provisions

That all the provisions as heretofore adopted by the City Council that are in conflict with the provisions of this Resolution are hereby repealed.

Section 7. Severability

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Resolution be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this Resolution as hereby adopted shall remain in full force and effect.

Section 8. Effective Date

That this Resolution shall take effect immediately upon the date of adoption.

Section 9. Certification

That the City Clerk of the City Council shall certify to the passage of this Resolution.

PASSED AND ADOPTED THIS _____ day of _____, 2020.

CITY OF MORENO VALLEY
CITY COUNCIL

Dr. Yxstian A. Gutierrez
Mayor of the City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Exhibits:
Exhibit A: Conditions of Approval PEN19-0204

Exhibit A
CONDITIONS OF APPROVAL

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

Page 1

CITY OF MORENO VALLEY
 CONDITIONS OF APPROVAL
 Conditional Use Permit (PEN19-0204)

EFFECTIVE DATE:

EXPIRATION DATE:

COMMUNITY DEVELOPMENT DEPARTMENTPlanning Division

1. A change or modification to the land use or the approved site plans may require a separate approval. Prior to any change or modification, the property owner shall contact the City of Moreno Valley Community Development Department to determine if a separate approval is required.
2. Any expansion to this use or exterior alterations will require the submittal of a separate application(s) and shall be reviewed and approved under separate permit(s). (MC 9.02.080)
3. The developer, or the developer's successor-in-interest, shall be responsible for maintaining any undeveloped portion of the site in a manner that provides for the control of weeds, erosion and dust. (MC 9.02.030)
4. This approval shall expire three years after the approval date of this project unless used or extended as provided for by the City of Moreno Valley Municipal Code; otherwise it shall become null and void and of no effect whatsoever. Use means the beginning of substantial construction contemplated by this approval within the three-year period, which is thereafter pursued to completion, or the beginning of substantial utilization contemplated by this approval. (MC 9.02.230)
5. In the event the use hereby permitted ceases operation for a period of one (1) year or more, or as defined in the current Municipal Code, this permit may be revoked in accordance with provisions of the Municipal Code.
6. All landscaped areas shall be maintained in a healthy and thriving condition, free from weeds, trash and debris. (MC 9.02.030)
7. The site shall be developed in accordance with the approved plans on file in the Community Development Department - Planning Division, the Municipal Code regulations, General Plan, and the conditions contained herein. Prior to any use of the project site or business activity being commenced thereon, all Conditions of Approval shall be completed to the satisfaction of the Planning Official. (MC 9.14.020)

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

Page 2

8. Any signs indicated on the submitted plans are not included with this approval. Any signs, whether permanent (e.g. wall, monument) or temporary (e.g. banner, flag), require separate application and approval by the Planning Division. No signs are permitted in the public right of way. (MC 9.12)
9. All site plans, grading plans, landscape and irrigation plans, fence/wall plans, lighting plans and street improvement plans shall be coordinated for consistency with this approval.

Special Conditions

10. To reduce single event noise impacts to below the level of 55 dBA beyond the boundaries of the property, delivery operations shall be limited to between the hours of 7am and 8pm. Loading or unloading activities shall be conducted from the truck bays or designated loading. (MC 9.10.140,)
11. Drive-up or drive-through speaker system shall not be detectable above daytime ambient noise levels beyond the property line boundaries, and shall not exceed fifty-five (55) dBA at any one time beyond the boundaries of the property line. (MC9.09.080 C.6 and 9.10.140)
12. Prior to the start of any construction, temporary security fencing shall be erected. The fencing shall be a minimum of six (6) feet high with locking, gated access and shall remain through the duration of construction. Security shall remain in place until the project is completed or the above conditions no longer exist. (Security fencing is required if there is: construction, unsecured structures, unenclosed storage of materials and/or equipment, and/or the condition of the site constitutes a public hazard).
13. The site has been approved for an approximately 3,000 square foot drive through restaurant with an outdoor seating area per the approved site plan. A change or modification shall require separate approval. For a Conditional Use Permit, violation may result in revocation of the Conditional Use Permit.
14. One outdoor trash receptacle shall be provided shall be provided for every ten (10) required parking spaces, with a minimum of one receptacle provided to be located front portion of the site for use by patrons. (MC 9.09.080 C 5.)
15. Drive through hours of operation shall be limited to 5:00am to 12:00am (Midnight) seven (7) days a week. Condition is subject to the City Council's approval of a proposed Municipal Code Amendment currently under consideration by the City.

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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Prior to Grading Permit

16. Prior to issuance of any grading permit, all Conditions of Approval and Mitigation Measures shall be printed on the grading plans.
17. Prior to the issuance of grading permits, decorative (e.g. colored/scored concrete or as approve by the Planning Official) pedestrian pathways across circulation aisles/paths shall be provided throughout the development to connect dwellings with open spaces and/or recreational uses or commercial/industrial buildings with open space and/or parking. and/or the public right-of-way. The pathways shall be shown on the precise grading plan. (GP Objective 46.8, DG)
18. Prior to issuance of any grading permits, mitigation measures contained in the Mitigation Monitoring Program approved with this project shall be implemented as provided therein. A mitigation monitoring fee, as provided by City ordinance, shall be paid by the applicant within 30 days of project approval. No City permit or approval shall be issued until such fee is paid. (CEQA)
19. Prior to issuance of grading permits, the developer shall pay the applicable Stephens' Kangaroo Rat (SKR) Habitat Conservation Plan mitigation fee. (Ord)
20. If potential historic, archaeological, Native American cultural resources or paleontological resources are uncovered during excavation or construction activities at the project site, work in the affected area must cease immediately and a qualified person (meeting the Secretary of the Interior's standards (36CFR61)) shall be consulted by the applicant to evaluate the find, and as appropriate recommend alternative measures to avoid, minimize or mitigate negative effects on the historic, prehistoric, or paleontological resource. Determinations and recommendations by the consultant shall be immediately submitted to the Planning Division for consideration, and implemented as deemed appropriate by the Community Development Director, in consultation with the State Historic Preservation Officer (SHPO) and any and all affected Native American Tribes before any further work commences in the affected area.

If human remains are discovered during grading and other construction excavation, no further disturbance shall occur until the County Coroner has made necessary findings as to origin. If the County Coroner determines that the remains are potentially Native American, the California Native American Heritage Commission shall be notified within 5-days of the published finding to be given a reasonable opportunity to identify the "most likely descendant." The "most likely descendant" shall then make recommendations, and engage in consultations concerning the treatment of the remains (California Public Resources Code 5097.98). (GP Objective 23.3, CEQA).

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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21. Within thirty (30) days prior to any grading or other land disturbance, a pre-construction survey for Burrowing Owls shall be conducted pursuant to the established guidelines of Multiple Species Habitat Conservation Plan. The pre-construction survey shall be submitted to the Planning Division prior to any disturbance of the site and/or grading permit issuance.
22. Prior to the issuance of grading permits, the site plan and grading plans shall show decorative hardscape (e.g. colored concrete, stamped concrete, pavers or as approved by the Planning Official) consistent and compatible with the design, color and materials of the proposed development for all driveway ingress/egress locations of the project.
23. Prior to issuance of grading permits, the developer shall submit wall/fence plans to the Planning Division for review and approval as follows:
 - a. A maximum 6 foot high solid decorative block perimeter wall with pilasters and a cap shall be required adjacent to all residential zoned areas.
 - b. 3-foot high decorative wall, solid hedge or berm shall be placed in any setback areas between a public right of way and a parking lot for screening.
 - c. Any proposed retaining walls shall also be decorative in nature, while the combination of retaining and other walls on top shall not exceed the height requirement.
 - d. Walls and fences for visual screening are required when there are adjacent residential uses or residentially zone property. The height, placement and design will be based on a site specific review of the project. All walls are subject to the approval of the Planning Official. (DC 9.08.070)
24. Prior to the issuance of grading permits, a temporary project identification sign shall be erected on the site in a secure and visible manner. The sign shall be conspicuously posted at the site and remain in place until occupancy of the project. The sign shall include the following:
 - a. The name (if applicable) and address of the development.
 - b. The developer's name, address, and a 24-hour emergency telephone number.
25. Prior to issuance of grading permits, the location of the trash enclosure shall be included on the plans.
26. Prior to issuance of any building permit, all Conditions of Approval and Mitigation Measures shall be printed on the building plans.

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

Page 5

27. Prior to the issuance of building permits, proposed covered trash enclosures shall be included in the Planning review of the Fence and Wall plan or separate Planning submittal. The trash enclosure(s), including the roof materials, shall be compatible with the architecture, color and materials of the building(s) design. Trash enclosure areas shall include landscaping on three sides. Approved design plans shall be included in a Building submittal (Fence and Wall or building design plans). (GP Objective 43.6, DG)
28. Prior to issuance of any building permits, final landscaping and irrigation plans shall be submitted for review and approved by the Planning Division. After the third plan check review for landscape plans, an additional plan check fee shall apply. The plans shall be prepared in accordance with the City's Landscape Requirements and shall include:
- a. A three (3) foot high decorative wall, solid hedge or berm shall be placed in any setback areas between a public right of way and a parking lot for screening.
 - b. Finger and end planters with required step outs and curbing shall be provided every 12 parking stalls as well as at the terminus of each aisle.
 - c. Drought tolerant landscape shall be used. Sod is limited to gathering areas with limited use.
 - d. Street trees shall be provided every 40 feet on center in the right of way.
 - e. On-site trees shall be planted at an equivalent of one (1) tree per thirty (30) linear feet of the perimeter of a parking lot and per thirty linear feet of a building dimension for the portions of the building visible from a parking lot or right of way. Trees may be massed for pleasing aesthetic effects.
 - f. Enhanced landscaping shall be provided at all driveway entries and street corner locations. The review of all utility boxes, transformers etc. shall be coordinated to provide adequate screening from public view.
 - g. Landscaping on three sides of any trash enclosure.
 - h. All site perimeter and parking lot landscape and irrigation shall be installed prior to the release of certificate of any occupancy permits for the site or pad in question.
29. Prior to issuance of building permits, the Planning Division shall review and approve the location and method of enclosure or screening of transformer cabinets, commercial gas meters and back flow preventers as shown on the final working drawings. Location and screening shall comply with the following criteria:

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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transformer cabinets and commercial gas meters shall not be located within required setbacks and shall be screened from public view either by architectural treatment or landscaping; multiple electrical meters shall be fully enclosed and incorporated into the overall architectural design of the building(s); back-flow preventers shall be screened by landscaping. (GP Objective 43.30)

30. Prior to issuance of a building permit, the developer/property owner or developer's successor-in-interest shall pay all applicable impact fees due at permit issuance, including but not limited to Multi-species Habitat Conservation Plan (MSHCP) mitigation fees. (Ord)
31. Prior to building final, the developer/owner or developer's/owner's successor-in-interest shall pay all applicable impact fees, including but not limited to Transportation Uniform Mitigation fees (TUMF), and the City's adopted Development Impact Fees. (Ord)
32. Prior to issuance of building permits, for projects that will be phased, a phasing plan shall be submitted to and approved by the Planning Division if occupancy is proposed to be phased.
33. Included with the building plan check submittal, a detailed, on-site, computer generated, point-by-point comparison lighting plan, including exterior building, parking lot, and landscaping lighting. The lighting plan shall be generated on the plot plan and shall be integrated with the final landscape plan. The plan shall indicate the manufacturer's specifications for light fixtures used, shall include style, illumination, location, height and method of shielding per the City's Municipal Code requirements. After the third plan check review for lighting plans, an additional plan check fee will apply. (MC 9.08.100, 9.16.280)
34. Prior to issuance of building permits, screening details shall be addressed on the building plans for roof top equipment submitted for Planning Division review and approval through the building plan check process. All equipment shall be completely screened so as not to be visible from public view, and the screening shall be an integral part of the building.

Prior to Building Final or Occupancy

35. Prior to building final, all required landscaping and irrigation shall be installed per plan, certified by the Landscape Architect and inspected by the Planning Division. (MC 9.03.040, MC 9.17).
36. Prior to building final, Planning approved/stamped landscape plans shall be provided to the Community Development Department – Planning Division on a CD disk.

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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37. Prior to building final, all required and proposed fences and walls shall be constructed according to the approved plans on file in the Planning Division. (MC 9.080.070).
38. Prior to building final, all required landscaping and irrigation shall be installed per plan, certified by the Landscape Architect and inspected by the Planning Division. (MC 9.03.040, MC 9.17).
39. Prior to building final, Planning approved/stamped landscape plans shall be provided to the Community Development Department – Planning Division on a CD disk.
40. Prior to building final, all required and proposed fences and walls shall be constructed according to the approved plans on file in the Planning Division. (MC 9.080.070).

Building Division

41. The proposed non-residential project shall comply with the latest Federal Law, Americans with Disabilities Act, and State Law, California Code of Regulations, Title 24, Chapter 11B for accessibility standards for the disabled including access to the site, exits, bathrooms, work spaces, etc.
42. Prior to submittal, all new development, including residential second units, are required to obtain a valid property address prior to permit application. Addresses can be obtained by contacting the Building Safety Division at 951.413.3350.
43. Contact the Building Safety Division for permit application submittal requirements.
44. Any construction within the city shall only be as follows: Monday through Friday seven a.m. to seven p.m.(except for holidays which occur on weekdays), eight a.m. to four p.m.; weekends and holidays (as observed by the city and described in the Moreno Valley Municipal Code Chapter 2.55), unless written approval is first obtained from the Building Official or City Engineer.
45. Building plans submitted shall be signed and sealed by a California licensed design professional as required by the State Business and Professions Code.
46. The proposed development shall be subject to the payment of required development fees as required by the City's current Fee Ordinance at the time a building application is submitted or prior to the issuance of permits as determined by the City.
47. The proposed project will be subject to approval by the Eastern Municipal Water

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

Page 8

District and all applicable fees and charges shall be paid prior to permit issuance. Contact the water district at 951.928.3777 for specific details.

48. All new structures shall be designed in conformance to the latest design standards adopted by the State of California in the California Building Code, (CBC) Part 2, Title 24, California Code of Regulations including requirements for allowable area, occupancy separations, fire suppression systems, accessibility, etc. The current code edition is the 2019 CBC.
49. The proposed non-residential project shall comply with 2019 California Green Building Standards Code, Section 5.106.5.3, mandatory requirements for Electric Vehicle Charging Station (EVCS).
50. The proposed project's occupancy shall be classified by the Building Official and must comply with exiting, occupancy separation(s) and minimum plumbing fixture requirements. Minimum plumbing fixtures shall be provided per the 2019 California Plumbing Code, Table 422.1. The occupant load and occupancy classification shall be determined in accordance with the California Building Code.
51. Prior to permit issuance, every applicant shall submit a properly completed Waste Management Plan (WMP), as a portion of the building or demolition permit process. (MC 8.80.030)

ECONOMIC DEVELOPMENT DEPARTMENT (EDD)

52. New Moreno Valley businesses may work with the Economic Development Department to coordinate job recruitment fairs.
53. New Moreno Valley businesses may adopt a "First Source" approach to employee recruitment that gives notice of job openings to Moreno Valley residents for one week in advance of the public recruitment.
54. New Moreno Valley businesses are encouraged to hire local residents.
55. New Moreno Valley businesses are encouraged to provide a job fair flyer and/or web announcement to the City in advance of job recruitments, so that the City can assist in publicizing these events.
56. New Moreno Valley businesses may utilize the workforce recruitment services provided by the Moreno Valley Employment Resource Center ("ERC").

The ERC offers no cost assistance to businesses recruiting and training potential employees. Complimentary services include:

- Job Announcements

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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- Applicant testing / pre-screening
- Interviewing
- Job Fair support
- Training space

FIRE DEPARTMENT**Fire Prevention Bureau**

57. Prior to issuance of building permits, plans specifying the required structural materials for building construction in high fire hazard severity zones shall be submitted to the Fire Prevention Bureau for approval. (CFC, 4905)
58. Prior to issuance of Certificate of Occupancy or Building Final, all commercial buildings shall display street numbers in a prominent location on the street side and rear access locations. The numerals shall be a minimum of twelve inches in height. (CFC 505.1, MVMC 8.36.060[I])
59. Prior to issuance of Certificate of Occupancy or Building Final, the applicant/developer shall install a fire alarm system monitored by an approved Underwriters Laboratory listed central station based on a requirement for monitoring the sprinkler system, occupancy or use. Fire alarm panel shall be accessible from exterior of building in an approved location. Plans shall be submitted to the Fire Prevention Bureau for approval prior to installation. (CFC Chapter 9 and MVMC 8.36.100)
60. A fire extinguisher with a minimum rating of 3A:40BC shall be mounted in an accessible location within the cell site enclosure next to the fueled generator.
61. Prior to issuance of Building Permits, the applicant/developer shall participate in the Fire Impact Mitigation Program. (Fee Resolution as adopted by City Council)
62. Dead-end streets and/or fire apparatus access roads in excess of 150 feet in length shall be provided with an approved turnaround for fire apparatus.
63. Prior to issuance of Certificate of Occupancy or Building Final, the applicant/developer shall be responsible for obtaining underground and/or above ground tank permits for the storage of combustible liquids, flammable liquids, or any other hazardous materials from both the County of Riverside Community Health Agency Department of Environmental Health and the Fire Prevention Bureau. (CFC 105)
64. All Fire Department access roads or driveways shall not exceed 12 percent grade. (CFC 503.2.7 and MVMC 8.36.060[G])

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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65. The Fire Department emergency vehicular access road shall be (all weather surface) capable of sustaining an imposed load of 80,000 lbs. GVW, based on street standards approved by the Public Works Director and the Fire Prevention Bureau. The approved fire access road shall be in place during the time of construction. Temporary fire access roads shall be approved by the Fire Prevention Bureau. (CFC 501.4, and MV City Standard Engineering Plan 108d)
66. The angle of approach and departure for any means of Fire Department access shall not exceed 1 ft drop in 20 ft (0.3 m drop in 6 m), and the design limitations of the fire apparatus of the Fire Department shall be subject to approval by the AHJ. (CFC 503 and MVMC 8.36.060)
67. Prior to construction, all locations where structures are to be built shall have an approved Fire Department access based on street standards approved by the Public Works Director and the Fire Prevention Bureau. (CFC 501.4)
68. Prior to issuance of Building Permits, the applicant/developer shall provide the Fire Prevention Bureau with an approved site plan for Fire Lanes and signage. (CFC 501.3)
69. Prior to issuance of Certificate of Occupancy or Building Final, "Blue Reflective Markers" shall be installed to identify fire hydrant locations in accordance with City specifications. (CFC 509.1 and MVLT 440A-0 through MVLT 440C-0)
70. Final fire and life safety conditions will be addressed when the Fire Prevention Bureau reviews building plans. These conditions will be based on occupancy, use, California Building Code (CBC), California Fire Code (CFC), and related codes, which are in effect at the time of building plan submittal.
71. The Fire Code Official is authorized to enforce the fire safety during construction requirements of Chapter 33. (CFC Chapter 33 & CBC Chapter 33)
72. Fire lanes and fire apparatus access roads shall have an unobstructed width of not less than twenty-four (24) feet and an unobstructed vertical clearance of not less than thirteen (13) feet six (6) inches. (CFC 503.2.1 and MVMC 8.36.060[E])
73. Prior to issuance of the building permit for development, independent paved access to the nearest paved road, maintained by the City shall be designed and constructed by the developer within the public right of way in accordance with City Standards. (MVMC 8.36.060, CFC 501.4)
74. Prior to issuance of a Certificate of Occupancy or Building Final, a "Knox Box Rapid Entry System" shall be provided. The Knox-Box shall be installed in an accessible location approved by the Fire Code Official. All exterior security emergency access

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- gates shall be electronically operated and be provided with Knox key switches for access by emergency personnel. (CFC 506.1)
75. The minimum number of fire hydrants required, as well as the location and spacing of fire hydrants, shall comply with the C.F.C., MVMC, and NFPA 24. Fire hydrants shall be located no closer than 40 feet to a building. A fire hydrant shall be located within 50 feet of the fire department connection for buildings protected with a fire sprinkler system. The size and number of outlets required for the approved fire hydrants are (6" x 4" x 2 ½" x 2 ½") (CFC 507.5.1, 507.5.7, Appendix C, NFPA 24-7.2.3, MVMC 912.2.1)
 76. Fire Department access driveways over 150 feet in length shall have a turn-around as determined by the Fire Prevention Bureau capable of accommodating fire apparatus. (CFC 503 and MVMC 8.36.060, CFC 501.4)
 77. During phased construction, dead end roadways and streets which have not been completed shall have a turn-around capable of accommodating fire apparatus. (CFC 503.1 and 503.2.5)
 78. If construction is phased, each phase shall provide an approved emergency vehicular access way for fire protection prior to any building construction. (CFC 501.4)
 79. Plans for private water mains supplying fire sprinkler systems and/or private fire hydrants shall be submitted to the Fire Prevention Bureau for approval. (CFC 105 and CFC 3312.1)
 80. The Fire Prevention Bureau is required to set a minimum fire flow for the remodel or construction of all commercial buildings per CFC Appendix B and Table B105.1. The applicant/developer shall provide documentation to show there exists a water system capable of delivering said waterflow for 2 hour(s) duration at 20-PSI residual operating pressure. The required fire flow may be adjusted during the approval process to reflect changes in design, construction type, or automatic fire protection measures as approved by the Fire Prevention Bureau. Specific requirements for the project will be determined at time of submittal. (CFC 507.3, Appendix B)
 81. Prior to building construction, dead end roadways and streets which have not been completed shall have a turnaround capable of accommodating fire apparatus. (CFC 503.2.5)
 82. Prior to issuance of Building Permits, the applicant/developer shall furnish one copy of the water system plans to the Fire Prevention Bureau for review. Plans shall:
 - a. Be signed by a registered civil engineer or a certified fire protection engineer;
 - b. Contain a Fire Prevention Bureau approval signature block; and
 - c. Conform to hydrant type, location, spacing of new and existing hydrants and minimum fire flow

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required as determined by the Fire Prevention Bureau. The required water system, including fire hydrants, shall be installed, made serviceable, and be accepted by the Moreno Valley Fire Department prior to beginning construction. They shall be maintained accessible.

PUBLIC WORKS DEPARTMENT**Land Development**

83. Aggregate slurry, as defined in Section 203-5 of Standard Specifications for Public Works Construction, shall be required prior to 90% security reduction or the end of the one-year warranty period of the public streets as approved by the City Engineer. If slurry is required, a slurry mix design shall be submitted for review and approved by the City Engineer. The latex additive shall be Ultra Pave 70 (for anionic) or Ultra Pave 65 K (for cationic) or an approved equal per the geotechnical report. The latex shall be added at the emulsion plant after weighing the asphalt and before the addition of mixing water. The latex shall be added at a rate of two to two-and-one-half (2 to 2½) parts to one-hundred (100) parts of emulsion by volume. Any existing striping shall be removed prior to slurry application and replaced per City standards.
84. The developer shall comply with all applicable City ordinances and resolutions including the City's Municipal Code (MC) and if subdividing land, the Government Code (GC) of the State of California, specifically Sections 66410 through 66499.58, said sections also referred to as the Subdivision Map Act (SMA). [MC 9.14.010]
85. The final approved conditions of approval (COAs) issued and any applicable Mitigation Measures by the Planning Division shall be photographically or electronically placed on mylar sheets and included in the Grading and Street Improvement plans.
86. The developer shall monitor, supervise and control all construction related activities, so as to prevent these activities from causing a public nuisance, including but not limited to, insuring strict adherence to the following:
- (a) Removal of dirt, debris, or other construction material deposited on any public street no later than the end of each working day.
 - (b) Observance of working hours as stipulated on permits issued by the Land Development Division.
 - (c) The construction site shall accommodate the parking of all motor vehicles used by persons working at or providing deliveries to the site.
 - (d) All dust control measures per South Coast Air Quality Management District (SCAQMD) requirements during the grading operations.
- Violation of any condition, restriction or prohibition set forth in these conditions shall

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- subject the owner, applicant, developer or contractor(s) to remedy as noted in City Municipal Code 8.14.090. In addition, the City Engineer or Building Official may suspend all construction related activities for violation of any condition, restriction or prohibition set forth in these conditions until such time as it has been determined that all operations and activities are in conformance with these conditions.
87. Drainage facilities (e.g., catch basins, water quality basins, etc.) with sump conditions shall be designed to convey the tributary 100-year storm flows. Secondary emergency escape shall also be provided.
 88. In the event right-of-way or offsite easements are required to construct offsite improvements necessary for the orderly development of the surrounding area to meet the public health and safety needs, the developer shall make a good faith effort to acquire the needed right-of-way in accordance with the Land Development Division's administrative policy. If unsuccessful, the Developer shall enter into an agreement with the City to acquire the necessary right-of-way or offsite easements and complete the improvements at such time the City acquires the right-of-way or offsite easements which will permit the improvements to be made. The developer shall be responsible for all costs associated with the right-of-way or easement acquisition. [GC 66462.5]
 89. If improvements associated with this project are not initiated within two (2) years of the date of approval of the Public Improvement Agreement (PIA), the City Engineer may require that the engineer's estimate for improvements associated with the project be modified to reflect current City construction costs in effect at the time of request for an extension of time for the PIA or issuance of a permit. [MC 9.14.210(B)(C)]
 90. The developer shall protect downstream properties from damage caused by alteration of drainage patterns (i.e. concentration or diversion of flow, etc). Protection shall be provided by constructing adequate drainage facilities, including, but not limited to, modifying existing facilities or by securing a drainage easement. [MC 9.14.110]
 91. Public drainage easements, when required, shall be a minimum of 25 feet wide and shall be shown on the map and plan, and noted as follows: "Drainage Easement – no structures, obstructions, or encroachments by land fills are allowed." In addition, the grade within the easement area shall not exceed a 3:1 (H:V) slope, unless approved by the City Engineer.
 92. The maintenance responsibility of the proposed storm drain line shall be clearly identified. Storm drain lines within private property will be privately maintained and those within public streets will be publicly maintained.
 93. The proposed on-site private storm drain system shall make a single connect to the

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existing 60" storm drain in Perris Blvd. The connection may require a construction permit issued from RCFC&WCD and an encroachment permit by the City. A storm drain manhole shall be placed at the right-of-way line to mark the beginning of the publicly maintained portion of this storm drain.

94. This project shall submit civil engineering design plans, reports and/or documents (prepared by a registered/licensed civil engineer) for review and approval by the City Engineer per the current submittal requirements, prior to the indicated threshold or as required by the City Engineer. The submittal consists of, but is not limited to, the following:
- a. Lot Line Adjustment recorded prior to building permit issuance;
 - b. Precise grading w/ erosion control plan prior to grading permit issuance;
 - c. Public Improvement Plans (e.g., STREET/STORM DRAIN w/ STRIPING, RCFC STORM DRAIN, SEWER/WATER, etc.) prior to Encroachment Permit issuance;
 - d. Final drainage study prior to grading plan approval;
 - e. Final WQMP prior to grading plan approval;
 - f. Legal Documents (e.g., EASEMENT(s), DEDICATION(s), LOT LINE ADJUSTMENT, VACATION, etc.) prior to Building Permit issuance;
 - g. As-Built revision for all plans prior to Occupancy release;
95. Water quality best management practices (BMPs) designed to meet Water Quality Management Plan (WQMP) requirements for single-family residential development shall not be used as a construction BMP. Water quality BMPs shall be maintained for the entire duration of the project construction and be used to treat runoff from those developed portions of the project. Water quality BMPs shall be protected from upstream construction related runoff by having proper best management practices in place and maintained. Water quality BMPs shall be graded per the approved design plans and once landscaping and irrigation has been installed, it and its maintenance shall be turned over to an established Homeowner's Association (HOA). The Homeowner's Association shall enter into an agreement with the City for basin maintenance.

Prior to Grading Plan Approval

96. Resolution of all drainage issues shall be as approved by the City Engineer.
97. A final detailed drainage study (prepared by a registered/licensed civil engineer) shall be submitted for review and approved by the City Engineer. The study shall include, but not be limited to: existing and proposed hydrologic conditions as well as hydraulic calculations for all drainage control devices and storm drain lines. The study shall analyze 1, 3, 6 and 24-hour duration events for the 2, 5, 10 and 100-year storm events [MC 9.14.110(A.1)]. A digital (pdf) copy of the approved drainage study shall be submitted to the Land Development Division.

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98. Emergency overflow areas shall be shown at all applicable drainage improvement locations in the event that the drainage improvement fails or exceeds full capacity.
99. The final project-specific Water Quality Management Plan (WQMP) shall be consistent with the approved P-WQMP, as well as in full conformance with the document: "Water Quality Management Plan - A Guidance Document for the Santa Ana Region of Riverside County" dated October 22, 2012. The F-WQMP shall be submitted and approved prior to application for and issuance of grading permits. At a minimum, the F-WQMP shall include the following: Site Design BMPs; Source Control BMPs, Treatment Control BMPs, Operation and Maintenance requirements for BMPs and sources of funding for BMP implementation.
- a. The Applicant has proposed to incorporate the use of Bioretention. Final design and sizing details of all BMPs must be provided in the first submittal of the F-WQMP. The Applicant acknowledges that more area than currently shown on the plans may be required to treat site runoff as required by the WQMP guidance document.
 - b. The Applicant shall substantiate the applicable Hydrologic Condition of Concerns (HCOC) in Section F of the F-WQMP. <The HCOC designates that the project will be exempt from mitigation requirements based on Exemption 3>.
 - c. All proposed LID BMP's shall be designed in accordance with the RCFC&WCD's Design Handbook for Low Impact Development Best Management Practices, dated September 2011.
 - d. The proposed LID BMP's as identified in the project-specific P-WQMP shall be incorporated into the Final WQMP.
 - e. The NPDES notes per City Standard Drawing No. MVFE-350-0 shall be included in the grading plans.
 - f. Post-construction treatment control BMPs, once placed into operation for post-construction water quality control, shall not be used to treat runoff from construction sites or unstabilized areas of the site.
 - g. Prior to precise grading plan approval, the grading plan shall show any proposed trash enclosure to include a cover (roof) and sufficient size for dual bin (1 for trash and 1 for recyclables). The architecture shall be approved by the Planning Division and any structural approvals shall be made by the Building and Safety Division.
100. The developer shall ensure compliance with the City Grading ordinance, these Conditions of Approval, drainage slope shall be per the latest Calif Building Code, City Standards and the following criteria:
- a. The project street and lot grading shall be designed in a manner that perpetuates the existing natural drainage patterns with respect to tributary drainage area and outlet points. Unless otherwise approved by the City Engineer, lot lines shall be located at the top of slopes.
 - b. Any grading that creates cut or fill slopes adjacent to the street shall provide

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erosion control, sight distance control, and slope easements as approved by the City Engineer.

c. All improvement plans are substantially complete and appropriate clearance letters are provided to the City.

d. A soils/geotechnical report (addressing the soil's stability and geological conditions of the site) shall be submitted to the Land Development Division for review. A digital (pdf) copy of the soils/geotechnical report shall be submitted to the Land Development Division.

101. Grading plans (prepared by a registered/licensed civil engineer) shall be submitted for review and approved by the City Engineer per the current submittal requirements.
102. The developer shall select Low Impact Development (LID) Best Management Practices (BMPs) designed per the latest version of the Water Quality Management Plan (WQMP) - a guidance document for the Santa Ana region of Riverside County.
103. The developer shall pay all remaining plan check fees.
104. A Storm Water Pollution Prevention Plan (SWPPP) shall be prepared in conformance with the State's current Construction Activities Storm Water General Permit. A copy of the current SWPPP shall be kept at the project site and be available for review upon request.
105. Any proposed trash enclosure shall include a solid cover (roof) and sufficient size for dual bin (one for trash and one for recyclables). The architecture shall be approved by the Planning Division and any structural approvals shall be made by the Building & Safety Division.

Prior to Grading Permit

106. A receipt showing payment of the Area Drainage Plan (ADP) fee to Riverside County Flood Control and Water Conservation District shall be submitted. [MC 9.14.100(O)]
107. For non-subdivision projects, a copy of the Covenants, Conditions and Restrictions (CC&Rs) shall be submitted for review by the City Engineer. The CC&Rs shall include, but not be limited to, access easements, reciprocal access, private and/or public utility easements as may be relevant to the project.
108. If the developer chooses to construct the project in phases, a Construction Phasing Plan for the construction of on-site public or private improvements shall be submitted for review and approved by the City Engineer.

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109. The developer shall pay current DIF fees adopted by the City Council. [Ord. 695 § 1.1 (part), 2005] [MC 3.38.030, 040, 050]
110. A digital (pdf) copy of all approved grading plans shall be submitted to the Land Development Division.
111. Security, in the form of a cash deposit (preferable), bond or letter of credit shall be submitted as a guarantee of the implementation and maintenance of erosion control measures. At least twenty-five (25) percent of the required security shall be in the form of a cash deposit with the City. [MC 8.21.160(H)]
112. Security, in the form of a cash deposit (preferable), bond or letter of credit shall be submitted as a guarantee of the completion of the grading operations for the project. [MC 8.21.070]
113. The developer shall pay all applicable inspection fees.
114. The developer shall pay current TUMF fees adopted by the City Council. [Ord. 835 § 2.1, 2012] [MC 3.44.060]

Prior to Improvement Plan Approval

115. The developer is required to bring any existing access ramps adjacent to and fronting the project to current ADA (Americans with Disabilities Act) requirements. However, when work is required in an intersection that involves or impacts existing access ramps, all access ramps in that intersection shall be retrofitted to comply with current ADA requirements, unless otherwise approved by the City Engineer.
116. The developer shall submit clearances from all applicable agencies, and pay all applicable plan check fees.
117. The street improvement plans shall comply with current City policies, plans and applicable City standards (i.e. MVSI-160 series, etc.) throughout this project.
118. The design plan and profile shall be based upon a centerline, extending beyond the project boundaries a minimum distance of 300 feet at a grade and alignment approved by the City Engineer.
119. Drainage facilities (i.e. catch basins, etc.) with sump conditions shall be designed to convey the tributary 100-year storm flows. Secondary emergency escape shall also be provided.
120. The hydrology study shall be designed to accept and properly convey all off-site drainage flowing onto or through the site. In the event that the City Engineer permits the use of streets for drainage purposes, the provisions of current City standards

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- shall apply. Should the quantities exceed the street capacity or the use of streets be prohibited for drainage purposes, as in the case where one travel lane in each direction shall not be used for drainage conveyance for emergency vehicle access on streets classified as minor arterials and greater, the developer shall provide adequate facilities as approved by the City Engineer. [MC 9.14.110 A.2]
121. All public improvement plans (prepared by a licensed/registered civil engineer) shall be submitted for review and approved by the City Engineer per the current submittal requirements.
 122. Any missing or deficient existing improvements along the project frontage within the Project's frontage of Perris Blvd and Dracace Avenue shall be constructed or secured for construction. The City Engineer may require the ultimate structural section for pavement to half-street width plus 18 feet or provide core test results confirming that existing pavement section is per current City Standards; additional signing & striping to accommodate increased traffic imposed by the development, etc.
 123. For non-subdivision projects, all street dedications shall be free of encumbrances, irrevocably offered to the public and shall continue in force until the City accepts or abandons such offers, unless otherwise approved by the City Engineer.
 124. The plans shall indicate any restrictions on trench repair pavement cuts to reflect the City's moratorium on disturbing newly-constructed pavement less than three (3) years old and recently slurry sealed streets less than one (1) year old. Pavement cuts may be allowed for emergency repairs or as specifically approved in writing by the City Engineer. Special requirements shall be imposed for repaving, limits to be determined by the City Engineer.
 125. All dry and wet utilities shall be shown on the plans and any crossings shall be potholed to determine actual location and elevation. Any conflicts shall be identified and addressed on the plans. The pothole survey data shall be submitted to Land Development with the public improvement plans for reference purposes only. The developer is responsible to coordinate with all affected utility companies and bear all costs of any utility relocation.
 126. The Applicant shall construct a bus turnout, per City Standards (MVSI-161-0) and as directed by the Transportation Engineering Division. Bus turnout will be located north of the Perris Blvd and Dracaea Avenue intersection, along the northbound travel lane and north of the Project's first driveway.
 127. The Applicant shall construct the Perris Blvd median from the south side of the Project's Perris Blvd driveway, north of the intersection of Dracaea Avenue, north to the intersection of Atwood Avenue (approximately 460 feet). The median shall be constructed per City Standards and as directed by the City's Transportation

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Engineering Division.

Prior to Encroachment Permit

128. A digital (pdf) copy of all approved improvement plans shall be submitted to the Land Development Division.
129. All applicable inspection fees shall be paid.
130. For non-subdivision projects, execution of a Public Improvement Agreement (PIA) and/or security (in the form of a cash deposit or other approved means) may be required as determined by the City Engineer. [MC 9.14.220]
131. The plans shall indicate any restrictions on trench repair pavement cuts to reflect the City's moratorium on disturbing newly-constructed pavement less than three (3) years old and recently slurry sealed streets less than one (1) year old. Pavement cuts may be allowed for emergency repairs or as specifically approved in writing by the City Engineer. Special requirements shall be imposed for repaving, limits to be determined by the City Engineer.
132. Any work performed within public right-of-way requires an encroachment permit.

Prior to Building Permit

133. An engineered-fill certification, rough grade certification and compaction report shall be submitted for review and approved by the City Engineer. A digital (pdf) copy of the approved compaction report shall be submitted to the Land Development Division. All pads shall meet pad elevations per approved grading plans as noted by the setting of "blue-top" markers installed by a registered land surveyor or licensed civil engineer.
134. For non-subdivision projects, the developer shall guarantee the completion of all related public improvements required for this project by executing a Public Improvement Agreement (PIA) with the City and posting the required security. [MC 9.14.220]
135. For Commercial/Industrial projects, the owner may have to secure coverage under the State's General Industrial Activities Storm Water Permit as issued by the State Water Resources Control Board.
136. For non-subdivision projects, all street dedications shall be free of encumbrances, irrevocably offered to the public and shall continue in force until the City accepts or abandons such offers, unless otherwise approved by the City Engineer.

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137. A walk through with a Land Development Inspector shall be scheduled to inspect existing improvements within public right of way along project frontage. Any missing, damaged or substandard improvements including ADA access ramps that do not meet current City standards shall be required to be installed, replaced and/or repaired. The applicant shall post security to cover the cost of the repairs and complete the repairs within the time allowed in the public improvement agreement used to secure the improvements.
138. Certification to the line, grade, flow test and system invert elevations for the water quality control BMPs shall be submitted for review and approved by the City Engineer (excluding models homes).

Prior to Occupancy

139. All outstanding fees shall be paid.
140. All required as-built plans (prepared by a registered/licensed civil engineer) shall be submitted for review and approved by the City Engineer per the current submittal requirements.
141. The final/precise grade certification shall be submitted for review and approved by the City Engineer.
142. For commercial, industrial and multi-family projects, in compliance with Proposition 218, the developer shall agree to approve the City of Moreno Valley NPDES Regulatory Rate Schedule that is in place at the time of certificate of occupancy issuance. Under the current permit for storm water activities required as part of the National Pollutant Discharge Elimination System (NPDES) as mandated by the Federal Clean Water Act, this project is subject to the following requirements:
- a. Select one of the following options to meet the financial responsibility to provide storm water utilities services for the required continuous operation, maintenance, monitoring system evaluations and enhancements, remediation and/or replacement, all in accordance with Resolution No. 2002-46.
 - i. Participate in the mail ballot proceeding in compliance with Proposition 218, for the Common Interest, Commercial, Industrial and Quasi-Public Use NPDES Regulatory Rate Schedule and pay all associated costs with the ballot process; or
 - ii. Establish an endowment to cover future City costs as specified in the Common Interest, Commercial, Industrial and Quasi-Public Use NPDES Regulatory Rate Schedule.
 - b. Notify the Special Districts Division of the intent to request building permits 90 days prior to their issuance and the financial option selected. The financial option selected shall be in place prior to the issuance of certificate of occupancy. [California Government Code & Municipal Code]

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143. The developer shall complete all public improvements in conformance with current City standards, except as noted in the Special Conditions, including but not limited to the following:
- a. Street improvements including, but not limited to: pavement, base, curb and/or gutter, cross gutters, spandrel, sidewalks, drive approaches, pedestrian ramps, street lights (SCE: LS-2) shall be installed per the City Standard MVSL-400B-2 and any power pole mounted street lights shall be removed, signing, striping, under sidewalk drains, landscaping and irrigation, medians, pavement tapers/transitions and traffic control devices as appropriate.
 - b. Storm drain facilities including, but not limited to: storm drain pipe, storm drain laterals, open channels, catch basins and local depressions.
 - c. City-owned utilities.
 - d. Sewer and water systems including, but not limited to: sanitary sewer, potable water and recycled water.
 - e. Under grounding of all existing and proposed utilities adjacent to and on-site. [MC 9.14.130]
 - f. Relocation of overhead electrical utility lines including, but not limited to: electrical, cable and telephone.
144. For commercial, industrial and multi-family projects, a “Stormwater Treatment Device and Control Measure Access and Maintenance Covenant”, shall be recorded to provide public notice of the maintenance requirements to be implemented per the approved final project-specific WQMP. A boilerplate copy of the covenants and agreements can be obtained by contacting the Land Development Division.
145. The applicant shall ensure the following, pursuant to Section XII. I. of the 2010 NPDES Permit:
- a. Field verification that structural Site Design, Source Control and Treatment Control BMPs are designed, constructed and functional in accordance with the approved Final Water Quality Management Plan (WQMP).
 - b. Certification of best management practices (BMPs) from a state licensed civil engineer. An original WQMP BMP Certification shall be submitted for review and approved by the City Engineer.
146. The Developer shall comply with the following water quality related items:
- a. Notify the Land Development Division prior to construction and installation of all structural BMPs so that an inspection can be performed.
 - b. Demonstrate that all structural BMPs described in the approved final project-specific WQMP have been constructed and installed in conformance with the approved plans and specifications;
 - c. Demonstrate that Developer is prepared to implement all non-structural BMPs described in the approved final project-specific WQMP; and

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- d. Demonstrate that an adequate number of copies of the approved final project-specific WQMP are available for future owners/occupants.
- e. Clean and repair the water quality BMP's, including re-grading to approved civil drawing if necessary.
- f. Obtain approval and complete installation of the irrigation and landscaping.

Special Districts Division

147. This project is conditioned for a proposed district to provide a funding source for the operation and maintenance of public improvements and/or services associated with new development in that territory. The Developer shall satisfy this condition with one of the options outlined below.

a. Participate in a special election for maintenance/services and pay all associated costs of the election process and formation, if any. Financing may be structured through a Community Facilities District, Landscape and Lighting Maintenance District, or other financing structure as determined by the City; or

b. Establish an endowment fund to cover the future maintenance and/or service costs.

The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org when submitting the application for building permit issuance. If the first building permit is pulled prior to formation of the district, this condition will not apply. If the district has been or is in the process of being formed the Developer must inform the Special Districts Division of its selected financing option (a. or b. above). The option for participating in a special election requires 90 days to complete the special election process. This allows adequate time to be in compliance with the provisions of Article 13C of the California Constitution.

The financial option selected shall be in place prior to the issuance of the first certificate of occupancy for the project.

148. Commercial (BP) If Land Development, a Division of the Public Works Department, requires this project to supply a funding source necessary to provide for, but not limited to, stormwater utilities services for the continuous operation, remediation and/or replacement, monitoring, systems evaluations and enhancement of on-site facilities and performing annual inspections of the affected areas to ensure compliance with state mandated stormwater regulations, a funding source needs to be established. The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org of its selected financial option for the National Pollution Discharge Elimination System (NPDES) program when submitting the application for the first building permit issuance (see Land

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Development's related condition). Participating in a special election the process requires a 90 day period prior to the City's issuance of a building permit. This allows adequate time to be in compliance with the provisions of Article 13D of the California Constitution. (California Health and Safety Code Sections 5473 through 5473.8 (Ord. 708 Section 3.1, 2006) & City of Moreno Valley Municipal Code Title 3, Section 3.50.050.)

149. If a landscaped median is required, for those areas to be maintained by the City and prior to the issuance of the first Building Permit, Planning Division (Community Development Department), Special Districts Division (the Public Works Department) and Transportation Division (the Public Works Department) shall review and approve the final median, parkway, slope, and/or open space landscape/irrigation plans as designated on the tentative map or in these Conditions of Approval prior to the issuance of the first Building Permit.
150. This project has been identified to be included in the formation of a Community Facilities District (Mello-Roos) for Public Safety services, including but not limited to Police, Fire Protection, Paramedic Services, Park Rangers, and Animal Control services. The property owner(s) shall not protest the formation; however, they retain the right to object to the rate and method of maximum special tax. In compliance with Proposition 218, the property owner shall agree to approve the mail ballot proceeding (special election) for either formation of the CFD or annexation into an existing district. The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org when submitting the application for building permit issuance to determine the requirement for participation. If the first building permit is pulled prior to formation of the district, this condition will not apply. If the condition applies, the special election will require a minimum of 90 days prior to issuance of the first building permit. This allows adequate time to be in compliance with the provisions of Article 13C of the California Constitution. (California Government Code Section 53313 et. seq.)
151. If a landscaped median is required, the Developer, or the Developer's successors or assignees shall be responsible for all parkway and/or median landscape maintenance for a period of one (1) year commencing from the time all items of work have been completed to the satisfaction of Special Districts staff as per the City of Moreno Valley Public Works Department Landscape Design Guidelines, or until such time as the District accepts maintenance responsibilities.
152. If a landscaped median is required, parkway, median, slope and/or open space landscape areas maintained as part of the City of Moreno Valley Community Facilities District 2014-01 shall be required to have independent utility systems, including but not limited to water, electric, and telephone services. An independent irrigation controller and pedestal will also be required. Combining utility systems with existing or future landscape areas not associated with the City of Moreno

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Valley Community Facilities District (CFD) landscaping will not be permitted.

153. If a landscaped median is required, plans for parkway, median, slope, and/or open space landscape areas designated in the project's Conditions of Approval for incorporation into a City Coordinated landscape maintenance program, shall be prepared and submitted in accordance with the City of Moreno Valley Public Works Department Landscape Design Guidelines. The guidelines are available on the City's website at www.moval.org/sd or from the Special Districts Division (951.413.3480 or specialdistricts@moval.org).
154. The ongoing maintenance of any landscaping required to be installed behind the curb shall be the responsibility of the property owner.
155. If a landscaped median is required, plan check fees for review of parkway/median landscape plans for improvements that shall be maintained by the City of Moreno Valley are due upon the first plan submittal. (MC 3.32.040)
156. The parcel(s) associated with this project have been incorporated into the Moreno Valley Community Services District Zone A (Parks & Community Services) and Zone C (Arterial Street Lighting). All assessable parcels therein shall be subject to annual parcel taxes for Zone A and Zone C for operations and capital improvements.
157. If a landscaped median is required, parkway, open space, and/or median landscaping specified in the project's Conditions of Approval shall be constructed in compliance with the approved landscape plans and completed prior to the issuance of the first Certificate of Occupancy/Building Final for this project.
158. If a landscaped median is required, landscape and irrigation plans for parkway, median, slope, and/or open space landscape areas designated to be maintained by the City shall be placed on compact disk (CD) in pdf format. The CD shall include "As Built" plans, revisions, and changes. The CD will become the property of the City of Moreno Valley and the Moreno Valley Community Services District.
159. If a landscaped median is required, inspection fees for the monitoring of landscape installation associated with the City of Moreno Valley maintained parkways/medians are due prior to the required pre-construction meeting. (MC 3.32.040)
160. Major Infrastructure Financing District. This project has been identified to potentially be included in the formation of a special financing district for the construction and maintenance of major infrastructure improvements which may include but are not limited to thoroughfares, bridges, and certain flood control improvements. The property owner(s) shall participate in such district and pay any special tax, assessment, or fee levied upon the project property for such district. At the time of the public hearing to consider formation of or annexation into the district, the

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qualified elector(s) will not protest the formation or annexation, but will retain the right to object to any eventual tax/assessment/fee that is not equitable should the financial burden of the tax/assessment/fee not be reasonably proportionate to the benefit the affected property obtains from the improvements to be installed and/or maintained. The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org when submitting an application for the first building permit to determine whether the development will be subjected to this condition. If subject to the condition, the special election requires a minimum 90 day process in compliance with the provisions of Article 13C of the California Constitution.

161. NEW STREET LIGHT INSTALLATION FEES. Prior to the issuance of the first building permit for this project, the Developer shall pay New Street Light Installation Fees for all applicable Residential and Arterial Street Lights required for this development. Payment shall be made to the City of Moreno Valley and collected by the Land Development Division. Fees are based upon the Advanced Energy fee rate in place at the time of payment, as set forth in the current Listing of City Fees, Charges, and Rates adopted by City Council. The Developer shall provide a copy of the receipt to the Special Districts Division (specialdistricts@moval.org). Any change in the project which may increase the number of street lights to be installed will require payment of additional Advanced Energy fees at the then current fee. Questions may be directed to the Special Districts Division at 951.413.3480 or specialdistricts@moval.org.

162. This project is conditioned to provide a funding source for the following special financing program(s):

a. Street Lighting Services for capital improvements, energy charges, and maintenance.

b. If a landscaped median is required, Landscape Maintenance Services for parkway, open space, and/or median landscaping on Perris Blvd.

The Developer's responsibility is to provide a funding source for the capital improvements and the continued maintenance. The Developer shall satisfy this condition with one of the options below.

i. Participate in a special election (mail ballot proceeding) and pay all associated costs of the special election and formation, if any. Financing may be structured through a Community Services District zone, Community Facilities District, Landscape and Lighting Maintenance District, or other financing structure as determined by the City; or

ii. Establish a Property Owner's Association (POA) or Home Owner's Association

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0204)

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(HOA) which will be responsible for any and all operation and maintenance costs

The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org of its selected financial option when submitting the application for building permit issuance. The option for participating in a special election requires approximately 90 days to complete the special election process. This allows adequate time to be in compliance with the provisions of Article 13C of the California Constitution.

The financial option selected shall be in place prior to the issuance of the first certificate of occupancy for the project.

163. Street Light Authorization forms for all street lights that are conditioned to be installed as part of this project must be submitted to the Special Districts Division for approval, prior to street light installation. The Street Light Authorization form can be obtained from the utility company providing electric service to the project, either Moreno Valley Utility or Southern California Edison. For questions, contact the Special Districts Division at 951.413.3480 or specialdistricts@moval.org.

Transportation Engineering Division

164. Prior to issuance of a construction permit, construction traffic control plans prepared by a qualified, registered Civil or Traffic engineer may be required for plan approval or as required by the City Traffic Engineer.
165. All project driveways shall conform to Section 9.11.080, and Table 9.11.080-14 of the City's Development Code – Design Guidelines and City of Moreno Valley Standard Plans No. MVSI-112A~D-0 for commercial driveway approaches. Driveway on Perris Boulevard shall be restricted to right-in and right-out movements only by raised median installation.
166. All proposed on-site traffic signing and striping should be accordance with the latest California Manual on Uniform Traffic Control Devices (CAMUTCD).
167. Prior to final approval of any landscaping or monument sign plans, the project plans shall demonstrate that sight distance at the project driveways conforms to City Standard Plan No. MVSI-164A, B, C-0.
168. A bus turnout is required along the project frontage per MVSI-161-0. Bus turn-out shall be installed on the east side of Perris Boulevard, along the property frontage.
169. Applicant shall modify geometrics at Perris Boulevard and Dracaea Avenue intersection to provide for eastbound and westbound left-turn lanes.

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170. Applicant shall construct raised median on Perris Boulevard from Dracaea Avenue to north of intersection with Atwood Avenue.
 - a) At Atwood Avenue and Perris Boulevard intersection, median shall provide for full access north and south left-turn, through, and right-turn movements. The northbound and southbound left-turn lanes would have approximately 100 feet of queuing space plus taper.
 - b) Atwood Avenue should be restricted to right-in/right-out east and westbound movements with a directional median opening on Perris Boulevard.
 - c) Southbound left-turn lane at Perris Boulevard intersection with Dracaea Avenue shall provide at least 125 feet of queuing space plus taper.
171. The first parking stall/drive aisle juncture shall be 60 feet from the property line per Municipal Code Section 9.11.080 - A.18 or as approved by the City Engineer.
172. Prior to issuance of building permits, signing and striping plans shall be prepared by a registered, qualified civil or electrical engineer and submitted to the City for the required offsite improvements on Dracaea Avenue, Perris Boulevard, and Atwood Avenue.
173. Prior to issuance of a certificate of occupancy, a bus bay shall be installed per City of Moreno Valley Standard MVSI-161-0 on the east side of Perris Boulevard north of Dracaea Avenue, or as approved by the City Engineer.
174. Prior to issuance of certificate of occupancy, communication conduit shall be installed along project frontage on Perris Boulevard per City Standard Plan No. MVSI-186-0. Any improvements undertaken by this project shall be consistent with the City's standards for this facility.
175. Prior to issuance of a certificate of occupancy, median improvements on Perris Boulevard shall be installed per the approved plans and accepted by the City Engineer.
176. Prior to issuance of a certificate of occupancy, all signing and striping improvements on Dracaea Avenue and Perris Boulevard shall be installed per the approved plans and accepted by the City Engineer.
177. Prior to issuance of a certificate of occupancy, fair share payments for improvements identified on the Traffic Study shall be paid:
 - a) Perris Boulevard and Cottonwood Avenue Intersection.
 - b) Perris Boulevard and Eucalyptus Avenue Intersection.

RESOLUTION NUMBER 2020-

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, APPROVING CONDITIONAL USE PERMIT PEN19-0205 FOR A FUELING STATION WITH A 4,088 SQUARE FOOT CONVENIENCE STORE LOCATED ON THE NORTH EAST CORNER OF PERRIS BOULEVARD AND DRACAEA AVENUE (APNS 479-120-027, 029, 042, AND 043)

WHEREAS, the City of Moreno Valley (“City”) is a general law city and a municipal corporation of the State of California; and

WHEREAS, Cadence Acquisition LLC, (“Developer”) has filed an application for the approval of Conditional Use Permits PEN19-0205 (“Applications”) for the development of a 4088 square foot convenience store (“Project”) located at the north east corner of Perris Boulevard and Dracaea Street (“Site”); and

WHEREAS, Section 9.02.060 (Conditional Use Permits) of the Moreno Valley Municipal Code acknowledges that the purpose of conditional use permit is to allow the establishment of uses that may have special impacts or uniqueness such that their effect on the surrounding environment cannot be determined in advance of the use being proposed for a particular location and that the conditional use permit application process involves the review of location, design and configuration of improvements related to the project, and the potential impact of the project on the surrounding area based on fixed and established standards; and

WHEREAS, the Application has been evaluated in accordance with Section 9.02.060 (Conditional Use Permits) of the Municipal Code with consideration given to the City’s General Plan, Zoning Ordinance, and other applicable laws and regulations; and

WHEREAS, Section 9.02.060 of the Municipal Code imposes conditions of approval upon projects for which a Conditional Use Permit (CUP) is required, which conditions may be imposed by the City Council to address on-site improvements, off-site improvements, the manner in which the site is used and any other conditions as may be deemed necessary to protect the public health, safety and welfare and ensure that the proposed Project will be developed in accordance with the purpose and intent of Title 9 (“Planning and Zoning”) of the Municipal Code; and

WHEREAS, at its November 12, 2020 meeting, the Planning Commission considered the Conditional Use Permit for the Perris and Dracaea commercial project and recommended approval to the City Council; and

WHEREAS, Staff has presented for the City Council’s consideration Conditions of Approval to be imposed upon Conditional Use Permit PEN20-0205 (“CUP”), which conditions have been deemed necessary to protect the public health, safety and welfare and ensure that the proposed Project will be developed in accordance with the purpose and intent of Title 9 (Planning and Zoning) of the Municipal Code; and

WHEREAS, pursuant to the provisions of Section 9.02.200 (Public Hearing and Notification Procedures) of the Municipal Code and Government Code section 65905, a public hearing was scheduled for December 15, 2020, and notice thereof was duly published and posted, and mailed to all property owners of record within 600 feet of the Site; and

WHEREAS, on December 15, 2020, the public hearing to consider the Application was duly conducted by the City Council at which time all interested persons were provided with an opportunity to testify and to present evidence; and

WHEREAS, consistent with the requirements of Section 9.02.060 (Conditional Use Permits) of the Municipal Code, at the public hearing the City Council considered Conditions of Approval to be imposed upon Conditional Use Permit PEN20-0205 (CUP), which conditions were prepared by Planning Division staff who deemed said conditions to be necessary to protect the public health, safety and welfare and to ensure the proposed Project will be developed in accordance with the purpose and intent of Title 9 (“Planning and Zoning”) of the Municipal Code; and

WHEREAS, at the public hearing, the City Council considered whether each of the requisite findings specified in Section 9.02.060 of the Municipal Code and set forth herein could be made with respect to the proposed Project as conditioned by Conditions of Approval.

WHEREAS, on December 15, 2020, in accordance with the provisions of the California Environmental Quality Act (CEQA¹) and CEQA Guidelines,² the City Council considered and approved the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program for the project.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

Section 1. Recitals and Exhibits

That the foregoing Recitals and attached Exhibits are true and correct and are hereby incorporated by this reference.

Section 2. Notice

¹ Public Resources Code §§ 21000-21177
² 14 California Code of Regulations §§15000-15387

That pursuant to Government Code section 66020(d)(1), notice is hereby given that the proposed project is subject to certain fees, dedications, reservations and other exactions as provided herein.

Section 3. Evidence

That the City Council has considered all of the evidence submitted into the administrative record for the proposed CUP, including, but not limited to, the following:

- (a) Moreno Valley General Plan and all other relevant provisions contained therein;
- (b) Title 9 (Planning and Zoning) of the Moreno Valley Municipal Code and all other relevant provisions referenced therein;
- (c) Application for the approval of Conditional Use Permit (CUP) PEN19-0205 and all documents, records and references contained therein;
- (d) Conditions of Approval for CUP PEN19-0205, attached hereto as Exhibit A and Exhibit B respectively;
- (e) Staff Report prepared for the City Council's consideration and all documents, records and references related thereto, and Staff's presentation at the public hearing;
- (f) Testimony and/or comments from Applicant and its representatives during the public hearing; and
- (g) Testimony and/or comments from all persons that was provided in written format or correspondence, at, or prior to, the public hearing.

Section 4. Findings

That based on the foregoing Recitals and the Evidence contained in the Administrative Record as set forth above, the City Council makes the following findings in approving CUP PEN19-0205

- (a) The proposed Project is consistent with the goals, objectives, policies and programs of the General Plan;
- (b) The proposed Project complies with all applicable zoning and other regulations;
- (c) The proposed Project will not be detrimental to the public health, safety or welfare or materially injurious to properties or improvements in the vicinity; and
- (d) The location, design and operation of the proposed Project will be compatible with existing and planned land uses in the vicinity.

Section 5. Approval

That based on the foregoing Recitals, Evidence contained in the Administrative Record and Findings set forth above, the City Council hereby approves CUP PEN19-0205 subject to the Conditions of Approval for CUP PEN19-0205 attached hereto as Exhibit A.

Section 6. Repeal of Conflicting Provisions

That all the provisions as heretofore adopted by the City Council that are in conflict with the provisions of this Resolution are hereby repealed.

Section 7. Severability

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Resolution be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this Resolution as hereby adopted shall remain in full force and effect.

Section 8. Effective Date

That this Resolution shall take effect immediately upon the date of adoption.

Section 9. Certification

That the City Clerk of the City Council shall certify to the passage of this Resolution.

PASSED AND ADOPTED THIS _____ day of _____, 2020.

CITY OF MORENO VALLEY
CITY COUNCIL

Dr. Yxstian A. Gutierrez
Mayor of the City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Exhibits:
Exhibit A: Conditions of Approval PEN19-0205

Exhibit A
CONDITIONS OF APPROVAL

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Conditional Use Permit (PEN19-0205)

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CITY OF MORENO VALLEY
 CONDITIONS OF APPROVAL
 Conditional Use Permit (PEN19-0205)

EFFECTIVE DATE:

EXPIRATION DATE:

COMMUNITY DEVELOPMENT DEPARTMENTPlanning Division

1. A change or modification to the land use or the approved site plans may require a separate approval. Prior to any change or modification, the property owner shall contact the City of Moreno Valley Community Development Department to determine if a separate approval is required.
2. Any expansion to this use or exterior alterations will require the submittal of a separate application(s) and shall be reviewed and approved under separate permit(s). (MC 9.02.080)
3. The developer, or the developer's successor-in-interest, shall be responsible for maintaining any undeveloped portion of the site in a manner that provides for the control of weeds, erosion and dust. (MC 9.02.030)
4. This approval shall expire three years after the approval date of this project unless used or extended as provided for by the City of Moreno Valley Municipal Code; otherwise it shall become null and void and of no effect whatsoever. Use means the beginning of substantial construction contemplated by this approval within the three-year period, which is thereafter pursued to completion, or the beginning of substantial utilization contemplated by this approval. (MC 9.02.230)
5. In the event the use hereby permitted ceases operation for a period of one (1) year or more, or as defined in the current Municipal Code, this permit may be revoked in accordance with provisions of the Municipal Code. (applicable to CUP's)
6. All landscaped areas shall be maintained in a healthy and thriving condition, free from weeds, trash and debris. (MC 9.02.030)
7. The site shall be developed in accordance with the approved plans on file in the Community Development Department - Planning Division, the Municipal Code regulations, General Plan, and the conditions contained herein. Prior to any use of the project site or business activity being commenced thereon, all Conditions of Approval shall be completed to the satisfaction of the Planning Official. (MC 9.14.020)

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Conditional Use Permit (PEN19-0205)

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8. Any signs indicated on the submitted plans are not included with this approval. Any signs, whether permanent (e.g. wall, monument) or temporary (e.g. banner, flag), require separate application and approval by the Planning Division. No signs are permitted in the public right of way. (MC 9.12)
9. All site plans, grading plans, landscape and irrigation plans, fence/wall plans, lighting plans and street improvement plans shall be coordinated for consistency with this approval.

Special Conditions

10. To reduce single event noise impacts to below the level of 55 dBA beyond the boundaries of the property, delivery operations shall be limited to between the hours of 7am and 8pm. Loading or unloading activities shall be conducted from the truck bays or designated loading. (MC 9.10.140,)
11. The site has been approved for a a fueling station with an approximately 4,088 square foot convenience store with off-site beer and wine sales designed per the approved plans. A change or modification shall require separate approval. For a Conditional Use Permit, violation may result in revocation of the Conditional Use Permit.
12. Prior to the start of any construction, temporary security fencing shall be erected. The fencing shall be a minimum of six (6) feet high with locking, gated access and shall remain through the duration of construction. Security shall remain in place until the project is completed or the above conditions no longer exist. (Security fencing is required if there is: construction, unsecured structures, unenclosed storage of materials and/or equipment, and/or the condition of the site constitutes a public hazard).
13. One outdoor trash receptacle shall be provided shall be provided for every ten (10) required parking spaces, with a minimum of one receptacle provided to be located front portion of the site for use by patrons. (MC 9.09.080 C 5.)

Prior to Grading Permit

14. Prior to issuance of any grading permit, all Conditions of Approval and Mitigation Measures shall be printed on the grading plans.
15. Prior to issuance of any grading permits, mitigation measures contained in the Mitigation Monitoring Program approved with this project shall be implemented as provided therein. A mitigation monitoring fee, as provided by City ordinance, shall be paid by the applicant within 30 days of project approval. No City permit or

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Conditional Use Permit (PEN19-0205)

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approval shall be issued until such fee is paid. (CEQA)

16. Prior to issuance of grading permits, the developer shall pay the applicable Stephens' Kangaroo Rat (SKR) Habitat Conservation Plan mitigation fee. (Ord)
17. If potential historic, archaeological, Native American cultural resources or paleontological resources are uncovered during excavation or construction activities at the project site, work in the affected area must cease immediately and a qualified person (meeting the Secretary of the Interior's standards (36CFR61)) shall be consulted by the applicant to evaluate the find, and as appropriate recommend alternative measures to avoid, minimize or mitigate negative effects on the historic, prehistoric, or paleontological resource. Determinations and recommendations by the consultant shall be immediately submitted to the Planning Division for consideration, and implemented as deemed appropriate by the Community Development Director, in consultation with the State Historic Preservation Officer (SHPO) and any and all affected Native American Tribes before any further work commences in the affected area.

If human remains are discovered during grading and other construction excavation, no further disturbance shall occur until the County Coroner has made necessary findings as to origin. If the County Coroner determines that the remains are potentially Native American, the California Native American Heritage Commission shall be notified within 5-days of the published finding to be given a reasonable opportunity to identify the "most likely descendant." The "most likely descendant" shall then make recommendations, and engage in consultations concerning the treatment of the remains (California Public Resources Code 5097.98). (GP Objective 23.3, CEQA).

18. Within thirty (30) days prior to any grading or other land disturbance, a pre-construction survey for Burrowing Owls shall be conducted pursuant to the established guidelines of Multiple Species Habitat Conservation Plan. The pre-construction survey shall be submitted to the Planning Division prior to any disturbance of the site and/or grading permit issuance.
19. Prior to the issuance of grading permits, the site plan and grading plans shall show decorative hardscape (e.g. colored concrete, stamped concrete, pavers or as approved by the Planning Official) consistent and compatible with the design, color and materials of the proposed development for all driveway ingress/egress locations of the project.
20. Prior to issuance of grading permits, the developer shall submit wall/fence plans to the Planning Division for review and approval as follows:
 - a. A maximum 6 foot high solid decorative block perimeter wall with pilasters

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Conditional Use Permit (PEN19-0205)

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and a cap shall be required around the perimeter of the development.

b. 3-foot high decorative wall, solid hedge or berm shall be placed in any setback areas between a public right of way and a parking lot for screening.

c. Any proposed retaining walls shall also be decorative in nature, while the combination of retaining and other walls on top shall not exceed the height requirement.

d. Walls and fences for visual screening are required when there are adjacent residential uses or residentially zone property. The height, placement and design will be based on a site specific review of the project. All walls are subject to the approval of the Planning Official. (DC 9.08.070)

21. Prior to the issuance of grading permits, a temporary project identification sign shall be erected on the site in a secure and visible manner. The sign shall be conspicuously posted at the site and remain in place until occupancy of the project. The sign shall include the following:
 - a. The name (if applicable) and address of the development.
 - b. The developer's name, address, and a 24-hour emergency telephone number.
22. Prior to issuance of grading permits, the location of the trash enclosure shall be included on the plans.
23. Prior to the issuance of grading permits, decorative (e.g. colored/scored concrete or as approve by the Planning Official) pedestrian pathways across circulation aisles/paths shall be provided throughout the development to connect dwellings with open spaces and/or recreational uses or commercial/industrial buildings with open space and/or parking. and/or the public right-of-way. The pathways shall be shown on the precise grading plan. (GP Objective 46.8, DG)
24. Prior to issuance of any building permit, all Conditions of Approval and Mitigation Measures shall be printed on the building plans.
25. Prior to the issuance of building permits, proposed covered trash enclosures shall be included in the Planning review of the Fence and Wall plan or separate Planning submittal. The trash enclosure(s), including the roof materials, shall be compatible with the architecture, color and materials of the building(s) design. Trash enclosure areas shall include landscaping on three sides. Approved design plans shall be included in a Building submittal (Fence and Wall or building design plans). (GP Objective 43.6, DG)
26. Prior to issuance of any building permits, final landscaping and irrigation plans shall

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Conditional Use Permit (PEN19-0205)

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be submitted for review and approved by the Planning Division. After the third plan check review for landscape plans, an additional plan check fee shall apply. The plans shall be prepared in accordance with the City's Landscape Requirements and shall include:

- a. A three (3) foot high decorative wall, solid hedge or berm shall be placed in any setback areas between a public right of way and a parking lot for screening.
 - b. Finger and end planters with required step outs and curbing shall be provided every 12 parking stalls as well as at the terminus of each aisle.
 - c. Drought tolerant landscape shall be used. Sod is limited to gathering areas with limited use.
 - d. Street trees shall be provided every 40 feet on center in the right of way.
 - e. On-site trees shall be planted at an equivalent of one (1) tree per thirty (30) linear feet of the perimeter of a parking lot and per thirty linear feet of a building dimension for the portions of the building visible from a parking lot or right of way. Trees may be massed for pleasing aesthetic effects.
 - f. Enhanced landscaping shall be provided at all driveway entries and street corner locations. The review of all utility boxes, transformers etc. shall be coordinated to provide adequate screening from public view.
 - g. Landscaping on three sides of any trash enclosure.
 - h. All site perimeter and parking lot landscape and irrigation shall be installed prior to the release of certificate of any occupancy permits for the site or pad in question.
27. Prior to issuance of building permits, the Planning Division shall review and approve the location and method of enclosure or screening of transformer cabinets, commercial gas meters and back flow preventers as shown on the final working drawings. Location and screening shall comply with the following criteria: transformer cabinets and commercial gas meters shall not be located within required setbacks and shall be screened from public view either by architectural treatment or landscaping; multiple electrical meters shall be fully enclosed and incorporated into the overall architectural design of the building(s); back-flow preventers shall be screened by landscaping. (GP Objective 43.30)
28. Prior to issuance of a building permit, the developer/property owner or developer's successor-in-interest shall pay all applicable impact fees due at permit issuance, including but not limited to Multi-species Habitat Conservation Plan (MSHCP)

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mitigation fees. (Ord)

29. Prior to building final, the developer/owner or developer's/owner's successor-in-interest shall pay all applicable impact fees, including but not limited to Transportation Uniform Mitigation fees (TUMF), and the City's adopted Development Impact Fees. (Ord)
30. Prior to issuance of building permits, for projects that will be phased, a phasing plan shall be submitted to and approved by the Planning Division if occupancy is proposed to be phased.
31. Prior to or at building plan check submittal, two copies of a detailed, on-site, computer generated, point-by-point comparison lighting plan, including exterior building, parking lot, and landscaping lighting, shall be submitted to the Planning Division for review and approval prior to the issuance of a building permit. The lighting plan shall be generated on the plot plan and shall be integrated with the final landscape plan. The plan shall indicate the manufacturer's specifications for light fixtures used, shall include style, illumination, location, height and method of shielding per the City's Municipal Code requirements. After the third plan check review for lighting plans, an additional plan check fee will apply. (MC 9.08.100, 9.16.280)
32. Prior to issuance of building permits, screening details shall be addressed on the building plans for roof top equipment submitted for Planning Division review and approval through the building plan check process. All equipment shall be completely screened so as not to be visible from public view, and the screening shall be an integral part of the building.

Building Division

33. The proposed non-residential project shall comply with the latest Federal Law, Americans with Disabilities Act, and State Law, California Code of Regulations, Title 24, Chapter 11B for accessibility standards for the disabled including access to the site, exits, bathrooms, work spaces, etc.
34. Prior to submittal, all new development, including residential second units, are required to obtain a valid property address prior to permit application. Addresses can be obtained by contacting the Building Safety Division at 951.413.3350.
35. Contact the Building Safety Division for permit application submittal requirements.
36. Any construction within the city shall only be as follows: Monday through Friday seven a.m. to seven p.m.(except for holidays which occur on weekdays), eight a.m. to four p.m.; weekends and holidays (as observed by the city and described in the

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Conditional Use Permit (PEN19-0205)

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- Moreno Valley Municipal Code Chapter 2.55), unless written approval is first obtained from the Building Official or City Engineer.
37. Building plans submitted shall be signed and sealed by a California licensed design professional as required by the State Business and Professions Code.
 38. The proposed development shall be subject to the payment of required development fees as required by the City's current Fee Ordinance at the time a building application is submitted or prior to the issuance of permits as determined by the City.
 39. The proposed project will be subject to approval by the Eastern Municipal Water District and all applicable fees and charges shall be paid prior to permit issuance. Contact the water district at 951.928.3777 for specific details.
 40. All new structures shall be designed in conformance to the latest design standards adopted by the State of California in the California Building Code, (CBC) Part 2, Title 24, California Code of Regulations including requirements for allowable area, occupancy separations, fire suppression systems, accessibility, etc. The current code edition is the 2019 CBC.
 41. The proposed non-residential project shall comply with 2019 California Green Building Standards Code, Section 5.106.5.3, mandatory requirements for Electric Vehicle Charging Station (EVCS).
 42. The proposed project's occupancy shall be classified by the Building Official and must comply with exiting, occupancy separation(s) and minimum plumbing fixture requirements. Minimum plumbing fixtures shall be provided per the 2019 California Plumbing Code, Table 422.1. The occupant load and occupancy classification shall be determined in accordance with the California Building Code.
 43. Prior to permit issuance, every applicant shall submit a properly completed Waste Management Plan (WMP), as a portion of the building or demolition permit process. (MC 8.80.030)

ECONOMIC DEVELOPMENT DEPARTMENT (EDD)

44. New Moreno Valley businesses may work with the Economic Development Department to coordinate job recruitment fairs.
45. New Moreno Valley businesses may adopt a "First Source" approach to employee recruitment that gives notice of job openings to Moreno Valley residents for one week in advance of the public recruitment.
46. New Moreno Valley businesses are encouraged to hire local residents.

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47. New Moreno Valley businesses are encouraged to provide a job fair flyer and/or web announcement to the City in advance of job recruitments, so that the City can assist in publicizing these events.
48. New Moreno Valley businesses may utilize the workforce recruitment services provided by the Moreno Valley Employment Resource Center ("ERC").

The ERC offers no cost assistance to businesses recruiting and training potential employees. Complimentary services include:

- Job Announcements
- Applicant testing / pre-screening
- Interviewing
- Job Fair support
- Training space

FIRE DEPARTMENT**Fire Prevention Bureau**

49. Prior to issuance of building permits, plans specifying the required structural materials for building construction in high fire hazard severity zones shall be submitted to the Fire Prevention Bureau for approval. (CFC, 4905)
50. Prior to issuance of Certificate of Occupancy or Building Final, all commercial buildings shall display street numbers in a prominent location on the street side and rear access locations. The numerals shall be a minimum of twelve inches in height. (CFC 505.1, MVMC 8.36.060[I])
51. Prior to issuance of Certificate of Occupancy or Building Final, the applicant/developer shall install a fire alarm system monitored by an approved Underwriters Laboratory listed central station based on a requirement for monitoring the sprinkler system, occupancy or use. Fire alarm panel shall be accessible from exterior of building in an approved location. Plans shall be submitted to the Fire Prevention Bureau for approval prior to installation. (CFC Chapter 9 and MVMC 8.36.100)
52. Prior to issuance of Certificate of Occupancy or Building Final, the applicant/developer shall install a fire sprinkler system based on square footage and type of construction, occupancy or use. Fire sprinkler plans shall be submitted to the Fire Prevention Bureau for approval prior to installation. (CFC Chapter 9, MVMC 8.36.100[D])

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53. Provide fire flow confirmation from EMWD for 1500 gpm at 20 psi for 2 hours
54. Identify locations of existing and proposed fire hydrants
55. The Fire Department emergency vehicular access road shall be (all weather surface) capable of sustaining an imposed load of 80,000 lbs. GVW, based on street standards approved by the Public Works Director and the Fire Prevention Bureau. The approved fire access road shall be in place during the time of construction. Temporary fire access roads shall be approved by the Fire Prevention Bureau. (CFC 501.4, and MV City Standard Engineering Plan 108d)
56. Prior to construction, all locations where structures are to be built shall have an approved Fire Department access based on street standards approved by the Public Works Director and the Fire Prevention Bureau. (CFC 501.4)
57. Prior to issuance of Building Permits, the applicant/developer shall provide the Fire Prevention Bureau with an approved site plan for Fire Lanes and signage. (CFC 501.3)
58. Prior to issuance of Certificate of Occupancy or Building Final, "Blue Reflective Markers" shall be installed to identify fire hydrant locations in accordance with City specifications. (CFC 509.1 and MVLT 440A-0 through MVLT 440C-0)
59. The Fire Code Official is authorized to enforce the fire safety during construction requirements of Chapter 33. (CFC Chapter 33 & CBC Chapter 33)
60. Fire lanes and fire apparatus access roads shall have an unobstructed width of not less than twenty-four (24) feet and an unobstructed vertical clearance of not less than thirteen (13) feet six (6) inches. (CFC 503.2.1 and MVMC 8.36.060[E])
61. Prior to issuance of the building permit for development, independent paved access to the nearest paved road, maintained by the City shall be designed and constructed by the developer within the public right of way in accordance with City Standards. (MVMC 8.36.060, CFC 501.4)
62. Prior to issuance of a Certificate of Occupancy or Building Final, a "Knox Box Rapid Entry System" shall be provided. The Knox-Box shall be installed in an accessible location approved by the Fire Code Official. All exterior security emergency access gates shall be electronically operated and be provided with Knox key switches for access by emergency personnel. (CFC 506.1)
63. The minimum number of fire hydrants required, as well as the location and spacing of fire hydrants, shall comply with the C.F.C., MVMC, and NFPA 24. Fire hydrants shall be located no closer than 40 feet to a building. A fire hydrant shall be located within 50 feet of the fire department connection for buildings protected with a fire

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- sprinkler system. The size and number of outlets required for the approved fire hydrants are (6" x 4" x 2 ½" x 2 ½") (CFC 507.5.1, 507.5.7, Appendix C, NFPA 24-7.2.3, MVMC 912.2.1)
64. During phased construction, dead end roadways and streets which have not been completed shall have a turn-around capable of accommodating fire apparatus. (CFC 503.1 and 503.2.5)
 65. If construction is phased, each phase shall provide an approved emergency vehicular access way for fire protection prior to any building construction. (CFC 501.4)
 66. Plans for private water mains supplying fire sprinkler systems and/or private fire hydrants shall be submitted to the Fire Prevention Bureau for approval. (CFC 105 and CFC 3312.1)
 67. The Fire Prevention Bureau is required to set a minimum fire flow for the remodel or construction of all commercial buildings per CFC Appendix B and Table B105.1. The applicant/developer shall provide documentation to show there exists a water system capable of delivering said waterflow for 2 hour(s) duration at 20-PSI residual operating pressure. The required fire flow may be adjusted during the approval process to reflect changes in design, construction type, or automatic fire protection measures as approved by the Fire Prevention Bureau. Specific requirements for the project will be determined at time of submittal. (CFC 507.3, Appendix B)
 68. Prior to building construction, dead end roadways and streets which have not been completed shall have a turnaround capable of accommodating fire apparatus. (CFC 503.2.5)
 69. Prior to issuance of Building Permits, the applicant/developer shall furnish one copy of the water system plans to the Fire Prevention Bureau for review. Plans shall:
 - a. Be signed by a registered civil engineer or a certified fire protection engineer;
 - b. Contain a Fire Prevention Bureau approval signature block; and
 - c. Conform to hydrant type, location, spacing of new and existing hydrants and minimum fire flow required as determined by the Fire Prevention Bureau. The required water system, including fire hydrants, shall be installed, made serviceable, and be accepted by the Moreno Valley Fire Department prior to beginning construction. They shall be maintained accessible.

PUBLIC WORKS DEPARTMENT**Land Development**

70. Aggregate slurry, as defined in Section 203-5 of Standard Specifications for Public

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Works Construction, shall be required prior to 90% security reduction or the end of the one-year warranty period of the public streets as approved by the City Engineer. If slurry is required, a slurry mix design shall be submitted for review and approved by the City Engineer. The latex additive shall be Ultra Pave 70 (for anionic) or Ultra Pave 65 K (for cationic) or an approved equal per the geotechnical report. The latex shall be added at the emulsion plant after weighing the asphalt and before the addition of mixing water. The latex shall be added at a rate of two to two-and-one-half (2 to 2½) parts to one-hundred (100) parts of emulsion by volume. Any existing striping shall be removed prior to slurry application and replaced per City standards.

71. The developer shall comply with all applicable City ordinances and resolutions including the City's Municipal Code (MC) and if subdividing land, the Government Code (GC) of the State of California, specifically Sections 66410 through 66499.58, said sections also referred to as the Subdivision Map Act (SMA). [MC 9.14.010]
72. The final approved conditions of approval (COAs) issued and any applicable Mitigation Measures by the Planning Division shall be photographically or electronically placed on mylar sheets and included in the Grading and Street Improvement plans.
73. The developer shall monitor, supervise and control all construction related activities, so as to prevent these activities from causing a public nuisance, including but not limited to, insuring strict adherence to the following:
 - (a) Removal of dirt, debris, or other construction material deposited on any public street no later than the end of each working day.
 - (b) Observance of working hours as stipulated on permits issued by the Land Development Division.
 - (c) The construction site shall accommodate the parking of all motor vehicles used by persons working at or providing deliveries to the site.
 - (d) All dust control measures per South Coast Air Quality Management District (SCAQMD) requirements during the grading operations.

Violation of any condition, restriction or prohibition set forth in these conditions shall subject the owner, applicant, developer or contractor(s) to remedy as noted in City Municipal Code 8.14.090. In addition, the City Engineer or Building Official may suspend all construction related activities for violation of any condition, restriction or prohibition set forth in these conditions until such time as it has been determined that all operations and activities are in conformance with these conditions.
74. Drainage facilities (e.g., catch basins, water quality basins, etc.) with sump conditions shall be designed to convey the tributary 100-year storm flows. Secondary emergency escape shall also be provided.
75. In the event right-of-way or offsite easements are required to construct offsite

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improvements necessary for the orderly development of the surrounding area to meet the public health and safety needs, the developer shall make a good faith effort to acquire the needed right-of-way in accordance with the Land Development Division's administrative policy. If unsuccessful, the Developer shall enter into an agreement with the City to acquire the necessary right-of-way or offsite easements and complete the improvements at such time the City acquires the right-of-way or offsite easements which will permit the improvements to be made. The developer shall be responsible for all costs associated with the right-of-way or easement acquisition. [GC 66462.5]

76. If improvements associated with this project are not initiated within two (2) years of the date of approval of the Public Improvement Agreement (PIA), the City Engineer may require that the engineer's estimate for improvements associated with the project be modified to reflect current City construction costs in effect at the time of request for an extension of time for the PIA or issuance of a permit. [MC 9.14.210(B)(C)]
77. The developer shall protect downstream properties from damage caused by alteration of drainage patterns (i.e. concentration or diversion of flow, etc). Protection shall be provided by constructing adequate drainage facilities, including, but not limited to, modifying existing facilities or by securing a drainage easement. [MC 9.14.110]
78. Public drainage easements, when required, shall be a minimum of 25 feet wide and shall be shown on the map and plan, and noted as follows: "Drainage Easement – no structures, obstructions, or encroachments by land fills are allowed." In addition, the grade within the easement area shall not exceed a 3:1 (H:V) slope, unless approved by the City Engineer.
79. The maintenance responsibility of the proposed storm drain line shall be clearly identified. Storm drain lines within private property will be privately maintained and those within public streets will be publicly maintained.
80. The proposed on-site private storm drain system shall make a single connect to the existing 60" storm drain in Perris Blvd. The connection may require a construction permit issued from RCFC&WCD and an encroachment permit by the City. A storm drain manhole shall be placed at the right-of-way line to mark the beginning of the publicly maintained portion of this storm drain.
81. This project shall submit civil engineering design plans, reports and/or documents (prepared by a registered/licensed civil engineer) for review and approval by the City Engineer per the current submittal requirements, prior to the indicated threshold or as required by the City Engineer. The submittal consists of, but is not limited to, the following:
 - a. Lot Line Adjustment recorded prior to building permit issuance;

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- b. Precise grading w/ erosion control plan prior to grading permit issuance;
 - c. Public Improvement Plans (e.g., STREET/STORM DRAIN w/ STRIPING, RCFC STORM DRAIN, SEWER/WATER, etc.) prior to Encroachment Permit issuance;
 - d. Final drainage study prior to grading plan approval;
 - e. Final WQMP prior to grading plan approval;
 - f. Legal Documents (e.g., EASEMENT(s), DEDICATION(s), LOT LINE ADJUSTMENT, VACATION, etc.) prior to Building Permit issuance;
 - g. As-Built revision for all plans prior to Occupancy release;
82. Water quality best management practices (BMPs) designed to meet Water Quality Management Plan (WQMP) requirements for single-family residential development shall not be used as a construction BMP. Water quality BMPs shall be maintained for the entire duration of the project construction and be used to treat runoff from those developed portions of the project. Water quality BMPs shall be protected from upstream construction related runoff by having proper best management practices in place and maintained. Water quality BMPs shall be graded per the approved design plans and once landscaping and irrigation has been installed, it and its maintenance shall be turned over to an established Homeowner's Association (HOA). The Homeowner's Association shall enter into an agreement with the City for basin maintenance.

Prior to Grading Plan Approval

83. Resolution of all drainage issues shall be as approved by the City Engineer.
84. A final detailed drainage study (prepared by a registered/licensed civil engineer) shall be submitted for review and approved by the City Engineer. The study shall include, but not be limited to: existing and proposed hydrologic conditions as well as hydraulic calculations for all drainage control devices and storm drain lines. The study shall analyze 1, 3, 6 and 24-hour duration events for the 2, 5, 10 and 100-year storm events [MC 9.14.110(A.1)]. A digital (pdf) copy of the approved drainage study shall be submitted to the Land Development Division.
85. Emergency overflow areas shall be shown at all applicable drainage improvement locations in the event that the drainage improvement fails or exceeds full capacity.
86. The final project-specific Water Quality Management Plan (WQMP) shall be consistent with the approved P-WQMP, as well as in full conformance with the document: "Water Quality Management Plan - A Guidance Document for the Santa Ana Region of Riverside County" dated October 22, 2012. The F-WQMP shall be submitted and approved prior to application for and issuance of grading permits. At a minimum, the F-WQMP shall include the following: Site Design BMPs; Source Control BMPs, Treatment Control BMPs, Operation and Maintenance requirements

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for BMPs and sources of funding for BMP implementation.

a. The Applicant has proposed to incorporate the use of Bioretention. Final design and sizing details of all BMPs must be provided in the first submittal of the F-WQMP. The Applicant acknowledges that more area than currently shown on the plans may be required to treat site runoff as required by the WQMP guidance document.

b. The Applicant shall substantiate the applicable Hydrologic Condition of Concerns (HCOC) in Section F of the F-WQMP. <The HCOC designates that the project will be exempt from mitigation requirements based on Exemption 3>.

c. All proposed LID BMP's shall be designed in accordance with the RCFC&WCD's Design Handbook for Low Impact Development Best Management Practices, dated September 2011.

d. The proposed LID BMP's as identified in the project-specific P-WQMP shall be incorporated into the Final WQMP.

e. The NPDES notes per City Standard Drawing No. MVFE-350-0 shall be included in the grading plans.

f. Post-construction treatment control BMPs, once placed into operation for post-construction water quality control, shall not be used to treat runoff from construction sites or unstabilized areas of the site.

g. Prior to precise grading plan approval, the grading plan shall show any proposed trash enclosure to include a cover (roof) and sufficient size for dual bin (1 for trash and 1 for recyclables). The architecture shall be approved by the Planning Division and any structural approvals shall be made by the Building and Safety Division.

87. The developer shall ensure compliance with the City Grading ordinance, these Conditions of Approval, drainage slope shall be per the latest Calif Building Code, City Standards and the following criteria:

a. The project street and lot grading shall be designed in a manner that perpetuates the existing natural drainage patterns with respect to tributary drainage area and outlet points. Unless otherwise approved by the City Engineer, lot lines shall be located at the top of slopes.

b. Any grading that creates cut or fill slopes adjacent to the street shall provide erosion control, sight distance control, and slope easements as approved by the City Engineer.

c. All improvement plans are substantially complete and appropriate clearance letters are provided to the City.

d. A soils/geotechnical report (addressing the soil's stability and geological conditions of the site) shall be submitted to the Land Development Division for review. A digital (pdf) copy of the soils/geotechnical report shall be submitted to the Land Development Division.

88. Grading plans (prepared by a registered/licensed civil engineer) shall be submitted for review and approved by the City Engineer per the current submittal

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requirements.

89. The developer shall select Low Impact Development (LID) Best Management Practices (BMPs) designed per the latest version of the Water Quality Management Plan (WQMP) - a guidance document for the Santa Ana region of Riverside County.
90. The developer shall pay all remaining plan check fees.
91. A Storm Water Pollution Prevention Plan (SWPPP) shall be prepared in conformance with the State's current Construction Activities Storm Water General Permit. A copy of the current SWPPP shall be kept at the project site and be available for review upon request.
92. Any proposed trash enclosure shall include a solid cover (roof) and sufficient size for dual bin (one for trash and one for recyclables). The architecture shall be approved by the Planning Division and any structural approvals shall be made by the Building & Safety Division.

Prior to Grading Permit

93. A receipt showing payment of the Area Drainage Plan (ADP) fee to Riverside County Flood Control and Water Conservation District shall be submitted. [MC 9.14.100(O)]
94. For non-subdivision projects, a copy of the Covenants, Conditions and Restrictions (CC&Rs) shall be submitted for review by the City Engineer. The CC&Rs shall include, but not be limited to, access easements, reciprocal access, private and/or public utility easements as may be relevant to the project.
95. If the developer chooses to construct the project in phases, a Construction Phasing Plan for the construction of on-site public or private improvements shall be submitted for review and approved by the City Engineer.
96. The developer shall pay current DIF fees adopted by the City Council. [Ord. 695 § 1.1 (part), 2005] [MC 3.38.030, 040, 050]
97. A digital (pdf) copy of all approved grading plans shall be submitted to the Land Development Division.
98. Security, in the form of a cash deposit (preferable), bond or letter of credit shall be submitted as a guarantee of the implementation and maintenance of erosion control measures. At least twenty-five (25) percent of the required security shall be in the form of a cash deposit with the City. [MC 8.21.160(H)]
99. Security, in the form of a cash deposit (preferable), bond or letter of credit shall be

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submitted as a guarantee of the completion of the grading operations for the project. [MC 8.21.070]

100. The developer shall pay all applicable inspection fees.
101. The developer shall pay current TUMF fees adopted by the City Council. [Ord. 835 § 2.1, 2012] [MC 3.44.060]

Prior to Improvement Plan Approval

102. The developer is required to bring any existing access ramps adjacent to and fronting the project to current ADA (Americans with Disabilities Act) requirements. However, when work is required in an intersection that involves or impacts existing access ramps, all access ramps in that intersection shall be retrofitted to comply with current ADA requirements, unless otherwise approved by the City Engineer.
103. The developer shall submit clearances from all applicable agencies, and pay all applicable plan check fees.
104. The street improvement plans shall comply with current City policies, plans and applicable City standards (i.e. MVSI-160 series, etc.) throughout this project.
105. The design plan and profile shall be based upon a centerline, extending beyond the project boundaries a minimum distance of 300 feet at a grade and alignment approved by the City Engineer.
106. Drainage facilities (i.e. catch basins, etc.) with sump conditions shall be designed to convey the tributary 100-year storm flows. Secondary emergency escape shall also be provided.
107. The hydrology study shall be designed to accept and properly convey all off-site drainage flowing onto or through the site. In the event that the City Engineer permits the use of streets for drainage purposes, the provisions of current City standards shall apply. Should the quantities exceed the street capacity or the use of streets be prohibited for drainage purposes, as in the case where one travel lane in each direction shall not be used for drainage conveyance for emergency vehicle access on streets classified as minor arterials and greater, the developer shall provide adequate facilities as approved by the City Engineer. [MC 9.14.110 A.2]
108. All public improvement plans (prepared by a licensed/registered civil engineer) shall be submitted for review and approved by the City Engineer per the current submittal requirements.
109. Any missing or deficient existing improvements along the project frontage within the

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Project's frontage of Perris Blvd and Dracaea Avenue shall be constructed or secured for construction. The City Engineer may require the ultimate structural section for pavement to half-street width plus 18 feet or provide core test results confirming that existing pavement section is per current City Standards; additional signing & striping to accommodate increased traffic imposed by the development, etc.

110. For non-subdivision projects, all street dedications shall be free of encumbrances, irrevocably offered to the public and shall continue in force until the City accepts or abandons such offers, unless otherwise approved by the City Engineer.
111. The plans shall indicate any restrictions on trench repair pavement cuts to reflect the City's moratorium on disturbing newly-constructed pavement less than three (3) years old and recently slurry sealed streets less than one (1) year old. Pavement cuts may be allowed for emergency repairs or as specifically approved in writing by the City Engineer. Special requirements shall be imposed for repaving, limits to be determined by the City Engineer.
112. All dry and wet utilities shall be shown on the plans and any crossings shall be potholed to determine actual location and elevation. Any conflicts shall be identified and addressed on the plans. The pothole survey data shall be submitted to Land Development with the public improvement plans for reference purposes only. The developer is responsible to coordinate with all affected utility companies and bear all costs of any utility relocation.
113. The Applicant shall construct a bus turnout, per City Standards (MVSI-161-0) and as directed by the Transportation Engineering Division. Bus turnout will be located north of the Perris Blvd and Dracaea Avenue intersection, along the northbound travel lane and north of the Project's first driveway.
114. The Applicant shall construct the Perris Blvd median from the intersection of Dracaea Avenue, north to the prolongation of the Parcel 2 (APN 479120042) northerly property line (approximately 330 feet). The median shall be constructed per City Standards and as directed by the City's Transportation Engineering Division.

Prior to Encroachment Permit

115. A digital (pdf) copy of all approved improvement plans shall be submitted to the Land Development Division.
116. All applicable inspection fees shall be paid.
117. For non-subdivision projects, execution of a Public Improvement Agreement (PIA)

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and/or security (in the form of a cash deposit or other approved means) may be required as determined by the City Engineer. [MC 9.14.220]

118. The plans shall indicate any restrictions on trench repair pavement cuts to reflect the City's moratorium on disturbing newly-constructed pavement less than three (3) years old and recently slurry sealed streets less than one (1) year old. Pavement cuts may be allowed for emergency repairs or as specifically approved in writing by the City Engineer. Special requirements shall be imposed for repaving, limits to be determined by the City Engineer.
119. Any work performed within public right-of-way requires an encroachment permit.

Prior to Building Permit

120. An engineered-fill certification, rough grade certification and compaction report shall be submitted for review and approved by the City Engineer. A digital (pdf) copy of the approved compaction report shall be submitted to the Land Development Division. All pads shall meet pad elevations per approved grading plans as noted by the setting of "blue-top" markers installed by a registered land surveyor or licensed civil engineer.
121. For non-subdivision projects, the developer shall guarantee the completion of all related public improvements required for this project by executing a Public Improvement Agreement (PIA) with the City and posting the required security. [MC 9.14.220]
122. For Commercial/Industrial projects, the owner may have to secure coverage under the State's General Industrial Activities Storm Water Permit as issued by the State Water Resources Control Board.
123. For non-subdivision projects, all street dedications shall be free of encumbrances, irrevocably offered to the public and shall continue in force until the City accepts or abandons such offers, unless otherwise approved by the City Engineer.
124. A walk through with a Land Development Inspector shall be scheduled to inspect existing improvements within public right of way along project frontage. Any missing, damaged or substandard improvements including ADA access ramps that do not meet current City standards shall be required to be installed, replaced and/or repaired. The applicant shall post security to cover the cost of the repairs and complete the repairs within the time allowed in the public improvement agreement used to secure the improvements.
125. Certification to the line, grade, flow test and system invert elevations for the water quality control BMPs shall be submitted for review and approved by the City

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Engineer (excluding models homes).

Prior to Occupancy

126. All outstanding fees shall be paid.
127. All required as-built plans (prepared by a registered/licensed civil engineer) shall be submitted for review and approved by the City Engineer per the current submittal requirements.
128. The final/precise grade certification shall be submitted for review and approved by the City Engineer.
129. For commercial, industrial and multi-family projects, in compliance with Proposition 218, the developer shall agree to approve the City of Moreno Valley NPDES Regulatory Rate Schedule that is in place at the time of certificate of occupancy issuance. Under the current permit for storm water activities required as part of the National Pollutant Discharge Elimination System (NPDES) as mandated by the Federal Clean Water Act, this project is subject to the following requirements:
- a. Select one of the following options to meet the financial responsibility to provide storm water utilities services for the required continuous operation, maintenance, monitoring system evaluations and enhancements, remediation and/or replacement, all in accordance with Resolution No. 2002-46.
 - i. Participate in the mail ballot proceeding in compliance with Proposition 218, for the Common Interest, Commercial, Industrial and Quasi-Public Use NPDES Regulatory Rate Schedule and pay all associated costs with the ballot process; or
 - ii. Establish an endowment to cover future City costs as specified in the Common Interest, Commercial, Industrial and Quasi-Public Use NPDES Regulatory Rate Schedule.
 - b. Notify the Special Districts Division of the intent to request building permits 90 days prior to their issuance and the financial option selected. The financial option selected shall be in place prior to the issuance of certificate of occupancy. [California Government Code & Municipal Code]
130. The developer shall complete all public improvements in conformance with current City standards, except as noted in the Special Conditions, including but not limited to the following:
- a. Street improvements including, but not limited to: pavement, base, curb and/or gutter, cross gutters, spandrel, sidewalks, drive approaches, pedestrian ramps, street lights(SCE: LS-2) shall be installed per the City Standard MVSL-400B-2 and any power pole mounted street lights shall be removed, signing, striping, under sidewalk drains, landscaping and irrigation, medians, pavement tapers/transitions and traffic control devices as appropriate.

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- b. Storm drain facilities including, but not limited to: storm drain pipe, storm drain laterals, open channels, catch basins and local depressions.
 - c. City-owned utilities.
 - d. Sewer and water systems including, but not limited to: sanitary sewer, potable water and recycled water.
 - e. Under grounding of all existing and proposed utilities adjacent to and on-site. [MC 9.14.130]
 - f. Relocation of overhead electrical utility lines including, but not limited to: electrical, cable and telephone.
131. For commercial, industrial and multi-family projects, a “Stormwater Treatment Device and Control Measure Access and Maintenance Covenant”, shall be recorded to provide public notice of the maintenance requirements to be implemented per the approved final project-specific WQMP. A boilerplate copy of the covenants and agreements can be obtained by contacting the Land Development Division.
132. The applicant shall ensure the following, pursuant to Section XII. I. of the 2010 NPDES Permit:
- a. Field verification that structural Site Design, Source Control and Treatment Control BMPs are designed, constructed and functional in accordance with the approved Final Water Quality Management Plan (WQMP).
 - b. Certification of best management practices (BMPs) from a state licensed civil engineer. An original WQMP BMP Certification shall be submitted for review and approved by the City Engineer.
133. The Developer shall comply with the following water quality related items:
- a. Notify the Land Development Division prior to construction and installation of all structural BMPs so that an inspection can be performed.
 - b. Demonstrate that all structural BMPs described in the approved final project-specific WQMP have been constructed and installed in conformance with the approved plans and specifications;
 - c. Demonstrate that Developer is prepared to implement all non-structural BMPs described in the approved final project-specific WQMP; and
 - d. Demonstrate that an adequate number of copies of the approved final project-specific WQMP are available for future owners/occupants.
 - e. Clean and repair the water quality BMP's, including re-grading to approved civil drawing if necessary.
 - f. Obtain approval and complete installation of the irrigation and landscaping.

Special Districts Division

134. This project is conditioned for a proposed district to provide a funding source for the operation and maintenance of public improvements and/or services associated with

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new development in that territory. The Developer shall satisfy this condition with one of the options outlined below.

a. Participate in a special election for maintenance/services and pay all associated costs of the election process and formation, if any. Financing may be structured through a Community Facilities District, Landscape and Lighting Maintenance District, or other financing structure as determined by the City; or

b. Establish an endowment fund to cover the future maintenance and/or service costs.

The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org when submitting the application for building permit issuance. If the first building permit is pulled prior to formation of the district, this condition will not apply. If the district has been or is in the process of being formed the Developer must inform the Special Districts Division of its selected financing option (a. or b. above). The option for participating in a special election requires 90 days to complete the special election process. This allows adequate time to be in compliance with the provisions of Article 13C of the California Constitution.

The financial option selected shall be in place prior to the issuance of the first certificate of occupancy for the project.

135. Commercial (BP) If Land Development, a Division of the Public Works Department, requires this project to supply a funding source necessary to provide for, but not limited to, stormwater utilities services for the continuous operation, remediation and/or replacement, monitoring, systems evaluations and enhancement of on-site facilities and performing annual inspections of the affected areas to ensure compliance with state mandated stormwater regulations, a funding source needs to be established. The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org of its selected financial option for the National Pollution Discharge Elimination System (NPDES) program when submitting the application for the first building permit issuance (see Land Development's related condition). Participating in a special election the process requires a 90 day period prior to the City's issuance of a building permit. This allows adequate time to be in compliance with the provisions of Article 13D of the California Constitution. (California Health and Safety Code Sections 5473 through 5473.8 (Ord. 708 Section 3.1, 2006) & City of Moreno Valley Municipal Code Title 3, Section 3.50.050.)
136. If a landscaped median is required, for those areas to be maintained by the City and prior to the issuance of the first Building Permit, Planning Division (Community Development Department), Special Districts Division (the Public Works Department) and Transportation Division (the Public Works Department) shall

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review and approve the final median, parkway, slope, and/or open space landscape/irrigation plans as designated on the tentative map or in these Conditions of Approval prior to the issuance of the first Building Permit.

137. This project has been identified to be included in the formation of a Community Facilities District (Mello-Roos) for Public Safety services, including but not limited to Police, Fire Protection, Paramedic Services, Park Rangers, and Animal Control services. The property owner(s) shall not protest the formation; however, they retain the right to object to the rate and method of maximum special tax. In compliance with Proposition 218, the property owner shall agree to approve the mail ballot proceeding (special election) for either formation of the CFD or annexation into an existing district. The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org when submitting the application for building permit issuance to determine the requirement for participation. If the first building permit is pulled prior to formation of the district, this condition will not apply. If the condition applies, the special election will require a minimum of 90 days prior to issuance of the first building permit. This allows adequate time to be in compliance with the provisions of Article 13C of the California Constitution. (California Government Code Section 53313 et. seq.)
138. If a landscaped median is required, the Developer, or the Developer's successors or assignees shall be responsible for all parkway and/or median landscape maintenance for a period of one (1) year commencing from the time all items of work have been completed to the satisfaction of Special Districts staff as per the City of Moreno Valley Public Works Department Landscape Design Guidelines, or until such time as the District accepts maintenance responsibilities.
139. If a landscaped median is required, parkway, median, slope and/or open space landscape areas maintained as part of the City of Moreno Valley Community Facilities District 2014-01 shall be required to have independent utility systems, including but not limited to water, electric, and telephone services. An independent irrigation controller and pedestal will also be required. Combining utility systems with existing or future landscape areas not associated with the City of Moreno Valley Community Facilities District (CFD) landscaping will not be permitted.
140. If a landscaped median is required, inspection fees for the monitoring of landscape installation associated with the City of Moreno Valley maintained parkways/medians are due prior to the required pre-construction meeting. (MC 3.32.040)
141. If a landscaped median is required, plans for parkway, median, slope, and/or open space landscape areas designated in the project's Conditions of Approval for incorporation into a City Coordinated landscape maintenance program, shall be prepared and submitted in accordance with the City of Moreno Valley Public Works Department Landscape Design Guidelines. The guidelines are available on the

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0205)

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City's website at www.moval.org/sd or from the Special Districts Division (951.413.3480 or specialdistricts@moval.org).

142. The ongoing maintenance of any landscaping required to be installed behind the curb shall be the responsibility of the property owner.
143. If a landscaped median is required, plan check fees for review of parkway/median landscape plans for improvements that shall be maintained by the City of Moreno Valley are due upon the first plan submittal. (MC 3.32.040)
144. The parcel(s) associated with this project have been incorporated into the Moreno Valley Community Services District Zone A (Parks & Community Services) and Zone C (Arterial Street Lighting). All assessable parcels therein shall be subject to annual parcel taxes for Zone A and Zone C for operations and capital improvements.
145. If a landscaped median is required, parkway, open space, and/or median landscaping specified in the project's Conditions of Approval shall be constructed in compliance with the approved landscape plans and completed prior to the issuance of the first Certificate of Occupancy/Building Final for this project.
146. If a landscaped median is required, landscape and irrigation plans for parkway, median, slope, and/or open space landscape areas designated to be maintained by the City shall be placed on compact disk (CD) in pdf format. The CD shall include "As Built" plans, revisions, and changes. The CD will become the property of the City of Moreno Valley and the Moreno Valley Community Services District.
147. MAJOR INFRASTRUCTURE FINANCING DISTRICT. This project has been identified to potentially be included in the formation of a special financing district for the construction and maintenance of major infrastructure improvements which may include but are not limited to thoroughfares, bridges, and certain flood control improvements. The property owner(s) shall participate in such district and pay any special tax, assessment, or fee levied upon the project property for such district. At the time of the public hearing to consider formation of or annexation into the district, the qualified elector(s) will not protest the formation or annexation, but will retain the right to object to any eventual tax/assessment/fee that is not equitable should the financial burden of the tax/assessment/fee not be reasonably proportionate to the benefit the affected property obtains from the improvements to be installed and/or maintained. The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org when submitting an application for the first building permit to determine whether the development will be subjected to this condition. If subject to the condition, the special election requires a minimum 90 day process in compliance with the provisions of Article 13C of the California Constitution.

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0205)

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148. NEW STREET LIGHT INSTALLATION FEES. Prior to the issuance of the first building permit for this project, the Developer shall pay New Street Light Installation Fees for all applicable Residential and Arterial Street Lights required for this development. Payment shall be made to the City of Moreno Valley and collected by the Land Development Division. Fees are based upon the Advanced Energy fee rate in place at the time of payment, as set forth in the current Listing of City Fees, Charges, and Rates adopted by City Council. The Developer shall provide a copy of the receipt to the Special Districts Division (specialdistricts@moval.org). Any change in the project which may increase the number of street lights to be installed will require payment of additional Advanced Energy fees at the then current fee. Questions may be directed to the Special Districts Division at 951.413.3480 or specialdistricts@moval.org.
149. This project is conditioned to provide a funding source for the following special financing program(s):
- a. Street Lighting Services for capital improvements, energy charges, and maintenance.
 - b. If a landscaped median is required, Landscape Maintenance Services for parkway, open space, and/or median landscaping on Perris Blvd.

The Developer's responsibility is to provide a funding source for the capital improvements and the continued maintenance. The Developer shall satisfy this condition with one of the options below.

- i. Participate in a special election (mail ballot proceeding) and pay all associated costs of the special election and formation, if any. Financing may be structured through a Community Services District zone, Community Facilities District, Landscape and Lighting Maintenance District, or other financing structure as determined by the City; or
- ii. Establish a Property Owner's Association (POA) or Home Owner's Association (HOA) which will be responsible for any and all operation and maintenance costs

The Developer must notify the Special Districts Division at 951.413.3480 or at specialdistricts@moval.org of its selected financial option when submitting the application for building permit issuance. The option for participating in a special election requires approximately 90 days to complete the special election process. This allows adequate time to be in compliance with the provisions of Article 13C of the California Constitution.

The financial option selected shall be in place prior to the issuance of the first certificate of occupancy for the project.

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0205)

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150. Street Light Authorization forms for all street lights that are conditioned to be installed as part of this project must be submitted to the Special Districts Division for approval, prior to street light installation. The Street Light Authorization form can be obtained from the utility company providing electric service to the project, either Moreno Valley Utility or Southern California Edison. For questions, contact the Special Districts Division at 951.413.3480 or specialdistricts@moval.org.

Transportation Engineering Division

151. Prior to issuance of a construction permit, construction traffic control plans prepared by a qualified, registered Civil or Traffic engineer may be required for plan approval or as required by the City Traffic Engineer.
152. All project driveways shall conform to Section 9.11.080, and Table 9.11.080-14 of the City's Development Code – Design Guidelines and City of Moreno Valley Standard Plans No. MVSI-112A~D-0 for commercial driveway approaches.
- Driveway on Perris Boulevard shall be restricted to right-in and right-out movements only by raised median installation.
153. All proposed on-site traffic signing and striping should be accordance with the latest California Manual on Uniform Traffic Control Devices (CAMUTCD).
154. Prior to final approval of any landscaping or monument sign plans, the project plans shall demonstrate that sight distance at the project driveways conforms to City Standard Plan No. MVSI-164A, B, C-0.
155. Sight distance at the proposed roadways and driveways shall conform to City of Moreno Valley Standard No. MVSI-164A,B,C-0 at the time of preparation of final grading, landscape, and street improvement plans.
156. Prior to issuance of building permits, signing and striping plans shall be prepared by a registered, qualified civil or electrical engineer and submitted to the City for the required offsite improvements on Dracaea Avenue, Perris Boulevard, and Atwood Avenue.
157. Prior to issuance of a certificate of occupancy, a bus bay shall be installed per City of Moreno Valley Standard MVSI-161-0 on the east side of Perris Boulevard north of Dracaea Avenue, or as approved by the City Engineer.
158. Prior to issuance of certificate of occupancy, communication conduit shall be installed along project frontage on Perris Boulevard per City Standard Plan No. MVSI-186-0. Any improvements undertaken by this project shall be consistent with

CONDITIONS OF APPROVAL

Conditional Use Permit (PEN19-0205)

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the City's standards for this facility.

159. Prior to issuance of a certificate of occupancy, median improvements on Perris Boulevard shall be installed per the approved plans and accepted by the City Engineer.
160. Prior to issuance of a certificate of occupancy, all signing and striping improvements on Dracaea Avenue and Perris Boulevard shall be installed per the approved plans and accepted by the City Engineer.
161. A bus turnout is required along the project frontage per MVSI-161-0. Bus turn-out shall be installed on the east side of Perris Boulevard, along the property frontage.
162. Applicant shall modify geometrics at Perris Boulevard and Dracaea Avenue intersection to provide for eastbound and westbound left-turn lanes.
163. Applicant shall construct raised median on Perris Boulevard from Dracaea Avenue to north of intersection with Atwood Avenue.
 - a) At Atwood Avenue and Perris Boulevard intersection, median shall provide for full access north and south left-turn, through, and right-turn movements. The northbound and southbound left-turn lanes would have approximately 100 feet of queuing space plus taper.
 - b) Atwood Avenue should be restricted to right-in/right-out east and westbound movements with a directional median opening on Perris Boulevard.
 - c) Southbound left-turn lane at Perris Boulevard intersection with Dracaea Avenue shall provide at least 125 feet of queueing space plus taper.
164. Prior to issuance of a certificate of occupancy, fair share payments for improvements identified on the Traffic Study shall be paid:
 - a) Perris Boulevard and Cottonwood Avenue Intersection.
 - b) Perris Boulevard and Eucalyptus Avenue Intersection.

NOTES

- PARKING LOTS, STREET AND BUILDINGS WILL BE WELL LIT, MINIMIZE SHADOWS CAST BY LANDSCAPING AND TREES ON PROPERTY, WALKWAYS, AND PUBLIC AREAS."

BUILDING DIV. NOTES

- CALIF. GREEN CODE - 5.106.4.1.2 -NEW BUILDING WITH 10 OR MORE EMPLOYEES PER SHIFT MUST PROVIDE LONG TERM BICYCLE PARKING.
- NONE OF THE BUILDINGS WILL HAVE OVER 10 EMPLOYEES PER SHIFT. THEREFORE, ONLY STANDARD ANCHORED SHORT TERM BIKE RACKS WILL BE PROVIDED.
- PROPOSED C-STORE WILL HAVE A TOTAL OF 4 EMPLOYEES PER SHIFT.
- PROPOSED DRIVE THRU WILL HAVE A TOTAL OF 9 EMPLOYEES PER SHIFT.

POLICE DEPT. NOTES

- SECURITY CAMERAS INSIDE THE BUSINESSES AND SEVERAL CAMERAS OUTSIDE.
- NUMBERS OR LETTERS SHOULD BE CLEARLY VISIBLE FROM THE STREET.
- MAXIMIZE THE NUMBER OF WINDOWS ON THE BUSINESSES SO CUSTOMERS CAN SEE OUT IN TO THE PARKING LOT WHILE INSIDE EATING. THIS WILL HELP PARKING LOT SECURITY BY GIVING PEOPLE THE OPPORTUNITY TO MAINTAIN VISUAL OF THEIR VEHICLES AND VALUABLES.
- ALL EXTERIOR DOORS SHALL HAVE A VANDAL RESISTANT LIGHT FIXTURE INSTALLED ABOVE THE DOOR. THE DOORS SHALL BE ILLUMINATED WITH A MINIMUM ONE FOOT CANDLE ILLUMINATION AT GROUND LEVEL, EVENLY DISPERSED.
- BUSINESS SHOULD HAVE AN ALARM SYSTEM THAT IS MONITORED BY A DESIGNATED PRIVATE ALARM COMPANY TO NOTIFY THE MORENO VALLEY POLICE DEPARTMENT OF ANY INTRUSIONS.

SITE DATA

PROJECT DATA	
ASSESSOR'S PARCEL NUMBER:	479-120-042, 027, 029, 043
ADDRESS:	NEC PERRIS BLVD. & DRACAEA AVE. MORENO VALLEY, CA 92553
DESCRIPTION:	NEW CONSTRUCTION
SPRINKLED:	N
CONSTRUCTION TYPE:	V-B
EXISTING ZONING:	(OC) OFFICE COMMERCIAL
PROPOSED ZONING:	(CC) COMMUNITY COMMERCIAL
PROPOSED PLAN LAND USE:	EXISTING: RESIDENTIAL OFFICE PROPOSED: COMMERCIAL

BUILDING DATA	
SITE AREA:	±101,934 (2.30 AC)
DRIVE THRU SITE AREA:	±56,393 (1.30 AC)
C-STORE SITE AREA:	±45,554 (1.04 AC)
PROPOSED C-STORE AREA:	±4,088 SF
CANOPY AREA:	±3,096 SF
PROPOSED DRIVE THRU REST. AREA:	±3,000 SF
ALLOWABLE BUILDING HEIGHT:	30' FT
LOT COVERAGE:	±7,088 SF (7.0 %)
LANDSCAPE PROVIDED:	±27,600 SF (26.5%)

LANDSCAPE SETBACKS	
FRONT	20' FT
REAR	20' FT
SIDE	15' FT
SIDE	20' FT

BUILDING SETBACKS	
FRONT	10' FT
REAR	20' FT (WITHIN BUILDING UNDER 30')
SIDE	15' FT
SIDE	20' FT

C-STORE PARKING REQUIRED:	21 STALLS 19 (1/225 SF OF 4,088 SF) 2 FOR SERVICE STATION
DRIVE THRU REST. REQUIRED:	34 STALLS @ 1/100 SF 3,000 SF @ 1/100 SF 400 SF @ 1/100 SF
C-STORE PARKING PROVIDED:	21 TOTAL STALLS
DRIVE THRU REST. PARKING PROVIDED:	34 TOTAL STALLS
TOTAL PARKING PROVIDED:	55 STALLS

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN IS SITUATED IN THE CITY OF MORENO VALLEY THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS

PARCEL 1:
THE WESTERLY 95 FEET OF LOT 414 OF EDGEMONT GARDENS, AS SHOWN BY MAP ON FILE IN BOOK 15, PAGE 90 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

PARCEL 2:
LOT 415 OF EDGEMONT GARDENS, AS SHOWN BY MAP ON FILE IN BOOK 15, PAGE 90 OF RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, EXCEPTING THEREFROM THE WESTERLY 50 FEET AS DEEDED TO THE COUNTY OF RIVERSIDE BY DEED, RECORDED JANUARY 31, 1972 AS INSTRUMENT NO. 12276 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

APN: 479-120-042 AND 479-120A-029

PARCEL 3:
LOTS 413 AND 414 OF EDGEMONT GARDENS, IN THE CITY OF MORENO VALLEY, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 15, PAGE 90 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THEREFROM THE EASTERLY 70 FEET OF THE SOUTHERLY 90 FEET OF SAID LOT 413;

ALSO EXCEPTING THEREFROM THE WESTERLY 95.00 FEET OF SAID LOT 414.

APN: 479-120-027

PARCEL 4:
LOT 406 OF EDGEMONT GARDENS, AS SHOWN BY MAP ON FILE IN BOOK 15, PAGE 90 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

EXCEPTING THEREFROM THE WESTERLY 50 FEET AS CONVEYED TO THE COUNTY OF RIVERSIDE BY DEED RECORDED JANUARY 31, 1972, AS INSTRUMENT NO. 12275, OFFICIAL RECORDS.

APN: 479-120-043-8

VICINITY MAP



LEGEND

	PROPERTY LINE		NEW TREE
	LANDSCAPE SETBACK		NEW PARKING LIGHTS
	ADA PATH OF TRAVEL		
	LANDSCAPE AREA		
	CONCRETE PAVING/SIDEWALK		

COMMERCIAL

PERRIS BOULEVARD

DRACAEA AVENUE

RESIDENTIAL

RESIDENTIAL

SITE PLAN

1"=20'

1

Job #: M - 1011
Date: 09/01/20
Scale: AS NOTED
Drawn By: SPD



SP-14

Plotted By: Smith, Hannah. Sheet Set: Rho. Layout: Layout1. September 01, 2020 12:39:50pm. K:\ORA_DEV\094934008 - D&T Moreno Valley\CAD\Plan Sheets\Preliminary Grading Plan.dwg. This document, together with the computer and design files presented herein, is an instrument of service intended only for the specific purpose and client for which it was prepared. Release of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

LEGEND

- PROPERTY LINE
CIVIL LIMIT OF WORK LINE
GRADE BREAK LINE
RIDGE LINE
FLOW LINE
STORM DRAIN LINE
PROPOSED SPOT GRADE
EXISTING SPOT GRADE
PROPOSED FLOW (DIRECTION AND SLOPE)
ASPHALT
HEAVY DUTY CONCRETE PAVEMENT
PROPOSED LANDSCAPING
TRUNCATED DOMES
STANDARD DUTY CONCRETE SIDEWALK
BIORETENTION AREA

GRADING NOTES

- 1 CONSTRUCT CONCRETE CURB
2 CONSTRUCT STANDARD DUTY CONCRETE SIDEWALK
3 PROPOSED LANDSCAPING
4 CONSTRUCT ASPHALT CONCRETE PAVEMENT
5 CONSTRUCT ACCESSIBLE CURB RAMP WITH DETECTABLE WARNINGS
6 INSTALL STANDARD 90' PARKING STALL
7 INSTALL 90' ACCESSIBLE PARKING STALL
8 INSTALL BIKE RACK/LONG TERM BIKE LOCKER PER ARCHITECTURAL PLANS

DRAINAGE NOTES

- D1 PROPOSED 2'-FT CHANNEL DRAIN
D2 CONSTRUCT 48" NYLOPLAST MANHOLE WITH WEIR AND ORIFICE OR APPROVED EQUAL
D3 INSTALL STORM DRAIN LINE. SIZE, SLOPE AND MATERIAL PER PLAN.
D4 CONNECT TO EXISTING 60" RCP STORM DRAIN MAIN
D5 CONSTRUCT WQMP RETENTION AREA PER DETAIL 1, SHEET C3.0
D6 CONSTRUCT STANDARD 48" MANHOLE. INVERTS PER PLAN.

ESTIMATED EARTHWORK QUANTITIES

CUT: 100 CY
FILL: 2300 CY
NET: 2200 CY (FILL)

NOTE: THE ABOVE QUANTITIES ARE APPROXIMATE IN PLACE VOLUMES CALCULATED FROM THE EXISTING GROUND TO THE PROPOSED FINISHED GRADE. EXISTING GROUND IS DEFINED BY THE CONTOURS AND SPOT GRADES ON THE BASE SURVEY. PROPOSED FINISHED GRADE IS DEFINED AS THE FINAL GRADE AS INDICATED ON THE GRADING PLAN(S).

THE EARTHWORK QUANTITIES ABOVE ARE FOR PERMIT PURPOSES ONLY. THEY HAVE NOT BEEN FACTORED TO ACCOUNT FOR CHANGES IN VOLUME DUE TO BULKING, CLEARING AND GRUBBING, SHRINKAGE, OVER-EXCAVATION AND RE-COMPACTON, AND CONSTRUCTION METHODS. NOR DO THEY ACCOUNT FOR THE THICKNESS OF PAVEMENT SECTIONS, FOOTINGS, SLABS, REUSE OF PULVERIZED MATERIALS THAT WILL UNDERLIE NEW PAVEMENTS, ETC. THE CONTRACTOR SHALL RELY ON THEIR OWN EARTHWORK ESTIMATES FOR BIDDING PURPOSES.

LEGAL DESCRIPTION

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SITE SUMMARY

FLOOD ZONE: X
LOCATION: 06065C0761G (EFFECTIVE 9/28/2008)
DRACAEAE AVE AND PERRIS BLVD
MORENO VALLEY, CA
DISTURBED AREA: 2.39 AC

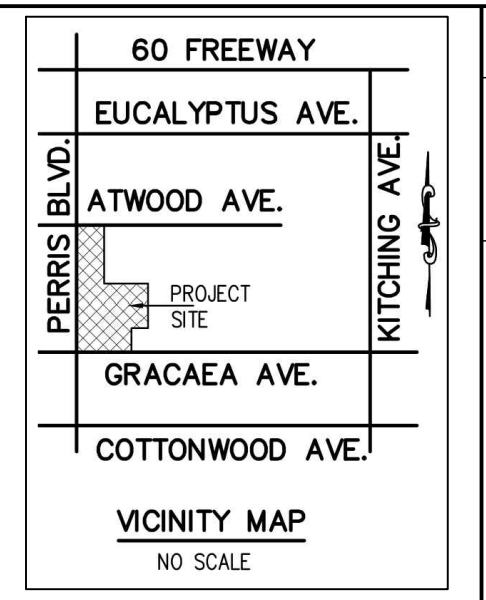
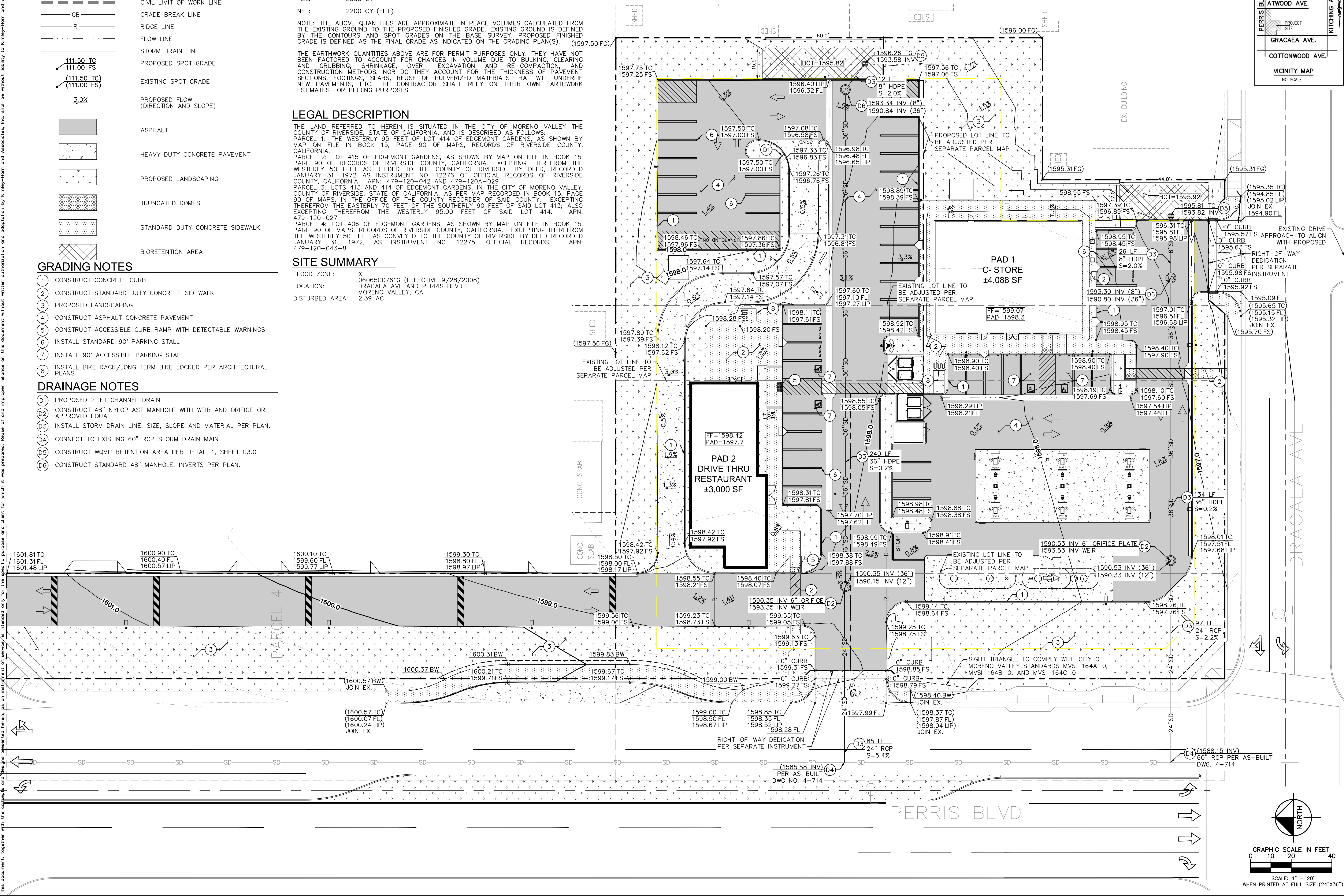


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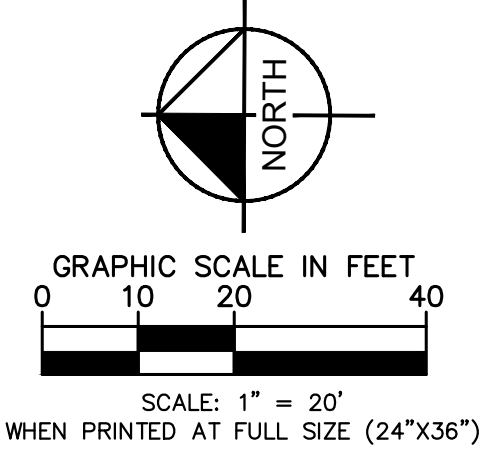
Kimley-Horn logo and contact information: 765 THE CITY DRIVE, SUITE 200, ORANGE, CA 92668. PHONE: 714-939-1030. FAX: 714-938-9468. WWW.KIMLEY-HORN.COM



Table with columns: SCALE, AS SHOWN, DESIGNED BY, DRAWN BY, CHECKED BY. Includes date 9/1/20.

CADENCE CAPITAL INVESTMENTS
6400 FIDDLERS GREEN CIR, STE 1820
GREEN VILLAGE, CO 80111

PRELIMINARY GRADING PLAN
PERRIS BLVD & DRACAEAE AVE.
MORENO VALLEY, CA 92553
SHEET NUMBER C1.0



Attachment: Project Plans (4510 : Conditional Use Permits for a Drive Through Restaurant, Fueling Station and Convenience Store)

ATWOOD AVE.

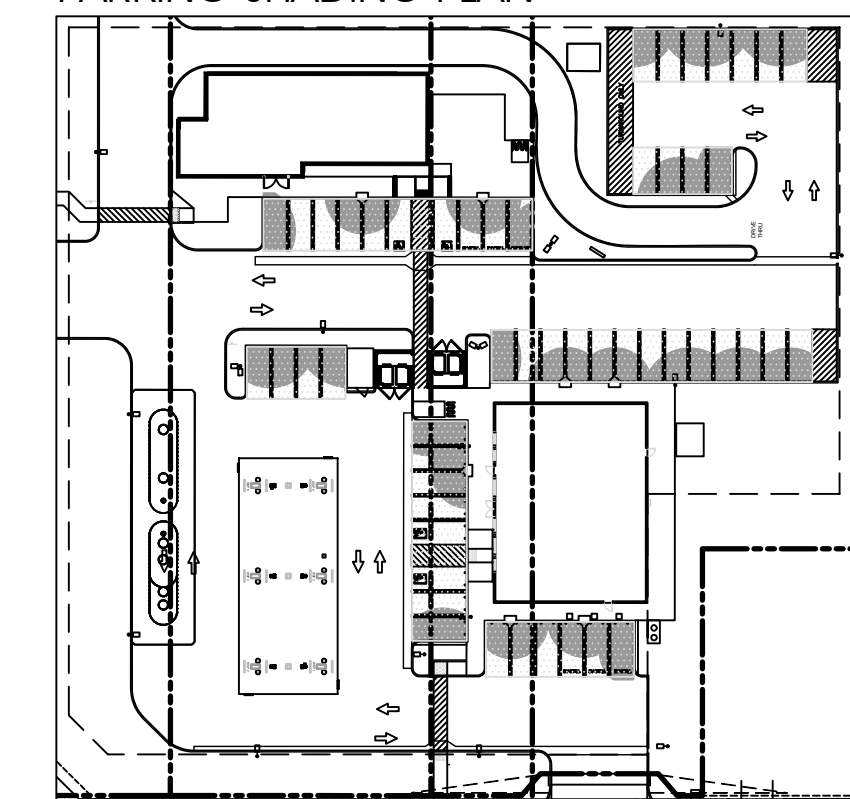
PLANT PALETTE

Symbol	Botanical Name	Common Name	WUCOLS Region 4
TREES			
	<i>Cercidium 'Desert Museum'</i>	Palo Verde	Low
	<i>Lagerstroemia i. 'Natchez'</i>	Crape Myrtle	Mod
	<i>Pinus canariensis</i>	Canary Island Pine	Low
	<i>Tristania conferta</i>	Brisbane Box	Mod
	<i>Geijera parviflora</i>	Australian Willow	Low
	<i>Ulmus parvifolia 'Drake'</i>	Evergreen Chinese Elm	Low
SHRUBS/ GROUNDCOVERS			
	<i>Acacia r. 'Lowboy'</i>	Prostrate Acacia	Low
	Agave species	Agave	Low
	Aloe species	Aloe	Low
	<i>Anigozanthos flavidus</i>	Kangaroo Paw	Low
	<i>Baccharis p. 'Twin Peaks'</i>	Coyote Bush	Low
	<i>Callistemon v. 'Little John'</i>	Dwarf Bottlebrush	Low
	<i>Cistus purpureus</i>	Orchid Rock Rose	Low
	<i>Dianella l. 'Variegata'</i>	Variegated Flax Lily	Mod
	<i>Dodonaea viscosa</i>	Purple Hopseed Bush	Mod
	Grevillea spp.	Grevillea	Low
	<i>Hesperaloe parviflora</i>	Red Yucca	Low
	Lantana 'New Gold'	Trailing Lantana	Low
	Lavandula 'Meerlo'	Meerlo Lavender	Low
	<i>Leucophyllum f. 'Compacta'</i>	Texas Ranger	Low
	<i>Myoporum parvifolium</i>	Myoporum	Low
	<i>Rosmarinus o. 'Huntington Carpet'</i>	Creeping Rosemary	Low
	<i>Russelia equisetiformis</i>	California fuchsia	Low
	<i>Salvia clevelandii</i>	Cleveland Sage	Low
	<i>Salvia greggii 'Flame'</i>	Furman's Red Autumn Sage	Low
	<i>Westringia fruticosa</i>	Coast Rosemary	Low
	<i>Muhlenbergia capilaris</i>	Pink Muhly	Low
	<i>Rhamnus californica</i>	Coffeeberry	Low

NOTES:
 PLANT MATERIAL NOT LISTED MAY BE USED, SUBJECT TO APPROVAL BY THE CITY.
 ALL LANDSCAPE PLANS AND INSTALLATIONS SHALL ADHERE TO CITY DESIGN GUIDELINES, CODES AND REGULATIONS.
 ALL LANDSCAPE AREAS SHALL RECEIVE AUTOMATIC IRRIGATION SYSTEM.
 ALL LANDSCAPE INSTALLATION SHALL BE PERMANENTLY MAINTAINED.

SITE AREA CALCULATIONS:
 TOTAL SITE AREA: 104,108 S.F.
 TOTAL PARKING AREA: 9,250 S.F.
 TOTAL LANDSCAPE AREA: 36,756 S.F. (35%)

PARKING SHADING PLAN



TREE SHADING CALCULATIONS:
 TOTAL PARKING AREA: 9,250 S.F.
 TOTAL PARKING SHADED AREA: 5,145 S.F. (56%)

VICINITY MAP

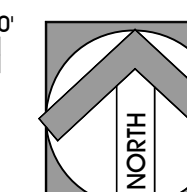


CONCEPTUAL LANDSCAPE PLAN

PERRIS BLVD & DRACAEA AVE.
 MORENO VALLEY, CA 92553



0 15 30 60 90
 SCALE: 1" = 30'
 CDPC PROJECT NO. 19030



September 01, 2020

conceptual design & planning company

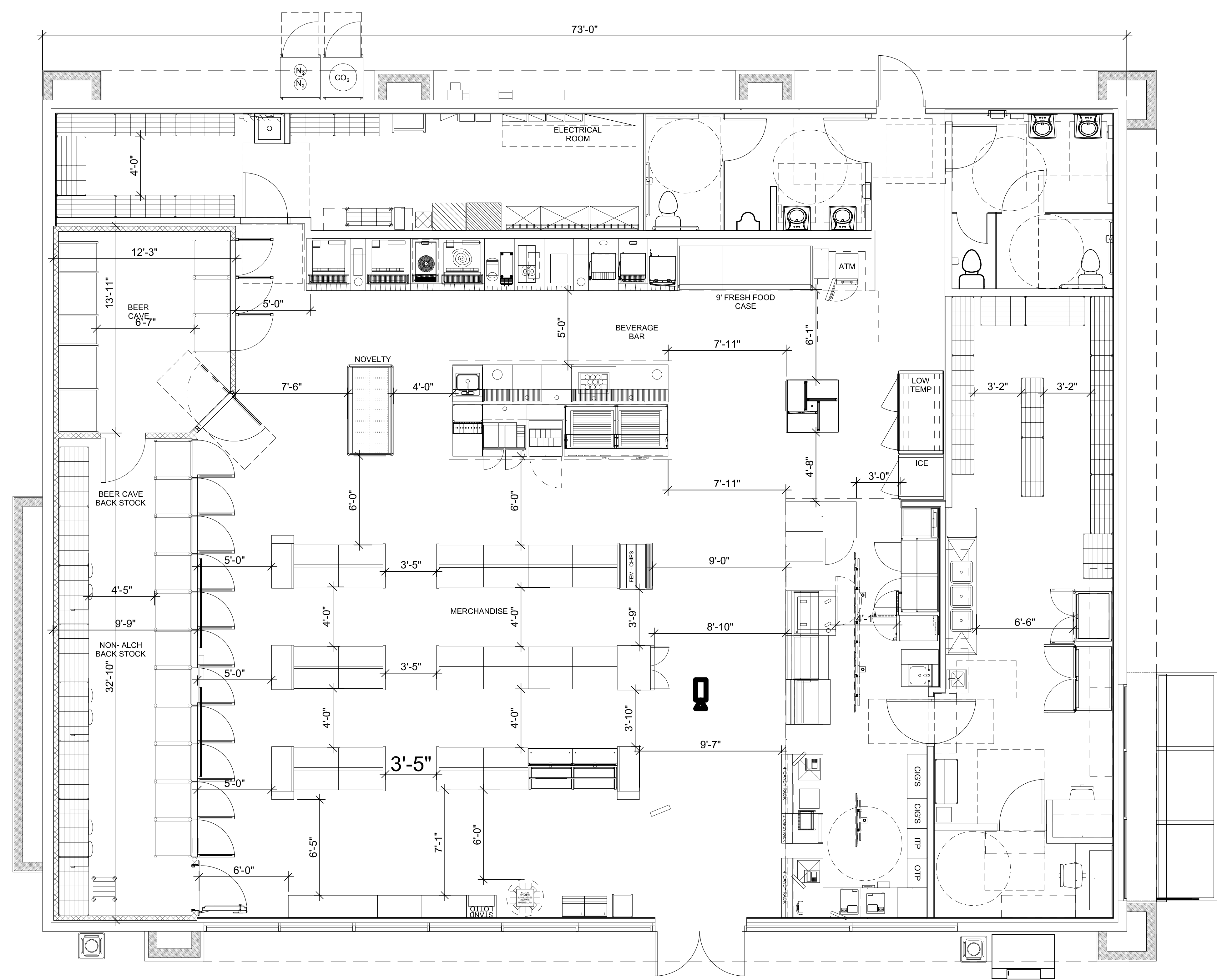
Corporate Office:
 3195-C Airport Loop Drive
 Studio One
 Costa Mesa, CA 92626
 T: 949-399-0870
 www.cdpcinc.com

COSTA MESA • ATASCADERO • SAN JOSE



L-1

1 OF 1



LAYOUT INFORMATION

ROLLER GRILLS	2 (SELF SERVE)
SANDWICH CASE	9'
ALC VAULT DRS	3
NON-ALC VAULT DRS	11
LOW TEMP DOORS	2
ICE MERCH. DOORS	1
NOVELTY CASE	1
BAKERY CASE	1 (LG)
SLURPEE BARRELS	8
GONDOLA UNITS (60"H)	36
END CAPS (60"H)	04
POWER WINGS	(03) - NOT IN TOTAL
LOW WALLS (36"H)	04
HIGH WALLS (72"H)	00
TOTAL	44

TOTAL SQ FT	= 4088 SF
SALES FLOOR AREA	= 2308 SF

GAS: YES LIQUOR: NO
 BEER: YES WINE: YES

OCCUPANCY LOAD (>49) = 42
 TRAVEL DISTANCE (<200) = 79'
 COMMON PATH OF TRAVEL (<75) = 48'
 RESTROOMS REQUIRED = 2
 EXITS REQUIRED = 2

OVERHEAD SHELVES = 50 FT
 FLOOR SHELVES = 80 FT

WALL TYPE LEGEND

- EXISTING WALL
- EXISTING COLUMN
- NEW WALL
- NEW PARTIAL HEIGHT WALL
- NEW COOLER WALL

OCCUPANCY CALCULATION

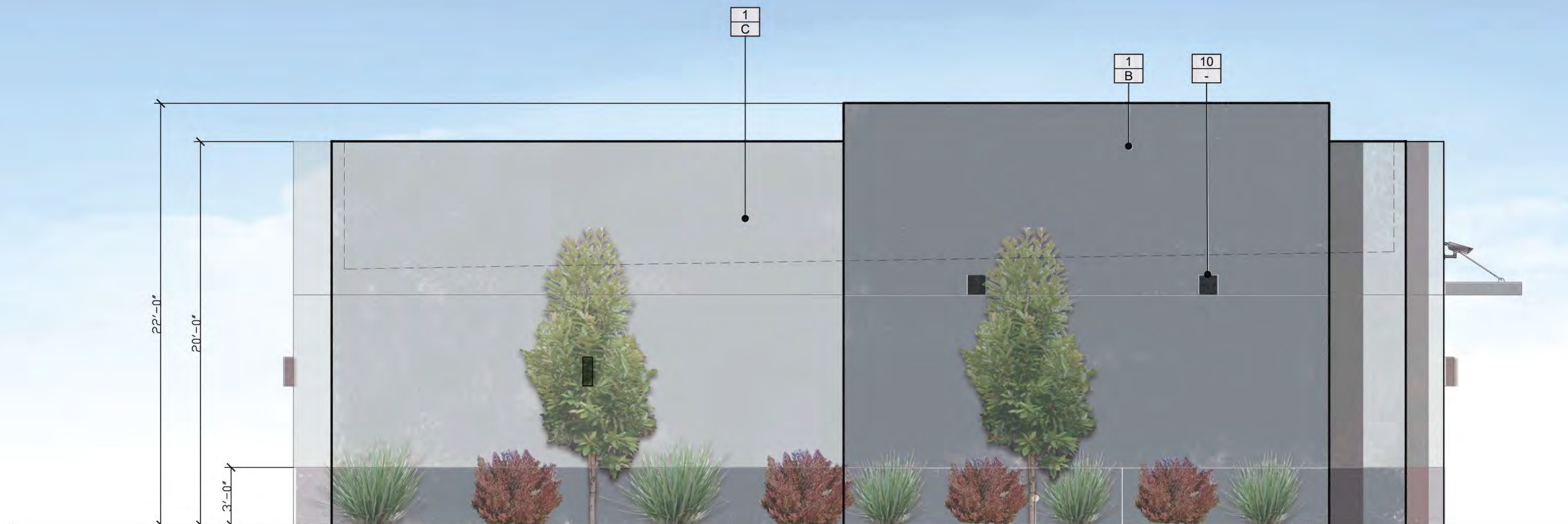
MERCHANDISE	2004 SF	/	60	=	34 PEOPLE
KITCHEN / SALES	304 SF	/	200	=	2 PEOPLE
STORAGE/ BACK ROOM	1319 SF	/	300	=	5 PEOPLE
OFFICE	91 SF	/	100	=	1 PERSON
RESTROOM	370 SF	/	N/A	=	0 PEOPLE
					TOTAL = 42 PEOPLE

Job #:	M - 1011	Date:	08/21/19	Scale:	AS NOTED	Drawn By:	SPD
CADENCE CAPITAL INVESTMENTS LLC 6400 FIDDLERS GREEN CIR, STE. 1820 GREENWOOD VILLAGE, CO 80011				PERRIS BLVD. & DRACAEA AVE. MORENO VALLEY, CA 92553			
				INTERIOR FLOOR PLAN			
INTERIOR FLOOR PLAN							1/4"=1'-0" 1



SOUTH ELEVATION

1/4"=1'-0" 1



NORTH ELEVATION

1/4"=1'-0" 2

KEYED NOTES

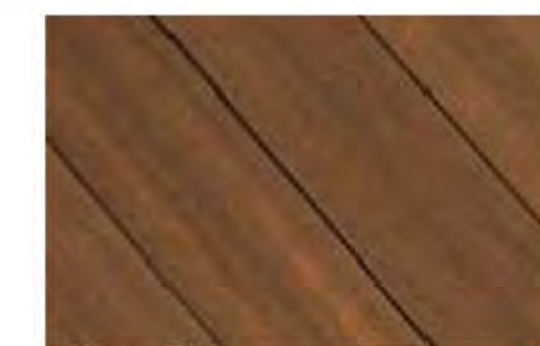
- 1 - EXTERIOR STUCCO TO BE PAINTED
- 2 - EMERGENCY EGRESS LIGHT
- 3 - PREFINISHED METAL CANOPY
- 4 - STREET ADDRESS WHITE VINYL
- 5 - WINDOW STRIPING AT TOP OF MULLION
- 6 - EXTERIOR WALL CYLINDER - KICHLER - 9264BA - BRONZE
- 7 - SERVICE & SECONDARY EGRESS DOOR
- 8 - SPANDREL STOREFRONT
- 9 - NOT USED
- 10 - EXTERIOR WALL PACK LIGHTS
- 11 - SIGNAGE - BACKLIT CHANNEL LETTERS
- 12 - WINDOW STRIPING
- 13 - EMERGENCY EGRESS DOOR
- 14 - INTERNAL DOWNSPOUT FOR PRIMARY DRAIN

NOTES

- ALL EXTERIOR DOORS SHALL HAVE A VANDAL RESISTANT LIGHT FIXTURE INSTALLED ABOVE THE DOOR. THE DOORS SHALL BE ILLUMINATED WITH A MINIMUM 1 FC ILLUMINATION AT GROUND LEVEL, EVENLY DISPERSED.
- NUMBER AND/OR LETTERS WILL BE CLEARLY VISIBLE FROM THE STREET.

EXTERIOR COLORS & FINISHES

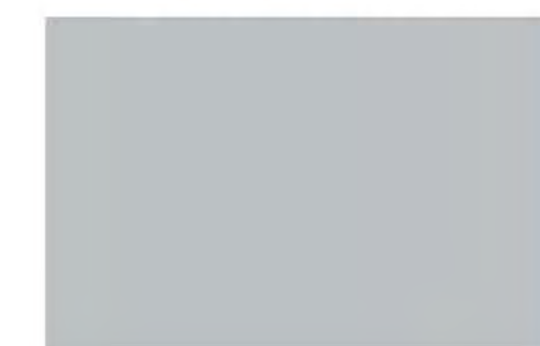
- CLADDING/SIDING
- A - FIBERON - WARM SIENA
- SHERWIN WILLIAMS PAINT
- B - #SW 6256 - SERIOUS GRAY
- SHERWIN WILLIAMS PAINT
- C - # SW 6254 - LAZY GRAY
- ANODIZED ALUMINUM STOREFRONT SYSTEM
- D - #40 DARK BRONZE AA-M10C21A44
- SHERWIN WILLIAMS PAINT
- E - # SW 7076 - CYBER SPACE



FIBERON - WARM SIENA



SHERWIN WILLIAMS PAINT
#SW 6256 - SERIOUS GRAY



SHERWIN WILLIAMS PAINT
#SW 6254 - LAZY GRAY



SHERWIN WILLIAMS PAINT
#SW 7076 - CYBER SPACE

CADENCE CAPITAL INVESTMENTS LLC
6400 FIDDLERS GREEN CIR, STE. 1820
GREEN VILLAGE, CO 80111

PERRIS BLVD. & DRACAEA AVE.
MORENO VALLEY, CA 92553

EXTERIOR ELEVATION



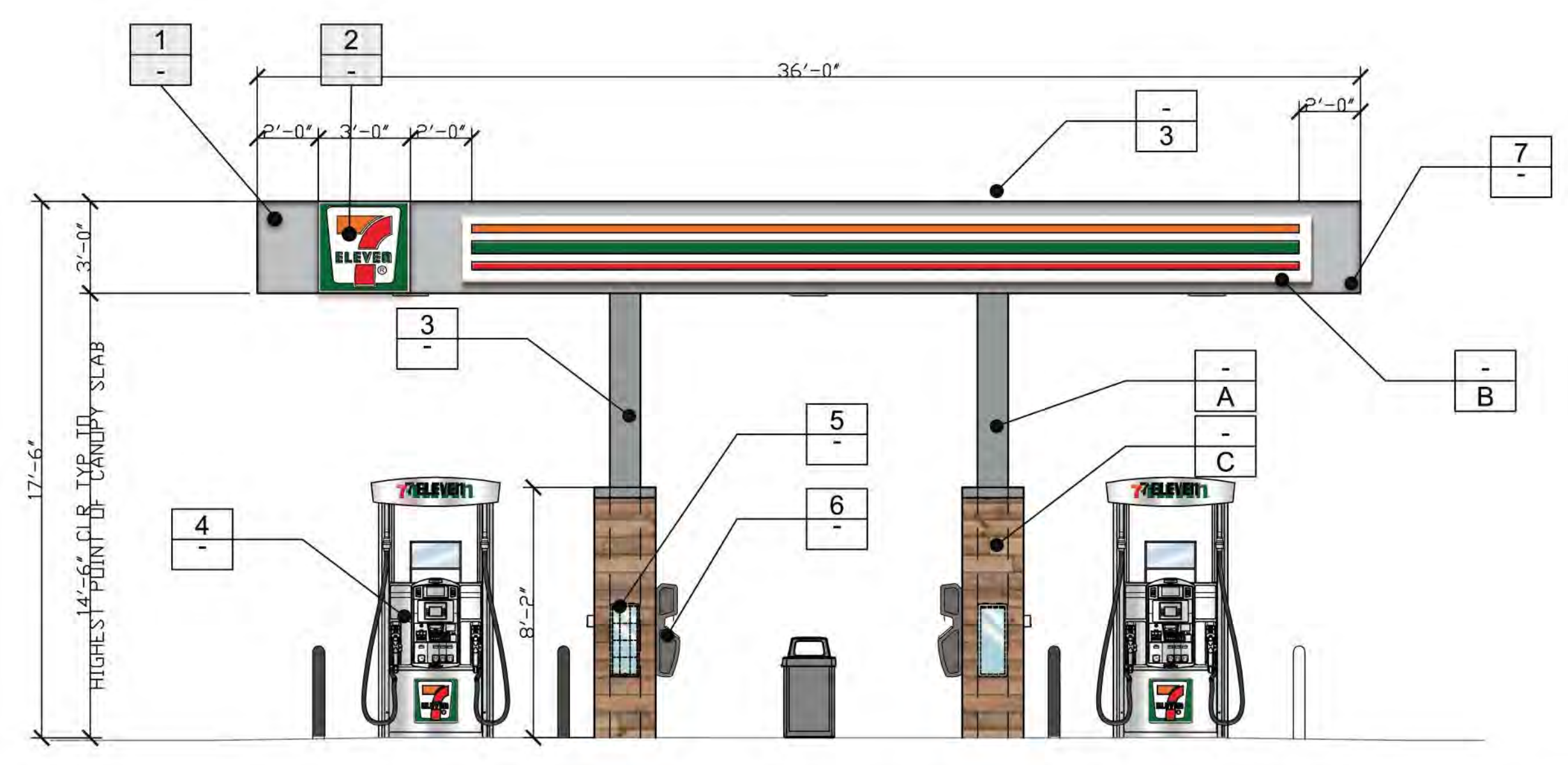
Job #: M - 1011
Scale: 06/07/19
Date: AS NOTED
Drawn By: SPD
Page Number: 5

ELEV

KEYED NOTES

- 1 - ACM CANOPY FASCIA WITH TATEYAMA IMAGE TRI-STRIPE
- 2 - ILLUMINATED LOGO
- 3 - CANOPY COLUMN
- 4 - GAS DISPENSER
- 5 - FIRE EXTINGUISHER-MOUNT MAX. 48" FROM GRADE
- 6 - WASH BUCKET-MOUNT MAX. 48" FROM GRADE
- 7 - OVERFLOW PROTECTION AS REQUIRED
- 8 - GAS DISPENSER OUTLINE

Rev. #	Date	Description



NORTH ELEVATION

1/4"=1'-0" 1

EXTERIOR COLORS & FINISHES

- SHERWIN WILLIAMS PAINT # SW 9162 - AFRICAN GRAY
- A - SHERWIN WILLIAMS PAINT # SW 6254 - LAZY GRAY
- CLADDING/SIDING FIBERON - WARM SIENA
- B - SHERWIN WILLIAMS PAINT # SW 6254 - LAZY GRAY
- C - CLADDING/SIDING FIBERON - WARM SIENA



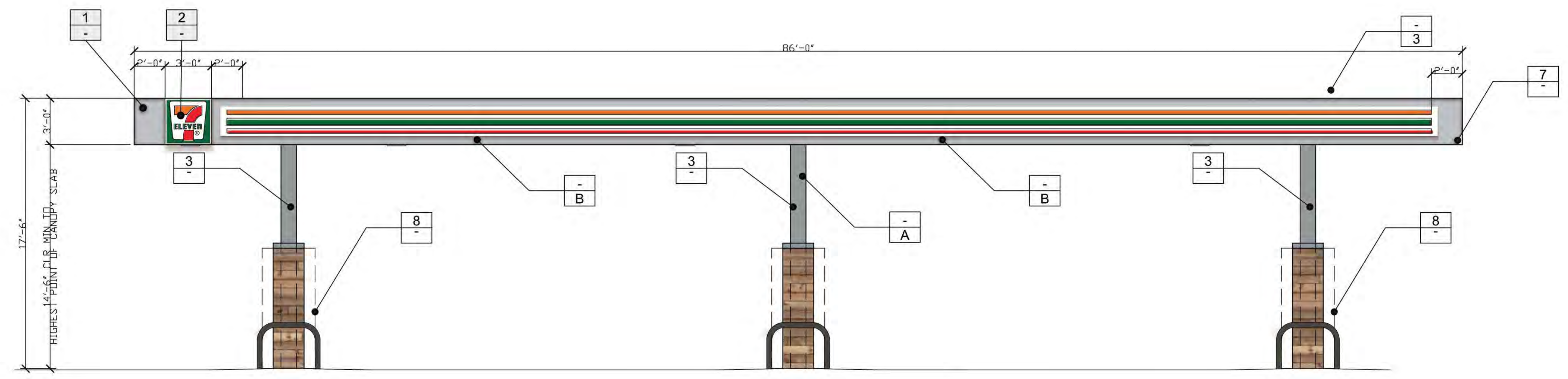
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SHERWIN WILLIAMS PAINT # SW 6254 - LAZY GRAY



FIBERON - WARM SIENA



WEST ELEVATION

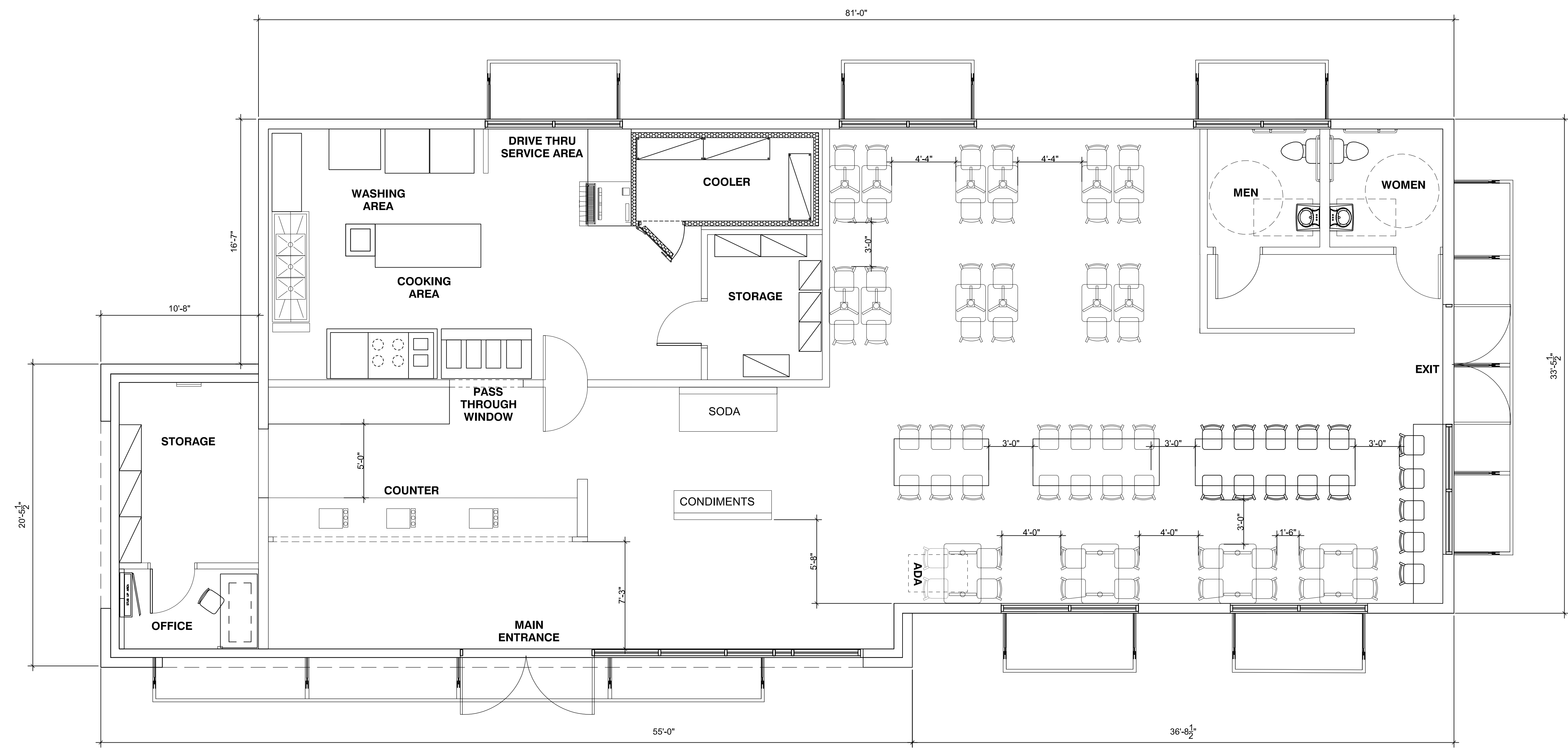
1/4"=1'-0" 2

CADENCE CAPITAL INVESTMENTS LLC
 6400 FIDDLERS GREEN CIR. STE. 1820
 GREEN VILLAGE, CO 80111
 PERRIS BLVD. & DRACAEA AVE.
 MORENO VALLEY, CA 92553
EXTERIOR ELEVATION FUEL-COLOR



Job #:	M - 1011
Scale:	06/07/19
Date:	AS NOTED
Drawn By:	SPD
Checked By:	

ELEV



Rev. #	Date	Description

CADENCE CAPITAL INVESTMENTS LLC
 6400 FIDDLERS GREEN CIR. STE. 1820
 GREEN VILLAGE, CO 80111

PERRIS BLVD. & DRACAENA AVE.
 MORENO VALLEY, CA 92553

INTERIOR FLOOR PLAN (DT RESTAURANT)



Job #: M - 1011
 Scale: 6/07/19
 Date: AS NOTED
 Drawn By: SPD
 Checked By:

Attachment: Project Plans (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling Station and Convenience Store)

INTERIOR FLOOR PLAN (DRIVE THRU RESTAURANT)

1/4"=1'-0"

1

IFP



SOUTH ELEVATION

1/4"=1'-0" 1



NORTH ELEVATION

1/4"=1'-0" 2

KEYED NOTES

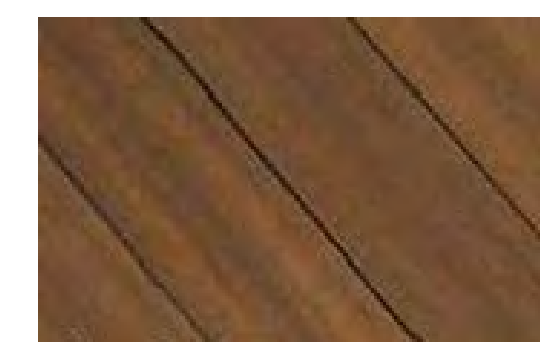
- 1 - EXTERIOR STUCCO TO BE PAINTED
- 2 - PREFINISHED METAL CANOPY
- 3 - EXTERIOR WALL CYLINDER - KICHLER - 9264BA - BRONZE
- 4 - SPANDREL STOREFRONT
- 5 - CITY SCAPES INLINE WALL MOUNTED TRELIS - CITY BROWN
- 6 - EXTERIOR WALL PACK LIGHTS
- 7 - DRIVE THRU PICK UP WINDOW

NOTES

- ALL EXTERIOR DOORS SHALL HAVE A VANDAL RESISTANT LIGHT FIXTURE INSTALLED ABOVE THE DOOR. THE DOORS SHALL BE ILLUMINATED WITH A MINIMUM 1 FC ILLUMINATION AT GROUND LEVEL, EVENLY DISPERSED.
- NUMBER AND/OR LETTERS WILL BE CLEARLY VISIBLE FROM THE STREET.

EXTERIOR COLORS & FINISHES

- CLADDING/SIDING
A FIBERON - WARM SIENA
- SHERWIN WILLIAMS PAINT
B #SW 6256 - SERIOUS GRAY
- SHERWIN WILLIAMS PAINT
C # SW 6254 - LAZY GRAY
- ANODIZED ALUMINUM STOREFRONT SYSTEM
D #40 DARK BRONZE AA-M10C21A44
- SHERWIN WILLIAMS PAINT
E # SW 7076 - CYBER SPACE



FIBERON - WARM SIENA



SHERWIN WILLIAMS PAINT
#SW 6256 - SERIOUS GRAY



SHERWIN WILLIAMS PAINT
#SW 6254 - LAZY GRAY



SHERWIN WILLIAMS PAINT
#SW 7076 - CYBER SPACE

Rev. #	Date	Description

CADENCE CAPITAL INVESTMENTS LLC
6400 FIDDLERS GREEN CIR, STE. 1820
GREEN VILLAGE, CO 80111

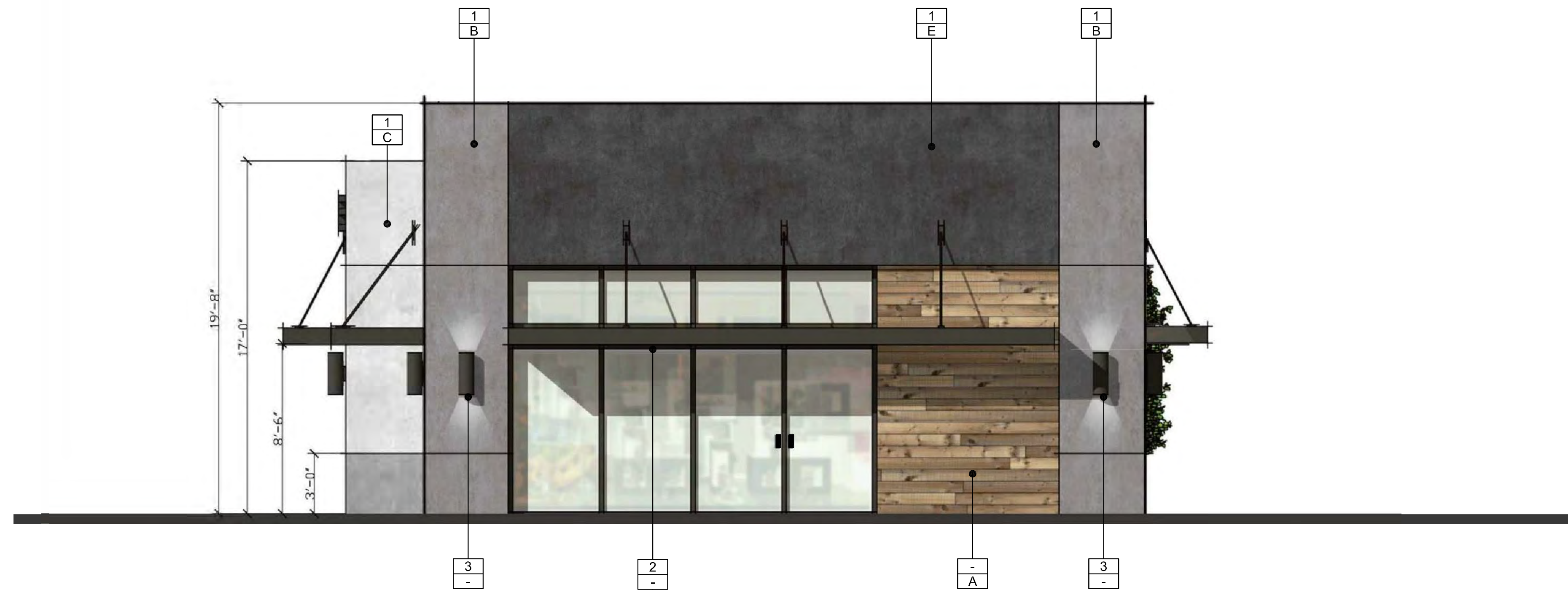
PERRIS BLVD. & DRACAEA AVE.
MORENO VALLEY, CA 92553

EXTERIOR ELEVATION (DT RESTAURANT)



Job #:	M - 1011
Scale:	06/07/19
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Drawn By:	SPD
Page Number:	4

ELEV



EAST ELEVATION

1/4"=1'-0" 1



WEST ELEVATION

1/4"=1'-0" 2

KEYED NOTES

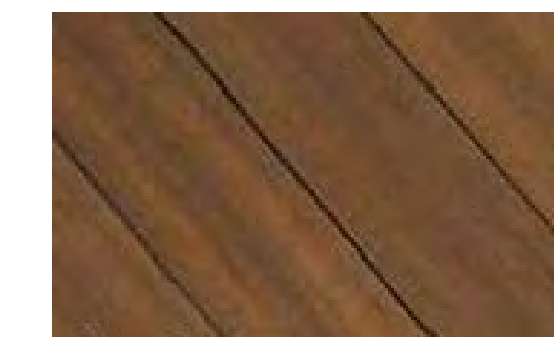
- 1 - EXTERIOR STUCCO TO BE PAINTED
- 2 - PREFINISHED METAL CANOPY
- 3 - EXTERIOR WALL CYLINDER - KICHLER - 9264BA - BRONZE
- 4 - SPANDREL STOREFRONT
- 5 - CITY SCAPES INLINE WALL MOUNTED TRELLIS - CITY BROWN
- 6 - EXTERIOR WALL PACK LIGHTS
- 7 - DRIVE THRU PICK UP WINDOW

NOTES

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- NUMBER AND/OR LETTERS WILL BE CLEARLY VISIBLE FROM THE STREET.

EXTERIOR COLORS & FINISHES

- CLADDING/SIDING
A FIBERON - WARM SIENA
- SHERWIN WILLIAMS PAINT
B #SW 6256 - SERIOUS GRAY
- SHERWIN WILLIAMS PAINT
C # SW 6254 - LAZY GRAY
- ANODIZED ALUMINUM STOREFRONT SYSTEM
D #40 DARK BRONZE AA-M10C21A44
- SHERWIN WILLIAMS PAINT
E # SW 7076 - CYBER SPACE



FIBERON - WARM SIENA



SHERWIN WILLIAMS PAINT
#SW 6256 - SERIOUS GRAY



SHERWIN WILLIAMS PAINT
#SW 6254 - LAZY GRAY



SHERWIN WILLIAMS PAINT
#SW 7076 - CYBER SPACE

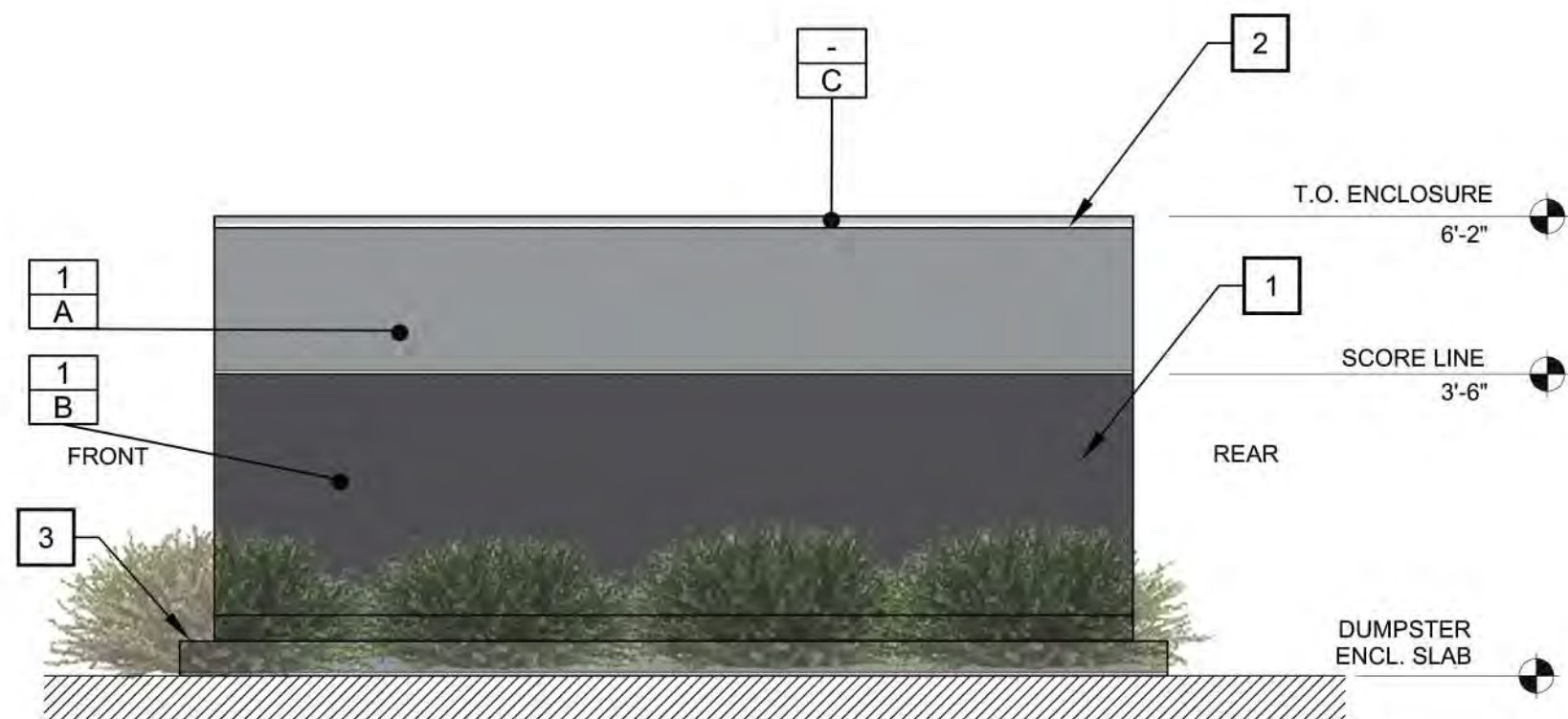
CADENCE CAPITAL INVESTMENTS LLC
6400 FIDDLERS GREEN CIR, STE 1820
GREEN VILLAGE, CO 80111

PERRIS BLVD. & DRACAEA AVE.
MORENO VALLEY, CA 92553

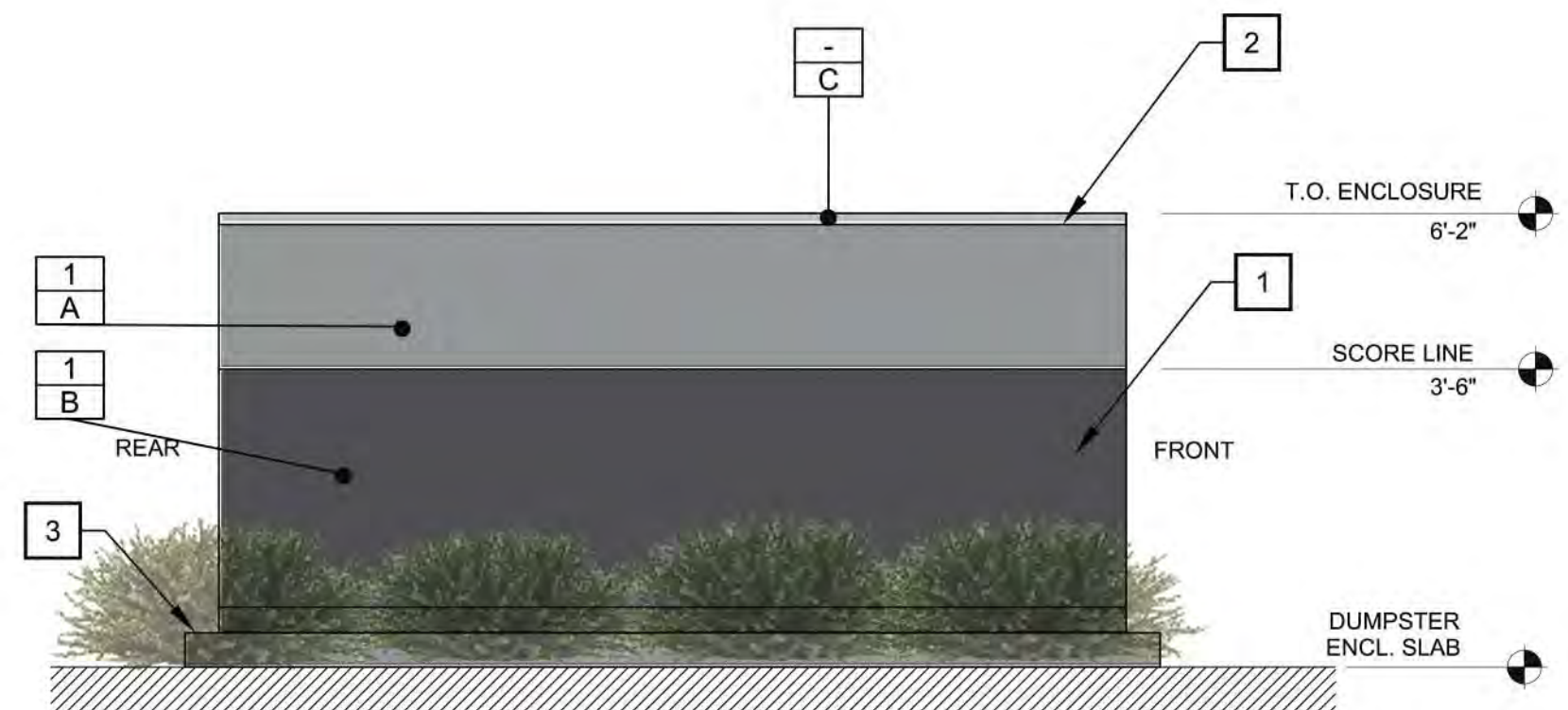


Job #: M - 1011
Scale: 06/07/19
Date: AS NOTED
Drawn By: SPD
Page Number: 5

ELEV



DRIVE THRU TRASH ENCLOSURE EAST 3/8"=1' 5



C STORE TRASH ENCLOSURE WEST 3/8"=1' 5

KEYED NOTES

- 1 8" R.C.M. BLOCK W/ STUCCO PAINT TO MATCH BUILDING.
- 2 2" CONCRETE CAP
- 3 6" PCC FOUNDATION, EXTENDING 6" BEYOND ENCLOSURE WALLS, OVER 90% COMPACTED BASE. WITH A MIN. SLOPE OF 2" TO DRAIN ENCLOSURE.
- 4 18 GA. METAL GATES AND STEEL MINI V-BEAM PANEL. HUNG ON 3" STEEL PIPES.
- 5 4' ACCESS OPENING
- 6 4"x4" STEEL PIPE

EXTERIOR COLORS & FINISHES

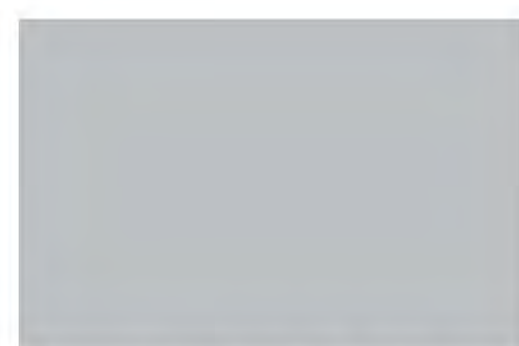
- SHERWIN WILLIAMS PAINT
- A # SW 9162 - AFRICAN GRAY
- SHERWIN WILLIAMS PAINT
- B # SW 7076 - CYBER SPACE
- SHERWIN WILLIAMS PAINT
- C # SW 6254 - LAZY GRAY
- ANODIZED ALUMINUM
- D DARK BRONZE



SHERWIN WILLIAMS PAINT # SW 9162 - AFRICAN GRAY



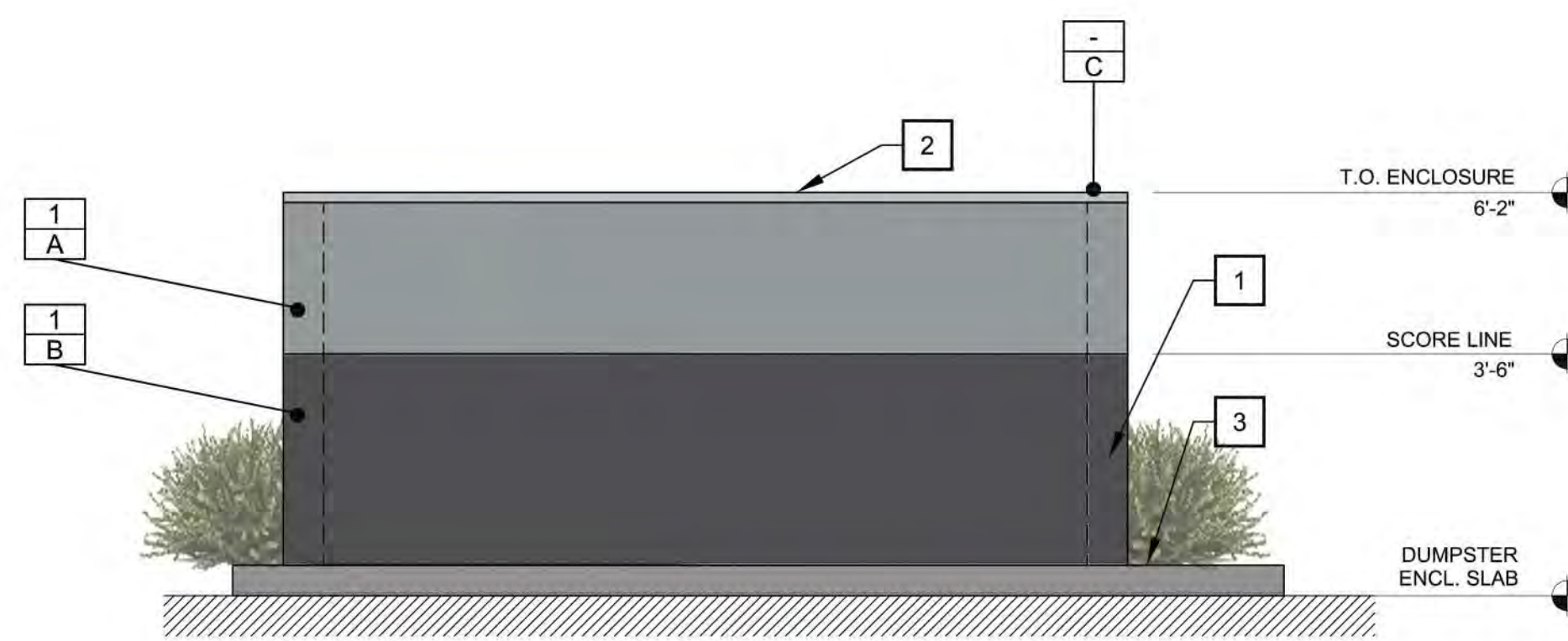
SHERWIN WILLIAMS PAINT # SW 7076 - CYBER SPACE



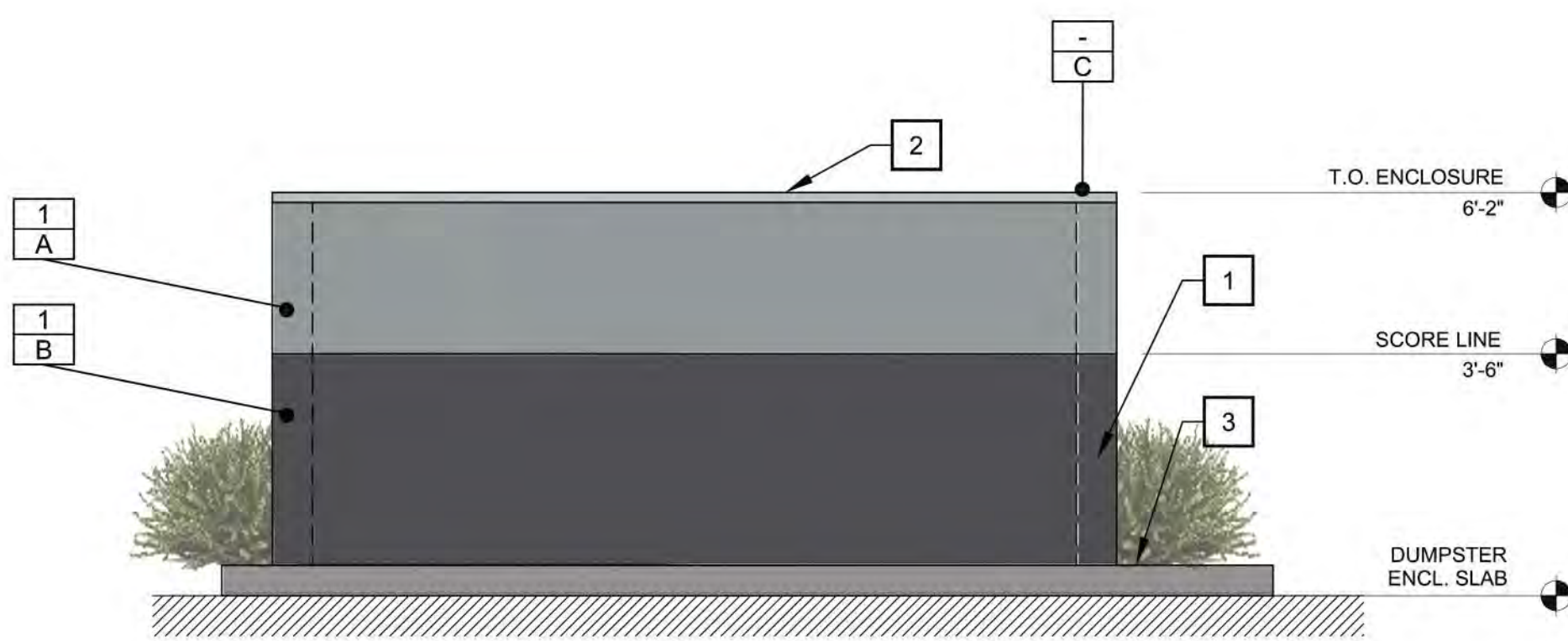
SHERWIN WILLIAMS PAINT # SW 6254 - LAZY GRAY



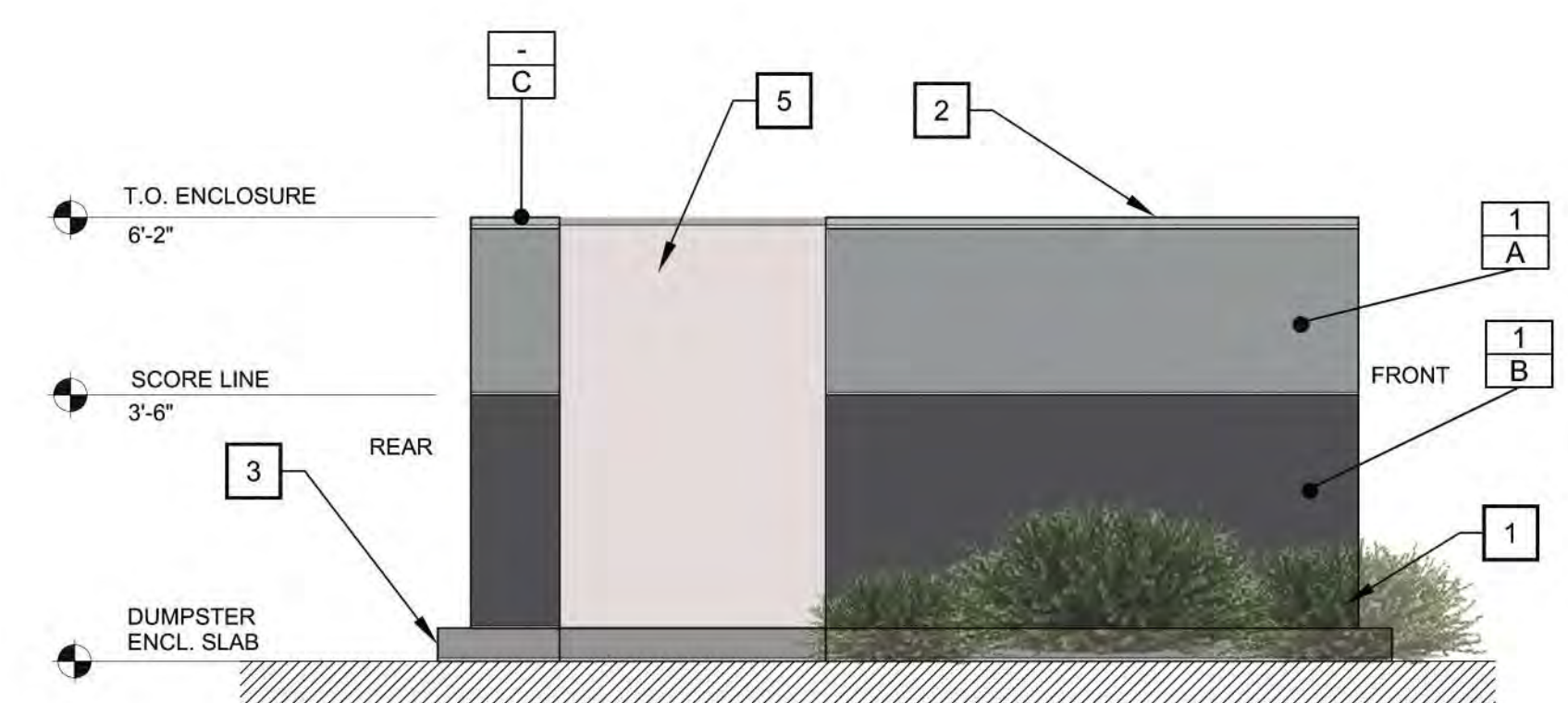
SHERWIN WILLIAMS PAINT # SW 6254 - LAZY GRAY



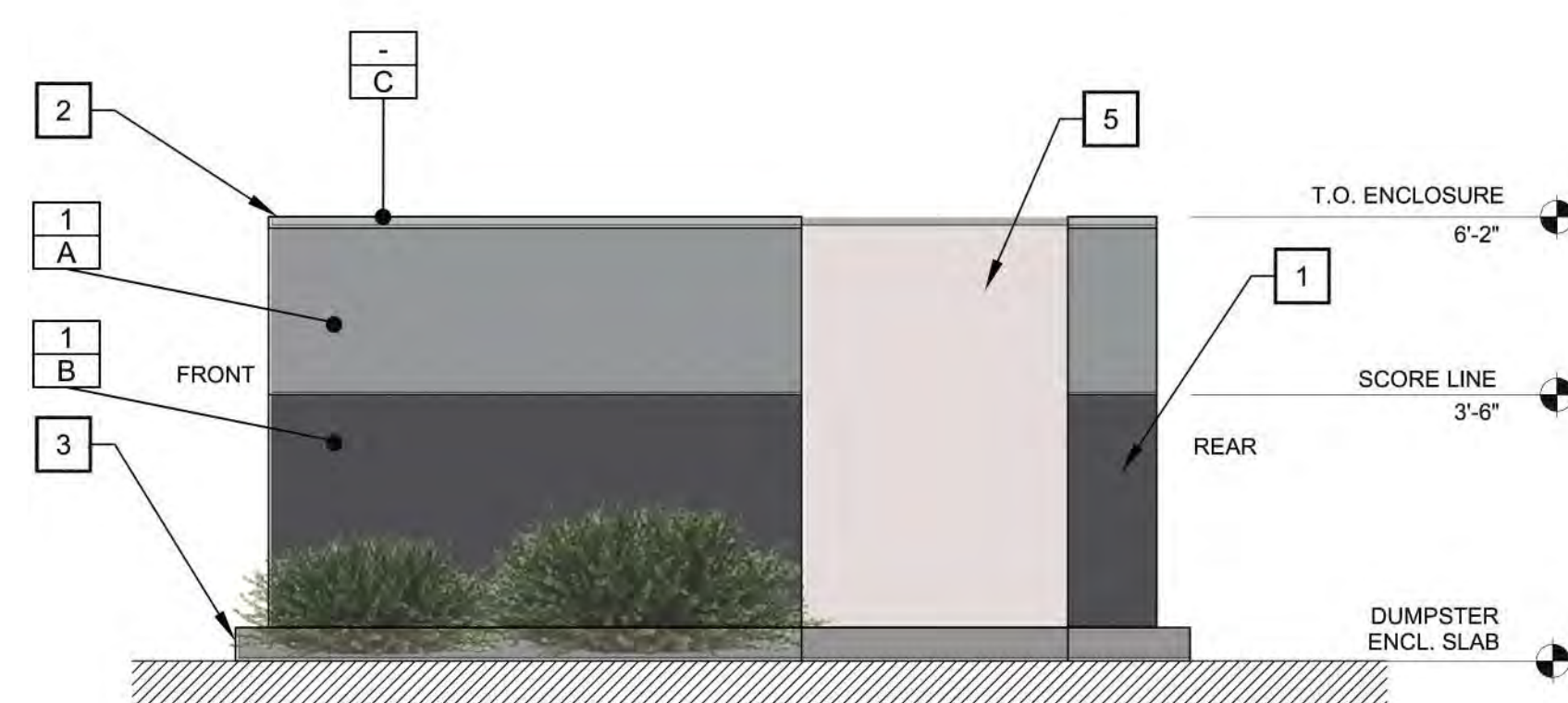
TRASH ENCLOSURE SOUTH 3/8"=1' 3



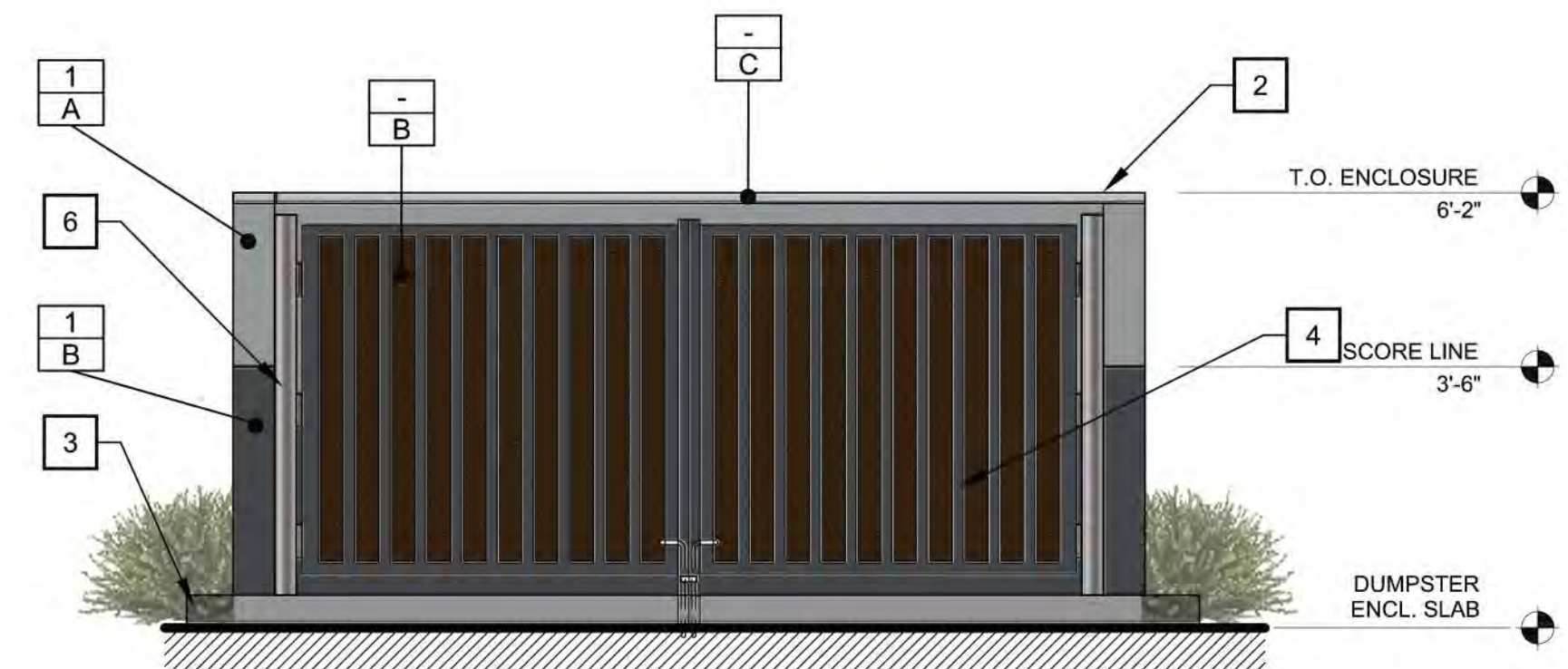
TRASH ENCLOSURE NORTH 3/8"=1' 3



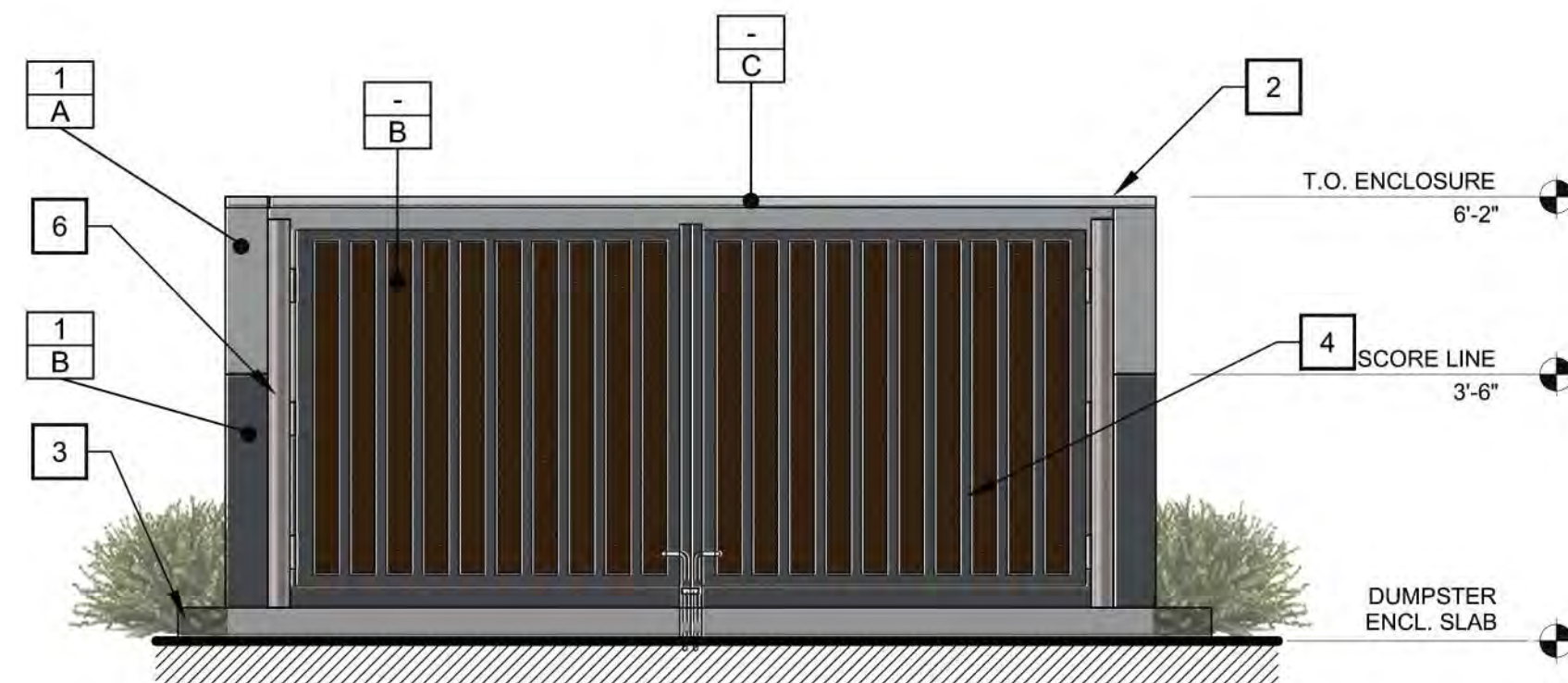
TRASH ENCLOSURE EAST 3/8"=1' 2



TRASH ENCLOSURE WEST 3/8"=1' 2



TRASH ENCLOSURE NORTH 3/8"=1' 1



TRASH ENCLOSURE SOUTH 3/8"=1' 1

Rev. #	Date	Description

CADENCE CAPITAL INVESTMENTS LLC
 6400 FIDDLERS GREEN CIR. STE. 1820
 GREEN VILLAGE, CO 80111
 PERRIS BLVD. & DRACAEVA AVE.
 MORENO VALLEY, CA 92553
 TRASH ENCLOSURE (COLOR)



Job #:	M - 1011
Scale:	08/21/19
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Drawn By:	SPD
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TE-1



PERIMETER WALL (NORTH) 3/8"=1' 2



PERIMETER WALL (EAST) 3/8"=1' 1

KEYED NOTES

- 1 8" R.C.M. BLOCK W/ STUCCO PAINT TO MATCH BUILDING.
- 2 CONCRETE FOOTING

EXTERIOR COLORS & FINISHES

- SHERWIN WILLIAMS PAINT
- A # SW 6254 - LAZY GRAY
- SHERWIN WILLIAMS PAINT
- B #SW 6256 - SERIOUS GRAY
- SHERWIN WILLIAMS PAINT
- C #SW 7076 - CYBER SPACE



SHERWIN WILLIAMS PAINT #SW 6254 - LAZY GRAY



SHERWIN WILLIAMS PAINT #SW 6256 - SERIOUS GRAY



SHERWIN WILLIAMS PAINT #SW 7076 - CYBER SPACE

Rev. #	Date	Description

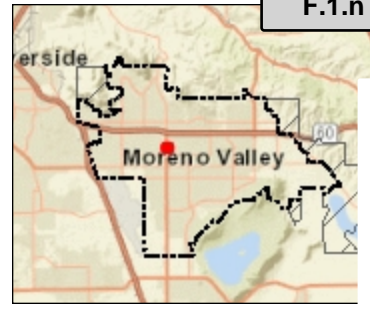
CADENCE CAPITAL INVESTMENTS LLC 6400 FIDDLERS GREEN CIR, STE 1820 GREENWOOD VILLAGE, CO 80111		PERRIS BLVD. & DRACAEA AVE. MORENO VALLEY, CA 92553
PERIMETER WALL		

Job #:	M - 1011
Date:	04/29/20
Scale:	AS NOTED
Drawn By:	SPD

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<p>PW</p>

PEN19-0204 - 0207



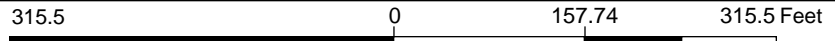
Legend

- Master Plan of Trails**
- Bridge
 - Improved
 - Multiuse
 - Proposed
 - Regional
 - State
- Road Labels**
- Parcels
 - City Boundary
 - Sphere of Influence

Image Source: Nearmap

Notes:

APN(s): 479120042, 043, 027, 029



DISCLAIMER: The information shown on this map was compiled from the City of Moreno Valley GIS and Riverside County GIS. The land base and facility information on this map is for display purposes only and should not be relied upon without independent verification as to its accuracy. Riverside County and City of Moreno Valley will not be held responsible for any claims, losses or damages resulting from the use of this map.

Attachment: Aerial Map (4210 : Conditional Use Permits for a Drive Through Restaurant, Fueling Station



City of Moreno Valley
 Community Development Department
 Planning Division
 City Hall Council Chamber
 14177 Frederick Street
 Moreno Valley, CA 92553

NOTICE OF PUBLIC HEARING (VIA TELECONFERENCE ONLY)

PURSUANT TO COVID-19 GOVERNOR EXECUTIVE ORDER N-29-20

Notice of Teleconferenced Public Hearing before the City Council of the City of Moreno Valley:

DATE & TIME: December 15, 2020 at 6:00 P.M. **VIA TELECONFERENCE ONLY**

COVID-19 TELECONFERENCE INSTRUCTIONS:

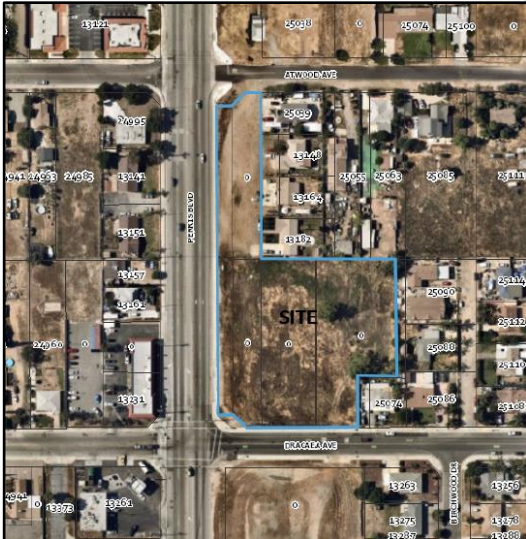
For Teleconference Meeting public participation instructions please see agenda <http://morenovalleyca.iqm2.com/Citizens/default.aspx>

PROJECT LOCATION: East side of Perris Boulevard between Dracaea Avenue Atwood Avenue, APN's 479-120-027, 029, 042, and 043

CASE NUMBER(s): PEN19-0206 General Plan Amendment, PEN19-0207 Change Zone, PEN19-0204 Conditional Use Permit, PEN19-0205 Conditional Use Permit

CASE PLANNER: Julia Descoteaux, Associate Planner (951) 413 3209
juliad@moval.org

<APN>
 <Property Owner>
 <Street Address>
 <City, State, Zip>



Attachment: Mailing Notice (4210 : Conditional Use Permits

NOTICE OF PUBLIC HEARING

F.1.o

PROPOSAL: The applicant is requesting approval of the following entitlements for an 2.04-acre site: 1) a General Plan Amendment* (GPA) amending Figure 2-2 "Land Use Map" of the Moreno Valley General Plan to change the land use designation of the project sit from Residential/ Office (R/O) to Commercial (C); 2) a Change of Zone amending the City of Moreno Valley Zoning Atlas to rezone th project site from Office (O) District and Office Commercial (OC) District to Community Commercial (CC) District; 3) a Conditional Us permit for a vehicle fueling station and convenience store; and 4) a Conditional Use Permit for a drive-through restaurant with outdoc seating.

ENVIRONMENTAL DETERMINATION: The project has been evaluated against criteria set forth in the California Environmental Qualit Act (CEQA) Guidelines Section 15070 and has determined that although the proposed project could have a significant effect on th environment, there will not be a significant effect in this case because mitigation measures have been required of the project that wi reduce potential impacts to a less than significant level. Therefore, a Mitigated Negative Declaration is recommended for the project.

PUBLIC HEARING: All interested parties will be provided an opportunity to submit oral testimony during the teleconferenced Publi Hearing and/or provide written testimony during or prior to the teleconferenced Public Hearing. The application file and relate environmental documents may be inspected by appointment at the Community Development Department at 14177 Frederick Stree Moreno Valley, California by calling (951) 413-3206 during normal business hours (7:30 a.m. to 5:30 p.m., Monday through Thursday

COVID-19 – IMPORTANT NOTICES: Please note that due the COVID-19 pandemic situation, staff will attempt to make reasonabl arrangements to ensure accessibility to inspect the aforementioned records. **In addition, special instructions on how to effectivel participate in the teleconferenced Public Hearing, as approved by Governor Executive Order N-25-20, will be posted a <http://morenovalleyca.igm2.com/Citizens/default.aspx> and will be described in the City Council agenda.**

PLEASE NOTE: The City Council may consider and approve changes to the proposed items under consideration during th teleconferenced Public Hearing.

GOVERNMENT CODE § 65009 NOTICE: If you challenge any of the proposed actions taken by the City Council in court, you may l limited to raising only those issues you or someone else raised during the teleconferenced Public Hearing described in this notice, or written correspondence delivered to the Planning Division of the City of Moreno Valley during or prior to, the teleconferenced Pub Hearing.

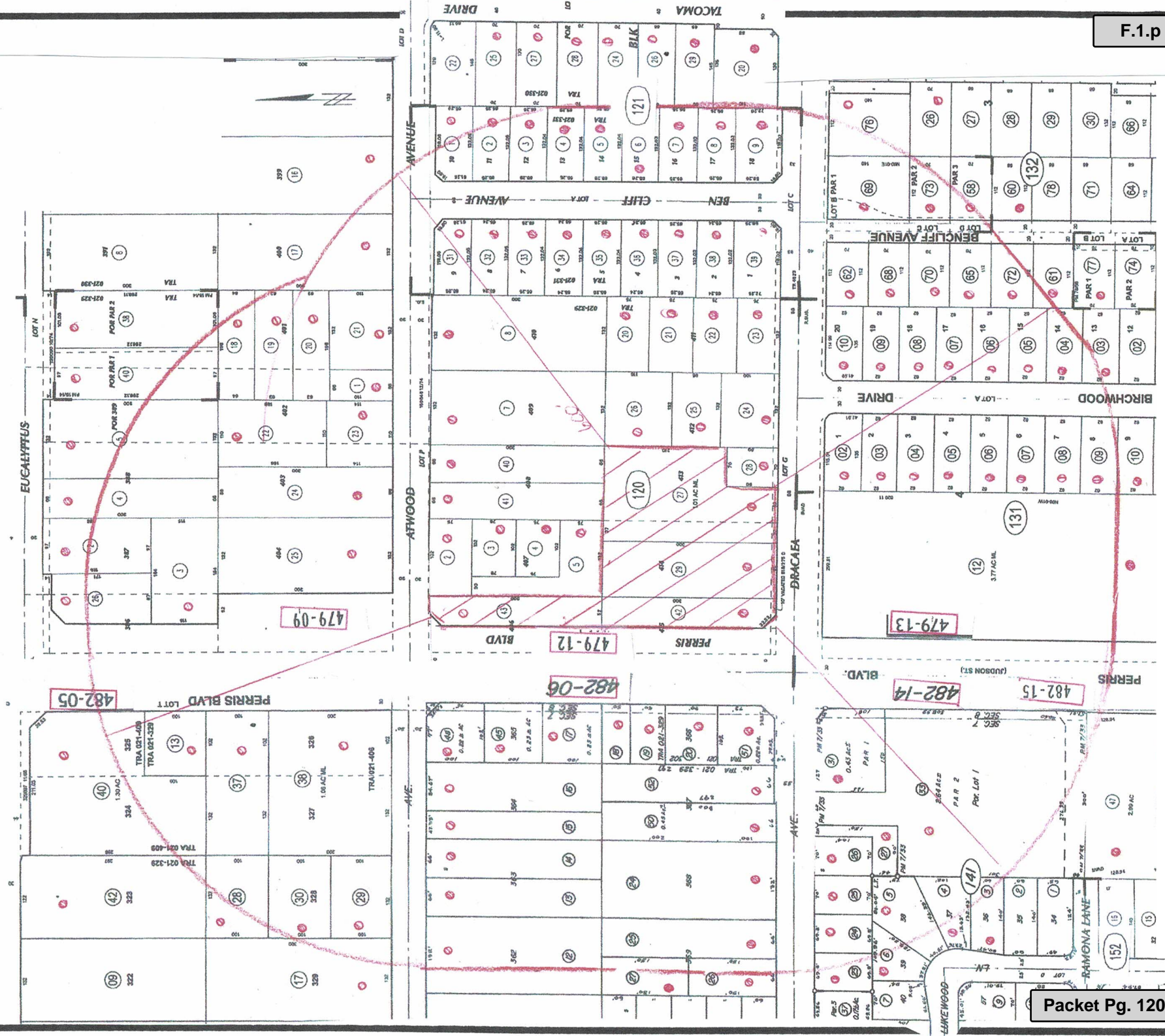
Upon request and in compliance with the Americans with Disabilities Act of 1990, any person with a disability who requires a modification c. accommodation in order to participate in a meeting should direct such request to Guy Pegan, ADA Coordinator, at 951.413.3120 at least 48 hours before the meeting. The 48-hour notification will enable the City to make reasonable arrangements to ensure accessi

600' Radius Map

APNS: 479-120-027, 029, 042, 043

Ownership Listing Service est. 1984

Cathy McDermott, Owner
P.O. Box 890684
Temecula CA 92589
Phone & Fax 951.699.8064



Attachment: 600 Foot Radius Map (4210 : Conditional Use Permits for a Drive Through Restaurant,



Report to City Council

TO: Mayor and City Council

FROM: Manuel A. Mancha, Community Development Director

AGENDA DATE: December 15, 2020

TITLE: MUNICIPAL CODE AMENDMENTS MODIFYING VARIOUS SECTIONS WITHIN TITLE 9 PLANNING AND ZONING: CHAPTER 9.02 PERMITS AND APPROVALS, CHAPTER 9.09 SPECIFIC USE DEVELOPMENT STANDARDS, CHAPTER 9.11 PARKING, PEDESTRIAN AND LOADING REQUIREMENTS, CHAPTER 9.12 SIGN REGULATIONS, AND TITLE 12 VEHICLES AND TRAFFIC, ADDING CHAPTER 12.24 DRIVING UPON ACCESS RAMPS PROHIBITED.

RECOMMENDED ACTION

Recommendations:

1. Introduce and conduct the first reading of Ordinance No. XXX amending Sections 9.02.030, 9.02.080, 9.02.150, 9.02.240, 9.02.280, 9.09.080, 9.11.040, 9.11.070, 9.12.060, and 9.12.070 of Title 9 the City of Moreno Valley Municipal Code to provide greater flexibility and clarity regarding existing requirements and to streamline certain entitlement procedures in order to promote economic development within the City;
2. Introduce and conduct the first reading of Ordinance No. XXX adding Chapter 12.24 of Title 12 of the City of Moreno Valley Municipal Code related to the prohibition of driving on City maintained sidewalks and pedestrian access ramps; and
3. Schedule the second reading and adoption of Ordinance Nos. XXX and XXX for the next regular Council meeting.

SUMMARY

The proposed Municipal Code Amendments revise various sections of Title 9 “Planning and Zoning” and Title 12 “Vehicles and Traffic.” The intent of the revisions to Title 9 are to provide greater flexibility and clarity regarding existing requirements and to streamline certain entitlement procedures in order to promote economic development within the City. These revisions will also make it less costly for the public with respect to processing certain entitlements. The revision to Title 12 will establish regulations making it unlawful to operate a vehicle on a City-maintained sidewalk or pedestrian access ramp without prior authorization.

DISCUSSION

On November 12, 2020, the Planning Commission considered staff-proposed amendments to Title 9 and recommended the City Council adopt the proposed modifications presented, excluding proposed modifications to Section 9.02.020 “Permitted uses” and Section 9.02.090 “Administrative variances.” The ordinance presented herein excludes the sections not recommended by the Planning Commission.

In addition to the modifications to Title 9 a second ordinance is proposed to amend Title 12 “Vehicles and Traffic” to add Chapter 12.24 “Driving upon access ramps prohibited.” This ordinance has been prepared separately as the modifications does not require consideration by the Planning Commission.

PROJECT DESCRIPTION

Project

Recommended Title 9 (Zoning) Modifications

The proposed Municipal Code Amendments recommended by the Planning Commission include revisions to certain provisions of Chapter 9.02 Permits and Approvals, Chapter 9.09 Specific Use Development Standards, Chapter 9.11 Parking, Pedestrian and Loading Requirements, and Chapter 9.12 Sign Regulations. The specifics of each proposed revision is discussed below.

Chapter 9.02 Permits and approvals.

Section 9.02.030 Development review process.

This proposed amendment will allow certain uses within existing commercial centers to be approved by the Community Development Director pursuant to the Minor Development Review Criteria, as currently set forth in Title 9 Planning and Zoning.

Section 9.02.080 Administrative plot plan.

This proposed amendment will streamline the review for minor administrative plot plans. Currently, Title 9 requires that modifications to an administrative plot plan be considered in the same manner as the original plot plan, which can be time consuming and costly, in the context of most administrative plot plan amendments. Under the proposed amendment, the Community Development Director will be permitted to approve an

administrative plot plan via an Administrative/Research Letter, which will be much more expedient and less costly than the current process.

Section 9.02.150 Temporary use permits.

This proposed amendment will allow outdoor provision of services in addition to outdoor merchandise sales which is currently permitted. Additionally, the proposed amendment will provide the Community Development Director with the discretion to approve other uses as temporary uses that are not identified in Table 9.02.150-3, including the temporary use of parking areas within certain developments. Finally, the proposed amendment will provide the Community Development Director with the discretion to extend the number of days a temporary use may be permitted.

Section 9.02.240 Appeals.

The proposed amendment will make the appeal period consistent with the appeal periods required by State law for subdivision maps. This means the current 15 day appeal periods will be reduced to 10 days.

Section 9.02.280 Substantial conformance.

The proposed amendment clarifies that certain expansions may be approved as part of a substantial conformance approval.

Chapter 9.09 Specific use development standards.

Section 9.09.080 Drive-in, drive-through, fast food and take-out restaurants.

The proposed amendment will provide the Planning Commission with the discretion to extend hours of operation for drive-throughs when adjacent to or across an alley from a residential zone or use; such hours are currently limited to 6am-10pm.

Chapter 9.11 Parking, pedestrian and loading requirements.

Section 9.11.040 Off-street parking requirements.

This proposed amendment will allow the Planning Commission to relax certain trailer parking requirements for industrial developments.

Section 9.11.070 Adjustments to off-street parking requirements.

This proposed amendment will authorize consultants, in addition to traffic engineers, to prepare parking studies and remove the maximum parking reduction percentage associated with transportation management plans.

Chapter 9.12 Sign regulations.

Section 9.12.060 Permitted signs.

This proposed amendment will authorize the Community Development Director to approve more than one freestanding sign, currently identified as “Freeway Signs” under Title 9. The Director will also have the discretion to approve the use 100 percent of the areas of such signs for changeable copy displays, provided that the respective sign also identifies at least one of the on-site businesses either as part of the changeable copy display or the physical structure of said sign. Changeable copy signs may be used to advertise establishments, products, services and activities that are sold, produced, and/or furnished on-site or off-site.

Section 9.12.070 Sign program.

This proposed amendment will allow for sign programs to be prepared for nonresidential properties developed with a minimum of two tenant suites. The current rule is that the site has to be a minimum of 15 acres in size.

Recommended Title 12 (Vehicles and Traffic) Modifications

Staff is recommending the City Council consider a modification to Title 12 to add Chapter 12.24 “Driving upon access ramps prohibited.” This proposed amendment codifies that it is unlawful for any person to operate a vehicle on a City-maintained sidewalk or pedestrian access ramp without prior authorization and establishes penalties should a person be convicted of such an offense.

ENVIRONMENTAL

Pursuant to Section 15378 of the California Environmental Quality Act, the proposed amendments to the Municipal Code are procedural in nature and would not involve any change to land use or development standards, thus there is no potential for these Municipal Code changes to either directly or indirectly result in a physical impact on the environment. Therefore, staff has determined that the proposed amendments are not a project under the California Environmental Quality Act.

ALTERNATIVES

1. Approve the proposed Recommended Actions as set forth in this staff report.
Staff recommends this alternative.
2. Do not approve the proposed Recommended Actions as set forth in this staff report.
Staff does not recommend this alternative.

FISCAL IMPACT

There are no fiscal impacts anticipated from the approval and adoption of this Ordinance. However, it is anticipated that the proposed modifications will streamline business and development regulations, which would be expected to have a positive impact on business and development and thus positive fiscal impact to the City.

NOTIFICATION

Notification was provided by a notice published in the newspaper and by publication of the agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Name Sean P. Kelleher
Title Senior Planner

Department Head Approval:
Name Manuel A. Mancha
Title Community Development Director

Concurred By:
Name Michael L. Wolfe
Title Public Works Director / City Engineer

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

Positive Environment. Create a positive environment for the development of Moreno Valley's future.

CITY COUNCIL STRATEGIC PRIORITIES

- 1. Economic Development
- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

ATTACHMENTS

- 1. Ordinance No. XXX - Title 9 Amendments
- 2. Ordinance No. XXX - Title 12 Amendments
- 3. Newspaper Notice

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/02/20 4:54 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/02/20 5:04 PM

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, AMENDING VARIOUS SECTIONS AND TABLES OF CHAPTER 9.02 PERMITS AND APPROVALS, CHAPTER 9.09 SPECIFIC USE DEVELOPMENT STANDARDS, CHAPTER 9.11 PARKING, PEDESTRIAN AND LOADING REQUIREMENTS, AND CHAPTER 9.12 SIGN REGULATIONS OF TITLE 9 PLANNING AND ZONING

WHEREAS, the City of Moreno Valley is a General Law city organized pursuant to Article XI of the California Constitution; and

WHEREAS, pursuant to the authority granted the City by Article XI, Section 7 of the California Constitution, the City has the police power to adopt regulations designed to promote the public convenience or the general prosperity, as well as regulations designed to promote the public health and safety; and

WHEREAS, pursuant to the provisions of Section 9.02.200 (Public Hearing and Notification Procedures) of the Municipal Code and Section 65905 of the Government Code , a public hearing was scheduled for December 15, 2020, and notice thereof was duly published and posted; and

WHEREAS, on December 15, 2020, the public hearing to consider the proposed amendments to various sections and tables of Chapter 9.02 Permits and Approvals, Chapter 9.09 Specific Use Development Standards, Chapter 9.11 Parking, Pedestrian and Loading Requirements, and Chapter 9.12 Sign Regulations of Title 9 Planning And Zoning (“Proposed Amendments”) was duly noticed and conducted by the City Council at which time all interested persons were provided an opportunity to be heard and to present evidence; and

WHEREAS, the intent of the Proposed Amendments is to provide some flexibility and clarity regarding existing development-related requirements, standards and specifications and to streamline certain entitlement procedures in order to promote and expedite continued economic development within the City; and

WHEREAS, the Proposed Amendments will also make it less costly for the public with respect to processing certain entitlement applications; and

WHEREAS, pursuant to Section 15378 of the California Environmental Quality Act, a “Project” means the whole of an action, which has a potential for resulting in either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment, but does not include organizational or administrative activities of government that will not result in direct or indirect physical changes in the environment such as the Proposed Amendments; and

WHEREAS, in the light of the foregoing, the Proposed Amendments does not constitute a “Project” under CEQA.

Attachment: Ordinance No. XXX - Title 9 Amendments [Revision 3] (4208 : PEN20-0189 Municipal Code Streamlining)

NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY DOES ORDAIN AS FOLLOWS:

Section 1. RECITALS

That the above recitals are true and correct and are incorporated herein as though set forth at length herein.

Section 2. AUTHORITY

That this Ordinance is adopted pursuant to the authority granted by Article 11, Section 7 of the Constitution of the State of California and California Government Code Section 37100, and is not intended to be duplicative of state law, nor is it preempted by state legislation.

Section 3. AMENDMENT OF TITLE 9 (PLANNING AND ZONING) OF THE MORENO VALLEY MUNICIPAL

That the following sections and tables of Title 9 (Planning and Zoning) of the Municipal Code is hereby amended in their entirety to read as follows:

Section 9.02.030 Development review process.

The purpose and intent of this section is to identify types of development review process or processing and to establish, by application type, the procedures and requirements for each type of development review process.

A. Minor Development Review Process.

1. Purpose and Intent. The purpose of minor development review is to provide a process for administrative review of development projects which are of limited size and scope. The intent of this process is to ensure that such limited projects comply with all applicable city guidelines, standards and ordinances; are not detrimental to the public health, safety or welfare; and are not materially damaging to surrounding properties or improvements. The minor development review process shall not be construed to include routine maintenance, reconditioning of an existing building, or other land use approvals construed to fall under Section 9.02.170 of the development code. Routine maintenance includes, but is not limited to, painting, stucco work, repairing existing buildings, and parking lot resurfacing/restriping.
2. Authority. The community development director is authorized to approve, approve with reasonable conditions, or disapprove applications for minor development review. In approving an application, the community development director may impose reasonable conditions

to ensure compliance with this title. Conditions may include requirement for open spaces, buffers, walls, fences and screening; requirements for street improvements and dedications, regulation of vehicular ingress, egress and traffic circulation; requirements for installation and maintenance of landscaping and erosion control measures; regulations of signs; regulations of hours of operation; establishment of time limits for performance or completion; and such other conditions as the community development director may deem necessary to make the determinations required by subsection (A)(6) of this section.

3. Minor Development Review Criteria. Unless otherwise specified for major development review pursuant to criteria outlined in subsection (B)(2) of this section, applications which include any of the following criteria shall be subject to the minor development review process and approval by the community development director:
 - a. Parking lot construction, reconstruction or expansion;
 - b. Construction, reconstruction or expansion of outdoor storage areas which are a permitted use in the applicable zone;
 - c. Construction and/or placement of satellite dishes, antennas, roof- or ground-mounted equipment visible from public view, or similar structures or equipment, as determined by the community development director;
 - d. New structures or additions which qualify for a categorical exemption pursuant to the California Environmental Quality Act (CEQA) and City of Moreno Valley "Rules to Implement CEQA";
 - e. Development of any other uses, facilities, or structures for which minor development review is specified elsewhere in this title;
 - f. Signs permitted subject to the provisions of Section 9.12.020 of this title;
 - g. Exterior remodeling of industrial, commercial or multifamily facilities;
 - h. Model homes and changes in production unit sizes within single-family residential tracts, except as otherwise provided in Section 9.08.210 of this title.
 - i. Projects within existing commercial centers when consistent with existing zoning.
4. Applications. An application for a minor development review shall be filed with the community development department in a manner prescribed by the community development director.

5. **Project/Design Review.** If it is determined by the community development director that the site contains unique or unusual characteristics and therefore requires additional design review, the community development director may refer the application to the project review staff committee.
 6. **Required Determinations.** Before granting approval of a minor development review application, the community development director shall make the following determinations:
 - a. That the proposed project is consistent with the goals, objectives, policies and programs of the Moreno Valley general plan and any applicable specific plan;
 - b. That the proposed project, together with the conditions applicable thereto, will not be detrimental to the public health, safety or welfare, or be materially injurious to properties or improvements in the vicinity;
 - c. That the proposed project is in compliance with each of the applicable provisions of this title.
- B. Major Development Review Process.**
1. **Purpose and Intent.** The major development review process is intended to implement general plan policies and other adopted policy and design standards, regulations and guidelines. To achieve quality development that is functionally as well as aesthetically enhancing to the community, and to minimize adverse effects on surrounding properties and the environment, the purposes of major development review are to ensure the following:
 - a. That the location and configuration of structures developed within the city are visually harmonious with their sites and with natural landforms and surrounding sites, structures, and streetscapes;
 - b. That the proposed design produces harmonious transitions in both the scale and character of development between adjacent land uses;
 - c. That site access and circulation thereon is safe and convenient for pedestrians, bicyclists and vehicles;
 - d. That adverse environmental effects are minimized;
 - e. That building site and architectural design is accomplished in an energy-efficient manner;
 - f. That the materials, textures, colors and details of proposed construction are an appropriate expression of the design concept and function, and

are, to the extent feasible, compatible with the adjacent and neighboring structures and functions;

- g. That development proposals do not unnecessarily block scenic views from other buildings or from public ways, or visually dominate their surroundings with respect to mass and scale, to an extent inappropriate to their use;
 - h. That the amount, location and configuration of open space and landscaping conforms to the requirements of this title, provides visually pleasing settings, and is appropriate to the design and function of the structure, site and surrounding area;
 - i. That the design and location of signs and their materials and colors are consistent with the scale and character of the buildings to which they are attached or are located on the same site, and to ensure visual harmony between signs and surrounding developments;
 - j. That excessive and unsightly grading of hillsides does not occur, and to ensure the preservation of the character of natural landforms and existing vegetation where feasible;
 - k. That excellence in architectural design is maintained in order to enhance the visual environmental of the city and to protect the economic value of existing structures;
 - l. That historically significant structures and sites are developed as much as possible in a manner consistent with their historic values;
 - m. That the public health, safety and general welfare are protected;
 - n. That development plans comply with applicable policies, standards, ordinances and design guidelines.
2. Authority.
 - a. Discretionary projects, which are not specifically subject to minor development review pursuant to the provisions of subsection A of this section, shall be subject to the major development review process.
 - b. Unless the city council is designated as the approving body, the planning commission is authorized to approve, conditionally approve or disapprove projects subject to the major development review process.
 3. Conditions of Approval. In approving an application subject to the major development review process, conditions may be imposed to ensure compliance with applicable city regulations. Conditions may include:

- a. Requirements for fences and walls, screening and buffering of adjacent properties, open spaces, and installation and maintenance of landscaping and erosion control measures;
 - b. Requirements for street improvements and dedications, regulation of vehicular ingress and egress, and traffic circulation;
 - c. Regulation of signs;
 - d. Regulation of hours or other characteristics of operation;
 - e. Establishment of time limits for performance or completion; and
 - f. Such other conditions as may be deemed necessary to ensure compatibility with surrounding uses; to preserve the public health, safety and welfare; and to enable the planning commission to make the findings supporting its decision, as required by subsection (B)(5) of this section.
4. Project/Design Review Procedure.
- a. Upon determination that an application is complete, the proposed project shall be forwarded to the project review and community development director for their review and comment.
 - b. In addition to the project review committee and the community development director, projects subject to the provisions of subsection shall also be submitted to such other city committees and boards, as may be required by the provisions of this title, as well as to such other committees and boards as the community development director determines to be appropriate.
 - c. Each committee or board to which a project is submitted pursuant to the provisions of this subsection shall review the case at its first available meeting and supply written recommendations to the community development director.
 - d. In addition to city committees and boards, the community development director shall forward the proposed project to such other public agencies whose operations or areas of responsibility could be affected by the proposed project for their review and comment.
 - e. If after review and consideration pursuant to subsections (B)(4)(a) through (B)(4)(d) of this section, the project is determined to be unacceptable, the community development director shall inform the applicant of identifiable issues, and suggest alternatives to resolve such issues. The applicant shall then be directed to return with revisions

and/or work with staff to resolve issues prior to public hearing or decision by the planning commission.

5. Findings. Following the noticed public hearing pursuant to Section 9.02.200 of this chapter and unless otherwise specified in this chapter, the planning commission shall make the following findings before approving a major development review application:
 - a. That the proposed project is consistent with the general plan;
 - b. That the proposed use is in compliance with each of the applicable provisions of this title;
 - c. That the proposed use, together with the conditions applicable thereto, will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the vicinity of the proposal.
- C. Development Review Index. The following list indicates the review process required for each application type shown.

**Development Review Index
Table 9.02.030-2**

Major*** Development Review	Minor Development Review
General Plan Amendment ** Specific Plan Zone Change Conditional Use Permit Plot Plan Variance * Tentative Tract Map * Tentative Parcel Map * Vesting Map * Reversion to Acreage Surface Mining Permit Development Agreements Extension of Time for Any Major Development Review * Refer to Chapter 9.14, Land Divisions, of this title for further information. ** Refer to Chapter 9.13, Specific Plans, of this title for further information. *** Requires a noticed public hearing pursuant to Section 9.02.200 of this chapter before planning commission and/or city council as established in each section of this title specifying findings for each major development review. **** Subject to the provisions contained in Section 9.08.210 of this title.	* Lot Line Adjustment * Lot Merger * Certificate of Compliance Home Occupation Permit Large Family Day Care Permit Temporary Use Permit Administrative Variance Administrative Plot Plan **** Model Home Complex Extension of Time for Any Major Development Review Projects Sign Permits

- D. Concurrently Filed Applications. An application which is dependent on approval of a change of zone or other enabling application(s) shall be

Attachment: Ordinance No. XXX - Title 9 Amendments [Revision 3] (4208 : PEN20-0189 Municipal Code Streamlining)

processed concurrently with such enabling application(s). Approval authority for such dependent application(s) shall be vested with the body authorized to approve the enabling application(s).

Section 9.02.080 Administrative plot plan.

- A. Purpose and Intent. The purpose of this section is to provide an administrative application under which development proposals listed as subject to the minor development review process may be processed. Unless a specific application for a particular use is identified within this title, the administrative plot plan application may be used to implement the minor development review process requirements.
- B. Authority. The community development director may approve administrative plot plans subject to the requirements, provisions and intentions of this title.
- C. An administrative plot plan may be approved if all of the following findings can be made:
1. The proposed project is consistent with the goals, objectives, policies and programs of the general plan;
 2. The proposed project complies with all applicable zoning and other regulations;
 3. The proposed project will not be detrimental to the public health, safety or welfare or materially injurious to properties or improvements in the vicinity;
 4. The project conforms with any applicable provisions of any city redevelopment plan;
 5. Landscaping Setbacks. In any commercial or industrial district, the community development director may decrease minimum setbacks by not more than ten (10) percent where the proposed setback area is in character with the surrounding neighborhood, and where such decrease will not unreasonably affect contiguous sites.
- D. Revisions or Modifications. Major revisions or modifications to an administrative plot plan shall be processed in the same manner as the original administrative plot plan. Minor revisions or modifications approvable by the community development director may be processed with an Administrative/Research Letter.
- E. Conditions of Approval. In approving an administrative plot plan, the approving authority shall impose conditions of approval for the project

regarding on-site improvements, off-site improvements, the manner in which the site is used and any other conditions as may be deemed necessary to protect the public health, safety and welfare and ensure that the project will be developed in accordance with the purpose and intent of this title.

Section 9.02.150 Temporary use permits.

- A. Purpose and Intent. The temporary use permit is intended to allow for short-term activities on privately owned property with appropriate regulations so that such activities will be compatible with the surrounding areas.
- B. Authority.
1. Authority for approval of temporary use permits shall be vested with the community development director through the minor development review process.
 2. A permit shall not be required for events that occur in theaters, meeting halls, or other permanent public assembly facilities. Temporary uses may be subject to additional permits, other city department approvals, licenses, and inspections, as required by any applicable laws or regulations.
- C. Permitted Temporary Uses. The following table identifies those uses which may be permitted subject to the issuance of a temporary use permit:

Temporary Uses Table 9.02.150-3

Permitted Temporary Uses (With a Temporary Use Permit)	Locations	Max. No. Days per Calendar Year ¹
Commercial and noncommercial Christmas tree sales, and incidental sales of Christmas lights, tree stands and decorations, but excluding gift items	All zones	30
Mobile health clinic	All commercial and industrial districts	14
Merchandise sale or provision of services - outdoors or in mobile or temporary enclosures - in conjunction with established businesses (see subsection D of this section)	All commercial districts	36 days per shopping or commercial center
Merchandise sale - outdoors or in mobile or temporary enclosures, sponsored by and on the premises of a bank, savings and loan association or credit union of merchandise typically financed by that institution in the normal course of its lending business (see subsection D of this section)	Banks, savings and loan associations and credit unions	12 days per shopping or commercial center
Real estate offices on the site of a proposed subdivision	All districts	n/a
Construction and security personnel offices on active construction sites	All districts	n/a
Temporary construction yards not located on active construction sites	All districts	n/a
Tent meetings	All districts	30
Commercial carnival, concert, exhibit, festival or similar event outdoors or in temporary enclosures	All commercial and industrial districts	14
Noncommercial carnival, fair, concert, exhibit, festival or similar; outdoors or in temporary enclosures	All districts	14
Pumpkin sales lots	All zones	30
Seasonal produce stands	All zones	120
Any other use deemed appropriate by the community development director.	All districts.	n/a
¹ The community development director may extend the maximum number of days per calendar year based on special circumstances.		

- D. Special Requirement for Merchandise Sales. The following shall apply to merchandise sales or provision of services, as delineated in the Temporary Uses Table 9.02.150-3:
1. “Merchandise sale in conjunction with established businesses” means an event managed and operated by the owner or operator of a permanently established business, on the premises of that business (or upon immediately adjacent common area of a shopping or commercial center in which the business is located), conducting the sale, lease, rental or other transfer of control of merchandise which is inventory of the established business or the provision of services and which is of the same or similar kind and quality normally offered as immediately available to the public by that business at that business site. Sales operated by outside vendors shall not be permitted under this provision. An outdoor sale of merchandise or provision of services on the premises of a business that ordinarily only displays merchandise and/or conducts sales or lease transactions for customer delivery or provides services at another site or at another time shall not be permitted under this provision. This subsection shall not apply to “merchandise sales on the premises of a bank, [etc.],” as listed in the Temporary Uses Table.
 2. Merchandise sales or provision of services sponsored and sanctioned by the Master Property Association or Property Manager for Shopping Centers, which are twenty (20) acres or larger and located within the Community Commercial (CC) district, shall be a maximum of thirty-six (36) days per calendar year.
 3. Food and Entertainment. Upon approval of the community development director and in compliance with all other laws and regulations, food or entertainment may be sold or provided by two or fewer secondary vendors incidental to the merchandise sale or provision of services, such as a hot dog cart, snow cone or popcorn wagon, pony ride, inflatable jumper, etc., provided that such uses occupy not more than twenty-five (25) percent of the total space occupied by the sale or four hundred (400) square feet, whichever is less.
 4. No secondary vendors, incidental to the merchandise sale or provisions of services provided, shall conduct business without a buffer of at least two hundred (200) feet from any established business on-site that sells similar products unless written consent from the established business(es), for a lesser buffer, has been presented to the city of Moreno Valley.
 5. Merchandise sales (including display areas) or provision of services shall not occupy landscaped areas or unimproved surfaces.

6. Merchandise sales or provision of services taking place upon parking surfaces shall be confined to improved parking surfaces. Merchandise sales or provision of services shall not occupy more than twenty (20) percent of the legally required improved parking spaces for the business conducting the sale or services, unless approved by the community development director. No merchandise sale or provision of services shall occupy parking spaces legally required for another business, including other businesses located in the same shopping or commercial center, or parking spaces otherwise required for the shopping or commercial center in which the business is located. Merchandise sales or provision of services may occupy on-site improved parking spaces that are not so legally required, subject to all other provisions of this chapter. No merchandise sale or provision of services shall occupy or encumber more than one hundred twenty-five (125) parking spaces.
 7. Merchandise sales or provision of services shall not negatively affect the vehicular and pedestrian circulation patterns of the subject site or nearby streets, or the usability of the remaining parking spaces for the site, and shall allow unabated access for public safety personnel and vehicles.
 8. Setup and Takedown. One day of setup before a merchandise sale and one day of takedown/cleanup after the sale shall not be counted against the total number of permitted sale days. No sales activity shall occur on such setup or takedown/cleanup days.
 9. No Use of Public Right-of-Way. Any and all personal properties or merchandise or services shall be solely contained on private property and shall not extend into the public right-of-way.
 10. Cleanup. The permittee shall be responsible for cleanup of the site within twenty-four (24) hours of termination of the event.
- E. Application Requirements. Applications for temporary use permits shall be filed a minimum of thirty (30) days prior to the date of the proposed event with the community development department. Applications must be accompanied by all appropriate fees and deposits, as determined by resolution of the city council. The application shall include, at a minimum, the following information:
1. A site plan identifying the area to be occupied, including the location of merchandise or provision of services, proposed signage, temporary structure(s) (e.g., tents, shade structures, vending stands, etc.) and all pedestrian areas, parking lot areas and/or drive aisles proposed to be closed, blocked, obstructed and/or barricaded and their proximity to major circulation aisles, public rights-of-way and buildings. The site plan shall provide proof of compliance with all requirements of applicable laws, ordinances and regulations;

2. Written authorization from the property owner or the property owner's duly authorized agent;
 3. Written operational/environmental statement identifying the proposed dates, defining the nature of the event or use and containing such other information as the community development director or designee shall consider necessary to determine the expected effects and impacts of the event or use;
 4. Proof of all applicable city business licenses.
- F. Criteria for Permit Issuance. The community development director shall consider the following criteria in rendering a decision relative to a temporary use permit application:
1. The operation of the requested use at the location proposed and within the time period specified will not jeopardize, endanger, or otherwise constitute a menace to the public health, safety or general welfare;
 2. The proposed site is adequate in size and shape to accommodate the temporary use without material detriment to the use and enjoyment of other properties located adjacent to and in the vicinity of the site;
 3. The proposed site is adequately served by streets or highways having sufficient width and improvements to accommodate the kind and quantity of traffic that the temporary use will or could reasonably be expected to generate;
 4. Adequate temporary parking to accommodate vehicular traffic to be generated by the use will be available either on-site or at alternate locations acceptable to the community development director and the city traffic engineer;
 5. The property shall be posted at least ten (10) days prior to issuance of a permit for a temporary outdoor event anticipated to accommodate two thousand five hundred (2,500) or more persons on a single site;
 6. Neither the applicant nor any person actually managing or operating the temporary use shall have been in violation of any prior temporary use permit within twelve (12) months of the date of application.
- G. Conditions of Approval. In approving an application for a temporary use permit, the community development director may impose conditions that are deemed necessary to ensure that the permit will be applied in accordance with the criteria outlined above. These conditions may involve any factors affecting the operation of the temporary use or event, and may include, but are not limited to:

1. Provision of temporary parking facilities, including vehicular ingress and egress;
2. Regulation of nuisance factors such as, but not limited to, prevention of glare or direct illumination of adjacent properties, noise, vibration, smoke, dust, dirt, odors, gases and heat;
3. Regulation of temporary buildings, structures and facilities, including placement, height and size, location of equipment and open spaces, including buffer areas and other yards;
4. Provision of sanitary and medical facilities;
5. Provision of solid waste collection and disposal;
6. Provision of security and safety measures, including deputized officers if necessary, as determined by the chief of police, with all costs borne by the applicant for security and police services;
7. Regulation of signs, including without limitation placement of any signage outside of the city limits;
8. Regulation of operating hours and days, including limitation of the duration of the temporary use to a shorter time period than that requested;
9. Submission of a performance bond or other surety device to assure that any temporary facilities or structures used for the proposed temporary use will be removed from the site within a reasonable time following the event and that the property will be restored to its former condition;
10. Submission of a site plan indicating any information required by this section; all events, structures, equipment, merchandise and activities shall be confined to the area designated on the approved site plan for that event;
11. A requirement that the approval of the requested temporary use permit is contingent upon compliance with applicable provisions of other ordinances;
12. All noncity sponsored groups and individuals who wish to utilize city of Moreno Valley, Moreno Valley community service district (MVCSD) or redevelopment agency (RDA) facilities shall be subject to the following requirements:
 - a. Noncity sponsored groups or individuals must complete an application which includes an indemnification and hold harmless clause protecting the city and MVCSD or RDA from the lessee's activities,

- b. Noncity sponsored groups or individuals must provide the city with evidence of adequate general liability insurance by either:
 - i. Providing the city with an original certificate of liability insurance and endorsement binder naming the city of Moreno Valley, MVCSD or RDA, where appropriate, as an additional insured,
 - ii. Participating in the city's special events insurance program if available and approved by the city manager;
- 13. Other conditions which will ensure the operation of the proposed temporary use in an orderly and efficient manner and in accordance with the intent and purpose of this section;
- 14. Timely payment of all business license fees, gross receipts taxes and sales and use taxes attributable to the temporary use.

Section 9.02.240 Appeals.

A. Appeal of Action.

- 1. Any affected person may appeal a decision of the community development director to the planning commission where the community development director's decision would otherwise be final.
- 2. Any affected person may appeal a decision of the planning commission to the city council.

B. Filing of Appeals. Appeals shall be addressed to the appellate body in a letter submitted to the community development director and shall be accompanied by the required fee. The appellant shall state the specific reasons for the appeal. Unless otherwise required by law, including as specified in Sections 9.02.040 and 9.14.050 of this title, appeals shall be filed with the community development director within ten (10) consecutive calendar days following the date of action for which an appeal is made, or, if no public hearing was held for the taking of such action, then within ten (10) consecutive calendar days following the date of deposit of notice of such action in the United States mail to the applicant, or any person who has requested notice.

C. Appeal Hearings. Public notice of an appeal hearing shall be given, as required by law.

D. Effective Date of Appealed Actions. Except as otherwise provided for in this title, an action which has been appealed shall not become effective until a final determination is made by the appellate body.

Section 9.02.280 Substantial conformance.

- A. Purpose and Intent. The substantial conformance is intended to address minor modifications to approved plot plans, conditional use permits and similar previously approved projects. The substantial conformance application is not intended to authorize a deviation from any applicable development standard specified in this title.
- B. Authority. Authority to approve a substantial conformance shall be vested in the community development director. A substantial conformance may be approved subject to further conditions of approval to ensure continued preservation of public health, safety and welfare.
- C. Review Requirements. A substantial conformance application shall be subject to minor development review procedures. A substantial conformance application may be filed in lieu of an applicable major development review application, provided that the proposal complies with the limitations described below:
1. That the proposal is not inconsistent with the expressed intent of the original project approval;
 2. That the proposal qualifies as a categorical exemption under the California Environmental Quality Act and/or the proposal is consistent with the environmental determination for the original project and where no further environmental determination is necessary; and
 3. That the proposed modifications do not have the potential to adversely affect surrounding land uses or improvements.
- D. Applicability. A substantial conformance approval may include expansions of approved projects, where the proposal meets zoning code requirements.

Section 9.09.080 Drive-in, drive-through, fast food and take-out restaurants.

- A. Purpose and Intent. The purpose of this section is to ensure that drive-in, drive-through, fast food and take-out restaurants do not result in adverse impacts on surrounding neighborhoods by reason of customer and employee parking demand, traffic generation, noise, light, litter, or cumulative impact of such demands in one area, consistent with the goals, objectives and policies of the general plan.
- B. Applicability. Drive-in, drive-through, fast food, or take-out restaurants may be permitted subject to the standards of the underlying district and special conditions listed below. The provisions of this section shall apply

to all drive-in, drive-through, fast food and take-out restaurants constructed or the use of which commenced after the effective date of this title and to any expansion of more than twenty (20) percent of the gross floor area or increase of more than twenty-five (25) percent of the number of seats in any such restaurant in use prior to the effective date of this title. Floor area added for the purpose of compliance with state or local health laws or access requirements of the disabled shall not be included in floor area calculations for purposes of determining applicability of this section.

- C. Minimum Development Standards. The following minimum development standards shall apply to all drive-in, drive-through, fast food and take-out restaurants.
1. Hours of Operation. When located on a site adjacent to, or separated by an alley from any residentially zoned property, a drive-in, drive-through, fast food or take-out restaurant shall not open prior to six a.m., nor remain open after ten p.m. unless extended hours are specifically approved by the Planning Commission.
 2. Driveways. Drive-in and drive-through restaurants sites shall have two points of ingress and/or egress.
 3. Queuing. Drive-up and drive-through restaurants shall have a capacity for queuing a minimum of eight vehicles awaiting service. Queuing area shall not interfere with on- or off-site circulation patterns and shall be reviewed and approved by the city traffic engineer prior to issuance of a building permit.
 4. Parking. A parking and vehicular circulation plan encompassing adjoining streets and alleys shall be submitted for review and approval by the city traffic engineer prior to approval of a conditional use permit.
 5. Trash Receptacle. A minimum of one outdoor trash receptacle shall be provided on-site. At least one additional on-site outdoor trash receptacle shall be provided for every ten (10) required parking spaces.
 6. Noise. Any drive-up or drive-through speaker system shall not be detectable above daytime ambient noise levels beyond the property boundaries. The system shall be designed to compensate for ambient noise levels in the immediate area, and shall not be located within one hundred (100) feet of any residential district or any property used for residential uses.

9.11.040 Off-street parking requirements.

A. Automobile Parking Requirements. Off-street automobile parking shall be provided in accordance with the requirements of this chapter. The following tables set forth the required off-street parking requirements and certain notations for various residential, commercial, industrial, public and quasi-public uses. Parking provided above required off-street must be constructed with permeable surfaces and/or enhanced landscaped retention and absorption areas:

Attachment: Ordinance No. XXX - Title 9 Amendments [Revision 3] (4208 : PEN20-0189 Municipal Code Streamlining)

**Table 9.11.040A-12
Off-Street Parking Requirements**

Residential Uses	Requirement	Covered Parking	Notes
Single-family	2/unit	Within an enclosed garage	
Accessory dwelling unit	1/bedroom		The accessory dwelling unit shall provide a minimum of one parking space per bedroom in addition to the parking required for the main dwelling, except as exempted by state law (refer to Section 9.09.130 Accessory dwelling units). Spaces may be provided as uncovered and/or tandem parking on a driveway.
Duplex	2/unit	Within an enclosed garage	
3 or more units: Studio 1 bedroom 2 bedrooms 3+ bedrooms	1.25/unit 1.5/unit 2.0/unit 2.5/unit	1 covered/unit 1 covered/unit 1 covered/unit 2 covered/unit	Guest parking is required for all units at 0.25 spaces/unit. Guest parking is included in the minimum required parking standard.
Senior housing: Studio 1 bedroom + bedrooms	1.0/unit 1.25/unit 1.5/unit	1 covered/unit 1 covered/unit 1 covered/unit	Guest parking is required for all units at 0.25 spaces/unit. Guest parking is included in the minimum required parking standard. Alternate parking requirements may be permitted subject to approval of a parking study pursuant to Section 9.11.070(A) of this chapter.
Mobile home parks	2.5/unit		Tandem spaces may be used to meet resident parking requirements.
Residential care homes	Parking requirements shall be determined by the community development director subject to an approved parking study.		
Live-work units (residential component)	2/unit	2 covered/unit	Guest parking is required for all units at 0.25 spaces/unit. Guest parking is NOT included in the minimum required parking standard and can be shared with the business aspect of the “live-work” parking standard.

<p>Residential component of mixed-use project</p>	<p>See multiple-family requirements in this table</p>	<p>See multiple-family requirements in this table</p>	<p>Guest parking is required for all units at 0.25 spaces/unit. Guest parking is included in the minimum required parking standard and may be shared with the nonresidential component. Alternate parking requirements may be permitted subject to approval of a parking study pursuant to Section 9.11.070(A) of this chapter.</p>
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**Table 9.11.040B-12
Off-Street Parking Requirements**

Commercial Uses	Requirement	Notes
General retail (unless specified elsewhere)	1/225 sq. ft. of gross floor area	
Automobile, boat, mobile home, or trailer sales, retail nurseries, or other similar outdoor commercial activities	1/2,000 sq. ft. of display area	1. Display area shall include all office, service and repair, or other related activities and areas that are accessible to the public. 2. No required off-street parking spaces shall be used for display, sales, service or repair of vehicles.
Automobile service stations, repair and service facilities	2 spaces + 4/service bay for 4 or less bays and 2/service bay for 5 or more bays	Any related retail activities shall be subject to the general retail parking standards (mini-markets, tire sales, and the like).
Automobile washing and waxing establishments: Self-serve Automated	2 spaces + 2/washing stall 10 + 1 per 2 employees	
Business and professional offices	1/250 sq. ft. of gross floor area	
Banks, savings and loans and medical/dental offices	1/225 sq. ft. of gross floor area	
Day care center	1/employee + 1/500 sq. ft. of gross floor area	Special design requirements shall apply for bus loading or parent drop-off points.
Eating and drinking establishments	1/100 sq. ft. of gross floor area up to 6,000 sq. ft. 1/75 sq. ft. of gross floor area over 6,000 sq. ft.	A minimum of 10 spaces required for stand-alone use. No additional parking required if outdoor dining area comprises no more than 15% of the interior gross floor area of the primary food service use; if outdoor dining area is over 15%, 1 space for every 60 sq. ft. or 1 space for every 3 seats, whichever is greater.
Eating and drinking establishments within shopping centers of	1/225 sq. ft. of gross floor area up to 15% of the shopping center gross building square footage	

Commercial Uses	Requirement	Notes
25,000 sq. ft. of building area or greater		
Hotel/motel	1/guest room	For facilities with 100+ parking spaces, two 12'x36' through stalls for RV parking are required. These stalls may be counted as 4 auto parking stalls.
Kennels	2 spaces/1,000 sq. ft.	2 spaces/1,000 sq. ft. of indoor animal enclosure.
Veterinary hospital and clinic	1/200 sq. ft. of gross floor area	
Mortuaries	1/4 seats + funeral procession queue capacity for 5 cars	
Nail salons	1 space/2 work stations	
Schools, private: Business and trade College Elementary/junior high Senior high	10 spaces + 24/classroom 10 spaces + 30/classroom 10 spaces + 2/classroom 10 spaces + 10/classroom	
Storage lots and mini-warehouses	1/100 storage spaces and 2/caretaker residence	2 spaces minimum.
Medical and health services: Convalescent and nursing homes Homeless shelter Hospitals Residential care facilities	1/3 beds 1/4 beds 1/bed see Residential Uses, Section 9.11.040 Table 9.11.040A-12	
Recreation: Arcades Bowling and billiards Commercial stables Golf course Golf driving range Golf, miniature Health club Parks—public and private	1/75 sq. ft. of gross floor area 5/alley + 2/billiard table 1/5 horse capacity for boarding on-site 6/hole 1/tee 3/hole 1/100 sq. ft. of gross floor area To be determined by the approval authority based	

Commercial Uses	Requirement	Notes
Skating rink Tennis, handball and racquetball facilities	upon an approved parking study. 1/100 sq. ft. of gross floor area 3/court	
Theaters	1/3 fixed seats	

**Table 9.11.040C-12
Off-Street Parking Requirements**

Industrial Uses	Requirement	Notes
Manufacturing	1/500 sq. ft. of gross floor area	Trailer parking: parking stalls for trailers shall be provided at a ratio of 1 stall per truck loading dock door unless otherwise approved by the Planning Commission. This is in addition to the loading parking stall already provided at the dock door.”
Research and development	1/350 sq. ft. of gross floor area	
Warehouse and distribution	1/1,000 sq. ft. of gross floor area for the first 20,000 sq. ft.; 1/ea. 2,000 sq. ft. of gross floor area for the second 20,000 sq. ft.; 1/ea. 4,000 sq. ft. of gross floor area for areas in excess of the initial 40,000 sq. ft.	

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**Table 9.11.040D-12
Off-Street Parking Requirements**

Public and Quasi-Public Uses	Requirement	Notes
Libraries, museums and galleries	1/300 sq. ft. of gross floor area	
Public utility facilities without an office on-site	2/employee on the largest shift + 1/company vehicle	A minimum of 2 spaces shall be required.
Auditorium, places of public assembly and places of worship	1/3 fixed seats or 1/35 sq. ft. of gross floor area of the assembly area or 1 space for every 4.5 lineal feet of benches/pews, whichever is greater	
Government offices	To be determined by a parking study approved by the community development director	

B. Schedule of Accessible Parking Requirements. The following requirements for accessible parking are intended to be consistent with the state requirements. Any conflicting provisions or future changes in state or federal requirements shall preempt the standards for provision of accessible parking spaces contained in this title.

1. Accessible parking for residential uses shall be provided at a rate of one space for each dwelling unit that is designed for accessibility and occupancy by the disabled, unless an adjustment is allowed, based on a parking study approved by the community development director.
2. Accessible parking for outpatient units and facilities providing medical care and other services for persons with mobility impairments shall be provided at a rate of ten (10) percent of the total number of parking spaces provided serving such outpatient unit or facility. Accessible parking for units and facilities that specialize in treatment or services for persons with mobility impairments shall be provided at a rate of twenty (20) percent of the total number of parking spaces provided serving each such unit or facility.
3. Accessible parking spaces for other uses shall be provided at the following rates:

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No. of Automobile Spaces Provided	No. of Accessible Spaces Provided
1—25	1
26—50	2
51—75	3
76—100	4
101—150	5
151—200	6
201—300	7
301—400	8
401—500	9
501—1,000	2% of total spaces
1,001 and over	20 plus 1 for each 100 spaces or fraction thereof over 1,001

4. Each accessible parking space shall be fourteen (14) feet wide, striped to provide a nine-foot wide parking area and a five-foot wide loading area (access aisle) and shall be a minimum of eighteen (18) feet in length. If two accessible spaces are located adjacent to each other, they may share the five-foot wide loading area, resulting in a width of twenty-three (23) feet for the two spaces. One in every eight handicapped spaces, but not less than one, shall be van accessible; served by a loading area not less than eight feet wide. If two van accessible parking spaces are located adjacent to each other, they may share a common eight-foot wide loading area.
5. When less than five parking spaces are provided, at least one shall be fourteen (14) feet wide, striped to provide a nine-foot parking area and a five-foot loading area. Such space shall not be required to be reserved or identified exclusively for use by persons with disabilities.
6. Accessible parking spaces serving a particular building shall be located on the shortest accessible route of travel from adjacent parking to an accessible entrance. In parking facilities that do not serve a particular building, accessible parking shall be located on the shortest accessible route of travel to an accessible pedestrian entrance of the parking facility. In buildings with multiple accessible entrances with adjacent parking, accessible parking spaces shall be dispersed and located closest to the accessible entrances.
7. In each parking area, a bumper or curb shall be provided and located to prevent encroachment of cars over the required width of walkways. The space shall be so located that persons with disabilities are not compelled to wheel or walk behind cars other than their own. Pedestrian ways that are accessible to people with disabilities shall be provided from each such parking space to the related facilities, including curb cuts or ramps as needed. Ramps shall not encroach into any parking space, with the

exception that ramps located at the front of accessible parking spaces may encroach into the length of such spaces when such encroachment does not limit the capability of a person with a disability to leave or enter their vehicle, thus providing equivalent facilitation. Where the building official determines that compliance with any regulation of this subsection would create an unreasonable hardship, a waiver may be granted when equivalent facilitation is provided.

8. The slope of an accessible parking stall shall be the minimum possible and shall not exceed one-quarter inch per foot (2.083% gradient) in any direction.
 9. Notwithstanding the off-street parking requirements of subsection A of this section, the number of parking spaces that are not accessible may be reduced to the extent necessary for modification of an existing facility to comply with the requirements described in this subsection.
 10. Where provided, one passenger drop-off and loading zone shall provide an access aisle at least five feet wide and twenty (20) feet long adjacent and parallel to the vehicle pull up space. Such zones shall be located on a surface with a slope not exceeding one vertical in fifty (50) horizontal and shall be located on an accessible route of travel to the entrance of the facility. If there are curbs between the access aisle and the vehicle pull-up space, then a curb ramp shall be provided. Valet parking facilities shall provide a passenger loading zone, as described herein.
- C. Low Emitting Fuel Efficient Carpool/Vanpool Vehicle Parking. Eight percent of required parking shall be designated for any combination of low-emitting, fuel efficient and carpool/vanpool vehicles for all new nonresidential development.

Section 9.11.070 Adjustments to off-street parking requirements.

Adjustments to off-street parking for uses included in this chapter may be granted if, in the opinion of the community development director, the proposed modification to the required number of parking or loading spaces is warranted. Requests for parking adjustments shall be reviewed and approved by the community development director based on the following requirements:

- A. Parking Studies. The number of spaces required by this chapter, as noted in Section 9.11.040(A) of this chapter, for provisions of off-street parking and loading spaces may be adjusted by the approval authority if it is demonstrated by a parking study, prepared by a qualified parking study consultant, that the proposed use would have a parking or loading space demand other than the requirements of this chapter.

B. Shared Parking.

1. Shared parking is encouraged to avoid the creation of unused parking spaces and their potential harmful effects such as increased construction and maintenance costs, heat and glare, and water run off requiring treatment of pollutants. A reduction in minimum parking requirements for individual uses may be granted by the community development director where joint use of parking facilities or other factors will mitigate peak parking demand.
2. Requests for parking reductions resulting from joint usage shall be supported by information prepared by a qualified parking study consultant. The investigation used to generate the required information shall generally follow the format described below. Shared parking requests shall be analyzed as follows:
 - a. Initial project review involves documentation and quantification of proposed land uses and anticipated functional relationships between the parking needs of different land uses. The initial review will also consist of data gathering regarding proximity to transit facilities, general location of parking facilities, surrounding land uses and mix, predicted pedestrian patterns, and similar variables which affect parking needs;
 - b. Adjustments for peak parking factor includes calculating the number of off-street parking spaces required for each land use within the area proposed for joint parking use based upon the requirements of Section 9.11.040 of this chapter. Other elements to be considered include seasonal adjustment for parking demand and a determination of the mode of transit used in reaching or departing the area being considered;
 - c. Analysis of hourly accumulation involves an estimation of hourly parking accumulations for each land use during a typical week day or weekend day; and
 - d. Estimate of shared parking merges the hourly parking demand estimate to calculate the overall parking required to be provided within the area being considered for shared parking facilities.
3. Up to fifty (50) percent of the parking facilities required by this chapter may be utilized as shared parking facilities subject to the requirements of this section. Except that, a church or an auditorium which is part of a public or private school may adjust the required parking by up to one hundred (100) percent of the parking facilities required by this chapter.
4. In granting parking reductions for shared use of parking facilities, the approval authority shall make one or more of the following findings:

- a. The parking study report justifies the requested parking reduction based upon the presence of two or more adjacent land uses which, because of their substantially different operating hours or different peak parking characteristics, will allow joint use of the same parking facilities;
 - b. The parking study report indicates that there are public transportation facilities and/or pedestrian circulation opportunities which justify the requested reduction of parking facilities;
 - c. The parking study report finds that the clustering of different land uses is such that a reduced number of parking spaces can serve multiple-trip purposes to the area in question.
5. As a condition of approval to the granting of a reduction in required parking, the city may require the granting of reciprocal access and parking agreements with surrounding properties.
- C. Transportation Management Plans.
1. The number of required parking spaces may be decreased subject to the approval of a transportation management plan supplied by the applicant. Such a plan may include, but is not limited to, car pooling, van pools, and staggered work hours.
 2. In evaluating the request, the approval authority shall consider, among other factors:
 - a. Projected effectiveness of car pool, van pool, staggered work hours, or similar transportation management programs;
 - b. Proximity to public transportation facilities which could be reasonably expected to serve a significant portion of employees or customers;
 - c. Evidence of the likelihood that employees or customers will utilize regular transportation alternatives to individual use of automobiles, including transportation management plans prepared pursuant to South Coast Air Quality Management District Rule XV.
- D. Off-Site Parking Facilities. Required parking for a development may be provided off the site in certain instances. Requests for off-site parking facilities shall meet the following requirements:
1. The off-site parking shall be located so that it will adequately serve the use for which it is intended. In making this determination, the approval authority shall consider the following:
 - a. Proximity of the off-site parking facilities;

- b. Ease of pedestrian access to the off-site parking facility;
 - c. The type of use which the off-site parking is intended to serve, recognizing that such facilities are generally not appropriate for high-turnover uses; and
 - d. The need for locating parking facilities off-site, and the resulting urban design benefits of off-site parking, if any.
2. As a condition of granting approval to the development of off-site parking facilities, the applicant and other involved parties shall be required to sign and record a reciprocal parking agreement ensuring the continued availability of the off-street parking facilities for the use they are intended to serve.

Section 9.12.060 Permitted signs.

A. General Provisions.

1. The following signs shall be permitted subject to a sign permit:
 - a. Monument signs;
 - b. Tenant identification (wall) signs;
 - c. Drive-through restaurant menu boards;
 - d. Freeway signs;
 - e. Gas station signs;
 - f. Theater marquees;
 - g. Internal guidance signs;
 - h. Directory signs;
 - i. Special event signs;
 - j. Off-site directional signs;
 - k. Banners.
2. Changeable Copy. The signs described in this section may include manual, electronic or mechanically activated changeable copy comprising not more than fifty (50) percent of the sign copy area. Such changeable copy shall not blink, flash or change in appearance more than once in three seconds. Manually activated changeable copy signs

shall use no more than two colors and shall be enclosed within a cabinet with a clear protective cover.

B. Monument Sign Requirements.

1. Commercial and Industrial Developments. One sign is allowed per driveway not to exceed a total per street frontage of two square feet of copy area and two and one-half square feet of sign area respectively for each one thousand (1,000) square feet of gross floor area within the development. With respect to a single building of less than ten thousand (10,000) square feet in gross floor area located on an individual parcel with street frontage, such sign need not be less than twenty (20) square feet in sign copy area and thirty-five (35) square feet in sign area per street frontage.
2. Residential Developments.
 - a. Neighborhood Identification Signs.
 - i. One non-illuminated neighborhood identification sign is permitted at each street entrance to each neighborhood.
 - ii. Neighborhood identification signs shall not exceed twenty-five (25) square feet in copy area, forty-five (45) feet in sign area and six feet in height.
 - iii. The content of such signs shall be limited to the name of the neighborhood.
 - iv. All neighborhood identification signs shall be designed for maximum vandal resistance and shall be made of masonry, cement, or other materials of comparable durability. Such signs may be either freestanding or affixed to the neighborhood perimeter wall.
 - v. All neighborhood identification signs shall comply with the sight distance requirements for traffic safety.
 - vi. No neighborhood identification sign shall be allowed unless a homeowners' association or community services district is responsible for sign maintenance.
 - vii. Any neighborhood identification sign located within a city right-of-way shall require an encroachment permit for such sign from the city engineer.
 - b. Multiple-Family Complex. One wall or monument sign, not exceeding twenty-five (25) square feet in area per display face, is allowed for each public street frontage. Monument signs may not exceed six feet in

overall height. In lieu of a freestanding sign or one large wall sign, two single-sided, wall mounted-signs not exceeding twenty-five (25) square feet per display face are allowed for each public street frontage when located at a project entry point. The content of such signs shall be limited to the name of the complex and the range of addresses within the complex.

- c. Temporary Model Home Complex. Two non-illuminated signs are permitted not to exceed twenty-five (25) square feet in copy area, forty-five (45) square feet in sign area and six feet in height at each major entrance to the complex. Such signs shall be removed at the completion of home sales.
3. Institutional Signs Within Residential Districts. One monument sign not to exceed thirty-six (36) square feet in copy area, forty-eight (48) square feet in sign area and eight feet in height is permitted to identify the premises of a place of religious worship or similar quasi-public institution.
4. Sign Height and Area.
 - a. The height of a monument sign is the vertical dimension measured from the average finished grade level to the highest point of the sign. The height of a monument sign shall not exceed fifteen (15) feet.
 - b. The maximum height of a sign located on a berm with a finished grade level more than two feet above the top of the street curb shall be reduced an amount equal to the distance that the grade level exceeds two feet above the top of curb.
 - c. Where topographic constraints make the established copy height standards impractical, the community development director may adjust the height requirements on a project by project basis.
 - d. The sign area of a monument sign may not exceed the limits prescribed in this section unless a determination is made by the decision-making body that an increase is needed to improve the compatibility of the sign with the architecture of the development where the sign is to be located. This provision shall not be construed to apply to the sign copy area.
 5. Addresses. Addresses with a minimum of six-inch letters shall be located above the copy area. If a series of addresses are located within the project, the address shall include the entire address range beginning with the lowest number. Addresses shall not be considered in the calculation of the copy area.

6. Vacant Spaces. Any vacant tenant spaces on a multitenant monument sign shall appear opaque until occupied using a material and texture consistent with the rest of the sign copy area.
 7. Opaque Backgrounds. The sign copy area shall be designed with opaque backgrounds such that when illuminated from behind, only the sign text is illuminated against a dark (unlighted) background.
 8. Application to Multitenant Centers. Monument sign standards apply to any development designed as an integrated center with shared parking and access. Leasing to individual tenants or subdivision of the center shall not establish separate sign privileges for each tenant or parcel.
 9. Setback Requirements. Monument signs may be placed at the ultimate street right-of-way line, except that they shall not encroach within the limited use area described in the landscape development guidelines and specifications.
- C. Tenant Identification (Wall) Sign Requirements.
1. Signs on Buildings Up to Two Stories High. Each tenant may erect a wall sign on the front, side and rear of the building space occupied by such tenant with a sign area not to exceed ten (10) percent of the building face occupied by such tenant, except that such sign need not be less than twenty (20) square feet in area.
 2. Signs Within Any District on Buildings Over Two Stories High.
 - a. One wall sign not to exceed two percent of the building face may be placed above the windows of the highest floor on each exterior wall (front, rear and side) of the building. Such sign(s) shall display the name of the building or the major tenant.
 - b. Up to four wall signs per building, each not to exceed twenty (20) square feet in area, may be placed below the second floor to identify building tenants.
 3. Residential Uses. One wall sign is permitted per street frontage of a multiple-family complex not to exceed twelve (12) square feet in area. The content of such signs shall be limited to the name of the complex and the range of addresses within the complex.
 4. Approved Types of Wall Signs. Wall signs shall consist of individually mounted channel letters, carved or routed wood, neon, sculptured cans, can signs and awning signs.
 5. Wall Sign Specifications.

- a. The copy area of a can wall sign shall use an opaque background. The retainer shall be decorative.
 - b. Individually mounted letters may be constructed of metal, plastic or foam, provided that the letters are a minimum of one inch in depth and the density of the plastic or foam is three pounds or greater. Alternative materials may be approved provided they are equivalent in durability to the above-referenced materials.
 - c. Carved or routed wood signs shall be constructed of redwood, cedar, balsa or an equivalent material. Wood signs shall be coated with sealer to minimize weathering. Plywood signs are prohibited.
 - d. Letters or graphics on an awning sign shall be painted, printed or affixed flat against the surface of an awning. An awning is a roof-like cover constructed of non-rigid material over a supporting framework that projects from the exterior wall of a building.
6. Raceways and Conduit. Raceways and electrical conduit shall not be visible.
- D. Drive-Through Restaurant Menu Boards. Two additional signs shall be permitted for the purpose of displaying the type and price of products sold on-site to drive-through customers. Such signs may include a speaker system to allow drive-through customers to order food and beverages. Such signs shall not exceed forty-eight (48) square feet in area and eight feet in height inclusive of the base. If the restaurant elects to build only a single menu board, the sign shall not exceed sixty-four (64) square feet and the height shall not exceed eight feet inclusive of the base.
 - E. Freeway Signs. One freestanding on-site sign shall be permitted per parcel or business complex, unless otherwise approved by the community development director, provided that the sign is located within six hundred sixty (660) feet of a freeway right-of-way. Such sign shall not exceed forty-five (45) feet in height and one hundred fifty (150) feet in sign area. The sign area may not exceed the limits prescribed in this section unless a determination is made by the community development director that an increase is needed to improve the compatibility of the sign with the architecture of the development where the sign is to be located.
1. The community development director may approve the use of 100 percent of the area of a freeway sign for changeable copy displays, provided that the respective sign also identifies at least one of the on-site businesses either as part of the changeable copy display or the

physical structure of said sign. Changeable copy signs may be used to advertise establishments, products, services and activities that are sold, produced, and/or furnished on-site or off-site.

F. Gas Station Signs.

1. Monument Signs. Gas stations shall be allowed one monument sign per street frontage to identify the business and the state-mandated price identification. Each sign shall not exceed forty (40) square feet in copy area and seventy-five (75) square feet in sign area, except that up to forty-five (45) square feet in copy area may be allowed where there is joint use of a gas station with other businesses.
2. Gas Pump Island Signs. Signs are allowed on or above the fuel pumps not to exceed a maximum aggregate surface area of four-square feet per linear foot of pump island.
3. Gas Pump Canopy (Liter Box) Signs. Letters and symbols placed on the canopy over the fuel pumps shall not exceed twenty (20) percent of the total surface area of each face of the canopy.

G. Theater Marquees. Theater marquees shall be subject to review by the community development director.

H. Internal Guidance Signs. Internal guidance signs may be erected to direct pedestrian or vehicular traffic within the internal circulation system of a business or residential complex. Internal guidance signs shall list one or more of the businesses or buildings on the premises and indicate the recommended route to the businesses or buildings. Such signs shall not exceed fifteen (15) feet in height. Such signs shall be oriented for viewing from within the premises and shall not be readily visible from outside of the premises in which they are located. Internal guidance signs located twenty (20) feet or more from the public right-of-way and less than four square feet in sign area do not require a sign permit.

I. Directory Signs.

1. Vehicular-Oriented Directory Signs. One vehicular-oriented directory sign may be required near each major entrance of a multiple-structure project. One vehicular-oriented directory sign shall be permitted near each major entrance of a multitenant, business complex. Such signs shall not exceed forty-eight (48) square feet in sign area and eight feet in height. A vehicular-oriented directory sign shall not be placed at the driveway entrance but shall be in an easily accessible location adjacent to the driveway. Such sign may contain a list and map and accompanying legend indicating the name of the development, streets, buildings, unit numbers and fire hydrant locations within the development. Vehicle-oriented directory signs shall be oriented for

viewing from within the complex and not from the street outside of the complex.

2. Pedestrian-Oriented Directory Signs. One pedestrian-oriented directory sign not to exceed ten (10) square feet in copy area shall be permitted for each multitenant building in a business or residential complex. Such sign shall list each business or residence located within the building and its address.
- J. Projecting Signs. A projecting sign may be permitted in lieu of a monument sign based on a determination by the decision-making body that the physical limitations of the site make it impractical to erect a monument sign on the premises. The copy area and sign area shall not exceed the size of the monument sign.
- K. Special Event Signs.
 1. Special event signs are permitted subject to the following:
 - a. Definition. A “special promotion” means a commercial event for which the special use of special event signs which are otherwise prohibited by this chapter, are permitted with a granting of a permit by the community development department prior to such displays. No special promotion shall exceed thirty (30) days during any calendar year at any one address or location within the city;
 - b. The community development director shall issue permits for “special event signs” not to exceed thirty (30) days during any calendar year. The applicant for such special event signs may elect to determine how the days shall be allocated to that particular address or premises within the city. However, no more than three permits may be issued per calendar year;
 - c. Applications for “special event sign” permits shall be filed with the community development department, at least five days prior to the beginning of the event; provided, however, that the community development director may exempt an applicant from the five days application prior to the beginning of an event provided the applicant files a declaration under penalty of perjury that the nature of his or her business activities does not permit advance knowledge by the applicant of the time of any particular “special event” and that such applicant agrees that he will not exceed the total number of thirty (30) days within any calendar year;
 - d. All special event signs shall comply with the following requirements and restrictions:

- i. The applicant shall obtain any other required permits, licenses, written approvals from the city or other agencies and observe all laws concerning health and safety.
 - ii. Written approval from the property owner or authorized agent shall be submitted with the permit application.
 - iii. A copy of the approved permit application will be furnished by the community development department. This copy, and all other required permits, must be displayed in a conspicuous place on the premises throughout the duration of the event.
 - iv. Signs, advertising devices and other approved outdoor displays shall substantially conform in size and location to the site plan sketched on or attached to the permit and conform with any restrictions stated upon the permit.
 - v. Signs, advertising devices and other approved outdoor displays shall be erected or placed only on property in possession or control of the permittee. No off-site signs or displays shall be permitted.
 - vi. Within ten (10) feet of any vehicular access or five feet of any public street property line, no sign, advertising device, or other approved outdoor display shall exceed thirty (30) inches in height above street curb. No public right-of-way shall be used for locating any sign or display.
 - vii. Signs or banners shall be permitted with an area of one square foot for each lineal foot of store or building front, owned or operated by the permittee, up to a maximum of eighty (80) square feet.
 - viii. All signs, or other approved outdoor displays shall be erected and maintained in a clean, safe manner and in good repair at all times.
 - ix. The community development director may impose special requirements and restrictions when unusual conditions exist at or near the proposed event location. Such restrictions shall be listed on the approved permit application and shall be adhered to throughout the duration of the event.
 - x. Search lights may be permitted concurrently with other signs as part of a special event promotion.
2. Special event signs for grand openings shall be permitted in addition to the time frames specified above, provided that no additional time shall be granted for inflatable signs.
 - a. No sign shall be displayed more than thirty (30) calendar days;

- b. The event is for the original opening of a business at a particular location, within thirty (30) days after occupancy. Existing businesses may qualify if the ownership and the name of the business are changed. A grand opening is not an annual or occasional sales promotion or the opening of a related store at another location;
 - c. The requirements of special event signs are met.
3. Inflatable Signs. Inflatables shall be allowed with a special event sign permit, provided that:
- a. Inflatables shall not be displayed for more than thirty (30) days per calendar year;
 - b. Balloons and blimps shall not exceed a maximum height of fifty (50) feet above grade;
 - c. Large (greater than forty (40) inches in diameter) balloons and blimps shall be permitted for commercial uses only;
 - d. Any size balloon or blimp may be illuminated but may not have been constructed of reflective material.
- L. Off-Site Directional Signs. Only off-site directional signs which are in conformance with this section may be erected or maintained within the city. Off-site directional signs shall only be permitted for residential subdivisions, public and quasi-public uses or facilities. The following standards shall apply to the construction and installation of off-site directional signs:
- 1. The city shall designate an organization for administration of the terms of this section, except that the organization shall have no enforcement powers hereunder. The duties of the organization under this section include, but are not limited to, the following:
 - a. Timely, equitable and nondiscriminatory processing of applications to install a directional sign on a kiosk;
 - b. Obtaining sites and approvals for kiosk locations;
 - c. Timely construction and installation of kiosks and directional signs; and
 - d. Maintenance of kiosks, kiosk sites and directional signs in a neat, clean and orderly condition.
 - 2. The duties imposed upon the organization pursuant to this section may be exercised by a third party, subject to prior approval of such third party by the public works director.

3. The design of kiosks and directional signs shall be prepared by the organization and submitted to the city for written approval by the public works director.
4. Kiosks and directional signs shall conform to the following general standards:
 - a. Kiosks shall contain no more than eight directional signs per face;
 - b. No kiosk shall have more than one face, except that additional faces, not to exceed three in number, may be approved for specific locations by the planning commission;
 - c. No kiosk shall exceed nine feet in height or five feet in width;
 - d. Each directional sign shall be nine inches high and five feet long;
 - e. Directional signs may contain the following information: name of use; applicant logo; and a directional arrow;
 - f. No tag sign, streamer, device, display board, or other appurtenance may be added to or placed upon any kiosk or kiosk site, except as approved in writing by the public works director;
 - g. Kiosks will be permitted in all land use districts and on private or public property or right-of-way, subject in each case to written permission of the owner of such property or right-of-way and subject to written approval of the city. Permission of the property owner for each kiosk site shall be filed with the public works director. Approval of the city may be obtained in the following manner:
 - i. By designation as an approved site by the public works director,
 - ii. For kiosks of one face, by the public works director, and
 - iii. For kiosks of two or more faces, by the planning commission, except that the public works director may give interim approval of such sites for a period of thirty (30) days or less;
 - h. All liabilities, costs and expenses arising out of the siting, installation and construction of kiosks and directional signs, and out of administering the provisions of this section, other than enforcement expenses related to violations of this section, shall be borne by the organization; the organization shall enter into an agreement with the city, under which it indemnifies, defends and holds harmless the city, in such form as approved by the public works director and city attorney, and shall provide public liability insurance in the minimum amount of three hundred thousand dollars (\$300,000.00) naming the city as additional

insured and in such form and with a company or companies approved by the director of public works and city attorney; and the city shall have no liability therefor.

- i. In addition to other penalties provided by law, including those set forth in this section, any directional sign erected, constructed, installed or maintained in violation of this section shall be deemed a public nuisance and may be summarily abated as such by the city.

M. Banners.

1. General Provisions.

- a. Banners shall be maintained free from deterioration, disrepair or other condition that would create a nuisance as described in Section 6.04.040 of this code.
- b. Banners shall be attached to buildings unless otherwise specified in this section. The banners shall be securely fastened at all four corners to the wall of the building on which it is located. The method of attachment shall prevent the banner from flapping in the wind.
- c. A banner shall not obscure windows, doors, lighting fixtures, other signs, nor shall it be displayed above the walls of the building on which it is located.

2. Promotional Advertising Banners.

- a. A “promotional advertising banner” means a banner advertising the name of a business or a product or service provided on the premises.
- b. No promotional advertising banner shall be displayed unless authorized by permit issued by the community development department. Each may cover more than one banner. A banner permit shall be effective for as long as the business receiving the permit has a valid business license for the location. A new permit shall be required if the business moves to a new location. Banners shall be maintained in good condition and in conformance with the approved permit.
- c. Banners shall be displayed on the wall(s) of the building space occupied by the business advertised on the banner, not to exceed one banner per wall and two banners per business. Each promotional advertising banner shall not exceed ten (10) percent of the area of the building face on which it is placed.
- d. In the case of a business engaged in a substantially outdoor enterprise, the community development director may permit a promotional advertising banner to be placed in a location other than the wall of a

building occupied by such business and of a size that would be enjoyed by a typical indoor business situated on a site of the same size.

- e. A copy of the approved banner permit shall be displayed in a conspicuous place on the premises in full public view for as long as the permit is in effect.
 - f. A promotional advertising banner shall not be displayed in lieu of a permanent wall or canopy sign except during the first sixty (60) days of issuance of the certificate of occupancy for the business.
 - g. A promotional advertising banner shall not be displayed facing a freeway.
3. Quasi-Public Uses. One banner not to exceed sixteen (16) square feet in sign area may be displayed per street frontage in conjunction with a quasi-public use.

N. Off-Site Real Estate Signs.

- 1. An off-site real estate sign is a sign advertising real estate that is for sale, rent, lease or exchange where the advertised property is not the same property on which the sign is located.
- 2. No off-site real estate sign may be illuminated.
- 3. No off-site real estate sign shall be allowed without written consent of the property owner.
- 4. No off-site real estate sign shall be installed in a manner that creates a hazard for vehicle or pedestrian traffic. All off-site real estate signs shall comply with the sight distance requirements for traffic safety.
- 5. Off-site real estate signs are prohibited within the public right-of-way.
- 6. No off-site real estate sign shall exceed twenty-four (24) square feet in area or eight feet in height.
- 7. Off-site real estate signs shall be made of weather-resistant materials, maintained in good condition and kept free of graffiti. No paper, cardboard, lightweight plastic or similar fragile material shall be used. Off-site real estate signs shall be coated with materials that allow graffiti to be removed easily.
- 8. The content of each off-site real estate sign shall be limited to the information identified in Section 713 of the California Civil Code: a statement that the property is for sale, lease or exchange; directions to

the property; and the owner's or agent's name, address and telephone number.

9. Off-site real estate signs shall be removed within ten (10) days of the execution of the sale, lease, exchange or rental agreement for the property for which the sign is erected.
- O. Signs in the Public Right-of-Way.
1. A monument sign that is otherwise permissible pursuant to subsection (B)(1) of this section and located in the public-right-of-way may be permitted in the following circumstances:
 - a. The sign is located within a public right-of-way controlled by the city of Moreno Valley;
 - b. The sign is located along Sunnymead Boulevard between Frederick Street and Perris Boulevard;
 - c. There is no practicable location on private property to locate the sign;
 - d. The sign design and location do not obstruct or impede any utility, utility access, pedestrian walkways or pedestrian or vehicle sight lines;
 - e. The sign design and location are not located over or upon any other easement without written authorization for such from the owner of the easement;
 - f. An encroachment permit is obtained, all fees paid, and all required insurance and other requirements are kept current and valid;
 - g. A sign permit is obtained in accordance with this chapter.
 2. In order to apply for a permit for a sign in the public right-of-way pursuant to this section, an application must first be made for an encroachment permit and all criteria for such encroachment permit must be met.
 3. In the event that any of the requirements or terms of the encroachment permit are not met or are not continually maintained in accordance with the encroachment permit, any sign permit shall become void and such sign shall become a public nuisance and may be removed by the city at any time at the sign owner's expense.
 4. Any such sign in the public right-of-way shall be immediately removed from the public right-of-way upon request by the city for any public purpose and shall not be entitled to any compensation.

Section 9.12.070 Sign program.

- A. An integrated sign program may be requested by the property owner for all nonresidential projects with two or more tenant suites. The sign program shall be subject to review by the community development director. A sign program may deviate from any of the standards provided in this section.
- B. Sign programs in effect prior to adoption of this title shall be considered valid upon adoption of this title. Such programs may be converted to the standards given in this section if the landowner files a notice of intent with the community development director.

Section 4. CEQA COMPLIANCE:

That the City Council hereby finds and determines that pursuant to Section 15378 of the California Environmental Quality Act, the proposed amendments to the Municipal Code are procedural in nature and would not involve any change to land use or development standards, thus there is no potential for these Municipal Code changes to either directly or indirectly result in a physical impact on the environment. Therefore, the proposed amendments are not a project under the California Environmental Quality Act.

Section 5. SEVERABILITY

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Ordinance be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this ordinance as hereby adopted shall remain in full force and effect.

Section 6. REPEAL OF CONFLICTING PROVISIONS

That all the provisions of the Municipal Code as heretofore adopted by the City of Moreno Valley that are in conflict with the provisions of this Ordinance are hereby repealed.

Section 7. EFFECTIVE DATE

That this Ordinance shall take effect thirty (30) days after its second reading.

Section 8. CERTIFICATION

That the City Clerk shall certify to the passage of this Ordinance and shall cause the same to be published according to law.

INTRODUCED at a regular meeting of the City Council on _____, 2020 and PASSED, APPROVED, and ADOPTED by the City Council on _____, 2020, by the following roll call vote, to wit:

Dr. Yxstian A. Gutierrez
Mayor
City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Attachment: Ordinance No. XXX - Title 9 Amendments [Revision 3] (4208 : PEN20-0189 Municipal Code Streamlining)

ORDINANCE JURAT

STATE OF CALIFORNIA)

COUNTY OF RIVERSIDE) ss.

CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Ordinance No. YYYY-____ was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the _____ day of _____, YYYY, by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

Attachment: Ordinance No. XXX - Title 9 Amendments [Revision 3] (4208 : PEN20-0189 Municipal Code Streamlining)

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADDING CHAPTER 12.24 “DRIVING UPON ACCESS RAMPS PROHIBITED” TO TITLE 12 VEHICLES AND TRAFFIC OF THE MORENO VALLEY MUNICIPAL CODE

WHEREAS, the City of Moreno Valley is a General Law city organized pursuant to Article XI of the California Constitution; and

WHEREAS, pursuant to the authority granted the City by Article XI, Section 7 of the California Constitution, the City has the police power to adopt regulations designed to promote the public convenience or the general prosperity, as well as regulations designed to promote the public health and safety; and

WHEREAS, staff has proposed that a new Chapter 12.24 (Driving Upon Access Ramps Prohibited) be added to Title 12 (Vehicles and Traffic) of the Moreno Valley Municipal Code; and

WHEREAS, the intent of the proposed new chapter is to protect City property, reduce the risk of automobile accidents, enhance pedestrian safety, and reduce the taxpayers’ costs of maintaining and/or repairing certain City-maintained sidewalks and pedestrian access ramps caused by vehicles and

WHEREAS, the proposed new chapter will prohibit driving on City-maintained sidewalks and pedestrian access ramps without prior authorization many of which have been damaged by vehicles driving upon them; and

WHEREAS, pursuant to the provisions of Section 9.02.200 (Public Hearing and Notification Procedures) of the Municipal Code and Section 65905 of the Government Code, a public hearing was scheduled for December 15, 2020 and notice of said hearing was duly published and posted; and

WHEREAS, on December 15, 2020, the public hearing to consider the proposed new chapter was duly noticed and conducted by the City Council at which time all interested persons were provided an opportunity to be heard and to present evidence; and

WHEREAS, pursuant to Section 15378 of the California Environmental Quality Act, a “Project” means the whole of an action, which has a potential for resulting in either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment, but does not include organizational or administrative activities of government that will not result in direct or indirect physical changes in the environment such as the proposed new chapter; and

WHEREAS, in the light of the foregoing, the proposed new chapter does not constitute a “Project” under CEQA.

NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY DOES ORDAIN AS FOLLOWS:

Section 1. RECITALS

That the above recitals are true and correct and are incorporated herein as though set forth at length herein.

Section 2. AUTHORITY

That this Ordinance is adopted pursuant to the authority granted by Article 11, Section 7 of the Constitution of the State of California and California Government Code Section 37100, and it is not intended to be duplicative of state law, nor be preempted by state legislation.

Section 3. AMENDMENT OF TITLE 12 (VEHICLES AND TRAFFIC) OF THE MORENO VALLEY MUNICIPAL

That Title 12 (Vehicles and Traffic) of the Municipal Code is hereby amended to include the following new chapter:

Chapter 12.24 Driving upon access ramps prohibited.

12.24.010 Prohibition.

It shall be unlawful for any person to operate a motor vehicle, as defined in California Vehicle Code Section 415, on a City maintained sidewalk or City maintained pedestrian access ramp, without prior authorization.

12.24.020 Violations and penalties.

- A. It is unlawful for any person to violate any provision of this chapter. Any person violating any provision of this chapter shall be deemed guilty of an infraction or misdemeanor as hereinafter specified. Each person shall be deemed guilty of a separate offense for each and every day or portion thereof during which any violation of any of the provisions of this chapter is committed, continued or permitted.
- B. Any person so convicted shall be: (1) guilty of an infraction offense and punished by a fine not exceeding one hundred dollars (\$100.00) for a first violation; (2) guilty of an infraction offense and punished by a fine not exceeding two hundred dollars (\$200.00) for a second violation. The third and any additional violations shall constitute a misdemeanor offense and shall be punishable by a fine not exceeding one thousand dollars (\$1,000.00) or six months in jail, or both. Notwithstanding the above, a first offense may be charged and prosecuted as a misdemeanor. Payment of any penalty herein shall not relieve a person from the responsibility for correcting the situation.”

Section 4. CEQA COMPLIANCE:

That the City Council hereby finds and determines that pursuant to Section 15378 of the California Environmental Quality Act, that since this proposed new chapter will not involve any change to land use or development standards, there is no potential for this new chapter to either directly or indirectly result in a physical impact on the environment, and is therefore not constitute a “project” under the California Environmental Quality Act.

Section 5. SEVERABILITY

That the City Council declares that, should any provision, section, paragraph, sentence or word of this Ordinance be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this ordinance as hereby adopted shall remain in full force and effect.

Section 6. REPEAL OF CONFLICTING PROVISIONS

That all the provisions of the Municipal Code as heretofore adopted by the City of Moreno Valley that are in conflict with the provisions of this Ordinance are hereby repealed.

Section 7. EFFECTIVE DATE

That this Ordinance shall take effect thirty (30) days after its second reading.

Section 8. CERTIFICATION

That the City Clerk shall certify to the passage of this Ordinance and shall cause the same to be published according to law.

INTRODUCED at a regular meeting of the City Council on _____, 2020 and PASSED, APPROVED, and ADOPTED by the City Council on _____, 2020, by the following roll call vote, to wit:

Dr. Yxstian A. Gutierrez
Mayor
City of Moreno Valley

ATTEST:

Pat Jacquez-Nares, City Clerk

APPROVED AS TO FORM:

Steven B. Quintanilla, Interim City Attorney

Attachment: Ordinance No. XXX - Title 12 Amendments [Revision 5] (4208 : PEN20-0189 Municipal Code Streamlining)

ORDINANCE JURAT

STATE OF CALIFORNIA)

COUNTY OF RIVERSIDE) ss.

CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Ordinance No. YYYY-____ was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the _____ day of _____, YYYY, by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

Attachment: Ordinance No. XXX - Title 12 Amendments [Revision 5] (4208 : PEN20-0189 Municipal Code Streamlining)

**CITY OF MORENO VALLEY
CITY COUNCIL**

**VIA TELECONFERENCE ONLY
PURSUANT TO COVID-19
GOVERNOR EXECUTIVE ORDER N-29-20**

**NOTICE OF PUBLIC HEARING AND
ENVIRONMENTAL DETERMINATION**

NOTICE IS HEREBY GIVEN that a teleconferenced Public Hearing will be held by the City Council of the City of Moreno Valley on the date and time set forth below:

Date and Time: December 15, 2020 at 6:00 p.m.
Location: **VIA TELECONFERENCE ONLY**
Go to <http://morenovalleyca.igm2.com/Citizens/default.aspx> for instructions.
Item: Municipal Code Amendment (Case No. PEN20-0189)
Applicant: City of Moreno Valley
Location: City Wide
Proposal: A Municipal Code Amendment to amend various sections within Title 9 Planning and Zoning, Chapter 9.02 Permits and Approvals, Chapter 9.09 General Development Standards, Chapter 9.11 Parking, Pedestrian and Loading Requirements, and Chapter 9.12 Sign Regulations, and adding Chapter 12.24 Driving upon access ramps prohibited to Title 12 Vehicles and Traffic.

Council District: City Wide

ENVIRONMENTAL DETERMINATION: Pursuant to Section 15378 of the California Environmental Quality Act, a 'Project' means the whole of an action, which has a potential for resulting in either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment. A project does not include organizational or administrative activities of governments that will not result in direct or indirect physical changes in the environment. Therefore, staff has determined that the proposed amendments are not a project under the California Environmental Quality Act.

PUBLIC TESTIMONY: All interested parties will be provided an opportunity to submit oral testimony during the teleconferenced Public Hearing and/or provide written testimony during or prior to the teleconferenced Public Hearing. The application file and related environmental documents may be inspected by appointment at the Community Development Department at 14177 Frederick Street, Moreno Valley, California by calling (951) 413-3206 during normal business hours (7:30 a.m. to 5:30 p.m., Monday through Thursday).

COVID-19 – IMPORTANT NOTICES: Please note that due to the COVID-19 pandemic situation, staff will attempt to make reasonable arrangements to ensure accessibility to inspect the aforementioned records. **In addition, special instructions on how to effectively participate in the teleconferenced Public Hearing, as approved by Governor Executive Order No. N-25-20, will be posted at <http://morenovalleyca.igm2.com/Citizens/default.aspx> and will be described in the City Council agenda.**

PLEASE NOTE: The City Council may consider and recommend changes to the proposed items under consideration during the teleconferenced Public Hearing.

GOVERNMENT CODE § 65009 NOTICE: If you challenge any of the proposed actions taken by the City Council in court, you may be limited to raising only those issues you or someone else raised during the teleconferenced Public Hearing described in this notice, or in written correspondence delivered to the Planning Division of the City of Moreno Valley during or prior to, the teleconferenced Public Hearing.

ACCESSIBILITY: Upon request and in compliance with the Americans with Disabilities Act of 1990, any person with a disability who requires a modification or accommodation in order to participate in a meeting should direct

such request to Guy Pegan, ADA Coordinator, at (951) 413-3120 at least 48 hours before the meeting. The 48-hour notification will enable the City to make reasonable arrangements to ensure accessibility to this meeting.

STAFF CONTACT: Due to the COVID-19 pandemic situation, if you have questions regarding this Public Hearing, please contact Patty Nevins, Planning Official, by telephone at (951) 413-3354 or via email at pattyn@moval.org.

/s/Patty Nevins	Press-Enterprise	December 4, 2020
Patty Nevins	Newspaper	Date of Publication
Planning Official		
Community Development Department		

Attachment: Newspaper Notice [Revision 1] (4208 : PEN20-0189 Municipal Code Streamlining)



Report to City Council

TO: Mayor and City Council
 Mayor and City Council Acting in its Capacity as President and Members of the Board of Directors of the Moreno Valley Community Services District (CSD)
 Mayor and City Council Acting in its Capacity as Chairman and Commissioners of the Moreno Valley Housing Authority (HA)
 Mayor and City Council Acting in its Capacity as Members of the Moreno Valley Successor Agency

FROM: Marshall Eyerman, Assistant City Manager
 Mike Lee, City Manager

AGENDA DATE: December 15, 2020

TITLE: FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW AND APPROVAL OF THE FIRST QUARTER BUDGET AMENDMENTS AND REVIEW OF THE OPERATING AND CAPITAL CARRYOVERS FROM FISCAL YEAR 2019/20 (RESO NO. 2020-XX, CSD 2020-XX, HA 2020-XX, SA 2020-XX)

RECOMMENDED ACTION

Recommendations: That the City Council:

1. Receive and file the Fiscal Year 2020/21 First Quarter Budget Review.
2. Adopt Resolution No. 2020-XX. A resolution of the City Council of the City of Moreno Valley, California, adopting the revised budgets for fiscal year 2020/21.
3. Approve the City Position Summary.
4. Receive and file the Fiscal Year 2019/20 Carryover Memo.

Recommendations: That the CSD:

1. Adopt Resolution No. CSD 2020-XX. A resolution of the Moreno Valley Community Services District of the City of Moreno Valley, California, adopting the revised operating and capital budgets for Fiscal Year (FY) 2020/21.

Recommendation: That the HA:

1. Adopt Resolution No. HA 2020-XX. A resolution of the Moreno Valley Housing Authority of the City of Moreno Valley, California, adopting the revised operating and capital budget for Fiscal Year 2019/20.

Recommendations: That the City Council as Successor Agency:

1. Adopt Resolution No. Successor Agency 2020-XX. A Resolution of the City Council of the City of Moreno Valley, California, Serving as Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley, California, adopting the revised operating and capital budget for Fiscal Year 2019/20.

SUMMARY

This report provides the First Quarter Budget Report, which updates the Mayor and City Council regarding current year financial trends, and provides the Fiscal Year 2020/21 First Quarter Budget Review through September 30, 2020. This report also requests the approval of certain FY 2020/21 revenue and expenditure budget amendments.

At the end of FY 2019/20, all unexpended appropriations lapse and revert to fund balance. Once the fiscal year-end closing process has identified the items that are encumbered by purchase order, contract, or other commitments at June 30th, these items may become a carryover budget pursuant to City Council Resolution 2019-27 adopted May 7, 2019. A copy of the Carryover Memo for FY 19/20 has been included for transparency.

DISCUSSION

Based on a long-term practice of maintaining a structurally balanced budget and being identified as one of the most fiscally fit cities in the Country, on May 7, 2019, the City Council continued this trend and adopted the Two-Year Operating Budget for Fiscal Years 2019/20 – 2020/21, while adhering to those past practices. Additionally, as established after the last recession, the budget continues to fund and maintain separate reserves specifically established for periods of economic uncertainty or natural disasters. The budget included all component units of the City, including the General Fund, Community Services District, Housing Authority and Successor Agency. During the two-year budget period, the City Council will be updated of the City's financial condition through the process of Quarterly Budget Reviews. This ongoing process ensures a forum to review expenditure and revenue changes from the estimates made in the budget document. Additionally, any significant changes in projected revenue or unanticipated expenditures that may occur will be presented to the City Council.

This report provides the FY 2020/21 First Quarter Review for the first three months, July

thru September. The first quarter budget review will focus primarily on the City's General Fund. This review will also present three-month operational results from other key funds.

The City Council's direction of "Maintain a Balanced General Fund Budget" serves as a foundation for the fiscal status of City operations. Ongoing funding is directed to sustain ongoing operational expenses; one-time funding is directed toward one-time expenditures. This report identifies the budget adjustments as recommended by the City Manager.

FISCAL YEAR 2019/20 FIRST QUARTER REVIEW:

This First Quarter Report updates the Mayor and City Council regarding current year financial trends and provides the opportunity for the City Council to review the recommended actions as they relate to revenues and expenditures.

General Fund Revenue Update

Revenue receipts do not follow an even schedule. Although 25% of the fiscal year has elapsed, based on historic trends revenues are estimated to be at approximately 10% of the budgeted amount. Actual revenues received are currently 12% of budget. As the FY 2020/21 revenue estimates were developed prior to the adoption of the budget on May 7, 2019, the revenue estimates may require adjustments based on the FY 2020/21 actuals and revised projections throughout the year. Although there will be variances in some of the amounts budgeted, the total is expected to remain within 1% of the amended budget for the year. It should be noted that the lag in timing of revenue receipts helps illustrate the need for an operating cash reserve throughout each fiscal year.

General Fund Expenditure Update

Although not all expenditures follow a straight-line spending pattern, operating expenditures should track close to within 25% of budget for the year at the end of the first three months. As of September 30, 2020, total General Fund expenditures were at 17%. This pace is within expectations for most activities in the General Fund.

FISCAL YEAR 2020/21 BUDGET ADJUSTMENTS

The FY 2020/21 General Fund revenue budget, as amended, totals approximately \$111.3 million. Based on economic activity and revenue collections through September 2020, staff is recommending First Quarter budget increase of \$133,974 to approximately \$111.4 million. This increase is primarily driven by sales tax projections.

The FY 2020/21 General Fund expenditures budget as currently amended, and excluding one-time expenditures from fund balance, totals approximately \$111.5 million. The recommended First Quarter budget amendments decrease expenditures by

\$87,382 to \$111.4 million. This is primarily due to personnel savings from position changes. The fund continues to be structurally balanced, without the use of fund balance for ongoing operations. The specific budget adjustments for the General Fund are summarized in Exhibit A attached to the City Council Resolutions recommended for approval.

Fund	Type (Rev/Exp)	FY 2020/21 Amended Budget (1)	Proposed Adjustments	FY 2020/21 Amended Budget (2)	Proposed Adjustments (%)
General Fund	Rev	\$111,339,094	\$133,974	\$111,473,068	<1%
General Fund	Exp	\$111,560,450	(\$87,382)	\$111,473,068	<1%
	Net Total	(\$221,356)	\$46,592	\$0	

(1) Includes FY 2019/20 carryover amounts.

(1) Excludes the one-time costs for the Fire Truck replacement, which shall be paid from Fund Balance.

Summaries of Other Major Funds FY2020/21

The following provides a summary of some of the proposed budget adjustments to other major funds. A complete list of all changes are identified in Exhibit A to the Resolutions.

Other Grants (Fund 2300)

Transfers of \$1,000,000 to support the Senior Eats program and \$25,000 in support of the Financial Wellness program both off-set by revenue from State and Federal grants.

Development Impact Fee Funds (Fund 2901, 2902, 2905, 2909, 2911)

Increase revenue budget to match actual revenue received to date.

2007 Towngate Refunding (Fund 4106) and Successor Agency Admin Fund (Fund 4800)

Decrease in each fund of \$1,202,696 in ROP's funding. Sufficient funding is available in these funds to pay debt service, administrative costs, etc. through maturity.

Successor Agency Debt Service (Fund 4851) and Successor Agency 2017 Ref 2007 TABS (Fund 4852)

Decrease in each fund of \$763,000 based on established debt service schedule.

Special District Funds (Fund 5011, 5012, 5014, 5110, 5111, 5112, 5113, 5114)

Adjust revenue budgets to match applied fixed charges through the property tax rolls and adjust expenditure budgets based on operational need of the districts.

City Position Summary Action

The City Position Summary as amended by the City Council serves as an important internal control tool for City Council to establish authorized positions for the City while enabling staff to manage within the authorized and funded positions. The City Position Summary addresses career authorized positions and does not include temporary positions. As a result of operational changes, some positions are being requested to be adjusted and/or eliminated based on current and projected demands for those positions and services. Staff recommends the following updates to the City Position Summary:

Department/ Position Title	Adjustment	FY 2020/21 Requested Budget Adjustment	Notes
<u>City Attorney</u>			
City Attorney	(1)	\$ (127,585)	Vacant - Eliminate
Assistant City Attorney	(1)	\$ (102,862)	Vacant - Eliminate
Deputy City Attorney	(1)	\$ (54,800)	Vacant - Eliminate
Admin Asst (PT)	(1)	\$ (14,753)	Vacant - Eliminate
<u>City Manager</u>			
Exec Asst II	(1)	\$ (88,000)	Vacant - Eliminate
Media & Communications Manager	-	\$ 12,400	Reclass from C30 to C33
<u>Financial & Management Services</u>			
Financial Operations Div Mgr	(1)	\$ (150,000)	Vacant - Eliminate
Sr Admin Asst	(1)	\$ (72,000)	Vacant - Eliminate
Asst Buyer	(2)	\$ -	Re-Title/Re-Classify
Management Aide	2	\$ 5,000	
Storekeeper	(1)	\$ -	Re-Title/Re-Classify
Sr Office Asst	1	\$ 5,000	
Sr GIS Analyst	(1)	\$ -	Re-Title/Re-Classify
GIS Administrator	1	\$ -	
GIS Tech	(1)	\$ -	Re-Title/Re-Classify
GIS/Application Analyst	1	\$ 15,000	
<u>Parks & Community Services</u>			
Sr Cust Service	(1)	\$ (60,000)	Vacant - Eliminate
Sr Office Asst (PT)	(1)	\$ (40,000)	Vacant - Eliminate
Parks Project Coordinator	(1)	\$ -	Re-Title/Re-Classify
Management Analyst	1	\$ -	
Sr Cust Service (2)	(2)	\$ -	Re-Title/Re-Classify
Sr Office Asst	2	\$ -	
Total Adjustment	(9)	\$ (672,600)	

ALTERNATIVES

1. Approve Recommended Actions as set forth in this staff report, including the approval of the budget adjustments, as presented in Exhibit A. The approval of these items will allow ongoing activities to be carried out in the current fiscal year and the City is able to modify budgets and operations as necessary through this quarterly review, while retaining a structurally balanced General Fund budget. *Staff recommends this alternative.*

1. Do not approve proposed Recommended Actions as set forth in this staff report, including the resolutions adopting the budget adjustments to the budget, as presented in Exhibit A. *Staff does not recommend this alternative.*

FISCAL IMPACT

The City's Operating and Capital Budgets provide the funding and expenditure plan for all funds. As such, they serve as the City's financial plan for the fiscal year. The fiscal impacts for the proposed budget amendments are identified in Exhibit A.

NOTIFICATION

Publication of the agenda.

PREPARATION OF STAFF REPORT

Prepared By:
Stephanie Cuff
Management Analyst

Department Head Approval:
Marshall Eyerman
Assistant City Manager/Chief Financial Officer

Concurred By:
Brian Mohan
Financial Resources Division Manager

Approved by:
Mike Lee
City Manager

CITY COUNCIL GOALS

Revenue Diversification and Preservation. Develop a variety of City revenue sources and policies to create a stable revenue base and fiscal policies to support essential City services, regardless of economic climate.

Public Safety. Provide a safe and secure environment for people and property in the community, control the number and severity of fire and hazardous material incidents, and provide protection for citizens who live, work and visit the City of Moreno Valley.

Public Facilities and Capital Projects. Ensure that needed public facilities, roadway improvements, and other infrastructure improvements are constructed and maintained.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development

- 2. Public Safety
- 3. Library
- 4. Infrastructure
- 5. Beautification, Community Engagement, and Quality of Life
- 6. Youth Programs

Objective 1.1: Proactively attract high-quality businesses.

Objective 1.2: Market all the opportunities for quality industrial development in Moreno Valley by promoting all high-profile industrial and business projects that set the City apart from others.

Objective 1.6: Establish Moreno Valley as the worldwide model in logistics development.

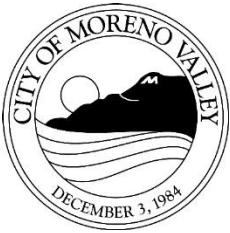
Objective 6.2: Improve health, wellness and fitness for Moreno Valley youth through recreation and sports programs.

ATTACHMENTS

- 1. FY20-21 First Qtr Financial Rep
- 2. City Resolution 2020-XX
- 3. Community Services District Resolution 2020-XX
- 4. Housing Authority Resolution 2020-XX
- 5. Successor Agency Resolution 2020-XX
- 6. Exhibit A - Proposed Amendments
- 7. City Position Summary FY 1920 - 2021
- 8. Carryover Memo-Exhibits

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/08/20 10:37 AM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/08/20 10:43 AM



City of Moreno Valley

Fiscal Year 2020/21

First Quarter Financial Summary

TO: Mayor and City Council

FROM: Marshall Eyerman, Assistant City Manager/Chief Financial Officer/City Treasurer

DATE: December 15, 2020

Note from the Chief Financial Officer/City Treasurer

Due to the public health emergency related to the COVID-19 pandemic, the State of California issued a Statewide “Stay-at-Home” directive (EO N-33-20) on March 19, 2020. At the time of this directive, economic forecasting models estimated revenue shortfalls at \$9.9 million for FY 2020/21. The City Council approved a budget plan to reduce revenues and expenditures accordingly in order to comply with our budget and fiscal policies. As we are now six months into the pandemic for this reporting period, the actual revenues have been negatively impacted, but not as severe as economist had originally thought.

The economic climate is ever changing during this pandemic due to constant changing of the opening/closing guidelines based on the number of positive cases that are continuing to increase. For these reasons, the City must remain optimistically cautious and only increase revenues when they are actually received. Staff will continue to monitor and request quarterly revenue adjustment based on this approach. These revenue adjustments will be offset by replenishing our expenditure levels accordingly.

INTRODUCTION

On May 7, 2019, the City Council adopted the Two-Year Operating Budget for Fiscal Years (FY) 2019/20 – 2020/21. During the two-year budget period, the City Council will be kept apprised of the City’s financial condition through the process of First Quarter, Mid-Year Budget and Third Quarter Reviews. This ongoing process ensures a forum to look at expenditure and revenue deviations from the estimates made in the budget document. Additionally, any significant changes in projected revenue or unanticipated expenditures that may occur will be shared with the City Council.

This report provides a review of the unaudited financial results at the First Quarter of FY 2020/21 (July 2019 – September 2020, 25% of the fiscal year).

CITYWIDE OPERATING EXPENDITURE SUMMARY

The following table contains a summary of the adopted budget, amended budget and the First Quarter Year expenditures. The totals represent each major fund type and component unit of the City.

Table 1. Citywide Operating Expenditures

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Fund/Component Unit				
General Fund	\$ 116,773,613	\$ 111,560,450	\$ 25,184,906	22.6%
Community Services District (CSD)	21,936,223	22,153,170	3,912,868	17.7%
Successor Agency	5,089,728	5,089,728	182,543	3.6%
Housing Fund	250,000	1,452,000	1,233,117	84.9%
Special Revenue Funds	41,270,569	85,816,209	8,925,713	10.4%
Capital Projects Funds	5,935,000	28,757,564	1,623,653	5.6%
Electric Utility Funds	32,751,206	42,066,082	10,326,450	24.5%
Internal Service Funds	10,013,938	12,958,397	3,124,553	24.1%
Debt Service Funds	4,327,834	4,327,834	-	0.0%
Total	\$ 238,348,111	\$ 314,181,434	\$ 54,513,803	17.4%

Actions taken by the City Council subsequent to the May 7, 2019 adoption of the two-year budget and included in the Amended Budget are:

- Throughout the fiscal year there are also budget amendments to reflect the acceptance of grants and adjustments to contractual services and material/supplies. The individual amendments are reviewed as part of separate City Council agenda items.
- The majority of this First Quarter update will focus on the General Fund, as it supports all basic services provided to City residents. Highlights for other key component funds will be discussed at a summary level as well.
- Due to the public health emergency related to the COVID-19 pandemic, the State of California issued a Statewide "Stay-at-Home" directive (EO N-33-20) on March 19, 2020. At the time of this directive, economic forecasting models estimated revenue shortfalls at \$9.9 million for FY 2020/21. The City Council approved a budget plan to reduce revenues and expenditures accordingly in order to comply with our budget and fiscal policies

GENERAL FUND OPERATING

Table 2. General Fund Operations

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Revenues:				
Taxes:				
Property Tax	\$ 16,553,200	\$ 16,637,300	\$ -	0.0%
Property Tax in-lieu	21,290,000	21,290,000	-	0.0%
Utility Users Tax	16,100,000	15,717,540	3,068,872	19.5%
Sales Tax	21,635,000	17,023,010	1,821,474	10.7%
Other Taxes	15,915,000	12,627,832	1,401,932	11.1%
Licenses & Permits	5,153,936	4,973,535	1,126,086	22.6%
Intergovernmental	313,000	2,891,550	1,470,043	50.8%
Charges for Services	13,045,435	11,613,657	3,090,294	26.6%
Use of Money & Property	5,097,806	5,122,806	982,818	19.2%
Fines & Forfeitures	604,500	301,343	79,454	26.4%
Miscellaneous	165,600	165,600	62,632	37.8%
Total Revenues	\$ 115,873,477	\$ 108,364,173	\$ 13,103,605	12.1%
Expenditures:				
Personnel Services	\$ 25,662,073	\$ 23,760,043	\$ 4,767,259	20.1%
Contractual Services	78,868,243	74,360,525	16,008,083	21.5%
Material & Supplies	2,097,824	3,993,528	644,870	16.1%
General Government	-	-	-	-
Debt Service	-	-	-	-
Fixed Charges	5,933,338	4,934,910	1,318,895	26.7%
Fixed Assets	50,000	57,546	48,944	85.1%
Total Expenditures	\$ 112,611,478	\$ 107,106,552	\$ 22,788,050	21.3%
<i>Excess (Deficiency) of Revenues Over (Under) Expenditures</i>	\$ 3,261,999	\$ 1,257,621	\$ (9,684,445)	
Transfers:				
Transfers In	\$ 913,836	\$ 2,974,921	\$ 12,501	0.4%
Transfers Out	4,162,135	4,453,898	2,396,856	53.8%
Net Transfers	\$ (3,248,299)	\$ (1,478,977)	\$ (2,384,355)	
Total Revenues & Transfers In	\$ 116,787,313	\$ 111,339,094	\$ 13,116,106	11.8%
Total Expenditures & Transfers Out	116,773,613	111,560,450	25,184,906	22.6%
Net Change of Fund Balance	\$ 13,700	\$ (221,356)	\$ (12,068,800)	

General Fund Operating Revenues

The General Fund is comprised of several revenue types. However, the main sources include property tax, utility users tax, and sales tax. Each of these is affected by different economic activity cycles and pressures.

Table 3. General Fund Operating Revenues

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Revenues:				
Taxes:				
Property Tax	\$ 16,553,200	\$ 16,637,300	\$ -	0.0%
Property Tax in-lieu	21,290,000	21,290,000	-	0.0%
Utility Users Tax	16,100,000	15,717,540	3,068,872	19.5%
Sales Tax	21,635,000	17,023,010	1,821,474	10.7%
Other Taxes	15,915,000	12,627,832	1,401,932	11.1%
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Intergovernmental	313,000	2,891,550	1,470,043	50.8%
Charges for Services	13,045,435	11,613,657	3,090,294	26.6%
Use of Money & Property	5,097,806	5,122,806	982,818	19.2%
Fines & Forfeitures	604,500	301,343	79,454	26.4%
Miscellaneous	165,600	165,600	62,632	37.8%
Total Revenues	\$ 115,873,477	\$ 108,364,173	\$ 13,103,605	12.1%

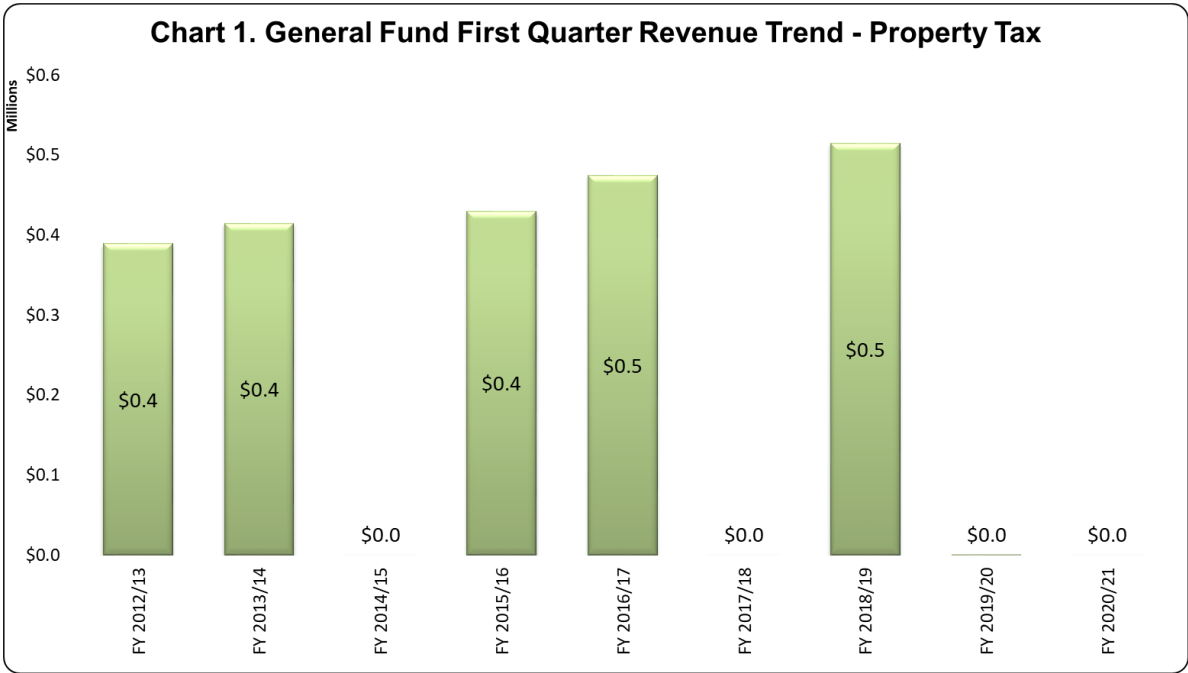
Property Taxes/Property Taxes In-Lieu

Property taxes were budgeted to increase by 2.7% from the FY 2019/20 Amended Budget. The annual schedule of property tax payments from the County of Riverside will provide payments to the City based on the following estimated schedule:

Secured Property Tax Payment Dates

Settlement 1	January
Settlement 2	May
Settlement 3	August
Teeter Settlement	October

Based on historical averages of actual receipts, the City is estimated to receive up to 1% of the budgeted property tax revenue through First Quarter. The City has currently received 0% through First Quarter. Property taxes will continue to be monitored as property valuations may adjust through the year based on property sales and assessment appeals filed with the County.

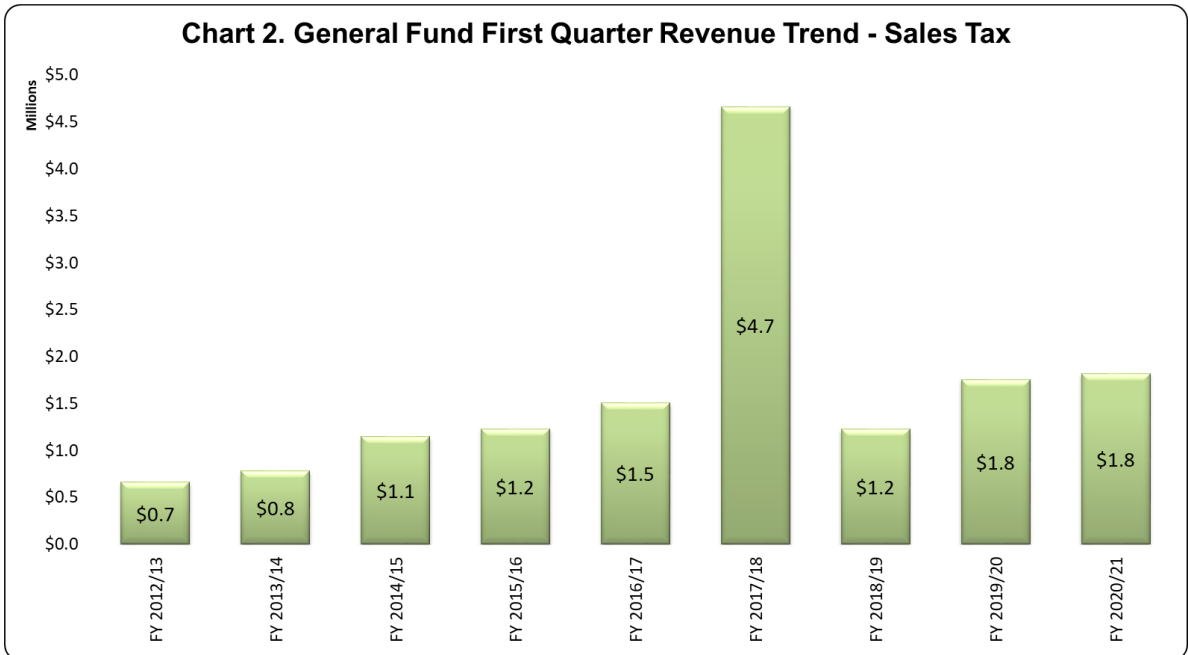


Note: FYs 2014/15, 2017/18, 2019/20 and 2020/21 did not receive any property tax revenues in the first quarter.

Sales Taxes

Sales taxes were budgeted to increase by 4% from the FY 2019/20 Amended Budget. Sales tax receipts will need to be continually monitored through the year to determine if current trends begin to plateau or begin to decrease.

Based on historical averages of actual receipts, the City is estimated to receive 9% of the budgeted sales tax revenue through First Quarter. The City has currently received 11% through d First Quarter.

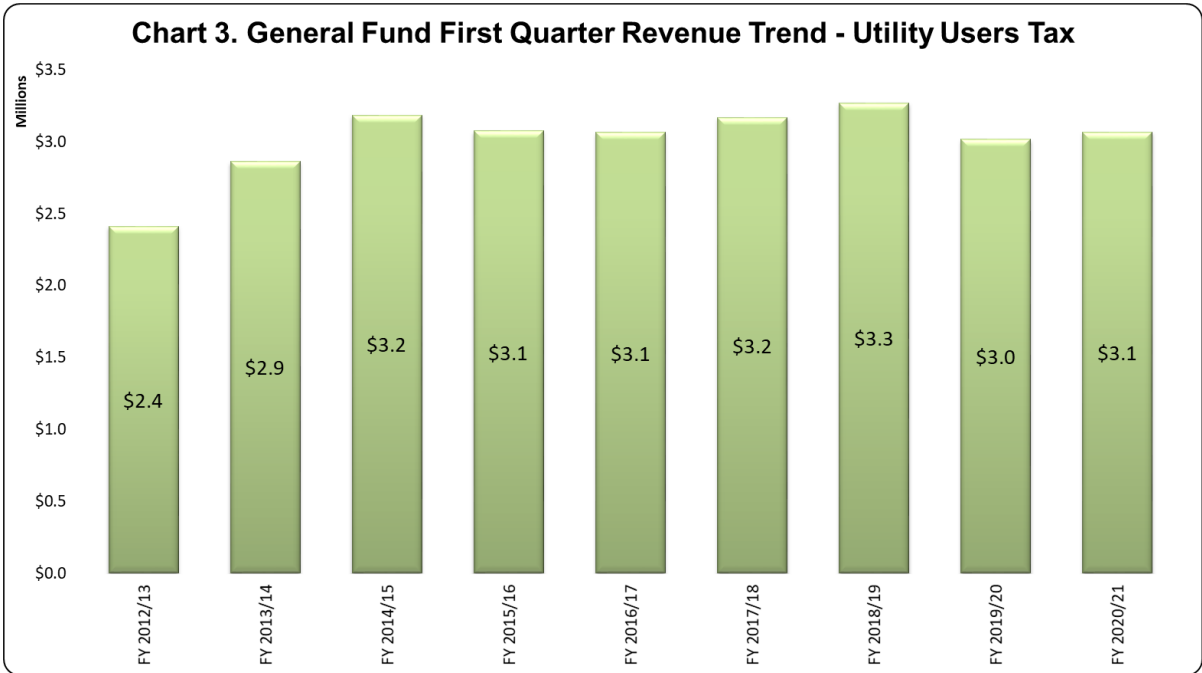


Note: For FY 2017/18, the reversal of revenues accrued to the prior fiscal year in accordance with the Government Accounting Standards Board (GASB) did not occur until the second quarter.

Utility Users Tax

Utility Users taxes were budgeted to increase less than 1% from the FY 2019/20 Amended Budget. This projection is primarily due to competitive forces within the communications markets. Both the wireless and wired markets experienced downturns year over year. Based on our discussions with utility tax experts, there are a couple of causes for this trend. First is competition and bundling practices within the market as more small players continue to join the market. Second is the migration of customers from contract plans to prepaid plans.

Based on historical averages of actual receipts, the City is estimated to receive 19% of the budgeted utility users tax revenue through First Quarter. The City has currently received 19.5% through First Quarter.

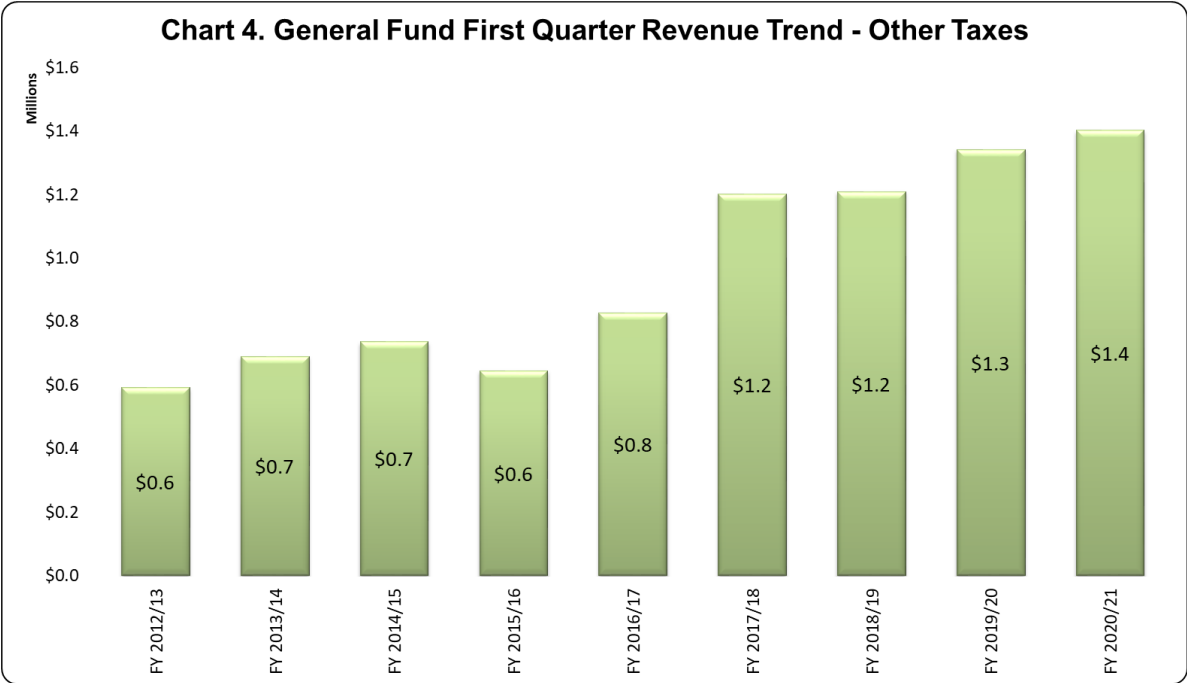


Other Taxes

Other taxes are primarily composed of Business Gross Receipts, Transient Occupancy Tax, Documentary Transfer Tax, and Franchise Fees. Collectively, other taxes were budgeted to increase 7.7% from the FY 2019/20 Amended Budget.

Based on historical averages of actual receipts, the City is estimated to receive 9% of the budgeted Other Taxes revenue through First Quarter. The City has currently received 11% through First Quarter.

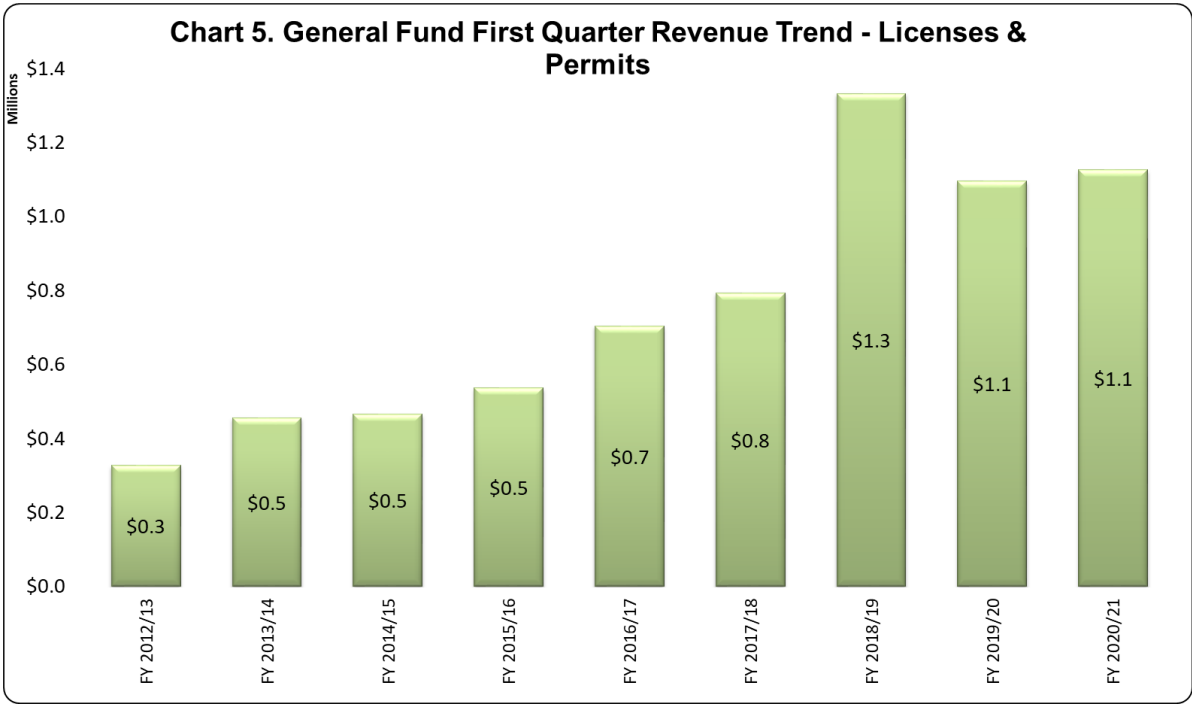
Attachment: FY20-21 First Qtrr Financial Rep (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW AND APPROVAL OF THE FIRST



Licenses & Permits

Licenses & Permits are primarily composed of Business and Animal Licenses, along with Building, Electrical, Mechanical, Plumbing and other permits. Collectively, Licenses & Permits were budgeted to decrease by 4% from the FY 2019/20 Amended Budget. This increase reflects increases due to Commercial Cannabis Business Permits. This budget will be monitored for a possible future increase.

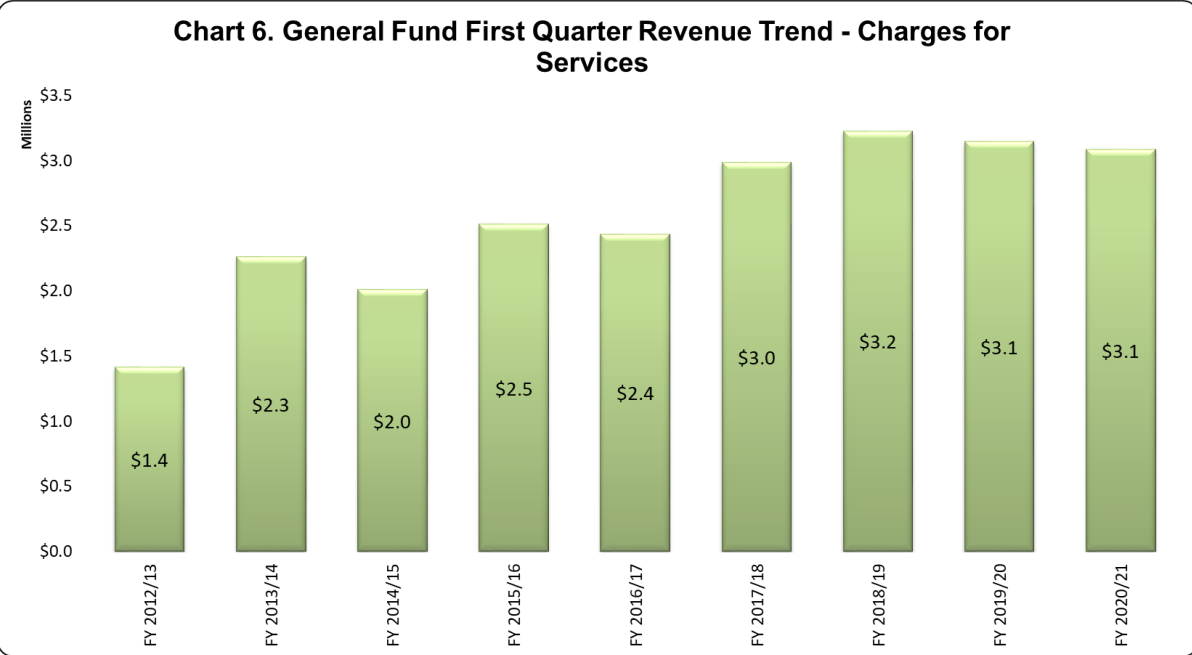
Based on historical averages of actual receipts, the City is estimated to receive 24% of the budgeted Licenses & Permits revenue through First Quarter. The City has currently received 23% through First Quarter. The collection rate is related primarily to building and business license permit activities.



Charges for Services

Charges for Services are primarily composed of Plan Check Fees, Inspection Fees, Administrative Charges to other funds, and Parking Control Fines. Collectively, Charges for Services were conservatively budgeted to decrease by 2% from the FY 2019/20 Amended Budget.

Based on historical averages of actual receipts, the City is estimated to receive 23% of the budgeted Charges for Services revenue through First Quarter. The City has currently received 27% through First Quarter.



Use of Money and Property

Investment income continues to remain low due to extremely low rates of return for fixed income investments, but has increased over the last year due to actions taken by the Federal Open Market Committee (FOMC). The investments achieved a Yield to Maturity (YTM) for September 2020 of 1.87%. In addition, the City maintained funds in the State Local Agency Investment Fund Pool (LAIF) with a YTM of .69%. In the first three months, the FOMC decreased the federal funds target rate by 225 basis points to 0.0% - 0.25%. This is a very low rate of return compared to historical experience, but is indicative of how investment income is performing everywhere, which is the reason the City utilizes the active management approach.

General Fund Expenditures

Expenditures are being spent in-line with prior year expenditures. Each Department's activities will be monitored throughout the year as they may be impacted by different operational activities and project timelines.

Table 4. General Fund Expenditures

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Department				
City Council	\$ 1,037,483	\$ 1,112,313	\$ 143,881	12.9%
City Clerk	1,054,060	855,366	110,218	12.9%
City Manager	2,039,696	1,814,639	318,329	17.5%
City Attorney	1,099,482	875,953	200,435	22.9%
Community Development	10,666,576	9,364,689	1,750,526	18.7%
Economic Development	2,464,235	2,188,524	332,627	15.2%
Financial & Management Services	11,818,382	12,501,344	2,176,684	17.4%
Public Works	7,771,229	7,636,732	1,659,176	21.7%
Non-Departmental	5,247,135	6,001,844	3,513,360	58.5%
Non-Public Safety Subtotal	\$ 44,758,786	\$ 42,351,404	\$ 10,206,226	24.1%
Public Safety				
Police	\$ 47,997,631	\$ 45,460,175	\$ 9,370,939	20.6%
Fire	24,017,196	23,748,871	5,607,742	23.6%
Public Safety Subtotal	\$ 72,014,827	\$ 69,209,046	\$ 14,978,681	21.6%
Total	\$ 116,773,613	\$ 111,560,450	\$ 25,184,906	

OTHER KEY FUNDS

The following summaries describe other major funds in the City.

Moreno Valley Community Services District

The Moreno Valley Community Services District (CSD) was formed by the voters in 1984 to collect fees and certain taxes to provide an array of services including parks, recreation and community services, streetlights, landscaping and ongoing maintenance. The CSD provides these services through separate “zones” that define the services that are provided.

For certain zones, the primary revenue source used to provide services to properties is parcel fees or taxes levied on properties via their annual tax bill. Proposition 218, passed by California voters in November 1996, and has posed a serious challenge to managing the future operation of the CSD zones. Prop. 218 requires any revenue increase to be addressed through a voting process by affected property owners. For a period following the initial implementation of Prop. 218, the CSD was successful in receiving approval for some new or increased revenues. There were also revenue increases due to the growth of developed parcels within the zones. However, due to cost increases that exceed any offsetting increases in the revenues over the past years, and the recent economic downturn slowing new parcel growth, property owners have been resistant to efforts to fully fund service levels.

Table 5. CSD Operations

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Revenues:				
Taxes:				
Property Tax	\$ 5,499,288	\$ 5,499,288	\$ -	0.0%
Other Taxes	6,719,200	6,719,200	-	0.0%
Charges for Services	5,698,574	5,698,574	70,166	1.2%
Use of Money & Property	1,022,020	1,056,530	161,252	15.3%
Fines & Forfeitures	30,000	30,000	-	0.0%
Miscellaneous	22,010	527,735	232	0.0%
Transfers In	2,040,727	1,612,490	375,456	23.3%
Total Revenues	\$ 21,031,819	\$ 21,143,817	\$ 607,106	2.9%
Expenditures:				
Library Services Fund (5010)	\$ 2,708,319	\$ 2,741,331	\$ 718,110	26.2%
Zone A Parks Fund (5011)	10,258,968	9,865,668	1,868,802	18.9%
LMD 2014-01 Residential Street Lighting Fund (5012)	1,516,919	1,516,919	264,005	17.4%
Zone C Arterial Street Lighting Fund (5110)	875,414	875,414	146,063	16.7%
Zone D Standard Landscaping Fund (5111)	1,426,105	1,510,545	187,950	12.4%
Zone E Extensive Landscaping Fund (5013)	289,398	317,350	40,280	12.7%
5014 LMD 2014-02	2,832,609	3,056,600	367,535	12.0%
Zone M Median Fund (5112)	468,229	569,853	45,650	8.0%
CFD No. 1 (5113)	1,482,660	1,621,888	268,090	16.5%
Zone S (5114)	77,602	77,602	6,383	8.2%
Total Expenditures	\$ 21,936,223	\$ 22,153,170	\$ 3,912,868	17.7%
Net Change or Adopted Use of Fund Balance	\$ (904,404)	\$ (1,009,353)	\$ (3,305,762)	

Community Services District Zone A – Parks & Community Services

The largest Zone within the CSD is Zone A. It accounts for the administration and maintenance of the Parks & Community Services facilities and programs. Funding sources for these services come from a combination of property taxes, fees for service and smaller amounts from other City funds.

Table 6. CSD Zone A Operations

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Revenues:				
Taxes:				
Property Tax	\$ 2,812,910	\$ 2,812,910	\$ -	0.0%
Other Taxes	4,977,000	4,977,000	-	0.0%
Charges for Services	1,148,683	1,156,483	41,422	3.6%
Use of Money & Property	925,659	947,718	146,135	15.4%
Miscellaneous	18,000	42,700	-	0.0%
Transfers In	528,237	12,678	-	0.0%
Total Revenues	\$ 10,410,489	\$ 9,949,489	\$ 187,557	1.9%
Expenditures:				
35010 Parks & Comm Svcs - Admin	\$ 750,716	\$ 703,732	\$ 134,597	19.1%
35210 Park Maintenance - General	3,851,191	3,608,683	794,211	22.0%
35211 Contract Park Maintenance	467,121	463,557	69,067	14.9%
35212 Park Ranger Program	361,567	333,840	58,954	17.7%
35213 Golf Course Program	490,200	423,031	102,167	24.2%
35214 Parks Projects	223,598	207,256	45,587	22.0%
35310 Senior Program	588,633	561,281	114,416	20.4%
35311 Community Services	500,997	473,402	56,713	12.0%
35312 Community Events	285,606	262,455	53,720	20.5%
35313 Conf & Rec Cntr	420,626	643,818	92,535	14.4%
35314 Conf & Rec Cntr - Banquet	377,698	358,338	72,164	20.1%
35315 Recreation Programs	1,210,541	1,149,566	197,481	17.2%
35317 July 4th Celebration	111,990	110,190	308	0.3%
35318 Sports Programs	587,584	537,999	63,566	11.8%
35319 Towngate Community Center	30,900	28,520	4,466	15.7%
35320 Amphitheater	-	-	143	0.0%
95011 Non-Dept Zone A Parks	-	-	8,710	0.0%
Total Expenditures	\$ 10,258,968	\$ 9,865,668	\$ 1,868,802	18.9%
Net Change or Adopted Use of Fund Balance	\$ 151,521	\$ 83,821	\$ (1,681,245)	

Electric Utility

The Moreno Valley Utility (MVU) manages the operation, maintenance and business planning of the City's electric utility. MVU's basic purpose is to purchase and distribute electricity to customers in newly developed areas of the City. The City began serving new customers in February 2004, and now serves more than 6851 customers. As it reaches fiscal and operational maturity, MVU will continue to be a key component of the City's economic development strategy. The City Council has established special tiered rates for electric utility customers based upon factors such as the number of jobs created.

The main revenue source for this fund is derived from charges for services. The customer base includes residential, commercial and industrial customers. The growth in customer base will continue to provide for the ability to create rate stabilization and replacement reserve funding.

Table 7. MVU Operations

	FY 2020/21 Adopted Budget	FY 2020/21 Amended Budget	Actuals as of 9/30/20 (unaudited)	% of Amended Budget
Revenues:				
Taxes:				
Charges for Services	\$ 35,344,376	\$ 35,344,376	\$ 11,107,887	31.4%
Use of Money & Property	158,000	158,000	19,256	12.2%
Miscellaneous	152,500	152,500	1,491,757	978.2%
Total Revenues	\$ 35,654,876	\$ 35,654,876	\$ 12,618,900	35.4%
Expenditures:				
45510 Electric Utility - General	\$ 26,287,723	\$ 28,155,343	\$ 8,183,313	29.1%
45511 Public Purpose Program	1,702,376	1,531,857	378,909	24.7%
45512 SCE Served Street Lights	400,000	487,889	203,504	41.7%
80005 CIP - Electric Utility	-	7,529,886	712,108	9.5%
96010 Non-Dept Electric	-	-	12,144	0.0%
96030 Non-Dept 2005 Lease Revenue Bonds	2,109,081	2,109,081	836,473	39.7%
96021 Non-Dept 2016 Tax LRB of 07 Tax	829,775	829,775	-	0.0%
96031 Non-Dept 2013 Refunding 05 LRB	21,336	21,336	-	0.0%
96032 Non-Dept 2014 Refunding 2005 LRB	119,174	119,174	-	0.0%
96040 Non-Dept 2015 Taxable LRB	447,769	447,769	-	0.0%
96050 Non-Dept 2018 Streetlight Fin	833,972	833,972	-	0.0%
Total Expenditures	\$ 32,751,206	\$ 42,066,082	\$ 10,326,450	24.5%
Net Change or Adopted Use of Fund Balance	\$ 2,903,670	\$ (6,411,206)	\$ 2,292,450	

MVU's revenues and expenses will fluctuate annually based on energy demands.

SUMMARY

The City of Moreno Valley has experienced certain levels of growth and continued to maintain a structurally balanced Budget without the use of reserves.

The economic climate is ever changing during this pandemic due to constant changing of the opening/closing guidelines based on the number of positive cases that are continuing to increase. For these reasons, the City must remain optimistically cautious and only increase revenues when they are actually received. Staff will continue to monitor and request quarterly revenue adjustment based on this approach. These revenue adjustments will be offset by replenishing our expenditure levels accordingly.

RESOLUTION NO. 2020-XX

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING THE REVISED OPERATING AND CAPITAL BUDGETS FOR FISCAL YEAR 2020/21

WHEREAS, the City Council approved the Operating and Capital Budgets for the City for Fiscal Year 2020/21, a copy of which, as may have been amended by the City Council, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the City Council approves amendments to the budgets throughout the fiscal year and such prior amendments are reflected within the current amended budget and further ratified as part of the adoption of the quarterly budget amendments; and

WHEREAS, the City Manager has heretofore submitted to the City Council proposed amendments to the Operating and Capital Budgets for the City for Fiscal Year 2020/21, a copy of which, as may have been amended by the City Council, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain estimates of the services, activities and projects comprising the budget, and contains expenditure requirements and the resources available to the City; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain the estimates of uses of fund balance as required to stabilize the delivery of City services during periods of operational deficits; and

WHEREAS, the City Council has made such revisions to the proposed amended Operating and Capital Budgets as so desired; and

WHEREAS, the amended Operating and Capital Budgets, as herein approved, will enable the City Council to make adequate financial plans and will ensure that City officers can administer their respective functions in accordance with such plans.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

1. The proposed amendments to the Operating and Capital Budgets, as Exhibit A to this Resolution and as on file in the Office of the City Clerk, and as may have been amended by the City Council, are hereby approved and adopted as the annual Operating and Capital Budgets of the City of Moreno Valley for Fiscal Year 2020/21.

1
Resolution No. 2020-XX
Date Adopted: December 15, 2020

- 2. The Proposed Amendments to City Position Summary included within the staff report and contained in the City Position Summary attached as Attachment 6 and on file in the Office of the City Clerk, and as may have been amended by the City Council, is hereby adopted as part of the Approved City Position Summary of the City of Moreno Valley for Fiscal Year 2020/21.
- 3. The amounts of proposed expenditures, which include the uses of fund balance specified in the approved budget, are hereby appropriated for the various budget programs and units for said fiscal year.
- 4. Within fifteen (15) days after the adoption of this Resolution, the City Clerk shall certify to the adoption hereof and, as so certified, cause a copy to be posted in at least three (3) public places within the City.

BE IT FURTHER RESOLVED that this Resolution shall take effect immediately upon its adoption.

APPROVED AND ADOPTED this 15th day of December 2020.

Mayor of the City of Moreno Valley

ATTEST:

City Clerk

APPROVED AS TO FORM:

City Attorney

2
Resolution No. 2020-XX
Date Adopted: December 15, 2020

Attachment: City Resolution 2020-XX (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW AND APPROVAL OF THE FIRST

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California, do hereby certify that Resolution No. 2020-XX was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting thereof held on the 15th day of December, 2020 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

CITY CLERK

(SEAL)

Resolution No. 2020-XX³
Date Adopted: December 15, 2020

RESOLUTION NO. CSD 2020-XX

A RESOLUTION OF THE MORENO VALLEY COMMUNITY SERVICES DISTRICT OF THE CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING THE REVISED OPERATING AND CAPITAL BUDGETS FOR FISCAL YEAR 2020/21

WHEREAS, the CSD Board approved the Operating and Capital Budgets for the City for Fiscal Year 2020/21, a copy of which, as may have been amended by the CSD Board, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the CSD Board approves amendments to the budgets throughout the fiscal year and such prior amendments are reflected within the current amended budget and further ratified as part of the adoption of the quarterly budget amendments; and

WHEREAS, the City Manager has heretofore submitted to the President and Board Members of the Moreno Valley Community Services District proposed amendments to the Operating and Capital Budgets for the District for Fiscal Year 2020/21, a copy of which, as may have been amended by the District's Board of Directors, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain estimates of the services, activities and projects comprising the budget, and contain expenditure requirements and the resources available to the Community Services District; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain the estimates of uses of fund balance as required to stabilize the delivery of CSD services during periods of operational deficits; and

WHEREAS, the President and Board of Directors have made such revisions to the proposed amended Operating and Capital Budgets as so desired; and

WHEREAS, the amended Operating and Capital Budgets, as herein approved, will enable the Community Services District to make adequate financial plans and will ensure that District officers can administer their respective functions in accordance with such plans.

NOW, THEREFORE, THE MORENO VALLEY COMMUNITY SERVICES DISTRICT OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

1. The proposed amendments to the Operating and Capital Budgets, as Exhibit

1
Resolution No. CSD 2020-XX
Date Adopted: December 15, 2020

A to this Resolution and as on file in the Office of the City Clerk, and as may have been amended by the Community Services District’s Board of Directors, is hereby approved and adopted as the annual Operating and Capital Budgets of the Moreno Valley Community Services District for the Fiscal Year 2020/21.

- 2. The amounts of proposed expenditures, which include the uses of fund balance specified in the approved budget, are hereby appropriated for the various budget programs and units for said fiscal year.
- 3. Within fifteen (15) days after the adoption of this Resolution, the City Clerk shall certify to the adoption hereof and, as so certified, cause a copy to be posted in at least three (3) public places within the City.

BE IT FURTHER RESOLVED that this Resolution shall take effect immediately upon its adoption.

APPROVED AND ADOPTED this 15th day of December, 2020.

 Mayor of the City of Moreno Valley,
 Acting in the capacity of President of the
 Moreno Valley Community Services District

ATTEST:

 City Clerk, acting in the capacity of
 Secretary of the Moreno Valley
 Community Services District

APPROVED AS TO FORM:

 City Attorney, acting in the capacity
 of General Counsel of the Moreno
 Valley Community Services District

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, Secretary of the Moreno Valley Community Services District, Moreno Valley, California do hereby certify that Resolution No. CSD 2020-XX was duly and regularly adopted by the Board of Directors of the Moreno Valley Community Services District at a regular meeting held on the 15th day of December, 2020 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Board members, Vice-President and President)

SECRETARY

(SEAL)

3
Resolution No. CSD 2020-XX
Date Adopted: December 15, 2020

RESOLUTION NO. HA 2020-XX
 A RESOLUTION OF THE HOUSING AUTHORITY OF THE
 CITY OF MORENO VALLEY, CALIFORNIA, ADOPTING
 THE REVISED OPERATING AND CAPITAL BUDGETS
 FOR FISCAL YEAR 2020/21

WHEREAS, the Housing Authority approved the Operating and Capital Budgets for the City for Fiscal Year 2020/21, a copy of which, as may have been amended by the Housing Authority, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the Housing Authority approves amendments to the budgets throughout the fiscal year and such prior amendments are reflected within the current amended budget and further ratified as part of the adoption of the quarterly budget amendments; and

WHEREAS, the City Manager has heretofore submitted to the Housing Authority proposed amendments to the Operating and Capital Budgets for the District for Fiscal Year 2020/21, a copy of which, as may have been amended by the Housing Authority, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain estimates of the services, activities and projects comprising the budget, and contain expenditure requirements and the resources available to the Housing Authority; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain the estimates of uses of fund balance as required to stabilize the delivery of Housing Authority services during periods of operational deficits; and

WHEREAS, the Housing Authority have made such revisions to the proposed amended Operating and Capital Budgets as so desired; and

WHEREAS, the amended Operating and Capital Budgets, as herein approved, will enable the Housing Authority to make adequate financial plans and will ensure that District officers can administer their respective functions in accordance with such plans.

NOW, THEREFORE, THE HOUSING AUTHORITY OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

1. The proposed amendments to the Operating and Capital Budgets, as Exhibit A to this Resolution and as on file in the Office of the City Clerk, and as may have been amended by the Housing Authority, is hereby approved and adopted as the annual Operating and Capital Budgets of the Moreno Valley Housing Authority for the Fiscal Year 2020/21.

1
 Resolution No. HA 2020-XX
 Date Adopted: December 15, 2020

- 2. The amounts of proposed expenditures, which include the uses of fund balance specified in the approved budget, are hereby appropriated for the various budget programs and units for said fiscal year.
- 3. Within fifteen (15) days after the adoption of this Resolution, the City Clerk shall certify to the adoption hereof and, as so certified, cause a copy to be posted in at least three (3) public places within the City.

BE IT FURTHER RESOLVED that this Resolution shall take effect immediately upon its adoption.

APPROVED AND ADOPTED this 15th day of December, 2020.

 Mayor of the City of Moreno Valley,
 Acting in the capacity of Chairman of the
 Housing Authority

ATTEST:

 City Clerk, acting in the capacity of
 Secretary of the Housing Authority

APPROVED AS TO FORM:

 City Attorney, acting in the capacity of
 General Counsel of the Housing Authority

2
 Resolution No. HA 2020-XX
 Date Adopted: December 15, 2020

Attachment: Housing Authority Resolution 2020-XX (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW AND APPROVAL OF THE

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, Secretary of the Housing Authority, Moreno Valley, California do hereby certify that Resolution No. HA 2020-XX was duly and regularly adopted by the Commissioners of the Housing Authority at a regular meeting held on the, 15th day of December, 2020 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Commissioners, Vice Chairperson and Chairperson)

SECRETARY

(SEAL)

Resolution No. HA 2020-XX³
Date Adopted: December 15, 2020

Attachment: Housing Authority Resolution 2020-XX (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW AND APPROVAL OF THE

RESOLUTION NO. SA 2020-XX
 A RESOLUTION OF THE SUCCESSOR AGENCY TO THE
 COMMUNITY REDEVELOPMENT AGENCY OF THE CITY
 OF MORENO VALLEY, CALIFORNIA, ADOPTING THE
 REVISED OPERATING AND CAPITAL BUDGETS FOR
 FISCAL YEAR 2020/21

WHEREAS, the Mayor and City Council of the City of Moreno Valley as Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley approved the Operating and Capital Budgets for the City for Fiscal Year 2020/21, a copy of which, as may have been amended by the Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the Mayor and City Council of the City of Moreno Valley as Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley approves amendments to the budgets throughout the fiscal year and such prior amendments are reflected within the current amended budget and further ratified as part of the adoption of the quarterly budget amendments; and

WHEREAS, the City Manager has heretofore submitted to the Mayor and City Council of the City of Moreno Valley as Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley proposed amendments to the Operating and Capital Budgets for the Agency for Fiscal Year 2020/21, a copy of which, as may have been amended by the Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley, is on file in the Office of the City Clerk and is available for public inspection; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain estimates of the services, activities and projects comprising the budget, and contain expenditure requirements and the resources available to the Successor Agency; and

WHEREAS, the said proposed amendments to the Operating and Capital Budgets contain the estimates of uses of fund balance as required to stabilize the delivery of Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley services during periods of operational deficits; and

WHEREAS, the Mayor and City Council have made such revisions to the proposed amended Operating and Capital Budgets as so desired; and

WHEREAS, the amended Operating and Capital Budgets, as herein approved, will enable the Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley to make adequate financial plans and will ensure that City officers can administer their respective functions in accordance with such plans.

1
 Resolution No. SA 2020-XX
 Date Adopted: December 15, 2020

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MORENO VALLEY, CALIFORNIA, DOES HEREBY RESOLVE AS FOLLOWS:

1. The proposed amendments to the Operating and Capital Budgets, as Exhibit A to this Resolution and as on file in the Office of the City Clerk, and as may have been amended by the Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley, is hereby approved and adopted as the annual Operating and Capital Budgets of the Moreno Valley Successor Agency to the Community Redevelopment Agency of the City of Moreno Valley for Fiscal Year 2020/21.
2. The amounts of proposed expenditures, which include the uses of fund balance specified in the approved budget, are hereby appropriated for the various budget programs and units for said fiscal year.
3. Within fifteen (15) days after the adoption of this Resolution, the City Clerk shall certify to the adoption hereof and, as so certified, cause a copy to be posted in at least three (3) public places within the City.

BE IT FURTHER RESOLVED that this Resolution shall take effect immediately upon its adoption.

APPROVED AND ADOPTED this 15th day of December, 2020.

Mayor of the City of Moreno Valley

ATTEST:

City Clerk

APPROVED AS TO FORM:

City Attorney

RESOLUTION JURAT

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF MORENO VALLEY)

I, Pat Jacquez-Nares, City Clerk of the City of Moreno Valley, California do hereby certify that Resolution No. SA 2020-XX was duly and regularly adopted by the City Council of the City of Moreno Valley at a regular meeting held on the 15th day of December, 2020, by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

(Council Members, Mayor Pro Tem and Mayor)

SECRETARY

(SEAL)

**CITY OF MORENO VALLEY
GENERAL FUND
FY 2020/21 Proposed Amendments**

Department	Fund	Account Description	General Ledger Account	Project	Fiscal Year 2020/21 Amended Budget	Proposed Amendment	Revised Budget	Description - Proposed Adjustment
Financial & Management Services	1010	Sales Tax - General	1010-99-99-91010-402000		\$ 17,148,010	\$ 104,016	\$ 17,252,026	Revenue projection based on HdL.
Financial & Management Services	1010	Transfers in - from ZONE "E" EXT LDSC FUND	1010-99-99-91010-805013		-	29,958	29,958	Transfer for Council approved levy.
REVENUE TOTAL					\$ 17,148,010	\$ 133,974	\$ 17,281,984	
Community Development	1010	Computer Hardware - Non-Cap <\$5k - New	1010-20-27-20211-631010		\$ -	\$ 50,000	\$ 50,000	Budgeting for expected expenses.
Financial & Management Services	1010	Transfers to - OTHER GRANTS FUND	1010-99-99-91010-902300		-	30,000	30,000	Transfer to support the Senior Eats program.
Financial & Management Services	1010	Transfers to LMD 2014-02	1010-99-99-91010-905014		210,000	(8,682)	201,318	Adjustment to be consistent with Engineer's report as approved by Council.
Financial & Management Services	1010	Vacancy Factor	1010-99-99-91010-680414		512,946	(672,600)	(159,654)	Personnel savings from position changes approved with this staff report.
Financial & Management Services	1010	Professional Svcs - Other	1010-99-99-91010-620299		-	500,000	500,000	Additional street cleanups.
Financial & Management Services	1010	Salaries-Overtime	1010-16-16-16210-611210		-	5,000	5,000	Based on actual trends.
Financial & Management Services	1010	Communications	1010-20-26-20110-620410		15,020	8,900	23,920	Radios for Public Safety Enterprise Communication.
EXPENSES TOTAL					\$ 737,966	\$ (87,382)	\$ 650,584	

Attachment: Exhibit A - Proposed Amendments [Revision 1] (4212 : FISCAL YEAR 2020/21 FIRST

**CITY OF MORENO VALLEY
NON-GENERAL FUND
FY 2020/21 Proposed Amendments**

Department	Fund	Account Description	General Ledger Account	Project	Fiscal Year 2020/21 Amended Budget	Proposed Amendment	Revised Budget	Description - Proposed Adjustment
Financial & Management Services	2512	Fed Grant-Operating Revenue	2512-99-99-92512-485000		\$ 3,148,650	\$ 146,975	\$ 3,295,625	Budgeting for expected revenue.
Financial & Management Services	2300	Transfers in - from GENERAL FUND	2300-99-99-92300-801010		-	30,000	30,000	Transfer to support the Senior Eats program.
Financial & Management Services	2300	Fed Grant-Operating Revenue	2300-50-92-75016-485000		59,783	750,000	809,783	Transfer to support the Senior Eats program.
Financial & Management Services	2300	State Grant-Operating Revenue	2300-50-92-75016-486000		19,928	220,000	239,928	Transfer to support the Senior Eats program.
Financial & Management Services	2300	Other Grant-Operating Revenue	2300-30-21-72507-489000		-	25,000	25,000	Grant - Financial Wellness Program.
Financial & Management Services	5014	Transfers in - from GENERAL FUND	5014-99-99-95014-801010		210,000	(8,682)	201,318	Adjustment to be consistent with Engineer's report as approved by Council.
Financial & Management Services	5012	Parcel Fees	5012-30-79-25703-500800		959,500	2,859	962,359	Adjust revenue to match applied fixed charges.
Financial & Management Services	5110	Special Taxes	5110-30-79-25703-404000		423,600	5,853	429,453	Adjust revenue to match applied fixed charges.
Financial & Management Services	5111	Parcel Fees	5111-30-79-25704-500800		1,186,000	(146,180)	1,039,820	Adjust revenue to match applied fixed charges.
Financial & Management Services	5112	Parcel Fees	5112-30-79-25719-500800		123,200	12,559	135,759	Adjust revenue to match applied fixed charges.
Financial & Management Services	5114	Parcel Fees	5114-30-79-25720-500800		64,300	(728)	63,572	Adjust revenue to match applied fixed charges.
Financial & Management Services	5014	Parcel Fees	5014-30-79-25721-500800		2,022,866	12,065	2,034,931	Adjust revenue to match applied fixed charges.
Financial & Management Services	2050	Special Taxes	2050-30-79-25722-404000		239,000	(47,170)	191,830	Adjust revenue to match applied fixed charges.
Financial & Management Services	4106	Transfers In - From SUCCESSOR AGENCY ADMIN FUND	4106-99-99-94106-804800		1,202,696	(1,202,696)	-	Adjustment in ROPS funding.
Financial & Management Services	5112	Transfers in - from CFD No. 2014-01	5112-99-99-95112-802050		1,990	2,840	4,830	Based on actual levies.
Parks & Community Services	5011	Rental Income	5011-50-58-35320-463000		-	41,805	41,805	Budgeting for expected revenue.
Parks & Community Services	2905	Transfers in - from PCS CAPITAL PROJ (PARK IMPRVMTS)	2905-99-95-92905-803015		-	8,938	8,938	Project 807 0026 50 57-3015 is completed and transferring in to Fund 2905.
Parks & Community Services	2905	Transfers in - from PCS CAPITAL PROJ (PARK IMPRVMTS)	2905-99-95-92905-803015		-	35,597	35,597	Project 807 0048-3015 is completed and transferring in balance to Fund 2905.
Parks & Community Services	2019	Transfers in - from PCS CAPITAL PROJ (QUIMBY)	2019-99-99-92019-803016		-	8,939	8,939	Project 807 0026-3016 is completed and transferring in balance to Fund 2919.
Parks & Community Services	2019	Transfers in - from PCS CAPITAL PROJ (QUIMBY)	2019-99-99-92019-803016		-	(7,089)	(7,089)	Project 807 0026-3016 is completed and transferring in balance to Fund 2919.
Financial & Management Services	6060	Transfers in - within cat ELECTRIC FUND	6060-99-99-96060-826010		-	634,000	634,000	2019 Lease Revenue Bonds.
Financial & Management Services	2008	Transfers in - from ZONE "E" EXT LDSC FUND	2008-99-99-92008-805013		-	60,440	60,440	Transfer for Council approved levy.
Financial & Management Services	5113	Transfers in - from ZONE "E" EXT LDSC FUND	5113-99-99-95113-805013		-	63,382	63,382	Transfer for Council approved levy.
Parks & Community Services	5011	P & R Advertising Sales	5011-50-58-35311-507320		-	7,500	7,500	MVU annual sponsorships to Parks & Community Services approved for 20-21.
Parks & Community Services	5011	P & R Advertising Sales	5011-50-58-35310-507320		-	4,200	4,200	MVU annual sponsorships to Parks & Community Services approved for 20-21.
Parks & Community Services	5011	Donations	5011-50-58-35312-580200		15,000	5,900	20,900	MVU annual sponsorships to Parks & Community Services approved for 20-21.
Parks & Community Services	5011	Donations	5011-50-58-35317-580200		-	1,300	1,300	MVU annual sponsorships to Parks & Community Services approved for 20-21.
Parks & Community Services	5011	Donations	5011-50-58-35318-580200		-	6,100	6,100	MVU annual sponsorships to Parks & Community Services approved for 20-21.

Attachment: Exhibit A - Proposed Amendments [Revision 1] (4212 : FISCAL YEAR 2020/21 FIRST

Department	Fund	Account Description	General Ledger Account	Project	Fiscal Year 2020/21 Amended Budget	Proposed Amendment	Revised Budget	Description - Proposed Adjustment
Parks & Community Services	5011	P & R Advertising Sales	5011-50-58-35311-507320		-	7,500	7,500	Economic Development annual sponsorships to Parks & Community Services approved for 20-21.
Public Works	2909	Transfers in - from FACILITY CONST FUND	2909-99-95-92909-803000		-	426,474	426,474	Project complete and transferring balance to Fund 2909.
Public Works	2902	Transfers in - from DIF TRAFFIC SIGNAL CAPITAL PRO	2902-99-95-92902-803302		-	190,468	190,468	Project complete and transferring balance to Fund 2902.
Public Works	2901	Dev Impact Fee: Comm-Regional	2901-99-95-92901-506040		-	24,402	24,402	Revised DIF revenue budgets based on historical trends.
Public Works	2902	Dev Impact Fee: Res-Single	2902-99-95-92902-506010		3,400	67,745	71,145	Revised DIF revenue budgets based on historical trends.
Public Works	2902	Dev Impact Fee: Comm-Regional	2902-99-95-92902-506040		-	16,581	16,581	Revised DIF revenue budgets based on historical trends.
Public Works	2911	Dev Impact Fee: Res-Single	2911-99-95-92911-506010		18,800	46,393	65,193	Revised DIF revenue budgets based on historical trends.
Public Works	2911	Dev Impact Fee: Res-Multiple	2911-99-95-92911-506020		1,800	8,020	9,820	Revised DIF revenue budgets based on historical trends.
Public Works	2911	Dev Impact Fee: Comm-Regional	2911-99-95-92911-506040		-	15,190	15,190	Revised DIF revenue budgets based on historical trends.
Public Works	2911	Dev Impact Fee: Indust-General	2911-99-95-92911-506060		-	27,220	27,220	Revised DIF revenue budgets based on historical trends.
REVENUE TOTAL					\$ 9,700,513	\$ 1,503,700	\$ 11,204,213	

Financial & Management Services	5014	Maint & Repair - Bldg & Ground	5014-30-79-79006-620910	SD LMD-Zone 01	\$ 166,000	\$ 18,000	\$ 184,000	Section change with net effect of zero.
Financial & Management Services	5014	Maint & Repair - Bldg & Ground	5014-30-79-79006-620910	SD LMD-Zone 02	166,000	95,850	261,850	Section change with net effect of zero.
Financial & Management Services	5014	Maint & Repair - Bldg & Ground	5014-30-79-79006-620910	SD LMD-Zone 03	166,000	52,150	218,150	Section change with net effect of zero.
Financial & Management Services	5014	CIP Other	5014-30-79-80006-720199		223,991	(166,000)	57,991	Section change with net effect of zero.
Financial & Management Services	5111	Maint & Repair - Bldg & Ground	5111-30-79-79006-620910		213,000	213,000	426,000	Section change with net effect of zero.
Financial & Management Services	5111	CIP Other	5111-30-79-80006-720199		84,440	(213,000)	(128,560)	Section change with net effect of zero.
Financial & Management Services	5112	Maint & Repair - Bldg & Ground	5112-30-79-79006-620910		133,000	133,000	266,000	Section change with net effect of zero.
Financial & Management Services	5112	CIP Other	5112-30-79-80006-720199		90,255	(133,000)	(42,745)	Section change with net effect of zero.
Financial & Management Services	4851	Expenditure Close to Bal	4851-99-99-94851-679010		-	(763,000)	(763,000)	Based on established debt service schedule.
Financial & Management Services	4852	Interest Expense	4852-99-99-94852-670410		1,506,930	(167,030)	1,339,900	Based on established debt service schedule.
Financial & Management Services	2512	CIP Other	2512-70-40-80003-720199		(96,975)	146,975	50,000	Budgeting for expected expenses, off-set by revenue.
Financial & Management Services	2300	Oper Mtrls - Other	2300-50-92-75016-630399		79,711	1,000,000	1,079,711	Transfer to support the Senior Eats program.
Financial & Management Services	2300	Professional Svcs - Other	2300-30-21-72507-620299		-	12,000	12,000	Grant - Financial Wellness Program.
Financial & Management Services	2300	Oper Suppl - Office	2300-30-21-72507-630210		-	500	500	Grant - Financial Wellness Program.
Financial & Management Services	2300	Oper Mtrls - Program Awards	2300-30-21-72507-630344		-	8,500	8,500	Grant - Financial Wellness Program.
Financial & Management Services	2300	Training & Travel	2300-30-21-72507-620510		-	4,000	4,000	Grant - Financial Wellness Program.
Financial & Management Services	5013	Transfers to GENERAL FUND	5013-99-99-95013-901010		-	29,958	29,958	Transfer for Council approved levy.
Financial & Management Services	5013	Transfers to STORM WATER MANAGEMENT	5013-99-99-95013-902008		-	60,440	60,440	Transfer for Council approved levy.

Attachment: Exhibit A - Proposed Amendments [Revision 1] (4212 : FISCAL YEAR 2020/21 FIRST

Department	Fund	Account Description	General Ledger Account	Project	Fiscal Year 2020/21 Amended Budget	Proposed Amendment	Revised Budget	Description - Proposed Adjustment
Financial & Management Services	5013	Transfers to CFD#1	5013-99-99-95013-905113		-	63,382	63,382	Transfer for Council approved levy.
Financial & Management Services	4800	Transfers to 2007 TOWNGATE SPCL TAX	4800-99-99-94800-904106		1,202,696	(1,202,696)	0	Adjustment in ROPS funding.
Financial & Management Services	2050	Transfers to ZONE "M" MEDIAN FUND	2050-99-99-92050-905112		1,990	2,840	4,830	Based on actuals.
Financial & Management Services	5111	Maint & Repair - Improvements	5111-30-79-25704-620920		1,000	1,500	2,500	Anticipated maintenance expense.
Financial & Management Services	5111	Maint & Repair - Bldg & Ground	5111-30-79-25704-620910		486,500	15,400	501,900	Tree maintenance services.
Financial & Management Services	5014	Maint & Repair - Bldg & Ground	5014-30-79-25721-620910	SD LMD-ZONE 02-Operations	1,421,130	6,200	1,427,330	Tree maintenance services.
Financial & Management Services	2050	Maint & Repair - Bldg & Ground	2050-30-79-25722-620910	SD CFD 2014 01-LM01H-Operations	83,240	3,300	86,540	Tree maintenance services.
Financial & Management Services	5013	CIP Other	5013-30-79-80006-720199	806 0001-5013-99	27,952	(27,952)	0	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5014	CIP Other	5014-30-79-80006-720199	806 0001-5014-99	223,991	(223,991)	0	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5111	CIP Other	5111-30-79-80006-720199	806 0001-5111-99	84,440	(84,440)	0	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5112	CIP Other	5112-30-79-80006-720199	806 0001-5112-99	90,255	(90,255)	0	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5013	Maint & Repair - Bldg & Ground	5013-30-79-79006-620910		-	27,952	27,952	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5014	Maint & Repair - Bldg & Ground	5014-30-79-79006-620910	SD LMD-ZONE 03-CIP	166,000	223,991	389,991	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5111	Maint & Repair - Bldg & Ground	5111-30-79-79006-620910		213,000	84,440	297,440	Allocating budget to new sections. Net effect zero.
Financial & Management Services	5112	Maint & Repair - Bldg & Ground	5112-30-79-79006-620910		133,000	90,255	223,255	Allocating budget to new sections. Net effect zero.
Financial & Management Services	2011	Salaries-Overtime	2011-16-16-16150-611210		1	25,000	25,001	Based on actual trends.
Financial & Management Services	6010	Transfers to 2019 TAXABLE LEASE REVENUE BOND	6010-99-99-96010-906060		-	634,000	634,000	2019 Lease Revenue Bonds.
Financial & Management Services	6060	Interest Expense	6060-99-99-96060-670410		-	634,000	634,000	2019 Lease Revenue Bonds.
Parks & Community Services	5011	Oper Mtrls - Other	5011-50-58-35311-630399		-	25,000	25,000	MVU annual sponsorships to Parks & Community Services approved for 20-21.
Parks & Community Services	5011	Salaries-Temporary	5011-50-58-35320-611310		-	6,750	6,750	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Salaries-Addl - Other	5011-50-58-35320-611699		-	3,780	3,780	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Marketing Svcs - Advertising	5011-50-58-35320-620610		-	600	600	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Oper Suppl - Office	5011-50-58-35320-630210		-	350	350	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Oper Suppl - Printing & Binding	5011-50-58-35320-630214		-	8,500	8,500	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Oper Mtrls - Janitorial	5011-50-58-35320-630314		-	1,000	1,000	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Oper Mtrls - Furn & Equip	5011-50-58-35320-630330		-	4,825	4,825	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	5011	Oper Mtrls - Rental Cleaning/Linen/Security	5011-50-58-35320-630360		-	16,000	16,000	Budgeting for expected expenses, off-set by revenue.
Parks & Community Services	3015	Transfers to DIF - PARKLAND FACILITIES	3015-99-99-93015-902905		-	8,938	8,938	Project 807 0026 50 57-3015 is completed and returning balance to original funding source.

Attachment: Exhibit A - Proposed Amendments [Revision 1] (4212 : FISCAL YEAR 2020/21 FIRST

Department	Fund	Account Description	General Ledger Account	Project	Fiscal Year 2020/21 Amended Budget	Proposed Amendment	Revised Budget	Description - Proposed Adjustment
Parks & Community Services	3015	Transfers to DIF - PARKLAND FACILITIES	3015-99-99-93015-902905		-	35,597	35,597	Project 807 0048-3015 is completed and returning balance to original funding source.
Parks & Community Services	3016	Transfers to - QUIMBY PARK FEES	3016-99-99-93016-902019		-	8,939	8,939	Project 807 0026-3016 is completed and returning balance to original funding source.
Parks & Community Services	3016	Transfers to - QUIMBY PARK FEES	3016-99-99-93016-902019		-	(7,089)	(7,089)	Project 807 0026-3016 is completed and returning balance to original funding source.
Parks & Community Services	3006	CIP Other	3006-50-57-80007-720199	807 0047-3006-99	127,604	107,983	235,587	Establishing project budget from FY19/20 savings.
Public Works	3000	Transfers to DIF - CITY HALL	3000-99-99-93000-902909		-	426,474	426,474	Project complete and transferring balance to Fund 2909.
Public Works	3302	CIP Other	3302-70-76-80008-720199	808 0015-3302-99	923,009	(121,045)	801,964	Project complete and transferring balance to Fund 2902.
Public Works	3302	CIP Other	3302-70-76-80008-720199	808 0016-3302-99	923,009	(69,423)	853,586	Project complete and transferring balance to Fund 2902.
Public Works	3302	Transfers to DIF - TRAFFIC SIGNALS	3302-99-99-93302-902902		-	190,468	190,468	Project complete and transferring balance to Fund 2902.
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0088-3008-99	1,010,182	2,912	1,013,094	True-up of project expenses.
Public Works	2000	CIP Other	2000-70-77-80001-720199	801 0085-2000A-99	7,573,649	138,320	7,711,969	Budgeting for expected expenses offset by RMRA revenue.
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0016-2001-99	354,217	(12,392)	341,825	Project complete and transferring balance to Sunnymead Project under Fund 2001.
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0014-2001-99	354,217	12,392	366,609	Project complete and transferring balance to Sunnymead Project under Fund 2001.
Public Works	2000	CIP Other	2000-70-77-80001-720199	801 0081-2000A-99	7,573,649	(37,763)	7,535,886	Citywide Pavement Rehabilitation FY18/19 is complete and transferring balance to Citywide Pavement Rehabilitation FY19/20.
Public Works	2000	CIP Other	2000-70-77-80001-720199	801 0085-2000A-99	7,573,649	37,763	7,611,412	Citywide Pavement Rehabilitation FY18/19 is complete and transferring balance to Citywide Pavement Rehabilitation FY19/20.
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0081-3008-99	1,010,182	(329,336)	680,846	Citywide Pavement Rehabilitation FY18/19 is complete and transferring balance to Citywide Pavement Rehabilitation FY19/20.
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0085-3008-99	1,010,182	329,336	1,339,518	Citywide Pavement Rehabilitation FY18/19 is complete and transferring balance to Citywide Pavement Rehabilitation FY19/20.
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0084-3008-99	1,010,182	(116,590)	893,592	Pavement Rehabilitation for Various Streets FY19/20 is complete and transferring balance to Pavement Rehabilitation for Various Streets F2021.
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0088-3008-99	1,010,182	116,590	1,126,772	Pavement Rehabilitation for Various Streets FY19/20 is complete and transferring balance to Pavement Rehabilitation for Various Streets F2021.
Public Works	6010	Maint & Repair - Streetlights	6010-70-80-45512-620950		87,889	150,000	237,889	Budgeting for streetlight maintenance.
Public Works	6010	Resource Adequacy	6010-70-80-45510-710146		1,227,000	37,800	1,264,800	Budgeting for expected expenses.
EXPENSES TOTAL					\$ 38,636,349	\$ 1,491,948	\$ 40,128,297	

Attachment: Exhibit A - Proposed Amendments [Revision 1] (4212 : FISCAL YEAR 2020/21 FIRST

City of Moreno Valley
FY 2019/20 - 2020/21
City Position Summary

Position Title	FY 2019/20 No.	FY 2020/21 Adj.	FY 2020/21 No.
Accountant I	1	-	1
Accountant II	1	-	1
Accounting Asst	3	-	3
Accounting Technician	1	-	1
Accounts Payable Supervisor	1	-	1
Administrative Asst	8	(1)	7
Administrative Services Dir	-	-	-
After School Prog Coordinator	-	-	-
After School Prog Specialist	-	-	-
After School Prog Supervisor	-	-	-
Animal Care Technician	5	-	5
Animal Care Technician Supervisor	1	-	1
Animal Control Officer	7	-	7
Animal Rescue Coordinator	1	-	1
Animal Services Assistant	4	-	4
Animal Services Dispatcher	1	-	1
Animal Services Division Manager	1	-	1
Animal Services Field Supervisor	1	-	1
Animal Services License Inspector	1	-	1
Animal Svcs Office Supervisor	1	-	1
Applications & DB Admin	1	-	1
Applications Analyst	1	-	1
Assistant City Attorney	1	(1)	-
Assistant City Clerk	-	-	-
Assoc Environmental Engineer	-	-	-
Associate Engineer	4	-	4
Associate Planner	4	-	4
Asst Buyer	2	(2)	-
Asst City Manager	-	-	-
Asst Crossing Guard Spvr	1	-	1
Asst Network Administrator	1	-	1
Assistant City Manager (Development Services)	1	-	1
Asst. Applications Analyst	-	-	-
Assistant City Manager (Administration)/Chief Financial Officer	1	-	1
Banquet Facility Rep	1	-	1
Budget Officer	-	-	-
Building & Neighborhood Services Div Mgr	-	-	-
Building Safety Supervisor	1	-	1
Building Div Mgr / Official	-	-	-
Building Inspector I I	4	-	4
Business License Liaison	1	-	1
Bus. Support & Neigh Prog Admin	-	-	-
Cable TV Producer	2	-	2
Capital Projects Division Manager	1	-	1
Chief Financial Officer/City Treasurer	-	-	-

City of Moreno Valley
FY 2019/20 - 2020/21
City Position Summary

Position Title	FY 2019/20 No.	FY 2020/21 Adj.	FY 2020/21 No.
Child Care Asst	4	-	4
Child Care Instructor I I	4	-	4
Child Care Program Manager	1	-	1
Child Care Site Supervisor	4	-	4
City Attorney	1	(1)	-
City Clerk	1	-	1
City Manager	1	-	1
Code & Neigh Svcs Division Manager	1	-	1
Code & Neigh Svcs Official	-	-	-
Code Compliance Field Sup.	-	-	-
Code Compliance Officer I/I I	4	-	4
Code Supervisor	-	-	-
Comm & Economic Dev Director	-	-	-
Community Dev Director	1	-	1
Community Enhancement Officer I	1	-	1
Community Enhancement Officer II	1	-	1
Community Services Coordinator	4	-	4
Community Svcs Supervisor	1	-	1
Construction Inspector	2	-	2
Construction Inspector Supervisor	1	-	1
Crossing Guard	35	-	35
Crossing Guard Supervisor	1	-	1
Customer Service Asst	-	-	-
Dep PW Dir /Asst City Engineer	-	-	-
Deputy City Attorney I	1	(1)	-
Deputy City Attorney I I I	-	-	-
Deputy City Clerk	1	-	1
Deputy City Manager	1	-	1
Deputy Finance Director	1	-	1
Dep. Comm & Economic Dev Director	-	-	-
Development Svcs Coordinator	-	-	-
Economic Dev Director	1	-	1
Economic Dev Division Mgr	2	-	2
Electric Utility Chief Engineer	1	-	1
Electric Utility Division Mgr	1	-	1
Electric Utility Program Coord	1	-	1
Emerg Mgmt & Vol Svc Prog Spec	1	-	1
Emerg Mgmt & Vol Svcs Prog Mgr	1	-	1
Engineering Division Manager/Assistant City Engineer	1	-	1
Engineering Technician I I	1	-	1
Enterprise Systems Admin	1	-	1
Environmental Analyst	-	-	-
Equipment Operator	4	-	4
Exec Asst to Mayor / City Council	1	-	1
Exec. Assistant to the City Manager	-	-	-

City of Moreno Valley
FY 2019/20 - 2020/21
City Position Summary

Position Title	FY 2019/20 No.	FY 2020/21 Adj.	FY 2020/21 No.
Executive Asst I	7	-	7
Executive Asst I I	1	(1)	-
Facilities Maint Mechanic	1	-	1
Facilities Maint Worker	3	-	3
Facilities Maintenance Spvr	-	-	-
Financial Analyst	-	-	-
Financial Operations Div Mgr	1	(1)	-
Financial Resources Div Mgr	1	-	1
Fire Inspector I	-	-	-
Fire Inspector I I	-	-	-
Fire Marshall	-	-	-
Fire Safety Specialist	-	-	-
Fleet Supervisor	-	-	-
Fleet & Facilities Maintenance Supervisor	1	-	1
GIS Administrator	-	1	1
GIS/Applications Analyst	-	1	1
GIS Specialist	1	-	1
GIS Technician	1	(1)	-
Housing Program Coordinator	-	-	-
Housing Program Specialist	-	-	-
Human Resources Analyst	1	-	1
Human Resources Director	-	-	-
Human Resources Division Manager	1	-	1
Human Resources Technician	-	-	-
Info Technology Technician	2	-	2
Landscape Development Coord	-	-	-
Landscape Irrigation Tech	-	-	-
Landscape Svcs Inspector	2	-	2
Landscape Svcs Supervisor	1	-	1
Lead Animal Care Technician	-	-	-
Lead Facilities Maint Worker	1	-	1
Lead Maintenance Worker	4	-	4
Lead Parks Maint Worker	6	-	6
Lead Traffic Sign/Marking Tech	2	-	2
Lead Vehicle / Equip Tech	1	-	1
Legal Secretary	-	-	-
Lib Serv Div Mgr	-	-	-
Librarian	-	-	-
Library Asst	-	-	-
Library Circulation Supervisor	-	-	-
Maint & Operations Div Mgr	1	-	1
Maintenance Worker I	-	-	-
Maintenance Worker II	1	-	1
Maintenance Worker I/II	17	-	17
Management Aide	5	2	7

City of Moreno Valley
FY 2019/20 - 2020/21
City Position Summary

Position Title	FY 2019/20 No.	FY 2020/21 Adj.	FY 2020/21 No.
Management Analyst	13	1	14
Management Asst	7	-	7
Media & Communications Division Manager	1	-	1
Media & Production Supervisor	-	-	-
Network Administrator	1	-	1
Office Asst	-	-	-
Paralegal	1	-	1
Park Ranger	3	-	3
Parking Control Officer	2	-	2
Parks & Community Services Deputy Director	1	-	1
Parks & Community Services Director	1	-	1
Parks & Community Services Division Manager	-	-	-
Parks Maintenance Division Manager	-	-	-
Parks Maint Supervisor	2	-	2
Parks Maint Worker	12	-	12
Parks Projects Coordinator	1	(1)	-
Payroll Supervisor	1	-	1
Permit Technician	5	-	5
Planning Commissioner	7	-	7
Planning Div Mgr / Official	1	-	1
Principal Accountant	1	-	1
Principal Planner	1	-	1
Public Information/Intergovernmental Relations Officer	1	-	1
Public Safety Contract Administrator	1	-	1
Purch & Facilities Div Mgr	1	-	1
PW Director / City Engineer	1	-	1
Recreation Program Coord	-	-	-
Recreation Program Leader	7	-	7
Recreation Supervisor	-	-	-
Recycling Specialist	1	-	1
Resource Analyst	-	-	-
Risk Division Manager	-	-	-
Security Guard	2	-	2
Spec Dist Budg & Accting Spvr	-	-	-
Spec Districts Div Mgr	1	-	1
Special Districts Prog Mgr	-	-	-
Sr Accountant	3	-	3
Sr Administrative Asst	15	(1)	14
Sr Applications Analyst	1	-	1
Sr Citizens Center Coord	-	-	-
Sr Code Compliance Officer	1	-	1
Sr. Community Enhancement Officer	1	-	1
Sr Construction Inspector	2	-	2
Sr Customer Service Asst	3	(3)	-
Sr Deputy City Clerk	1	-	1

City of Moreno Valley
FY 2019/20 - 2020/21
City Position Summary

Position Title	FY 2019/20 No.	FY 2020/21 Adj.	FY 2020/21 No.
Sr Electrical Engineer	1	-	1
Sr Engineer, P.E.	5	-	5
Sr Engineering Technician	1	-	1
Sr Equipment Operator	1	-	1
Sr Financial Analyst	-	-	-
Sr GIS Analyst	1	(1)	-
Sr Graphics Designer	1	-	1
Sr Human Resources Analyst	1	-	1
Sr IT Technician	-	-	-
Sr Landscape Svcs Inspector	-	-	-
Sr Management Analyst	3	-	3
Sr Office Asst	2	2	4
Sr Park Ranger	-	-	-
Sr Parking Control Officer	1	-	1
Sr Parks Maint Technician	2	-	2
Sr Payroll Technician	1	-	1
Sr Permit Technician	2	-	2
Sr Planner	1	-	1
Sr Recreation Program Leader	2	-	2
Sr Telecomm Technician	1	-	1
Sr Traffic Engineer	-	-	-
Sr Traffic Signal Technician	1	-	1
Storekeeper	1	(1)	-
Storm Water Prog Mgr	-	-	-
Strategic Initiatives Manager	1	-	1
Street Maintenance Supervisor	2	-	2
Sustainability & Intergovernmental Prog Mgr	-	-	-
Technology Services Div Mgr	-	-	-
Telecomm Engineer / Admin	1	-	1
Telecomm Technician	1	-	1
Traffic Operations Supervisor	1	-	1
Traffic Sign / Marking Tech I	1	-	1
Traffic Sign/Marking Tech II	2	-	2
Traffic Signal Technician	2	-	2
Trans Div Mgr / City Traf Engr	1	-	1
Treasury Operations Div Mgr	1	-	1
Tree Trimmer	-	-	-
Vehicle / Equipment Technician	3	-	3
Total	362	(9)	353



**Financial & Management
Services Department**

MEMORANDUM

To: Mike Lee, City Manager
 From: Marshall Eyerman, Assistant City Manager/Chief Financial Officer
 Brian Mohan, Financial Resources Division Manager
 Date: September 14, 2020
 Subject: Approval of Carryover Appropriations from Fiscal Year 2019/20

Council adopted Resolution 2019-27 on May 7, 2019 giving the Chief Financial Officer and City Manager the following authority:

Certain capital projects, programs and commitments have been previously approved by the City Council for appropriation in fiscal year 2018/19 and current adoption of fiscal year 2019/20, the Chief Financial Officer shall be authorized to carryover such appropriation budgets for these items as approved by the City Manager.

For your consideration, the attached carryover exhibits are enclosed for your review. Your signature below will provide the approval to record all requested carryover adjustments.

Mike Lee
City Manager

Enclosed: Exhibit A - Carryover
 Exhibit B - CIP Carryover

**CITY OF MORENO VALLEY
GENERAL FUND
FY 2020/21 Proposed Carryovers**

Department	Fund	Account Description	General Ledger Account	FY 2020/21 Amended Budget	Proposed Carryover	Revised Budget	Description - Proposed Adjustment
Financial & Management Services	1010	Transfers in - from FLEET OPS REPLACEMENT RESERVE	1010-99-99-91010-807430	\$ 906,336	\$ 2,011,085	\$ 2,917,421	Carrying over budget for approved vehicle replacement.
REVENUE TOTAL				\$ 906,336	\$ 2,011,085	\$ 2,917,421	
City Council	1010	Training & Travel	1010-10-01-10010-620510	\$ 1,464	\$ 296	\$ 1,760	For expected training expenses.
City Council	1010	Council - Dist 1 Discretionary	1010-10-01-10011-620111	3,000	1,989	4,989	Council Discretionary.
City Council	1010	Council - Dist 2 Discretionary	1010-10-01-10012-620112	3,000	2,805	5,805	Council Discretionary.
City Council	1010	Council - Dist 2 Discretionary-Carryover	1010-10-01-10012-620117	-	834	834	Expected expense approved in FY19/20.
City Council	1010	Council - Dist 3 Discretionary	1010-10-01-10013-620113	3,000	2,578	5,578	Council Discretionary.
City Council	1010	Council - Dist 4 Discretionary	1010-10-01-10014-620114	3,000	2,733	5,733	Council Discretionary.
City Council	1010	Council - Mayor Discretionary	1010-10-01-10015-620130	6,000	2,575	8,575	Council Discretionary.
Community Development	1010	Nuisance Abatement	1010-20-26-20110-625015	-	50,000	50,000	Nuisance abatements.
Fire	1010	Mach-Equip-Repl - Vehicles	1010-40-45-30110-660322	-	694,833	694,833	Carrying over budget for approved vehicle replacement.
Fire	1010	Mach-Equip-Repl - Other	1010-40-45-30110-660399	-	694,833	694,833	Carrying over budget for approved vehicle replacement.
Public Works	1010	Mach-Equip-Repl - Vehicles	1010-70-78-45370-660322	906,336	621,419	1,527,755	Carrying over budget for approved vehicle replacement.
EXPENSES TOTAL				\$ 925,800	\$ 2,074,895	\$ 3,000,695	

**NON-GENERAL FUND
FY 2020/21 Proposed Carryovers**

Department	Fund	Account Description	General Ledger Account	FY 2020/21 Amended Budget	Proposed Carryover	Revised Budget	Description - Proposed Adjustment
Community Development	2300	State Grant-Operating Revenue	2300-20-27-72203-486000	\$ -	\$ 625,000	\$ 625,000	Carrying over remaining budget for ongoing project.
Community Development	2300	State Grant-Operating Revenue	2300-20-38-73312-486000	26,000	7,328	33,328	Spay-Neuter program.
Community Development	2300	Other Grant-Operating Revenue	2300-20-38-73313-489000	-	36,709	36,709	For ongoing projects.
Community Development	2715	Fed Grant-Operating Revenue	2715-20-26-72115-485000	-	79,892	79,892	Funded by JAG grant.
Financial & Management Services	2506	Fed Grant-Operating Revenue	2506-99-99-92506-485000	820,692	1,082,680	1,903,372	Revenue from HOME for Courtyards at Cottonwood Project and Administrative agreement.
Financial & Management Services	2507	Fed Grant-Operating Revenue	2507-30-36-72701-485000	-	3,500,000	3,500,000	Revenue from NSP1 for Courtyards at Cottonwood Project.
Financial & Management Services	2512	Fed Grant-Operating Revenue	2512-99-99-92512-485000	2,725,573	727,204	3,452,777	Revenue for CARES Act grants.
Financial & Management Services	2512	Fed Grant-Operating Revenue	2512-99-99-92512-485000	2,725,573	423,077	3,148,650	Revenue for 19/20 carryover projects, CIP grants, Facilities grants.
Financial & Management Services	2514	Fed Grant-Operating Revenue	2514-30-36-72752-485000	-	630,838	630,838	Revenue for CARES Act projects.
Financial & Management Services	4105	Special Taxes	4105-99-99-94105-404000	97,730	14,044	111,774	Carrying over remaining budget for ongoing project.
Economic Development	2300	Other Grant-Operating Revenue	2300-22-25-72202-489000	-	10,738	10,738	Carrying over budget for approved vehicle replacement.
Fire	2503	Fed Grant-Operating Revenue	2503-40-47-74105-485000	-	12,222	12,222	EPMG Grant Revenue.
Fire	2503	Fed Grant-Operating Revenue	2503-40-47-74106-485000	-	30,479	30,479	SHSP Grant Revenue.
Parks & Community Services	2300	Fed Grant-Operating Revenue	2300-50-92-75014-485000	-	12,825	12,825	Support for Edgemont afterschool program via the 21st CCLC funds.
Parks & Community Services	2300	Fed Grant-Operating Revenue	2300-50-92-75016-485000	-	59,783	59,783	Senior Eats program.
Parks & Community Services	2300	State Grant-Operating Revenue	2300-50-92-75016-486000	-	19,928	19,928	Senior Eats program.
Police	2715	Fed Grant-Operating Revenue	2715-99-92-76511-485000	-	21,335	21,335	Coronavirus Emergency Supplemental Funding Grant Revenue.
Public Works	2000	Transfers in - from EQUIP REPLACEMENT (FURN & EQUIP)	2000-99-99-92000-807510	-	9,366	9,366	Move over remaining budget for truck equipment.
REVENUE TOTAL				\$ 6,395,568	\$ 7,303,448	\$ 13,699,016	

Attachment: Carryover Memo-Exhibits (4212) : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW

**NON-GENERAL FUND
FY 2020/21 Proposed Carryovers**

Department	Fund	Account Description	General Ledger Account	FY 2020/21 Amended Budget	Proposed Carryover	Revised Budget	Description - Proposed Adjustment
Fire	2503	Software Maint/Support/License	2503-40-47-74105-625010	\$ -	\$ 12,222	\$ 12,222	EPMG Grant Expenditures.
Fire	2503	Salaries-Regular	2503-40-47-74106-611110	-	30,479	30,479	SHSP Grant Revenue.
Police	2715	Oper Suppl - Other	2715-99-92-76511-630299	-	8,520	8,520	Coronavirus Emergency Supplemental Funding Grant Revenue.
Police	2715	Oper Mtrls - Furn & Equip	2715-99-92-76511-630330	-	2,815	2,815	Coronavirus Emergency Supplemental Funding Grant Revenue.
Police	2715	Mach-Equip-New - Furn & Equip	2715-99-92-76511-660310	-	10,000	10,000	Coronavirus Emergency Supplemental Funding Grant Revenue.
Community Development	2018	Contractual Svcs - Other	2018-99-99-92018-625099	-	1,325,405	1,325,405	For ongoing projects.
Community Development	2300	Professional Svcs - Other	2300-20-27-72203-620299	-	625,000	625,000	Carrying over remaining budget for ongoing project.
Community Development	2300	Professional Svcs - Veterinary Svcs	2300-20-38-73312-620250	26,000	7,328	33,328	Spay-Neuter program.
Community Development	2300	Professional Svcs - Veterinary Svcs	2300-20-38-73313-620250	-	16,760	16,760	Public outreach.
Community Development	2300	Maint & Repair - Bldg & Ground	2300-20-38-73313-620910	-	18,973	18,973	For ongoing remodel project.
Community Development	2300	Oper Mtrls - Furn & Equip	2300-20-38-73313-630330	-	976	976	For ongoing remodel project.
Community Development	2715	Salaries-Temporary	2715-20-26-72115-611310	-	79,892	79,892	Funded by JAG grant.
Economic Development	2300	Professional Svcs - Other	2300-22-25-72202-620299	-	10,738	10,738	Carrying over budget for approved vehicle replacement.
Economic Development	2512	Contractual Svcs - Other	2512-22-25-72612-625099	-	340,000	340,000	Carryover of remaining budget for ongoing CARES Act expenses.
Economic Development	2512	Salaries-Regular	2512-22-25-72612-611110	-	85,706	85,706	Carryover of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	2006	Professional Svcs - Legal Svcs	2006-30-79-25701-620230	-	50,000	50,000	Additional expected expenses.
Financial & Management Services	2006	Professional Svcs - Other	2006-30-79-25701-620299	-	93,000	93,000	Additional expected expenses.
Financial & Management Services	2006	Agency Svcs - Cnty	2006-30-79-25701-620320	-	1,000	1,000	Additional expected expenses.
Financial & Management Services	2506	Professional Svcs - Other	2506-30-36-72657-620299	62,069	82,679	144,748	For ongoing Courtyards project.
Financial & Management Services	2506	HOME Programs	2506-30-36-72657-733102	615,519	1,000,000	1,615,519	For ongoing Cottonwood project.
Financial & Management Services	2507	NSP Programs	2507-30-36-72701-733201	-	3,500,000	3,500,000	For ongoing Cottonwood project.
Financial & Management Services	2512	CDBG Public Services	2512-30-36-72611-730102	172,336	25,290	197,626	For ongoing CDBG projects.
Financial & Management Services	2512	Rehabilitation	2512-30-36-72611-730104	50,000	47,787	97,787	For ongoing CDBG projects.
Financial & Management Services	2512	Professional Svcs - Other	2512-30-36-72612-620299	-	150,000	150,000	Carryover of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	2512	CDBG Public Services	2512-30-36-72612-730102	-	32,000	32,000	Carryover of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	2512	Salaries-Regular	2512-30-36-72612-611110	-	89,498	89,498	Carrying over remaining budget for an ongoing project.
Financial & Management Services	2512	Computers - Misc Components	2512-30-39-72612-630910	-	30,000	30,000	Carryover of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	2514	Professional Svcs - Other	2514-30-36-72752-620299	-	61,583	61,583	Carrying over of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	2514	Marketing Svcs - Advertising	2514-30-36-72752-620610	-	1,500	1,500	Carrying over of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	2514	ESG Programs	2514-30-36-72752-733210	-	567,755	567,755	Carrying over of remaining budget for ongoing CARES Act expenses.
Financial & Management Services	4105	Admin Chrg - Special Dist	4105-99-99-94105-692020	28,080	14,044	42,124	Carrying over remaining budget for ongoing project.
Financial & Management Services	5112	Maint & Repair - Bldg & Ground	5112-30-79-25719-620910	-	11,369	11,369	Carrying over remaining budget for ongoing project.
Financial & Management Services	7110	Training & Travel - Citywide	7110-30-22-18120-620520	-	11,000	11,000	For expected training expenses.
Financial & Management Services	7210	Dues & Subscriptions	7210-30-39-25452-625030	-	703,515	703,515	Laser fiche cloud services.
Financial & Management Services	7430	Transfers to GENERAL FUND	7430-99-99-97430-901010	906,336	2,011,085	2,917,421	Carrying over budget for approved vehicle replacement.
Financial & Management Services	8884	Housing Programs	8884-30-36-20601-732101	-	1,202,000	1,202,000	Land dedication to Courtyards at Cottonwood Project.
Parks & Community Services	2300	Contractual Svcs - Other	2300-50-92-75014-625099	32,063	12,825	44,888	Support for Edgemont afterschool program via the 21st CCLC funds.
Parks & Community Services	2300	Oper Mtrls - Other	2300-50-92-75016-630399	-	79,711	79,711	Senior Eats program.
Parks & Community Services	3912	Oper Mtrls - Recreation	3912-50-58-35318-630312	-	6,142	6,142	Park equipment funded by Rock Ridge Park Endowment.
Public Works	2000	Mach-Equip-Repl - Furn & Equip	2000-70-78-45311-660320	-	9,366	9,366	Move over remaining budget for truck equipment.
Public Works	6010	Contractual Svcs - Other	6010-70-80-45510-625099	-	95,675	95,675	Carrying over remaining budget for ongoing MVU projects.
Public Works	6010	Purchased Power	6010-70-80-45510-710110	-	1,000,000	1,000,000	For expected expenses.
Public Works	6010	Emergency Outage Response	6010-70-80-45510-710134	-	49,858	49,858	Streetlight equipment.
Public Works	6010	Renewable Energy	6010-70-80-45510-710148	-	162,799	162,799	Renewable energy.
Public Works	6010	Maint & Repair - Streetlights	6010-70-80-45512-620950	-	87,889	87,889	Continued maintenance of streetlights.
Public Works	6011	Improvements Other than Bldg	6011-70-80-45510-660610	-	376,577	376,577	Continued developer projects.
Public Works	7510	Transfers to GAS TAX FUND	7510-99-97-88120-902000	-	9,366	9,366	Move over remaining budget for truck equipment.
EXPENSES TOTAL				\$ 1,892,403	\$ 14,150,127	\$ 16,042,530	

Attachment: Carryover Memo-Exhibits (4212) : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW

**CITY OF MORENO VALLEY
General Fund
FY 2020/21 Proposed Capital Improvement Plan (CIP) Carryovers**

Department	Fund	Account Description	General Ledger Account	Project Number	Fiscal Year (FY) 2020/21 Amended Budget	Proposed Carryover	Revised Budget	Project
Police	1010	CIP Other	1010-60-65-80003-720199	803 0047	\$ -	\$ 51,668	\$ 51,668	Public Safety Building Back Parking Lot Improvements
Public Works	1010	CIP Other	1010-70-77-80001-720199	801 0037 70 77	-	5,187	5,187	Public Works HLFV Interchanges
Public Works	1010	CIP Other	1010-70-76-80008-720199	808 0019	-	691	691	Road Safety Audit on Ironwood Avenue
EXPENSE TOTAL					\$ -	\$ 57,546	\$ 57,546	

**CITY OF MORENO VALLEY
Non-General Fund
FY 2020/21 Proposed Capital Improvement Plan (CIP) Carryovers**

Department	Fund	Account Description	General Ledger Account	Project Number	Fiscal Year (FY) 20/21 Amended Budget	Proposed Carryover	Revised Budget	Project
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	801 0021 70 77	\$ 7,393,000	\$ 16,800,000	\$ 24,193,000	SR-60/Moreno Beach IC Phase 2
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	801 0063	7,393,000	121,579	7,514,579	Cycle 1 ATP Citywide SRTS Pedestrian Facility Improvements
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	801 0073	7,393,000	1,132,457	8,525,457	Juan Bautista de Anza Multi-Use Trail - ATP 2
Non-Department	2301	State Grant-Capital Revenue	2301-99-99-92301-486010	801 0077	-	2,571,509	2,571,509	Juan Bautista de Anza Multi-Use Trail - ATP 3
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	801 0086	7,393,000	982,419	8,375,419	Juan Bautista de Anza Multi-Use Trail - ATP 4
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	802 0006	7,393,000	1,043,958	8,436,958	Bridge Preventative Maintenance Program - Implementation Phase
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	804 0008	7,393,000	105,000	7,498,000	Sunnymead Master Drainage Plan - Storm Drain Lines F and F-7
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	808 0018	7,393,000	3,579,982	10,972,982	Advanced Dilemma Zone Detection at Certain Intersections
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	808 0019	7,393,000	294,328	7,687,328	Road Safety Audit on Ironwood Avenue
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	808 0020	7,393,000	118,048	7,511,048	Road Safety Audit on Kitching Street
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	808 0026	7,393,000	429,310	7,822,310	South Lasselle Street Safety Corridor
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	808 0027	7,393,000	702,255	8,095,255	Guardrail Upgrades
Non-Department	2301	Fed Reimb-Capital	2301-99-99-92301-482020	808 0028	7,393,000	218,832	7,611,832	Upgrade Existing Marked Crosswalks on Arterials
Non-Department	2301	State Grant-Capital Revenue	2301-99-99-92301-486010	810 0015	-	99,232	99,232	Dracaea Avenue Neighborhood Greenway Corridor Study
Non-Department	2800	County Article 3	2800-99-99-92800-487100	801 0076	-	520,000	520,000	Heacock St Pedestrian and Bicycle Enhancements/ Gregory Lane
Non-Department	2800	County Article 3	2800-99-99-92800-487100	808 0017	-	199,094	199,094	Heacock St Pedestrian and Bicycle Enhancements/ Gregory Lane
Non-Department	3002	Reimbursement Agreement	3002-99-99-93002-500600	804 0014	-	474,386	474,386	Sunnymead - Flaming Arrow Drive Storm Drain
Non-Department	3003	Reimbursements - Other Govts	3003-99-99-93003-483010	801 0010 70 77	5,000,000	893,905	5,893,905	Heacock St. South Extension
Non-Department	3003	Reimbursements - Other Govts	3003-99-99-93003-483010	801 0021 70 77	5,000,000	2,028,428	7,028,428	SR-60/Moreno Beach IC Phase 2
Non-Department	3003	Reimbursements - Other Govts	3003-99-99-93003-483010	801 0021 70 77	5,000,000	198,206	5,198,206	SR-60/Moreno Beach IC Phase 2
Parks & Community Services	2300	Other Grant-Operating Revenue	2300-50-57-80007-489000	807 0049	-	25,000	25,000	Demonstration Garden
REVENUE TOTAL					\$ 103,716,000	\$ 32,537,928	\$ 136,253,928	

Attachment: Carryover Memo-Exhibits (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW

**Non-General Fund
FY 2020/21 Proposed Capital Improvement Plan (CIP) Carryovers**

Department	Fund	Account Description	General Ledger Account	Project Number	Fiscal Year (FY) 20/21 Amended Budget	Proposed Carryover	Revised Budget	Project
Financial & Management Services	3000	CIP Other	3000-30-56-80003-720199	803 0045	\$ -	\$ 800,028	\$ 800,028	Moreno Valley Library at Iris Plaza
Financial & Management Services	5013	CIP Other	5013-30-79-80006-720199	806 0001	-	27,952	27,952	Landscape Maintenance Districts Capital Improvement Renovation
Financial & Management Services	5014	CIP Other	5014-30-79-80006-720199	806 0001	-	223,991	223,991	Landscape Maintenance Districts Capital Improvement Renovation
Financial & Management Services	5111	CIP Other	5111-30-79-80006-720199	806 0001	-	84,440	84,440	Landscape Maintenance Districts Capital Improvement Renovation
Financial & Management Services	5112	CIP Other	5112-30-79-80006-720199	806 0001	-	90,255	90,255	Landscape Maintenance Districts Capital Improvement Renovation
Financial & Management Services	7220	CIP Other	7220-30-39-80003-720199	803 0011 30 39	-	19,735	19,735	Box Springs Communications Site
Financial & Management Services	7220	CIP Other	7220-30-39-80009-720199	809 0001 30 39	-	298,355	298,355	Citywide Fiber Optic Comm Exp
Financial & Management Services	7220	CIP Other	7220-30-39-80010-720199	810 0001 30 39	-	36,804	36,804	Citywide Camera Surveillance System
Parks & Community Services	2300	CIP Other	2300-50-57-80007-720199	807 0049	-	25,000	25,000	Demonstration Garden
Parks & Community Services	3000	CIP Other	3000-50-57-80003-720199	803 0037	-	5,596,854	5,596,854	Civic Center Amphitheater and Park
Parks & Community Services	3006	CIP Other	3006-50-57-80007-720199	807 0047	-	127,604	127,604	Moreno Valley Community Park Soccer Field Improvements
Parks & Community Services	3015	CIP Other	3015-50-57-80007-720199	807 0031 50 57	30,000	165,618	195,618	Rancho Verde Park
Parks & Community Services	3015	CIP Other	3015-50-57-80007-720199	807 0039	30,000	46,717	76,717	Hidden Springs Park II
Parks & Community Services	3015	CIP Other	3015-50-57-80007-720199	807 0043	30,000	24,745	54,745	Cottonwood Recreation Center Exterior Landscaping
Parks & Community Services	3015	CIP Other	3015-50-57-80007-720199	807 0049	30,000	175,266	205,266	Demonstration Garden
Parks & Community Services	3016	CIP Other	3016-50-57-80007-720199	807 0004 50 57	375,000	59,437	434,437	Replacement Playground Equipment
Parks & Community Services	3016	CIP Other	3016-50-57-80007-720199	807 0005 50 57	375,000	214,201	589,201	Annual ADA Park Improvements
Parks & Community Services	3016	CIP Other	3016-50-57-80007-720199	807 0043	375,000	50,375	425,375	Cottonwood Recreation Center Exterior Landscaping
Parks & Community Services	3016	CIP Other	3016-50-57-80007-720199	807 0045	375,000	162,490	537,490	Cottonwood Golf Center Irrigation Improvements
Parks & Community Services	3016	CIP Other	3016-50-57-80003-720199	803 0027	130,000	100,000	230,000	March Community Center Renovation
Parks & Community Services	3016	CIP Other	3016-50-57-80003-720199	803 0030	130,000	110,925	240,925	Park Restroom Renovations at Various Sites
Parks & Community Services	3016	CIP Other	3016-50-57-80003-720199	803 0031	130,000	16,668	146,668	Towngate Community Center Renovation
Parks & Community Services	3016	CIP Other	3016-50-57-80003-720199	803 0034	130,000	53,667	183,667	Replace Flooring at Various Community Services Facilities
Parks & Community Services	3016	CIP Other	3016-50-57-80001-720199	801 0073	-	5,553	5,553	Juan Bautista de Anza Multi-Use Trail - ATP 2
Parks & Community Services	3016	CIP Other	3016-50-57-80003-720199	803 0044	130,000	249,582	379,582	Electronic Marquee Sign
Parks & Community Services	5113	CIP Other	5113-50-57-80007-720199	807 0051	92,000	24,718	116,718	Celebration Park Splash Pad UV Purification System
Parks & Community Services	5113	CIP Other	5113-50-57-80007-720199	807 0052	92,000	4,510	96,510	Drinking Fountain Replacements at Various Parks
Parks & Community Services	5113	CIP Other	5113-50-57-80007-720199	807 0053	92,000	50,000	142,000	LED Lighting Upgrades at Various Parks
Parks & Community Services	5113	CIP Other	5113-50-57-80003-720199	803 0031	-	60,000	60,000	Towngate Community Center Renovation
Public Works	2000	CIP Other	2000-70-77-80001-720199	801 0008 70 77	3,492,736	564,600	4,057,336	Annual ADA Compliant Access Upgrades
Public Works	2000	CIP Other	2000-70-76-80001-720199	801 0015 70 76	50,000	150,000	200,000	Residential Traffic Mgmt Prgrm (Speed Humps)
Public Works	2000	CIP Other	2000-70-77-80001-720199	801 0081	3,492,736	46,956	3,539,692	Citywide Pavement Rehabilitation Program FY18/19
Public Works	2000	CIP Other	2000-70-77-80001-720199	801 0085	3,492,736	3,469,357	6,962,093	Citywide Pavement Rehabilitation Program FY19/20
Public Works	2000	CIP Other	2000-70-77-80002-720199	802 0002 70 77	10,000	20,477	30,477	Bridge Inspection Program
Public Works	2000	CIP Other	2000-70-77-80002-720199	802 0006	10,000	135,256	145,256	Bridge Preventative Maintenance Program - Implementation Phase
Public Works	2001	CIP Other	2001-70-77-80001-720199	801 0008 70 77	-	7,963	7,963	Annual ADA Compliant Access Upgrades
Public Works	2001	CIP Other	2001-70-76-80001-720199	801 0015 70 76	-	4,241	4,241	Residential Traffic Mgmt Prgrm (Speed Humps)
Public Works	2001	CIP Other	2001-70-78-80001-720199	801 0017 70 78	-	46,565	46,565	Annual Pavement Maintenance - Crack Seal
Public Works	2001	CIP Other	2001-70-77-80001-720199	801 0052 70 77	-	887	887	SR-60 / World Logistics Center Parkway Interchange
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0007 70 77	-	43,265	43,265	Moreno MDP Line K-1 Stage 3 K-4
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0008	-	35,000	35,000	Sunnymead Master Drainage Plan - Storm Drain Lines F and F-7
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0014	-	123,453	123,453	Sunnymead - Flaming Arrow Drive Storm Drain

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**Non-General Fund
FY 2020/21 Proposed Capital Improvement Plan (CIP) Carryovers**

Department	Fund	Account Description	General Ledger Account	Project Number	Fiscal Year (FY) 20/21 Amended Budget	Proposed Carryover	Revised Budget	Project
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0015	-	725	725	Sunnymead MDP Line B-16A
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0016	-	12,392	12,392	Moreno - Alessandro Interim Facility (Discovery Church)
Public Works	2001	CIP Other	2001-70-77-80004-720199	804 0017	-	139,382	139,382	Moreno MDP Line F-18 and F-19
Public Works	2001	CIP Other	2001-70-76-80008-720199	808 0013 70 76	-	159,826	159,826	Traffic Signal Equipment Upgrades
Public Works	2001	CIP Other	2001-70-76-80008-720199	808 0017	-	212,496	212,496	Pedestrian Hybrid Beacon on Cactus Ave at Woodland Park
Public Works	2005	CIP Other	2005-70-76-80008-720199	808 0004 70 76	20,000	97,684	117,684	Traffic Signal Coordination Program
Public Works	2008	CIP Other	2008-70-29-80004-720199	804 0018	80,000	80,000	160,000	Citywide Full Trash Capture Device Installation
Public Works	2301	CIP Other	2301-70-77-80001-720199	801 0021 70 77	7,393,000	16,800,000	24,193,000	SR-60/Moreno Beach IC Phase 2
Public Works	2301	CIP Other	2301-70-77-80001-720199	801 0063	7,393,000	118,768	7,511,768	Cycle 1 ATP Citywide SRTS Pedestrian Facility Improvements
Public Works	2301	CIP Other	2301-70-77-80001-720199	801 0073	7,393,000	1,126,625	8,519,625	Juan Bautista de Anza Multi-Use Trail - ATP 2
Public Works	2301	CIP Other	2301-70-77-80001-720199	801 0077	7,393,000	2,571,257	9,964,257	Juan Bautista de Anza Multi-Use Trail - ATP 3
Public Works	2301	CIP Other	2301-70-77-80001-720199	801 0086	7,393,000	982,419	8,375,419	Juan Bautista de Anza Multi-Use Trail - ATP 4
Public Works	2301	CIP Other	2301-70-77-80002-720199	802 0006	-	1,043,958	1,043,958	Bridge Preventative Maintenance Program - Implementation Phase
Public Works	2301	CIP Other	2301-70-77-80004-720199	804 0008	-	105,000	105,000	Sunnymead Master Drainage Plan - Storm Drain Lines F and F-7
Public Works	2301	CIP Other	2301-70-76-80008-720199	808 0018	-	3,576,409	3,576,409	Advanced Dilemma Zone Detection at Certain Intersections
Public Works	2301	CIP Other	2301-70-76-80008-720199	808 0019	-	294,285	294,285	Road Safety Audit on Ironwood Avenue
Public Works	2301	CIP Other	2301-70-76-80008-720199	808 0020	-	118,035	118,035	Road Safety Audit on Kitching Street
Public Works	2301	CIP Other	2301-70-76-80008-720199	808 0026	-	429,303	429,303	South Lasselle Street Safety Corridor
Public Works	2301	CIP Other	2301-70-76-80008-720199	808 0027	-	702,253	702,253	Guardrail Upgrades
Public Works	2301	CIP Other	2301-70-76-80008-720199	808 0028	-	217,929	217,929	Upgrade Existing Marked Crosswalks on Arterials
Public Works	2301	CIP Other	2301-70-76-80010-720199	810 0015	-	112,090	112,090	Dracaea Avenue Neighborhood Greenway Corridor Study
Public Works	2512	CIP Other	2512-70-40-80003-720199	803 0039	-	100,000	100,000	Main Library ADA Improvements
Public Works	2512	CIP Other	2512-70-77-80004-720199	804 0014	-	250,000	250,000	Sunnymead - Flaming Arrow Drive Storm Drain
Public Works	2800	CIP Other	2800-70-77-80001-720199	801 0076	-	520,000	520,000	Heacock St Pedestrian and Bicycle Enhancements/ Gregory Lane
Public Works	2800	CIP Other	2800-70-76-80008-720199	808 0017	-	199,094	199,094	Pedestrian Hybrid Beacon on Cactus Ave at Woodland Park
Public Works	3000	CIP Other	3000-70-40-80003-720199	803 0042	-	485,431	485,431	Corporate Yard Building/ Fleet Shop Remodel
Public Works	3000	CIP Other	3000-70-29-80003-720199	803 0043	-	189,600	189,600	Corporate Yard Master Plan Improvements
Public Works	3002	CIP Other	3002-70-77-80004-720199	804 0014	-	326,152	326,152	Sunnymead - Flaming Arrow Drive Storm Drain
Public Works	3002	CIP Other	3002-70-77-80004-720199	804 0016	-	342,777	342,777	Moreno - Alessandro Interim Facility (Discovery Church)
Public Works	3003	CIP Other	3003-70-77-80001-720199	801 0010 70 77	5,000,000	893,905	5,893,905	Heacock St. South Extension
Public Works	3003	CIP Other	3003-70-77-80001-720199	801 0021 70 77	5,000,000	2,226,634	7,226,634	SR-60/Moreno Beach IC Phase 2
Public Works	3004	CIP Other	3004-70-76-80001-720199	801 0073	-	4,873	4,873	Juan Bautista de Anza Multi-Use Trail - ATP 2
Public Works	3004	CIP Other	3004-70-76-80008-720199	808 0031	-	74,846	74,846	Transit Signal Priority Integration Phase 1
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0084	-	120,310	120,310	Pavement Rehabilitation for Various Streets (CDBG FY 19/20)
Public Works	3008	CIP Other	3008-70-77-80001-720199	801 0085	-	393,837	393,837	Citywide Pavement Rehabilitation Program FY19/20
Public Works	3008	CIP Other	3008-70-77-80002-720199	802 0003 70 77	-	4,565	4,565	SR-60/Nason Overcrossing Bridge
Public Works	3301	CIP Other	3301-70-77-80001-720199	801 0076	-	54,531	54,531	Heacock St Pedestrian and Bicycle Enhancements/ Gregory Lane
Public Works	3301	CIP Other	3301-70-77-80002-720199	802 0004	200,000	643,280	843,280	Indian Street/Cardinal Avenue Bridge (Over Lateral A)
Public Works	3302	CIP Other	3302-70-76-80008-720199	808 0015	-	121,045	121,045	ITS Deployment Phase 1B
Public Works	3302	CIP Other	3302-70-76-80008-720199	808 0016	-	69,423	69,423	Dynamic Traveler Alert Message Boards
Public Works	3302	CIP Other	3302-70-76-80008-720199	808 0025	-	472,901	472,901	Moreno Valley Ranch ITS
Public Works	3302	CIP Other	3302-70-76-80008-720199	808 0030	-	259,640	259,640	Pigeon Pass Road ITS

Attachment: Carryover Memo-Exhibits (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW

**Non-General Fund
FY 2020/21 Proposed Capital Improvement Plan (CIP) Carryovers**

Department	Fund	Account Description	General Ledger Account	Project Number	Fiscal Year (FY) 20/21 Amended Budget	Proposed Carryover	Revised Budget	Project
Public Works	3311	CIP Other	3311-70-77-80001-720199	801 0021 70 77	300,000	625,090	925,090	SR-60/Moreno Beach IC Phase 2
Public Works	3311	CIP Other	3311-70-77-80001-720199	801 0052 70 77	300,000	503,300	803,300	SR-60 / World Logistics Center Parkway Interchange
Public Works	3311	CIP Other	3311-70-77-80002-720199	802 0003 70 77	-	30,000	30,000	SR-60/Nason Overcrossing Bridge
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0044	-	440,980	440,980	Alessandro Crosstown Tie
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0045	-	1,056,299	1,056,299	Mobile Advanced Metering Infrastructure (AMI) System
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0046	-	2,498,862	2,498,862	Electrical System Automation
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0049	-	941,590	941,590	Day Street Line Extension
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0054	-	1,551,361	1,551,361	City Hall Annex Solar Carports
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0055	-	545,883	545,883	Eucalyptus Avenue Line Extension
Public Works	6011	CIP Other	6011-70-80-80005-720199	805 0056	-	494,911	494,911	Moreno Beach Bridge Conduit Project
EXPENSE TOTAL					\$ 60,959,208	\$ 58,895,466	\$ 119,854,674	

Attachment: Carryover Memo-Exhibits (4212 : FISCAL YEAR 2020/21 FIRST QUARTER BUDGET REVIEW



Report to City Council

TO: Mayor and City Council

FROM: Pat Jacquez-Nares, City Clerk

AGENDA DATE: December 15, 2020

TITLE: CITY COUNCIL REORGANIZATION - SELECTION OF MAYOR PRO TEM

RECOMMENDED ACTION

Recommendation: That the City Council:

1. Conduct the reorganization of the City Council by selecting one Council Member to serve a one-year term as Mayor Pro Tem.

SUMMARY

The City Council shall meet annually in December to choose one of its members as Mayor Pro Tem. The Mayor Pro Tem shall be installed, sworn and shall assume the office at that City Council meeting.

DISCUSSION

Section 4.1.3 of the Rules of Procedure provides that nominations for the office Mayor Pro Tem may be made by any member of the City Council and need not be seconded in order to be effective. Appointment shall be made by three or more affirmative votes on a motion to appoint. In the event that no person receives three or more votes in the selection process, the selection process shall be repeated immediately; provided, however, that the two persons receiving the highest number of votes in the preceding selection process shall be the only nominees. If, upon repeating the selection process Mayor Pro Tem, no person has yet received three affirmative votes for such office, the City Council may either repeat the selection process until the officer has been duly selected or may continue the selection to the next regular meeting of the City Council.

The new Mayor Pro Tem, shall serve until the next meeting scheduled for selection of the Mayor Pro Tem in December 2021.

ALTERNATIVES

1. Conduct the reorganization of the City Council by selecting a Mayor Pro Tem.
2. Continue the selection to the next regular meeting of the City Council if upon repeating the selection process, no person receives three affirmative votes for Mayor Pro Tem.

FISCAL IMPACT

There is no fiscal impact associated with the recommended action

NOTIFICATION

Publication of the Agenda

PREPARATION OF STAFF REPORT

Prepared By:
Pat Jacquez-Nares
City Clerk

Department Head Approval:
Pat Jacquez-Nares
City Clerk

CITY COUNCIL GOALS

Positive Environment. Create a positive environment for the development of Moreno Valley's future.

ATTACHMENTS

None

CITY COUNCIL GOALS

Positive Environment. Create a positive environment for the development of Moreno Valley's future.

CITY COUNCIL STRATEGIC PRIORITIES

1. Economic Development
2. Public Safety
3. Library
4. Infrastructure
5. Beautification, Community Engagement, and Quality of Life
6. Youth Programs

ATTACHMENTS

None

APPROVALS

Budget Officer Approval	<u>✓ Approved</u>	12/08/20 5:50 PM
City Attorney Approval	<u>✓ Approved</u>	
City Manager Approval	<u>✓ Approved</u>	12/08/20 5:51 PM